

Development & Heritage Standing Committee Meeting Agenda

Date: Monday, November 3, 2025

Time: 4:30 o'clock p.m.

Location: Council Chambers, 1st Floor, Windsor City Hall

All members will have the option of participating in person in Council Chambers or electronically and will be counted towards quorum in accordance with Procedure By-law 98-2011 as amended, which allows for electronic meetings. The minutes will reflect this accordingly. Any delegations have the option to participate in person or electronically.

MEMBERS:

Ward 1 – Councillor Fred Francis

Ward 4 - Councillor Mark McKenzie

Ward 7 - Councillor Angelo Marignani

Ward 9 - Councillor Kieran McKenzie

Ward 10 - Councillor Jim Morrison (Chairperson)

Member Anthony Arbour

Member Joseph Fratangeli

Member Daniel Grenier

Member John Miller

Member Charles Pidgeon

Member Robert Polewski

Member Khassan Saka

Member William Tape

ORDER OF BUSINESS

Item #	Item Description
1.	CALL TO ORDER

READING OF LAND ACKNOWLEDGMENT

We [I] would like to begin by acknowledging that the land on which we gather is the traditional territory of the Three Fires Confederacy of First Nations, which includes the Ojibwa, the Odawa, and the Potawatomi. The City of Windsor honours all First Nations, Inuit and Métis peoples and their valuable past and present contributions to this land.

2.	DISCLOSURES OF PECUNIARY INTEREST AND THE GENERAL NATURE THEREOF
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3.	REQUEST FOR DEFERRALS, REFERRALS OR WITHDRAWALS
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4.	COMMUNICATIONS
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5.	ADOPTION OF THE <i>PLANNING ACT</i> MINUTES
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5.1.	Adoption of the Development & Heritage Standing Committee minutes (<i>Planning Act</i>) of its meeting held October 6, 2025 (SCM 326/2025)
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6.	PRESENTATION DELEGATIONS (<i>PLANNING ACT</i> MATTERS)
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7.	<i>PLANNING ACT</i> MATTERS
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7.1.	Zoning By-law Amendment Application for 1730 Olive Road, Z-025/25 [ZNG-7329], Ward 8 (S 121/2025) <i>Author: Brian Nagata, Planner III - Development (Acting)</i>
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7.2.	Rezoning – 0 Spitfire Way – Z-020/25 ZNG/7318 – Ward 6 (S 120/2025) <i>Author: Adam Szymczak, Senior Planner - Development</i>
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7.3.	Zoning By-Law Amendment - Z 023/25 (ZNG-7327) 3858-3868 Peter St. - Ward 2 (S 119/2025) <i>Author: Averil Parent, Planner II - Development Review</i>
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- 7.4. Zoning By-Law Amendment - Z 021/25 (ZNG-7323) - 520 Grand Marais Rd. W - Ward 10 (**S 122/2025**) *Author: Averil Parent, Planner II - Development Review*

8. ADOPTION OF THE MINUTES

- 8.1. Adoption of the Development & Heritage Standing Committee minutes of its meeting held September 2, 2025 (**SCM 287/2025**)
- 8.2. Adoption of the Development & Heritage Standing Committee minutes of its meeting held October 6, 2025 (**SCM 325/2025**)

9. PRESENTATIONS AND DELEGATIONS (COMMITTEE ADMINISTRATIVE MATTERS)

10. HERITAGE ACT MATTERS

- 10.1. Notice of Intent to Partially Demolish a Heritage Listed Property – 734 Kildare Road, Warren Seagrave House - Ward 4 (**S 124/2025**) *Author: Tracy Tang, Planner III - Heritage (Acting)*

11. ADMINISTRATIVE ITEMS

- 11.1. Amendment to CR15/2020 for Closure of Perth Avenue located north of Chappus Street, Ward 1, SAS-5686 (**S 123/2025**) *Author: Brian Nagata, Planner III - Development (Acting)*

12. COMMITTEE MATTERS

13. QUESTION PERIOD

14. ADJOURNMENT



Committee Matters: SCM 326/2025

**Subject: Adoption of the Development & Heritage Standing Committee minutes
(*Planning Act*) of its meeting held October 6, 2025**

**Development & Heritage Standing Committee Meeting
(Planning Act Matters)**

Date: Monday, October 6, 2025

Time: 4:30 o'clock p.m.

Members Present:

Councillors

Ward 1 - Councillor Fred Francis

Ward 7 - Councillor Angelo Marignani

Ward 9 - Councillor Kieran McKenzie

Ward 10 - Councillor Jim Morrison (Chairperson)

Councillor Regrets

Ward 4 - Councillor Mark McKenzie

Members

Member Anthony Arbour

Member Regrets

Member Daniel Grenier

Member Robert Polewski

**PARTICIPATING VIA VIDEO CONFERENCE ARE THE FOLLOWING FROM
ADMINISTRATION:**

Matthew Johnson, Executive Director, Economic Development

Sandra Gebauer, Council Assistant

**ALSO PARTICIPATING IN COUNCIL CHAMBERS ARE THE FOLLOWING FROM
ADMINISTRATION:**

Jelena Payne, Deputy Chief Administrative Officer / Commissioner, Economic Development

Neil Robertson, City Planner

Greg Atkinson, Deputy City Planner – Development

Jason Campigotto, Deputy City Planner – Growth

Aaron Farough, Senior Legal Counsel

Sahar Jamshidi, Manager, Road Safety

Patrick Winters, Manager, Development

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Laura Diotte, Manager, Planning
Elara Mehrilou, Supervisor, Coordinator Maintenance
Shannon Mills, Technologist III
Frank Garado, Planner III – Policy & Special Studies
Adam Szymczak, Planner III - Development
Brian Nagata, Planner III - Development
Anna Ciacelli, Deputy City Clerk

Delegations—participating via video conference

None.

Delegations—participating in person

Item 7.1, 7.2, & 7.3 - Tracey Pillon-Abbs, Principal Planner, Pillon Abbs Inc.
Item 7.1 - Brent Klundert, President, BK Cornerstone
Item 7.1 - Andi Shallvari, Representing Property Owners
Item 7.2 - Dan Coccimiglio, Area Resident
Item 11.1 - Kip Brouwer, Area Resident
Item 11.1 - Grazyna Stachyra, Area Resident

1. CALL TO ORDER

The Chairperson calls the meeting of the Development & Heritage Standing Committee to order at 4:30 o'clock p.m.

2. DISCLOSURES OF PECUNIARY INTEREST AND THE GENERAL NATURE THEREOF

None disclosed.

3. REQUEST FOR DEFERRALS, REFERRALS OR WITHDRAWALS

See item 7.1.

4. COMMUNICATIONS

None presented.

5. ADOPTION OF THE *PLANNING ACT* MINUTES

5.1. Adoption of the Development & Heritage Standing Committee minutes (*Planning Act*) of its meeting held September 2, 2025

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Moved by: Councillor Angelo Marignani

Seconded by: Member Anthony Arbour

THAT the *Planning Act* minutes of the Development & Heritage Standing Committee meeting held September 2, 2025 **BE ADOPTED** as presented.

Carried.

7. PLANNING ACT MATTERS

7.1. ZBA Application - 475 Cabana Road W - Z012-2025 [ZNG/7306] -Ward 1

Frank Garardo (author), Planner III – Policy & Special Studies, presents application.

Tracey Pillon-Abbs (agent), presents an alternative concept plan.

Councillor Fred Francis calls a point of order. Councillor Francis states that the delegate presents an alternative concept plan than the previously submitted application, and states that Administration has not commented on changes and is unsure how to make a decision on the changes. Mr. Robertson states that the information presented was the previous concept and is unsure how widely the new concept has been shared with residents and Administration.

Councillor Francis makes a motion to refer the item back to a future meeting and allow for comments from Administration and the public on the new concept plan.

Moved by: Councillor Fred Francis

Seconded by: Councillor Angelo Marignani

- 1) THAT the report of the Planner III – Policy & Special Studies dated August 6, 2025 entitled, “ZBA Application - 475 Cabana Road West – Z 012-2025 [ZNG/7306] - Ward 1” **BE REFERRED** back to administration to allow for their review and comment regarding the new proposal; and that the information **BE BROUGHT FORWARD** to a future Development & Heritage Standing Committee meeting.

Carried.

Report Number: S 103/2025

Clerk's File: ZB/15009

7.2. ZBA Application - 619 Cabana Rd W – Z 013-2025 (ZNG/7307) - Ward 1

Frank Garardo (author), Planner III – Policy & Special Studies, presents application.

Tracey Pillon-Abbs (agent), states that this application conforms with the new residential intensification policy that the City has implemented along Cabana Road, with no commercial component. Ms. Pillon-Abbs states that the application is requesting a zoning amendment to permit the proposed development. Ms. Pillon-Abbs states that the application meets the new Official Plan

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Policy which does support this type of development. Ms. Pillon-Abbs addresses concerns from residents regarding the proposed concept plan.

Dan Coccimiglio (area resident) presents written submission. Mr. Coccimiglio has concerns of requested lot width, misguided lot area comparisons, the proposal does not conform with the Official Plan and zoning principles and will impact the character of the existing neighbourhood. Mr. Coccimiglio states that the over intensification will cause lack of privacy, additional traffic, more garbage and smaller units not geared toward family units. Mr. Coccimiglio states that this proposal oversteps guiding principles and provincial directives.

Councillor Fred Francis inquires whether this proposal is a concern as it will set a precedent for future proposed developments. Mr. Coccimiglio agrees and inquires what the purpose of by-laws are if site specific amendments are allowed and can have a drastic effect on the neighbourhood.

Councillor Francis inquires about the drastic changes this could cause to the neighbourhood. Mr. Coccimiglio states concerns of the over intensification of units can cause additional noise, garbage, traffic, pollution and only asks that the proposal fit with the character of the neighbourhood.

Councillor Francis inquires about the concerns of rear parking affecting residents along Kennedy Dr. W.. Mr. Coccimiglio states that once residents understand how parking in the rear yard means that the residential building will be further from neighbouring backyards then residents will be accepting of the parking lot. Mr. Coccimiglio states that garbage is another concern but understands that it will be handled in the Site Plan Control process.

Councillor Kieran McKenzie inquires about the building orientation and other potential concept plans. Ms. Pillon-Abbs states that other applicant determined that a lower profile building would create a blend better with surrounding properties due to massing and height of the building. Ms. Pillon-Abbs states that the configuration was due to the unique lot size, and the orientation of the buildings provided a better sighting of the buildings and friendly to pedestrians.

Councillor Kieran McKenzie inquires how many units would be provided if the building were to be four storeys. Ms. Pillon-Abbs states that there is a potential of thirty to forty units.

Councillor Francis agrees with Mr. Coccimiglio and inquires whether Administration has any responses to the concerns of residents. Mr. Robertson states that there are no recommendations of RD2.5 or RD2.1, the base zoning will remain, and the site-specific provisions will address the amendments in the recommendations. Mr. Robertson states that a number of the provisions are consistent with RD1.4 in terms of the setbacks which have been increased. Mr. Robertson states that as-of-right the proposal could have increased the number of units without being presented to the committee.

Councillor Francis inquires why there are site specific provisions created for this proposal to make something work at this site and will this set a precedence for future proposals. Mr. Robertson states that Administration is comfortable with the recommendations and the proposal addresses the impacts to the neighbourhood.

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Councillor Francis inquires about this proposal and its strong street presence and that no other buildings on Cabana Road have been built this close to the road. Mr. Robertson states that this is a trade-off to have the parking in the rear, where alternatively privacy is compromised with the building being so close to the rear lot line.

Moved by: Councillor Kieran McKenzie

Seconded by: Member Anthony Arbour

Decision Number: **DHSC 770**

- I. THAT Zoning By-law 8600 **BE AMENDED** by adding the following zoning exception to Section 91.10:

- x. **SOUTH SIDE OF CABANA ROAD WEST, BETWEEN DOUGALL AVENUE AND MCGRAW AVENUE**

For the lands comprising of the west Part Lot 5, Registered Plan 1478, a *Multiple Dwelling* containing up to 6 dwelling *units* shall be an additional permitted *main use*, and the following additional provisions shall apply to a *Multiple Dwelling* containing up to 6 *dwelling units*:

- a) *Lot Width: minimum* 15.0 m
 - b) *Lot Area: minimum* 540 m²
 - c) *Lot Coverage: maximum* 35.0%
 - d) *Building height: maximum* 9.0 m
 - e) *Front Yard Depth: minimum / maximum* 6.0 m / 7.0 m
 - f) *Rear Yard Depth for main building: minimum* 20.0 m
 - g) *Side Yard Width: minimum* 1.20 m
 - h) In any *required front yard*, a *parking space* is prohibited.
 - i) Notwithstanding Section 25.5.20.1.5, the *minimum* parking area separation from a *building* wall in which is located a main pedestrian entrance facing the *parking area* shall be 1.5 m.
 - j) A *screening fence* with a *minimum* height of 1.8 m shall be provided along the *south lot line*.

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- k) A landscaped area with a minimum width of 2.0 m shall be provided abutting the south *lot line*.
- l) A minimum of 50% of the north exterior wall elevation shall be covered in masonry, brick or any combination thereof. A minimum of 35% of east and west exterior wall elevations shall be covered in masonry, brick or any combination thereof.

[ZDM 9; ZNG/7307]

- II. THAT Zoning By-law 8600 **BE FURTHER AMENDED** by changing the zoning of the west Part Lot 5, Registered Plan 1478, situated on the south side of Cabana Road West between Dougall Avenue and McGraw Avenue, from RD1.4 to RD1.4x(x).
- III. THAT when Site Plan Control is applicable:
 - A. Prior to the submission of an application for site plan approval, at the discretion of the City Planner, Deputy City Planner, or Site Plan Approval Officer:
 - 1. Those documents submitted in support of the application for amendment to the Zoning By-law 8600 **BE UPDATED** to reflect the site plan for which approval is being sought, and any comments from municipal departments and external agencies included in Appendix D.

Carried.

Councillor Fred Francis voting nay.

Report Number: C 108/2025
Clerk's File: ZB/15008

7.3. Rezoning Application – 4325-4445 Cabana Rd E - Z-018/25 ZNG/7315 - Ward 9

Adam Szymczak (author), Senior Planner – Development, presents the application.

Tracey Pillon-Abbs (agent) states that the applicant supports Administration's recommendations and the intent of power generation facility is to convert energy from a primary source into a reuseable form of energy with operation one to two hours a day. The facility is located between two existing buildings and will not impact the surrounding community. Ms. Pillon-Abbs is available for questions.

Councillor Angelo Marignani inquires what kind of energy will be produced. Ms. Pillon-Abbs states that it can be natural gas or any type of energy. Ms. Pillon-Abbs states that no details have been released and it is subject to approval from the federal government.

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Councillor Marignani inquires what is likely the type of fuel being generate electricity at this facility. Ms. Pillon-Abbs states she is unable to answer the question and that the applicant requires the zoning amendment to move forward with the final proposal and design.

Councillor Marignani inquires if any indication about the type of fuel generated. Ms. Jelena Payne defers the question to Mr. Matthew Johnson as it is an Independent Electricity System Operator (IESO) initiative and the application will be going to Council on October 20, 2025, for endorsement. Chair Morrison states that delegate questions must continue prior to questions to Administration.

Councillor Kieran McKenzie inquires about the current uses. Ms. Pillon-Abbs states the current uses are transportation terminal, gymnastic club and contractors.

Councillor Kieran McKenzie inquires about compatibility with sensitive land uses and how this would be an appropriate use of the site for the proposed development. Ms. Pillon-Abbs states that there are several dwellings in the area. An open house was held to hear any concerns. No residents attended and no written comments were submitted. The location was chosen between two existing buildings to buffer any noise generated.

Councillor Kieran McKenzie inquires whether any mitigations for odour that may be generated. Ms. Pillon-Abbs states that there should be no odour coming from the stacks.

Councillor Marignani inquires about the fuel to be used. Mr. Matthew Johnson states the fuel is a containerized natural gas system and that there will be noise and odour buffering ventilation systems in place.

Councillor Marignani inquires if this is a similar project to the Ford Foundry Power Generation plant. Mr. Robertson states that the Ford Foundry was an expansion project and this project is not to the same scale but is similar in terms of operation.

Councillor Marignani inquires if there has been consultation with neighbours. Mr. Johnson states that as part of the application process community engagement is required, and in-person and virtual open houses were held with no concerns from residents.

Councillor Kieran McKenzie inquires about the mitigation measures. Mr. Johnson states that the exhaust and noise from the facility will be buffered and the containerized systems must meet all codes. As part of Site Plan Control (SPC), a noise study and fire code and fire safety plans are required.

Councillor Kieran McKenzie inquires whether residents would be impacted due to emissions. Mr. Johnson states that mitigation measures and code compliance requirements are put in place to protect the surrounding community, and IESO has final approval of the Request for Proposal process. City Administration can add further requirements during SPC.

Councillor Kieran McKenzie inquires about noise and whether the levels would remain the same or have a measurable impact on the current uses of the site. Mr. Johnson states that he is unfamiliar

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with the other uses currently on the site, but a site visit was conducted with the applicant at a similar facility in Leamington and the noise from that facility was not significantly higher. Mr. Johnson states that each unit is within a sound dampening container. Mr. Szymczak states that a noise study is a requirement.

Councillor Kieran McKenzie inquires about appropriate use of land and what tools are available to address the challenges to minimize impact on the neighbourhood. Mr. Szymczak states that SPC is an appropriate tool, which will request a noise study that must comply with provincial guidelines and recommend mitigation measures, if required.

Member Anthony Arbour inquires about the working hours of the facility and if it exceeds the working hours being requested and approved. Mr. Johnson states that the facility will operate under the procurement issued through the IESO and will only operate for the hours it has been approved for.

Member Arbour inquires whether the facility will be running throughout the day. Mr. Johnson states that he is unsure but can request the information from the applicant.

Councillor Marignani inquires about the airport zoning regulations. Mr. Szymczak states that anything proposed within a specific radius of the airport must be reviewed by Transport Canada and NAV Canada to ensure compliance with their requirements.

Councillor Marignani inquires if drainage will be directed towards Baseline Road. Mr. Szymczak states this will be addressed during SPC.

Chair Morrison inquires about the zoning of the property and why a power facility was not a permitted use. Mr. Szymczak states that By-law 85-18 was inherited when the lands were transferred from the Town of Tecumseh to the City of Windsor, and the list of permitted uses was what was in their industrial zone.

Chair Morrison inquires about the process of the application and when it comes back to Council will we require a Municipal Support Resolution (MSR) like the other power generation facility. Mr. Szymczak states the MSR is a separate approval process, and the Council report for the MSR will be considered at the next council meeting and that this planning report will be presented at the same council meeting.

Moved by: Councillor Fred Francis

Seconded by: Councillor Angelo Marignani

Decision Number: **DHSC 771**

1. THAT Zoning By-law 85-18 **BE AMENDED** by renumbering paragraph 3.101b to 3.101c and paragraph 3.101a to 3.101b and by adding the following new definition to Section 3:

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- 3.101a **POWER GENERATION FACILITY** is an industrial activity and means premises used to generate electricity and may include a power distribution station or a transformer station.
[ZNG/7315]
2. THAT Zoning By-law 85-18 **BE FURTHER AMENDED** by adding the following defined area to Section 14:
- 14.3.17 Defined Area M1-17 as shown on Zoning Map T12 of this By-law.
- a) Permitted Uses
- 1) All uses permitted in the Industrial (M1) zone save and except a dwelling or dwelling unit.
 - 2) *Power Generation Facility.*
- b) Permitted Buildings and Other Structures
- No building or structures shall be used or erected in Defined Area M1-17 except for the following purposes:
- 1) buildings and structures for the permitted uses.
 - 2) accessory buildings and structures for the permitted uses.
- c) Other Requirements
- 1) The regulations of subsection 14.1.3 to 14.2.4 and all other applicable regulations shall apply to any use permitted in subsection 14.3.17 a).
- (ZNG/7315)
3. THAT Zoning By-law 85-18 **BE FURTHER AMENDED** by changing the zoning of CON 7; PT GORE LOT 17; RP 12R8108; PARTS 4; 5; 7 & 8; PT PARTS 1 TO 3; 6 & 9; RP 12R28421; PARTS 4 TO 6; PIN 75235-0176, situated at the southwest corner of Cabana Road East and 8th Concession Road (4325-4445 Cabana Road East; Roll No. 090-010-04950) from M1 to M1-17.
4. THAT, when Site Plan Control is applicable:
- A. The following additional materials **BE SUBMITTED** with an application for site plan approval, and **BE SUBJECT** to the satisfaction of the City Planner, Deputy City Planner, or Site Plan Approval Officer:
- 1) A noise study.

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- 2) A copy of the confirmation email from Transport Canada that an Aeronautical Assessment Form (AAF) has been submitted to Transport Canada.
- 3) Confirmation of submission of a Land Use Form to the NAV CANADA Land Use Web Submission Portal.

B. The Site Plan Approval Officer **BE DIRECTED** to incorporate the following into an approved site plan and an executed and registered site plan agreement:

- 1) Any noise mitigation measures identified in a Noise Study, subject to the approval of the Site Plan Approval Officer.
- 2) Provision of Transport Canada's determination regarding the proposed Power Generation Facility to the satisfaction of the City Planner, Deputy City Planner, or Site Plan Approval Officer.
- 3) Provision of NAV CANADA's assessment of the proposed Power Generation Facility to the satisfaction of the City Planner, Deputy City Planner, or Site Plan Approval Officer.

Carried.

Report Number: S 109/2025
Clerk's File: Z/15030

8. ADJOURNMENT

There being no further business the meeting of the Development & Heritage Standing Committee (*Planning Act* Matters) portion is adjourned at 5:59 o'clock p.m.

Ward 10 – Councillor Jim Morrison
(Chairperson)

Deputy City Clerk / Supervisor of Council
Services

Subject: Zoning By-law Amendment Application for 1730 Olive Road, Z-025/25 [ZNG-7329], Ward 8

Reference:

Date to Council: November 3, 2025
Author: Brian Nagata, MCIP, RPP
Planner III - Development (A)
(519) 255-6543 ext. 6181

Planning & Building Services
Report Date: September 29, 2025
Clerk's File #: Z/15049

To: Mayor and Members of City Council

Recommendation:

- I. THAT Zoning By-law 8600 **BE AMENDED** by changing the zoning of Lots 318, 319, 431 & 432, Part of Lots 317, 320, 430 & 433, and Part of Closed Alley, Plan 1063 [PIN No. 01116-0581 LT (in part) & PIN No. 01116-0587 LT (in part)], situated on the south part of Kinsmen Norman Road Park and north half of abutting 7.62-metre-wide closed east/west alley, shown as the *Subject Site* on Appendix A, from GD1.1 to RD1.2.
- II. THAT before the issuance of a Building Permit, the City Solicitor **BE DIRECTED** to grant Bell Canada, ENWIN Utilities Ltd. and Managed Network Systems Inc. (MNSi), at no cost, a 4.57-metre-wide easement, measuring 1.52 metres and 3.05 metres from the shared rear lot line of the new single unit dwelling lots on Olive Road and Norman Road, respectively, shown as the *Subject Site* on Appendix A, to service the proposed development.

Executive Summary:

N/A

Background:

Application Information:

Location: 1730 Olive Road

[Lots 309 to 319 & 431 to 441, Part of Lots 320 & 430, and Part of Closed Alley, Plan 1063; Roll No. 010-450-06200; PIN No. 01116-0581 LT (in part) & PIN No. 01116-0587 LT (in part)]

Ward: 8
Planning District: East Windsor
Zoning District Map: 11
Owner: The Corporation of the City of Windsor
Applicant: Same as Owner
Authorized Agent: Dillon Consulting Limited (Amy Farkas)

Proposal:

The applicant is requesting an amendment to Zoning By-law 8600 to allow for the creation of six (6) single unit dwelling lots on the lands known municipally as Kinsmen Norman Road Park or 1730 Olive Road (the "Subject Property"). The requested amendment will be confined to the south part of the Subject Property and north half of the abutting 7.62 metre wide closed east/west alley, shown on Appendix A and hereinafter referred to as the "Subject Site".

The applicant is requesting a change in zoning for the Subject Site from Green District 1.1 (GD1.1) to Residential District 1.2 (RD1.2) to remain consistent with the zoning of properties around Kinsmen Norman Road Park.

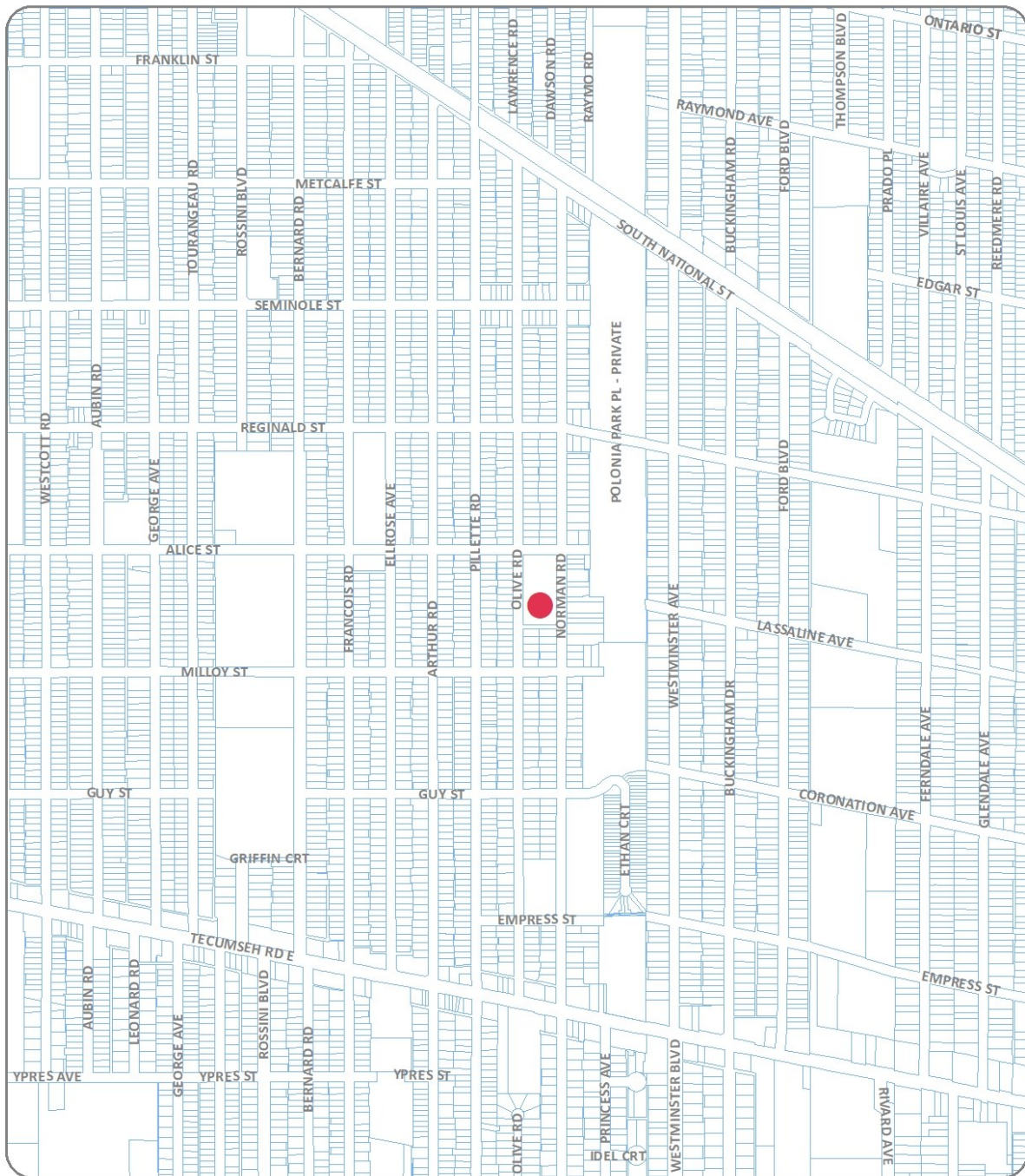
The applicant is specifically proposing to create six (6) serviced 12.0 metre wide lots off Olive Road and Norman Road. The lots will be sold with the intention that each be developed with a single unit dwelling.

Clause (c) of subsection (3) and clause (b) of subsection (5) to Section 50 of the Planning Act authorizes a municipality to dispose of land without having to obtain a consent or exemption from part-lot control.

Submitted Information: Conceptual Plans (See Appendix A), Parks Improvement Plan (See Appendix H), Planning Justification Report (See Appendix I), Public Open House - Engagement Summary (See Appendix J), Servicing and Composite Utility Plan (See Appendix K), Stormwater Management Report (See Appendix L) and Zoning By-law Amendment Application Form

Site Information:

Official Plan	Zoning	Current Use	Previous Use
Residential	Green District 1.1 (GD1.1)	Kinsmen Norman Road Park Closed Alley	Agricultural
Lot Width	Lot Depth	Lot Area	Lot Shape
67.7 m	141.6 m	9,586.3 m ²	Rectangular
<i>All measurements are based on Registered Plan of Subdivision No. 1063 (Roseland Heights), registered on October 23, 1922</i>			



KEY MAP - Z-025/25, ZNG/7329



● SUBJECT LANDS



PART OF ZONING DISTRICT MAP 11

N.T.S.

REQUESTED ZONING AMENDMENT

Applicant: City of Windsor



SUBJECT LANDS

PLANNING & BUILDING DEPARTMENT



DATE : SEPTEMBER, 2025
FILE NO. : Z-025/25, ZNG/7329



NEIGHBOURHOOD MAP - Z-025/25, ZNG/7329



SUBJECT LANDS

Neighbourhood Characteristics:

The subject property is located on the eastern side of the East Windsor neighbourhood. The East Windsor neighbourhood constitutes the area north of Tecumseh Road East, east of the former railway corridor that traversed the Chrysler Windsor Assembly Plant, south of the Detroit River and west of Raymo Road (north of the Via Rail corridor) and Jefferson Boulevard (south of the Via Rail corridor).

Surrounding Land Uses:

North:

- Low density residential

East:

- Low density residential

South:

- Low density residential
- Polonia Park

West:

- Low density residential

Municipal Infrastructure:

- Norman Road is classified as a local road, which has a two-lane cross section with curbs and gutters and sidewalks on both sides, and streetlights on the east side.
- Olive Road is classified as a local road, which has a two-lane cross section with curbs and gutters and sidewalks on both sides, and streetlights on the east side.
- Sanitary sewers, storm sewers and watermain are located within the Norman Road and Olive Road right-of-ways.

Discussion:

The defined role of the Provincial Planning Statement, 2024 (the “PPS”) is to provide *“policy direction on matters of provincial interest related to land use planning and development. As a key part of Ontario’s policy-led planning system, the PPS sets the policy foundation for regulating the development and use of land province-wide, helping achieve the provincial goal of meeting the needs of a fast-growing province while enhancing the quality of life for all Ontarians.”*

The following policies of the PPS are considered relevant in discussing provincial interests related to this amendment:

Chapter 2: Building Homes, Sustaining Strong and Competitive Communities

2.1 Planning for People and Homes

Policy 2.1.6 states:

Planning authorities should support the achievement of complete communities by:

- a) improving accessibility for people of all ages and abilities by addressing land use barriers which restrict their full participation in society; and*
 - a. Compliance with the Barrier-Free Design requirements of the *Ontario Building Code* and accessibility requirements under *Ontario Regulation 191/11 Integrated Accessibility Standards to the Accessibility for Ontarians with Disabilities Act, 2005* will be addressed through the Building Permit stage of the development process.

2.2 Housing

Policy 2.2.1 states:

Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected needs of current and future residents of the regional market area by:

- b) permitting and facilitating:*
 - 1. *all types of residential intensification, including ... redevelopment, which results in a net increase in residential units in accordance with policy 2.3.1.3;*
 - i. This amendment will allow for the redevelopment of the Subject Site at a higher density than currently exists.
- c) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation; and*
 - 1. This amendment will allow for new housing that efficiently uses the Subject Site, available municipal electrical, sanitary, storm and water services and nearby public service facilities (Corpus Christi Catholic Middle School, David Maxwell Public School, École élémentaire catholique Sainte-Thérèse, Gino A. Marcus Community Complex, Seminole Library, W.F. Herman Secondary School).
 - 2. This amendment will allow for housing that supports the use of existing active transportation infrastructure within proximity to the Subject Site.
 - 3. These factors avoid unnecessary land consumption (bike lanes on Pillette Road).

d) *requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations.*

1. This amendment will allow for a transit-supportive development through residential intensification within walking distance of four (4) transit stops on the Central 3 bus route.

i. 400.0 metres is typically used as an acceptable walking distance to a transit stop.

ii. This is reflected within Transit Windsor's 2019 Transit Master Plan and the City of Windsor's Active Transportation Master Plan.

2.3 Settlement Areas and Settlement Area Boundary Expansions

2.3.1 General Policies for Settlement Areas

Policy 2.3.1.1 states:

Settlement areas shall be the focus of growth and development. Within settlement areas, growth should be focused in, where applicable, strategic growth areas, including major transit station areas.

- The Subject Property is located within a Settlement area.

One or more of the responses provided to the policies of PPS Chapter 2 and/or the Official Plan referenced herein also speak to the following relevant PPS Policies:

Chapter 2: Building Homes, Sustaining Strong and Competitive Communities	
2.3 Settlement Areas and Settlement Area Boundary Expansions	
2.3.1 General Policies for Settlement Areas	
Policy 2.3.1.2 (<i>Land use patterns within settlement areas should be based on densities and a mix of land uses which :</i>)	Sub Policies a) (<i>efficiently use land and resources</i>), b) (<i>optimize existing and planned infrastructure and public service facilities</i>), c) (<i>support active transportation</i>), d) (<i>are transit-supportive, as appropriate</i>)
Policy 2.3.1.3 (<i>Planning authorities shall support general intensification and redevelopment to support the achievement of complete communities, including by planning for a range and mix of housing options</i>)	
2.9 Energy Conservation, Air Quality and Climate Change	
Policy 2.9.1 (<i>Planning authorities</i>	Sub Policy a) (<i>support the</i>

<i>shall plan to reduce greenhouse gas emissions and prepare for the impacts of a changing climate through approaches that :)</i>	<i>achievement of compact, transit-supportive, and complete communities)</i>
Chapter 3: Infrastructure and Facilities	
3.6 Sewage, Water and Stormwater	
<i>Policy 3.6.2 (Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. For clarity, municipal sewage services and municipal water services include both centralized servicing systems and decentralized servicing systems)</i>	

Official Plan

Relevant excerpts from Volume I of the Official Plan are attached as Appendix C. The following policies from these excerpts are considered relevant in discussing this amendment's conformity with the Official Plan.

Table 1 - Volume I Schedules

Schedule	Designation
Schedule A - Planning Districts & Policy Areas	East Windsor
Schedule A-1 - Special Policy Areas	N/A
Schedule B - Greenway System	N/A
Schedule C - Development Constraints	N/A
Schedule C-1 - Archaeological Potential	Archaeological Potential Zone (in part)
Schedule D - Land Use	Residential
Schedule E - City Centre Planning District	N/A
Schedule F - Roads & Bikeways	N/A
Schedule F-1 - Railways	N/A
Schedule G - Civic Image	N/A
Schedule H - Baseplan Development Phasing	N/A
Schedule J - Urban Structure Plan	N/A
Schedule K - Source Water Protection Areas	Intake Protection Zone 2

Volume I

Chapter 6 - Land Use:

6.1 Goals

This amendment complies with the following applicable land use goals:

- *Safe, caring and diverse neighbourhoods.* (Goal 6.1.1)
- *Housing suited to the needs of Windsor's residents.* (Goal 6.1.3)
- *To direct residential intensification to those areas of the City where transportation, municipal services, community facilities and goods and services are readily available.* (Goal 6.1.14)

6.3 Residential

6.3.1 Objectives

The amendment complies with the following applicable Residential land use objectives:

- *To promote compact neighbourhoods which encourage a balanced transportation system.* (Objective 6.3.1.2)
- *To promote residential redevelopment, infill and intensification initiatives in locations in accordance with this plan.* (Objective 6.3.1.3)

6.3.2 Policies

Permitted Uses

Uses permitted in the Residential land use designation identified on Schedule D: Land Use include Low Profile, and Medium Profile dwelling units. (Policy 6.3.2.1)

- A single unit dwelling is classified as a Low-Profile dwelling.

Types of Low-Profile Housing

For the purposes of this Plan, Low Profile housing development is further classified as follows: (Policy 6.3.2.3)

- *(a) small scale forms: single detached, semi-detached, duplex and row and multiplexes with up to 8 units; and*
- *(b) large scale forms: buildings with more than 8 units.*

Locational Criteria

... New residential development and intensification shall be located where: (Policy 6.3.2.4)

- *(a) There is access to a collector or arterial road;*

- The subject property is located within approximately 175.0 metres of Pillette Road, a Class II Arterial Road.
- *(b) Full municipal physical services can be provided;*
 - Refer to the response provided to PPS Policy 2.2.1.c) herein.
- *(c) Adequate community services and open spaces are available or are planned; and*
 - Refer to the response provided to PPS Policy 2.2.1.c) and the Surrounding Land Uses section herein.
- *(d) Public transportation service can be provided.*
 - Refer to the response provided to PPS Policy 2.2.1.d) herein.

Evaluation Criteria for a Neighbourhood Development Pattern

At the time of submission, the proponent shall demonstrate to the satisfaction of the Municipality that a proposed residential development within an area having a Neighbourhood development pattern is: (Policy 6.3.2.5)

- *(c) In existing neighbourhoods, compatible with the surrounding area in terms of scale, massing, height, siting, orientation, setbacks, parking and amenity areas.*
 - This amendment will allow for a development that is compatible with the established built environment found within the block.
 - This amendment results in an extension of the RD1.2 zoning district that surrounds Kinsmen Norman Road Park.
 - The proposed lots will comply with the RD1.2 zoning provisions.
 - The size of the proposed single unit dwelling lots is consistent with the residential properties within the 1700 blocks of Norman Road and Olive Road.
 - This is demonstrated through the analysis conducted for the said properties, included in the table *attached* hereto as Appendix G.
- *(d) provided with adequate off street parking;*
 - The proposed development will accommodate the required number of parking spaces onsite.
- *(e) capable of being provided with full municipal physical services and emergency services; and*
 - Refer to the response provided to PPS Policy 2.2.1.c) herein for details on the municipal physical services available to the subject property.

- The subject property is served by Essex-Windsor EMS, Windsor Fire & Rescue Services (Fire Hall No. 2) & Windsor Police Service.

Chapter 11 - Tools:

Land use compatibility throughout Windsor is an implementation goal to be achieved when administering a planning tool under this Chapter. Compatibility between land uses is also an objective of the Zoning By-law Amendment planning tool (Policy 11.6.1.2).

- Land use compatibility was considered as part of the evaluation of the applicable Official Plan and PPS policies referenced herein.

Policy 11.6.3.3 states:

- *When considering applications for Zoning By-law amendments, Council shall consider the policies of this Plan and will, without limiting the generality of the foregoing, consider such matters as the following:*
 - a) *The relevant evaluation criteria contained in the Land Use Chapter of this Plan, Volume II: Secondary Plans & Special Policy Areas and other relevant standards and guidelines;*
 - b) *Relevant support studies;*
 - c) *The comments and recommendations from municipal staff and circularized agencies;*
 - d) *Relevant provincial legislation, policies and appropriate guidelines; and*
 - e) *The ramifications of the decision on the use of adjacent or similar lands.*
 - This amendment is not anticipated to have any ramifications on the use of adjacent or similar lands.

The aforesaid matters were considered as part of the evaluation of the applicable Official Plan and PPS policies referenced herein.

Zoning By-Law

Relevant excerpts from Zoning By-law 8600 are *attached* as Appendix D.

The subject property is within a GD1.1 zone of Zoning By-law 8600, which does not permit a single detached dwelling use. The applicant is requesting an amendment to Zoning By-law 8600 to change the zoning to RD1.2 zone to allow for the creation of six (6) single unit dwelling lots.

The applicant has submitted a Planning Justification Report (PJR) in support of the requested amendment. The PJR has been considered and is supported in this report.

No other zoning deficiencies have been identified or supported.

A draft amending by-law is attached as Appendix F. Subsection 24 (1) of the *Planning Act*, R.S.O. 1990, c. P.13., prohibits a by-law from being passed that does not conform

with the Official Plan. As discussed through the Official Plan section herein, the proposed amendment conforms to the applicable policies of the Official Plan.

Risk Analysis:

N/A

Climate Change Risks

Climate Change Mitigation:

Residential intensification minimizes the impact on the community greenhouse gas emissions. Development within existing communities and neighbourhoods while using currently available infrastructure such as sewers, sidewalks, and public transit helps to mitigate development impact.

Situating development in areas with access to active transportation and transit increases the likelihood of residents utilizing various non-vehicular means of utilitarian transportation which collectively reduced the amount of greenhouse gas emissions within the City of Windsor.

Climate Change Adaptation:

N/A

Financial Matters:

N/A

Consultations:

Comments received from City Departments, external agencies and members of the public on this application were taken into consideration when preparing this report. A record of the comments is included as Appendix E herein.

Bell Canada, ENWIN Utilities Ltd. and Managed Network Systems Inc. (MNSi), have requested a combined 4.57-metre-wide easement, measuring 1.52 metres and 3.05 metres from the shared rear lot line of the new single unit dwelling lots on Olive Road and Norman Road respectively, shown as the *Subject Site* on Appendix A, to service the proposed development. The granting of the requested easement must be obtained prior to the issuance of a Building Permit as in Recommendation II of this report.

Bell Canada has requested Servicing Plans/Composite Utility Plan to confirm the provision of communication/telecommunication infrastructure needed to service the proposed development. The applicant submitted a Servicing and Composite Utility Plan with this amendment application. Approval of the Servicing and Composite Utility Plan by Bell Canada, must be obtained prior to the issuance of a Building Permit.

The Engineering - Development section requested a Storm Water Retention Scheme for the proposed development. The applicant submitted a Stormwater Management Report

with this amendment application. Approval of the Stormwater Management Report by the City Engineer, must be obtained prior to the issuance of a Building Permit.

There are no objections to the proposed amendment.

The applicant hosted an open house (in-person and virtual) on June 26, 2025, at St. Theresa's Catholic Church (1991 Norman Road). The open house was attended in-person by six (6) residents and online by one (1) resident via Zoom. Comments and questions from the attendees and received through email have been documented and responded to within the Public Open House - Engagement Summary prepared by Dillon Consulting Limited, dated July 2025.

Comments received were taken into consideration when preparing this report.

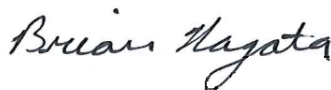
Public Notice: Statutory notice was advertised in the Windsor Star, a local daily newspaper. A courtesy notice was mailed to property owners and residents within 200 metres of the subject property.

Conclusion:

The *Planning Act* requires that a decision of Council in respect of the exercise of any authority that affects a planning matter, "shall be consistent with the" *Provincial Planning Statement 2024*. The recommended zoning amendment has been evaluated for consistency with the *Provincial Planning Statement 2024* and conformity with the policies of the City of Windsor Official Plan.

The recommended Zoning By-law amendment is consistent the PPS, conforms with the policy direction of the City of Windsor Official Plan, is compatible with existing and permitted uses in the surrounding neighbourhood and constitutes good planning.

Planning Act Matters:



Brian Nagata, MCIP, RPP
Planner III - Development (A)

I concur with the above comments and opinion of the Registered Professional Planner.

Greg Atkinson, MCIP, RPP
Deputy City Planner - Development

Neil Robertson, MCIP, RPP
City Planner

I am not a Registered Professional Planner and have reviewed as a Corporate Team Leader

JP

RM

Approvals:

Name	Title
Brian Nagata	Planner III - Development (A)
Greg Atkinson	Deputy City Planner - Development
Aaron Farough	Senior Legal Counsel, Legal & Real Estate
Neil Robertson	City Planner
Jelena Payne	Deputy CAO/ Commissioner, Economic Development
Ray Mensour	Chief Administrative Officer

Appendices:

- 1 Appendix A - Concept Plan
- 2 Appendix B - Site Images
- 3 Appendix C - Excerpts from Official Plan
- 4 Appendix D - Excerpts from Zoning By-law 8600
- 5 Appendix E - Consultations
- 6 Appendix F - Draft Amending By-law
- 7 Appendix G - Zoning Analysis
- 8 Appendix H - Parks Improvement Plan
- 9 Appendix I - Planning Justification Report
- 10 Appendix J - Public Open House - Engagement Summary
- 11 Appendix K - Servicing and Composite Utility Plan
- 12 Appendix L - Stormwater Management Report



THE CITY OF WINDSOR
KINSMEN NORMAN ROAD PARK
(1730 OLIVE ROAD)

ZONING BY-LAW AMENDMENT
FIGURE 1.0



EXISTING KINSMEN
NORMAN ROAD PARK
(±9,826m²)



PARKLAND (GD1.1)
(±7,396.0 sq.m)



SUBJECT SITE (RD1.2)
(±2,430.0 sq.m)

File Location:
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park-concept plan.dwg
June, 26, 2025 9:32 AM

SOURCE: THE COUNTY OF ESSEX INTERACTIVE MAPPING (2024)

MAP/DRAWING INFORMATION
THIS DRAWING IS FOR INFORMATION PURPOSES ONLY.
ALL DIMENSIONS AND BOUNDARY INFORMATION SHOULD
BE VERIFIED BY AN O.L.S. PRIOR TO CONSTRUCTION.

CREATED BY: ESB
CHECKED BY: PR
DESIGNED BY: ESB

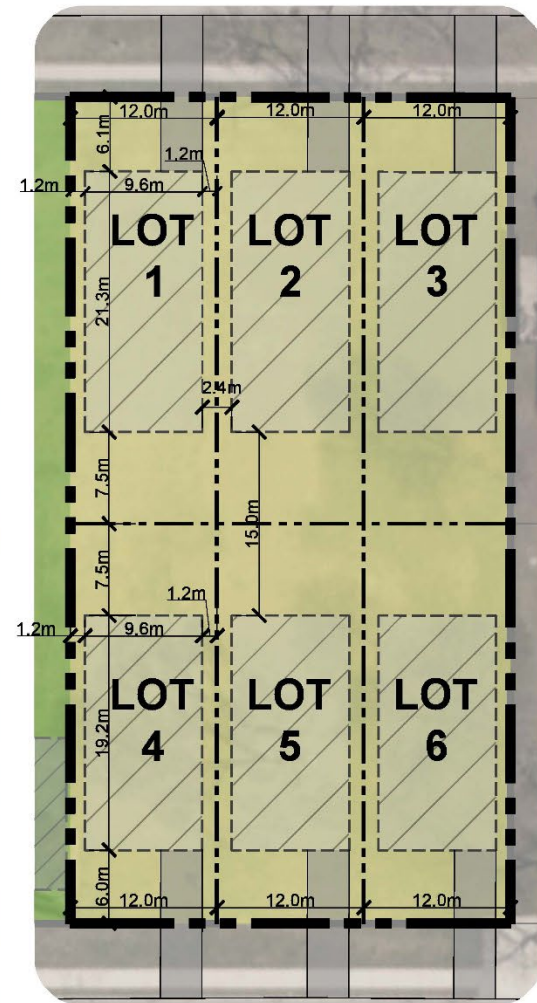
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COUNTERPOINT
LAND DEVELOPMENT BY

DILLON
CONSULTING

PROJECT: 25-1509
STATUS: DRAFT
DATE: 05/26/2025



SITE DATA MATRIX	
RESIDENTIAL DISTRICT 1.2 (RD1.2) PROVISIONS	
SUBJECT SITE AREA	2,430.0 sq.m
LOT WIDTH	12.0 m
MIN. LOT AREA	390.0 sq.m
MAX. LOT COVERAGE	45.00%
MAX. BUILDING HEIGHT	9.0 m
MIN. FRONT YARD DEPTH	6.0 m
MIN. REAR YARD DEPTH	7.5 m
MIN. SIDE YARD DEPTH	1.2 m
MAX. GROSS FLOOR AREA	400.0 sq.m
LOTS 1-3	
LOT DEPTH	34.8 m
LOT WIDTH	12.0 m
LOT AREA	417.0 sq.m
MAX. LOT COVERAGE	45% / 187.7 sq.m
LOTS 4-6	
LOT DEPTH	32.7 m
LOT WIDTH	12.0 m
LOT AREA	392.0 sq.m
MAX. LOT COVERAGE	45% / 178.4 sq.m

THE CITY OF WINDSOR
KINSMEN NORMAN ROAD PARK
(1730 OLIVE ROAD)

CONCEPT PLAN
FIGURE 2.0

EXISTING KINSMEN NORMAN ROAD PARK
(±9,826m²)

PARKLAND (GD1.1)
(±7,396.0 sq.m)

SUBJECT SITE (RD1.2)
(±2,430.0 sq.m)

PROPOSED SINGLE DETACHED DWELLING LOTS (6)

BUILDING SETBACKS

MAX. DEVELOPEABLE AREA

File Location:
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 park-concept plan.dwg
 June, 20, 2025 9:32 AM

SOURCE: THE COUNTY OF ESSEX INTERACTIVE MAPPING (2024)

MAPPING INFORMATION
 THIS DRAWING IS FOR INFORMATION PURPOSES ONLY.
 ALL DIMENSIONS AND BOUNDARY INFORMATION SHOULD
 BE VERIFIED BY AN O.L.S PRIOR TO CONSTRUCTION.

CREATED BY: EBB
 CHECKED BY: FR
 DESIGNED BY: EBB

SCALE: 1:400 (11x17)

COUNTERPOINT
 LAND DEVELOPMENT BY

DILLON
 CONSULTING

PROJECT: 25-1503
 STATUS: DRAFT
 DATE: 06/26/2025

APPENDIX “B”
Site Photos

(Google Street View - November 2023)



Figure 1 - Looking southeast towards Kinsmen Norman Road Park from Alice Street and Olive Road



Figure 2 - Looking southwest towards Kinsmen Norman Road Park from Alice Street and Norman Road

APPENDIX “C”

Excerpts from Official Plan

6. Land Use

6.0 Preamble

A healthy and livable city is one in which people can enjoy a vibrant economy and a sustainable healthy environment in safe, caring and diverse neighbourhoods. In order to ensure that Windsor is such a city, Council will manage development through an approach, which balances environmental, social and economic considerations. As such, the Land Use chapter of this Plan promotes a compact urban form and directs compatible development to appropriate locations within existing and future neighbourhoods.

This chapter of the Official Plan provides goals, objectives and policies for the land use designations identified on Schedule D: Land Use and Schedule E: City Centre Planning District and should be read in conjunction with the other parts of the Plan.

6.1 Goals

In keeping with the Strategic Directions, Council’s land use goals are to achieve:

<i>NEIGHBOURHOODS</i>	6.1.1	Safe, caring and diverse neighbourhoods.
<i>ENVIRONMENTALLY SUSTAINABLE</i>	6.1.2	Environmentally sustainable urban development.
<i>RESIDENTIAL</i>	6.1.3	Housing suited to the needs of Windsor’s residents.
<i>EMPLOYMENT</i>	6.1.4	The retention and expansion of Windsor’s employment base.
<i>COMMERCIAL</i>	6.1.5	Convenient and viable areas for the purchase and sale of goods and services.
<i>INSTITUTIONAL</i>	6.1.6	An integration of institutions within Windsor’s neighbourhoods.
<i>OPEN SPACE</i>	6.1.7	A variety of open space areas.
<i>WATERFRONT</i>	6.1.8	An accessible Detroit River, Lake St. Clair and a healthy waterfront.
<i>NATURAL HERITAGE</i>	6.1.9	The protection and conservation of environmentally significant and sensitive natural heritage features and functions.
<i>MIXED USE</i>	6.1.10	Pedestrian oriented clusters of residential, commercial, employment and institutional uses.
<i>CITY CENTRE PLANNING DISTRICT</i>	6.1.11	The City Centre as the vibrant focal point and symbol of Windsor.

<i>AIRPORT</i>	6.1.12	Protection and enhancement of Windsor Airport's role in serving passenger and cargo needs. (added by OPA #60-05/07/07-B/L85-2007-OMB Decision/Order No.2667, 10/05/2007)
<i>FUTURE GROWTH AREAS</i>	6.1.13	The provision of sufficient land in appropriate locations to accommodate future population and employment growth in Windsor. (added by OPA #60-05/07/07-B/L85-2007-OMB Decision/Order No.2667, 10/05/2007)
<i>RESIDENTIAL INTENSIFICATION</i>	6.1.14	To direct residential intensification to those areas of the City where transportation, municipal services, community facilities and goods and services are readily available. (added by OPA #159 – APPROVED July 11, 2022, B/L# 100-2022)

6.3 Residential

The lands designated as “Residential” on Schedule D: Land Use provide the main locations for housing in Windsor outside of the City Centre Planning District. In order to develop safe, caring and diverse neighbourhoods, opportunities for a broad range of housing types and complementary services and amenities are provided.

The following objectives and policies establish the framework for development decisions in Residential areas.

6.3.1 Objectives

<i>RANGE OF FORMS & TENURES</i>	6.3.1.1	To support a complementary range of housing forms and tenures in all neighbourhoods.
<i>NEIGHBOURHOODS</i>	6.3.1.2	To promote compact neighbourhoods which encourage a balanced transportation system.
<i>INTENSIFICATION, INFILL & REDEVELOPMENT</i>	6.3.1.3	To promote residential redevelopment, infill and intensification initiatives in locations in accordance with this plan. (Added by OPA#159 - APPROVED July 11, 2022, B/L#100-2022)
<i>MAINTENANCE & REHABILITATION</i>	6.3.1.4	To ensure that the existing housing stock is maintained and rehabilitated.
<i>SERVICE & AMENITIES</i>	6.3.1.5	To provide for complementary services and amenities which enhance the quality of residential areas.
<i>HOME BASED OCCUPATIONS</i>	6.3.1.6	To accommodate home based occupations.
<i>SUFFICIENT LAND SUPPLY</i>	6.3.1.7	To ensure that a sufficient land supply for residential and ancillary land uses is available to accommodate market demands over the 20 year period of this Plan.

6.3.2 Policies

In order to facilitate the orderly development and integration of housing in Windsor, the following policies shall apply.

<i>PERMITTED USES</i>	6.3.2.1	Uses permitted in the Residential land use designation identified on Schedule D: Land Use include Low Profile, and Medium Profile dwelling units. High Profile Residential Buildings shall be directed to locate in the City Centre, Mixed Use Centres and Mixed Use Corridors. (Added by OPA #159 – APPROVED July 11, 2022 , B/L# 100-2022)
<i>TYPES OF LOW PROFILE HOUSING</i>	6.3.2.3	For the purposes of this Plan, Low Profile housing development is further classified as follows: (a) small scale forms: single detached, semi-detached, duplex and row and multiplexes with up to 8 units; and (b) large scale forms: buildings with more than 8 units.
<i>LOCATIONAL CRITERIA</i>	6.3.2.4	Residential intensification shall be directed to the Mixed Use Nodes and areas in proximity to those Nodes. Within these areas Medium Profile buildings, up to four (4) storeys in height shall be permitted. These taller buildings shall be designed to provide a transition in height and massing from low-profile areas. New residential development and intensification shall be located where: (a) There is access to a collector or arterial road; (b) Full municipal physical services can be provided; (c) Adequate community services and open spaces are available or are planned; and (d) Public transportation service can be provided. (Added by OPA #159 – APPROVED July 11, 2022, B/L# 100-2022)
<i>EVALUATION CRITERIA FOR A NEIGHBOURHOOD DEVELOPMENT PATTERN</i>	6.3.2.5	At the time of submission, the proponent shall demonstrate to the satisfaction of the Municipality that a proposed residential development within an area having a Neighbourhood development pattern is: (a) feasible having regard to the other provisions of this Plan, provincial legislation, policies and appropriate guidelines and support studies for uses:

- (i) within or adjacent to any area identified on Schedule C: Development Constraint Areas and described in the Environment chapter of this Plan;
 - (ii) adjacent to sources of nuisance, such as noise, odour, vibration and dust;
 - (iii) within a site of potential or known contamination;
 - (iv) where traffic generation and distribution is a provincial or municipal concern; and
 - (v) adjacent to heritage resources.
- (b) in keeping with the goals, objectives and policies of any secondary plan or guideline plan affecting the surrounding area;
 - (c) In existing neighbourhoods, compatible with the surrounding area in terms of scale, massing, height, siting, orientation, setbacks, parking and amenity areas.

In Mature Neighbourhoods as shown on Schedule A-1, compatible with the surrounding area, as noted above, and consistent with the streetscape, architectural style and materials, landscape character and setback between the buildings and streets; (Added by OPA #159 – APPROVED July 11 2022, B/L# 100-2022)

- (d) provided with adequate off street parking;
- (e) capable of being provided with full municipal physical services and emergency services; and
- (f) Facilitation a gradual transition from Low Profile residential development to Medium and/or High Profile development and vice versa, where appropriate, in accordance with Design Guidelines approved by Council. (Added by OPA #159 – APPROVED July 11, 2022, B/L# 100-2022)

11 Tools

This chapter was amended as part of Official Plan Amendments #84 as part of a 5-year review of the Official Plan. Official Plan Amendment #84 was approved by the Ministry of Municipal Affairs and Housing on 07/09/2012.

11.6 Zoning

A municipality can regulate the use and development of lands, buildings and other structures under the zoning provisions of the *Planning Act*.

11.6.3 Zoning By-law Amendment Policies

- | | | |
|--------------------------------|----------|--|
| <i>AMENDMENTS MUST CONFORM</i> | 11.6.3.1 | All amendments to the Zoning By-law(s) shall conform with this Plan. The Municipality will, on each occasion of approval of a change to the zoning by-law(s), specify that conformity with the Official Plan is maintained or that the change will be in conformity upon the coming into effect of an amendment to the Official Plan. |
| <i>REVIEW PROCEDURE</i> | 11.6.3.2 | <p>All applications for Zoning By-law amendments shall be processed in accordance with the provisions of the <i>Planning Act</i>, and regulations pursuant thereto, and the procedural requirements of this Plan. In general, after an applicant's pre-application consultation meeting with municipal staff and submission of an application that is determined to be complete, all applications shall: Added by OPA 65 – 10/22/2007– By-law 192-2007</p> <ul style="list-style-type: none"> (a) Be circulated to appropriate agencies and those agencies be provided with sufficient time to respond; Added by OPA 65 – 10/22/2007– By-law 192-2007 (b) Be advertised and be presented to the public and the views of the public ascertained at a public meeting to be held in accordance with the <i>Planning Act</i>; and Added by OPA 65 – 10/22/2007– By-law 192-2007 (c) Be given due and thorough consideration by Council. Added by OPA 65 – 10/22/2007– By-law 192-2007 |
| <i>EVALUATION CRITERIA</i> | 11.6.3.3 | <p>When considering applications for Zoning By-law amendments, Council shall consider the policies of this Plan and will, without limiting the generality of the foregoing, consider such matters as the following:</p> <ul style="list-style-type: none"> (a) The relevant evaluation criteria contained in the Land Use Chapter of this Plan, Volume II: Secondary Plans & Special Policy Areas and other relevant standards and guidelines; (b) Relevant support studies; (c) The comments and recommendations from municipal staff and circularized agencies; (d) Relevant provincial legislation, policies and appropriate guidelines; and (e) The ramifications of the decision on the use of adjacent or similar lands. |

APPENDIX “D”

Excerpts from Zoning By-law 8600

SECTION 9 - GREEN DISTRICTS 1. (GD1.)

(B/L 8988 May 19/1987; B/L 33-2001 Oct 23/2001 OMB Order 1716 PL010233; B/L 370-2001 Nov 15/2001; B/L 181-2003 Jul 14/2003; B/L 164-2017 Dec 7/2017; B/L 52-2024 Apr 22/2024)

9.1 GREEN DISTRICT 1.1 (GD1.1)

[ZNG/6277]

(B/L 52-2024 APR 22/2024)

9.1.1 PERMITTED MAIN USES

Child Care Centre
Public Park

9.1.2 PERMITTED ACCESSORY USES

Any use accessory to a permitted main use

9.1.5 PROVISIONS

.3	Lot Coverage – maximum	25.0%
.4	Main Building Height – maximum	
	<i>Lot having a lot area of less than 0.50 ha</i>	9.0 m
	<i>Lot having a lot area of 0.50 ha or more</i>	14.0 m
.90	For any <i>lot</i> situated on the north side of Riverside Drive East and West between Rosedale Avenue and Lauzon Road, the extension of a <i>building</i> above the crown of the pavement within Riverside Drive is prohibited. This provision shall not apply to the following: fountain, <i>municipal infrastructure</i> , observation deck, playground equipment, sculpture and any other work of art, or washroom.	

SECTION 10 - RESIDENTIAL DISTRICTS 1. (RD1.)

(B/L 10358 Jul 16/1990; B/L 11093 Jul 20/1992; B/L 33-2001 Oct 23/2001, OMB Decision/Order No. 1716 Case No. PL010233; B/L 370-2001 Nov 15/2001; B/L 363-2002 Dec 31/2002; B/L 220-2002, Feb 24/2003; B/L 10-2004 OMB Order PL040143, File No. R040023, Decision/Order No. 0055, Issued Jan 12/2005 B/L 114-2016 Sep 19/2016); B/L 164-2017, Dec. 7/2017 [ZNG/5270]; B/L 95-2019, Sept. 27/2019 B/L 101-2022 Jul 11/2022; B/L 52-2024 Apr 22/2024)

10.2 RESIDENTIAL DISTRICT 1.2 (RD1.2)

10.2.1 PERMITTED USES

Existing Duplex Dwelling
Existing Semi-Detached Dwelling
One Single Unit Dwelling
Any use accessory to the preceding uses

10.2.5 PROVISIONS

	Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.1 Lot Width – minimum	9.0 m	15.0 m	12.0 m
.2 Lot Area – minimum	360.0 m ²	450.0 m ²	360.0 m ²
.3 Lot Coverage – maximum	45.0%	45.0%	45.0%
.4 Main Building Height – maximum	9.0 m	9.0 m	9.0 m
.5 Front Yard Depth – minimum	6.0 m	6.0 m	6.0 m
.6 Rear Yard Depth – minimum	7.50 m	7.50 m	7.50 m
.7 Side Yard Width – minimum	1.20 m	1.20 m	1.50 m
.8 Gross Floor Area – main building – maximum	400 m ²	400 m ²	400 m ²

(B/L 101-2022 July 11/2022; B/L 52-2024 Apr 22/2024)

APPENDIX “E”

Consultations

BELL CANADA

Comments to Planning Consultation Stage 1 Application (File No. PC-039/25)

We have reviewed the circulation regarding the above noted application. The following paragraphs are to be included as a condition of approval:

“The Owner acknowledges and agrees to convey any easement(s) as deemed necessary by Bell Canada to service this new development. The Owner further agrees and acknowledges to convey such easements at no cost to Bell Canada.

The Owner agrees that should any conflict arise with existing Bell Canada facilities where a current and valid easement exists within the subject area, the Owner shall be responsible for the relocation of any such facilities or easements at their own cost.” Upon receipt of this comment letter, the Owner is to provide Bell Canada with servicing plans/CUP at their earliest convenience to circulations@bell.ca to confirm the provision of communication/telecommunication infrastructure needed to service the development.

It shall be noted that it is the responsibility of the Owner to provide entrance/service duct(s) from Bell Canada’s existing network infrastructure to service this development.

In the event that no such network infrastructure exists, in accordance with the Bell Canada Act, the Owner may be required to pay for the extension of such network infrastructure.

If the Owner elects not to pay for the above noted connection, Bell Canada may decide not to provide service to this development.

To ensure that we are able to continue to actively participate in the planning process and provide detailed provisioning comments, we note that we would be pleased to receive circulations on all applications received by the Municipality and/or recirculation.

[Juan Corvalan - Senior Manager - Municipal Liaison]

CALDWELL FIRST NATION COMMUNITY

No comments provided

ENGINEERING - DEVELOPMENT

Comments to Planning Consultation Stage 1 Application (File No. PC-039/25)

Storm Water Retention Scheme required to be submitted with Zoning By-law Amendment Application.

[Juan Paramo - Development Engineer]

ENGINEERING - R.O.W.**Required Drawing Revisions:**

1. Driveway Approaches - Do not conform to City of Windsor Standards, which must be constructed with straight flares and no raised curbs within the right-of-way.
 - a. Modify as per Standard Engineering Drawing AS-221 or AS-222
 - b. Remove redundant curb cut approach off alley.
2. Sewer Connections - All existing and proposed storm, sanitary and water services must be identified on the drawings, as well as the associated mainline sewers/water mains.
 - a. Modify drawings to include all sewer connections and water services.
 - b. Identify any redundant connections to be abandoned in accordance with Engineering Best Practice BP1.3.3.

Right-of-Way Permit Requirements

Driveway Approaches - The Owner further agrees that driveway approaches shall be constructed in such width and location as shall be approved by the City Engineer, with straight flare driveway approaches and no raised curbs within the right-of-way. The Owner shall have the option of constructing said driveway approaches as follows:

1. Residential Property
 - a. Asphalt in accordance with City of Windsor Standard Drawing AS-221; or
 - b. Concrete in accordance with City of Windsor Standard Drawing AS-222

Servicing Charges - The applicant(s) shall note that they will be required to pay servicing charges for the existing storm and sanitary sewer if not paid previously for this site. These charges will be assessed prior to the issuance of a construction permit.

Sewer Connections - The site is serviced by a 300mm sanitary sewer and a 450mm storm sewer located within the Olive Road right-of-way and a 300mm sanitary sewer and a 450mm storm sewer located within the Norman Road right-of-way. All existing and proposed storm, sanitary and water services must be identified on the drawings, as well as the associated mainline sewers/water mains.

- Modify drawings to include all sewer connections and water services.
- Identify any redundant connections to be abandoned in accordance with Engineering Best Practice BP1.3.3.

[Mark Schaffhauser - Technologist II]

ENVIRONMENTAL POLICY

Comments to Planning Consultation Stage 1 Application (File No. PC-039/25)

Natural Areas has reviewed the Liaison and has no further requests for more information or studies.

During development, the following applies:

1. Should Species at Risk or their habitat be found at any time on or adjacent to the site, cease activity immediately and contact MECP at SAROntario@ontario.ca for recommendations on next steps to prevent contravention of the Endangered Species Act (2007). The City of Windsor SAR hotline (519-816-5352) can also be used for relevant questions and concerns.
2. For a list of Species at Risk and other provincially tracked species with potential to be around the site, use the Natural Heritage Information Centre (NHIC) Make A Map tool, found at https://www.lioapplications.lrc.gov.on.ca/Natural_Heritage/index.html?viewer=Natural_Heritage.Natural_Heritage&locale=en-CA
3. If trees or other vegetation (i.e., shrubs and unmaintained grasses) on/adjacent to the site are to be removed, damaged, or disturbed during the breeding bird season (April 1 – August 31), then sweeps for nesting birds should be conducted to prevent contravention of Migratory Bird Regulations (2022), the Migratory Birds Convention Act (1992), and section 7 of the Fish and Wildlife Conservation Act (1997). Visit <https://www.canada.ca/en/environment-climate-change/services/avoiding-harm-migratory-birds/reduce-risk-migratory-birds.html> for more information
4. Beaver dams and dens of fur-bearing mammals are protected under section 8 of the Fish and Wildlife Conservation Act (1997) and are not to be damaged or destroyed without the proper authorization and/or license.

[Karen Alexander - City Naturalist and Outreach Coordinator]

ENWIN UTILITIES LTD. - HYDRO ENGINEERING

Comments to Planning Consultation Stage 1 Application (File No. PC-039/25)

No Objection provided adequate clearances are achieved and maintained.

Please note the following.

1. ENWIN has single phase 16kV primary conductor on the Bell pole line at the rear of the properties on Olive Rd.
2. ENWIN has single phase 120/240V overhead secondary conductors on the Bell pole line at the rear of the properties on Olive Rd.

Prior to working in these areas, we would suggest notifying your contractor and referring to the *Occupational Health and Safety Act* and Regulations for Construction Projects to confirm clearance requirements during construction.

Also, we suggest referring to the *Ontario Building Code* for permanent required clearances for New Building Construction.

[Keegan Morency Kendall - Hydro Engineering Technologist]



ENWIN UTILITIES LTD. - WATER ENGINEERING

Comments to Planning Consultation Stage 1 Application (File No. PC-039/25)

ENWIN Water has no objections. The proposed properties will require water servicing.

[Bruce Ogg - Water Project Review Officer]

FORESTRY

Comments to Planning Consultation Stage 1 Application (File No. PC-039/25)

There are approximately 26 City owned trees on the property

Forestry has NO concerns at this time regarding the request for a Zoning By-law Amendment.

For future Development:

Based on the conceptual site plan provided it appears that approx. 5 trees will need to be removed for the proposed development. Removal & Canopy Replacement costs may be calculated based on cost-to-remove and a Diameter-for-Diameter replacement ratio for both City owned and Private trees.

A preliminary estimate of Removal and canopy replacement fees based on the Forestry's current tree inventory: \$ 44,982.72

Forestry will require detailed site plans prior to construction that show adequate Tree Root protection zones for each individual tree if they are to be preserved on-site and those in proximity on adjacent lots through the development process. Basic Tree protection includes exclusion fencing at the 'drip-line +1 meter' distance for each tree. Full details regarding the size of protection area required, the permitted activity within the protection zone and the type of fencing for protection zones can be provided by Forestry.

If the trees are not to be preserved or cannot be adequately protected through the development process a request must be made that specific trees be removed. Removal and canopy Replacement Fees may apply.

[Mitchell Schinkel - Forestry II]

PARKS DESIGN & DEVELOPMENT

Comments to Planning Consultation Stage 1 Application (File No. PC-039/25)

The washroom is outdated, and we have no issues with it. It is planned for removal in the near future.

[Hoda Kameli - Landscape Architect]

PLANNING DEPARTMENT - HERITAGE

Comments to Planning Consultation Stage 1 Application (File No. PC-039/25)

There is no apparent built heritage concern with this property. Please be advised that a portion of the subject property (narrow sliver approximately 5m parallel to the property line fronting Olive Rd) is within the Archaeological Potential Zone (APZ). Should no ground disturbing activities (including infrastructure developments such as sewer lines, gas lines, underground hydro lines, etc.) occur within this APZ, a Stage 1 Archaeological Assessment is not required.

Should any ground disturbing activities be proposed within this APZ, it is recommended that the applicant/development proponent consult a Licensed Archaeologist for an opinion. The WAMP is a risk assessment tool, and it is the onus of the applicant/development proponent to demonstrate why they believe that a Stage 1 Archaeological Assessment should not be needed and present the evidence to justify it.

Should the applicant/development proponent uncover archaeological resources and/or human remains during ground disturbing activities, whether or not a Stage 1

Archaeological Assessment was completed, it is the responsibility of the applicant/development to stop work immediately and follow the steps within the Windsor Archaeological Management Plan 2024 Update Appendix C: Contingency Plan. This includes the standard notification process described below as well as retention of a consultant archaeologist to undertake investigation and reporting.

1. Should archaeological resources be found during grading, construction or soil removal activities, all work in the area must stop immediately and the City's Planning & Building Department, the City's Manager of Culture and Events, and the Ontario Ministry of Citizenship and Multiculturalism must be notified and confirm satisfaction of any archaeological requirements before work can recommence.
2. In the event that human remains are encountered during grading, construction or soil removal activities, all work in that area must be stopped immediately and the site secured. The local police or coroner must be contacted to determine whether or not the skeletal remains are human, and whether the remains constitute a part of a crime scene. The Local police or coroner will then notify the Ontario Ministry of Citizenship and Multiculturalism and the Registrar at the Ministry of Government and Consumer Services if needed, and notification and satisfactory confirmation be given by the Ministry of Citizenship and Multiculturalism.

Contacts:

Windsor Planning & Building Department:

519-255-6543 x6179, ttang@citywindsor.ca, planningdept@citywindsor.ca

Windsor Manager of Culture and Events (A):

Michelle Staadegaard, (O) 519-253-2300x2726, (C) 519-816-0711,

mstaadegaard@citywindsor.ca

Ontario Ministry of Citizenship and Multiculturalism

Archaeology Programs Unit, 1-416-212-8886, Archaeology@ontario.ca

Windsor Police: 911

Ontario Ministry of Government & Consumer Services

A/Registrar of Burial Sites, War Graves, Abandoned Cemeteries and Cemetery Closures, 1-416-212-7499, Crystal.Forrest@ontario.ca

[Tracy Tang - Heritage Planner]

TRANSPORTATION PLANNING

Comments to Planning Consultation Stage 1 Application (File No. PC-039/25)

No supporting information required

[Elara Mehrilou - Transportation Planner I]

WALPOLE ISLAND FIRST NATION

No comments provided

APPENDIX "F"

Draft Amending By-law

B Y - L A W N U M B E R -2025

A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600
CITED AS THE "CITY OF WINDSOR ZONING BY-LAW"

Passed the day of , 2025.

WHEREAS it is deemed expedient to further amend By-law Number 8600 of the Council of The Corporation of the City of Windsor, cited as the "City of Windsor Zoning By-law" passed the 31st day of March, 1986, as heretofore amended:

THEREFORE the Council of The Corporation of the City of Windsor enacts as follows:

1. The said by-law is amended by changing the Zoning District Maps or parts thereof referred to in Section 1, of said by-law and made part thereof, so that the lands described in Column 3 are delineated by a broken line and further identified by the zoning symbol shown in Column 5:

1. Item Number	2. Zoning District Map Part	3. Lands Affected	4. Zoning Symbol	5. New Zoning Symbol
1	11	Lots 309 to 319 & 431 to 441, Part of Lots 320 & 430, and Part of Closed Alley, Plan 1063; PIN No. 01116-0581 LT (in part) & PIN No. 01116-0587 LT (in part) (located on the south part of Kinsmen Norman Road Park and north half of abutting 7.62-metre-wide closed east/west alley, shown as the Subject Lands on Schedule 2 herein)	GD1.1	RD1.2

DREW DILKENS, MAYOR

CLERK

First Reading - , 2025

Second Reading - , 2025

Third Reading - , 2025

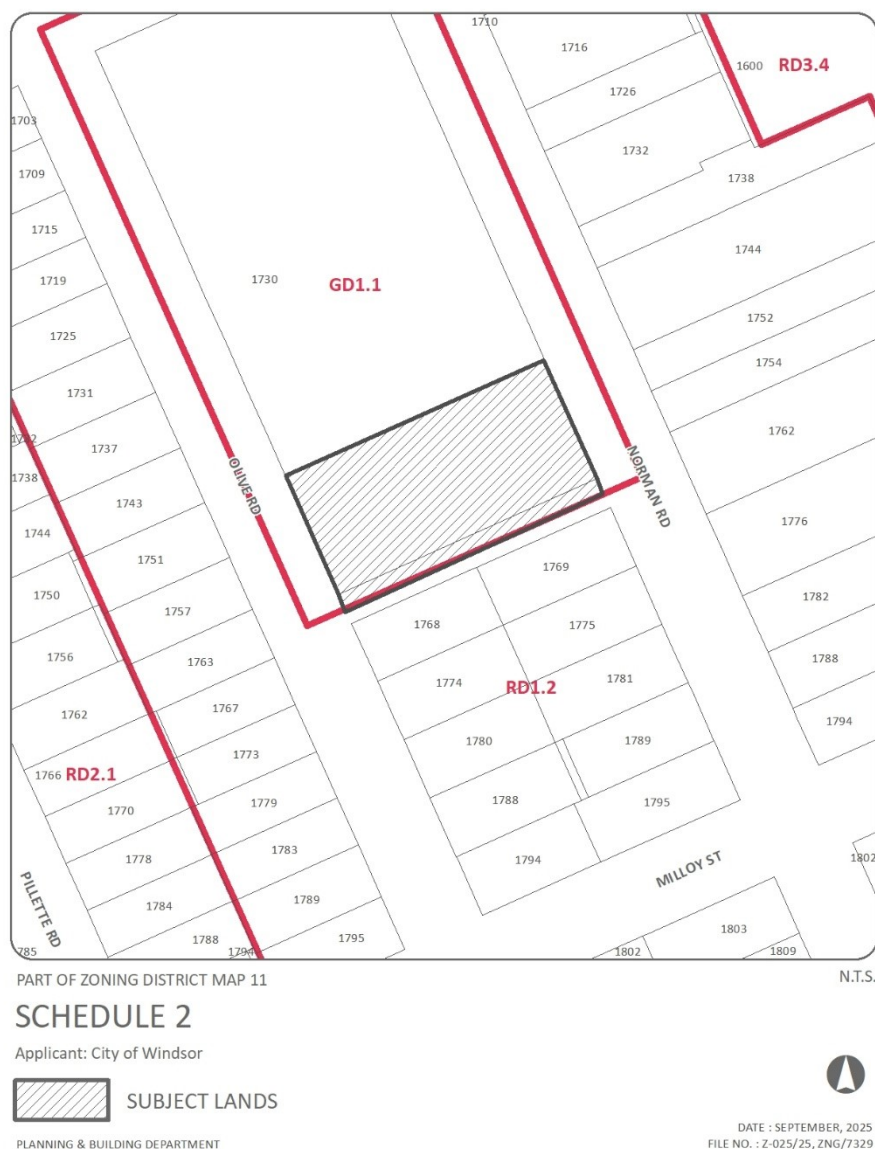
SCHEDULE 2

1. By-law _____ has the following purpose and effect:

To amend the zoning of the south part of Kinsmen Norman Road Park and north half of abutting 7.62-metre-wide closed east/west alley, legally described as Lots 309 to 319 & 431 to 441, Part of Lots 320 & 430, and Part of Closed Alley, Plan 1063; PIN No. 01116-0581 LT (in part) & PIN No. 01116-0587 LT (in part), shown as the Subject Lands on the Schedule below, so as to allow for the creation of six (6) single unit dwelling lots.

The applicant is specifically proposing to create three (3) serviced 12.0-metre-wide lots off Olive Road and Norman Road.

2. Key map showing the location of the lands to which By-law _____ applies.



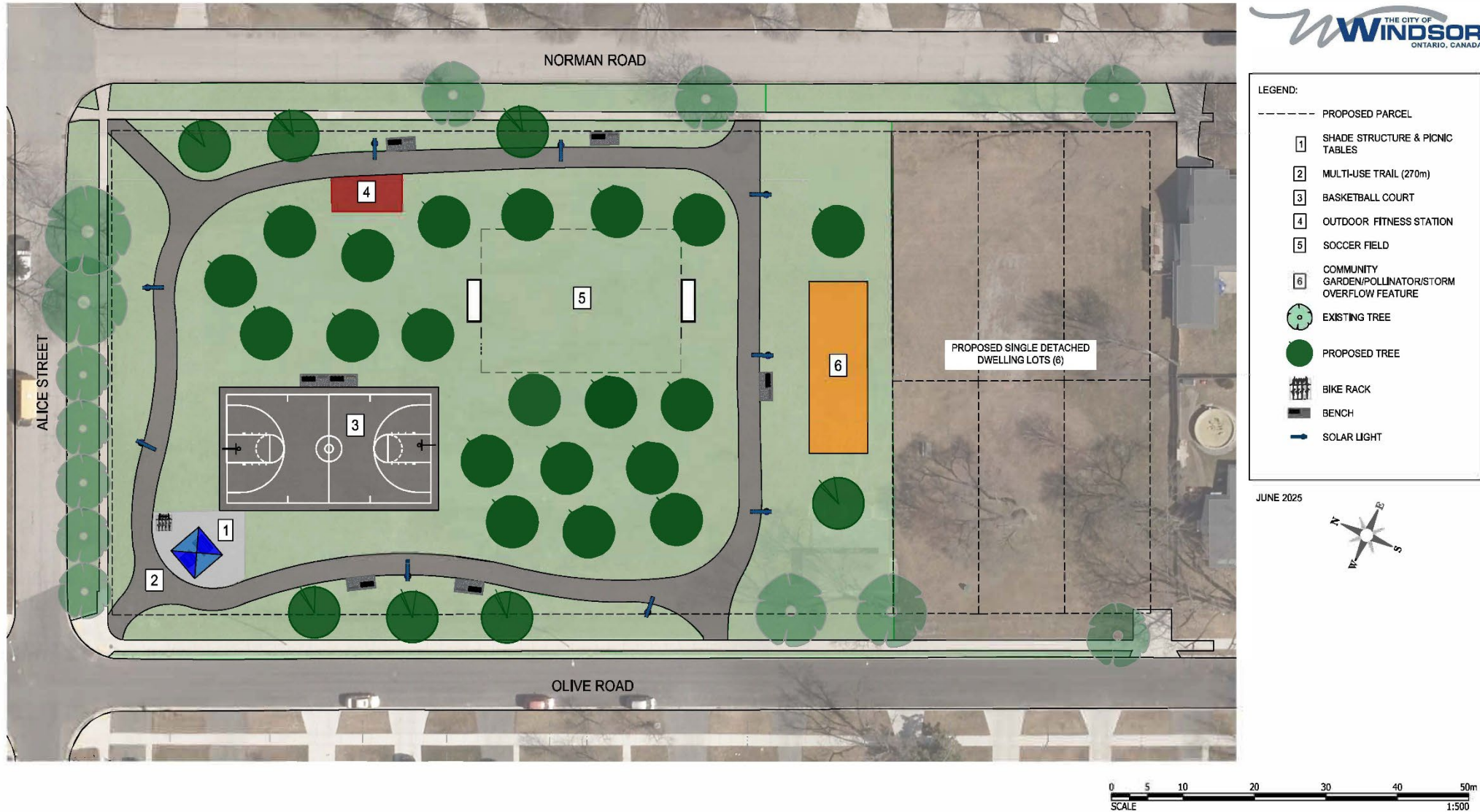
Zoning Analysis (1700 Blocks of Norman Road Olive Road)

Municipal Address	Use	Age of Main Building	Lot Width (m) (Approximate)	Lot Area (m2) (Approximate)	Density (Approximate) (uph)	Density with ADU's (Approximate) (uph)
1702 Norman Road	Single Unit Dwelling	1914	9.7	344.2	29.1	87.2
1706 Norman Road	Single Unit Dwelling	1919	9.6	343.2	29.1	87.4
1710 Norman Road	Single Unit Dwelling	1919	10.5	375.8	26.6	79.8
1726 Norman Road	Single Unit Dwelling	1919	10.7	489.9	20.4	61.2
1738 Norman Road	Single Unit Dwelling	1919	10.7	845.7	11.8	35.5
1732 Norman Road	Single Unit Dwelling	1919	19.8	881.0	11.4	34.1
1762 Norman Road	Single Unit Dwelling	1919	21.3	1,615.4	6.2	18.6
1716 Norman Road	Single Unit Dwelling	1919	24.7	1,073.5	9.3	27.9
1744 Norman Road	Single Unit Dwelling	1940	21.3	1,617.0	6.2	18.6
1789 Olive Road	Single Unit Dwelling	1947	13.6	442.2	22.6	67.8
1767 Olive Road	Single Unit Dwelling	1949	12.2	375.3	26.6	79.9
1773 Olive Road	Single Unit Dwelling	1949	12.2	375.3	26.6	79.9
1779 Olive Road	Single Unit Dwelling	1949	12.2	397.5	25.2	75.5
1709 Olive Road	Single Unit Dwelling	1949	13.6	442.2	22.6	67.8
1757 Olive Road	Single Unit Dwelling	1949	13.6	442.2	22.6	67.8
1703 Olive Road	Single Unit Dwelling	1949	13.7	447.2	22.4	67.1
1795 Olive Road	Single Unit Dwelling	1949	13.7	447.2	22.4	67.1
1725 Olive Road	Single Unit Dwelling	1949	14.0	430.7	23.2	69.7
1751 Olive Road	Single Unit Dwelling	1949	14.3	488.7	20.5	61.4
1737 Olive Road	Single Unit Dwelling	1949	14.3	467.0	21.4	64.2
1743 Olive Road	Single Unit Dwelling	1949	14.3	467.0	21.4	64.2
1731 Olive Road	Single Unit Dwelling	1949	14.3	468.1	21.4	64.1
1788 Norman Road	Single Unit Dwelling	1949	14.6	508.7	19.7	59.0
1782 Norman Road	Single Unit Dwelling	1949	14.6	508.7	19.7	59.0
1794 Norman Road	Single Unit Dwelling	1949	14.9	519.2	19.3	57.8
1768 Olive Road	Single Unit Dwelling	1949	15.1	492.7	20.3	60.9
1769 Norman Road	Single Unit Dwelling	1949	15.1	523.2	19.1	57.3
1794 Olive Road	Single Unit Dwelling	1949	15.2	469.1	21.3	64.0
1774 Olive Road	Single Unit Dwelling	1949	15.2	497.7	20.1	60.3
1780 Olive Road	Single Unit Dwelling	1949	15.2	497.7	20.1	60.3
1788 Olive Road	Single Unit Dwelling	1949	15.2	497.7	20.1	60.3
1789 Norman Road	Single Unit Dwelling	1949	15.2	498.0	20.1	60.2
1781 Norman Road	Single Unit Dwelling	1949	15.2	527.2	19.0	56.9

Zoning Analysis (1700 Blocks of Norman Road Olive Road)

1775 Norman Road	Single Unit Dwelling	1949	15.2	527.9	18.9	56.8
1795 Norman Road	Single Unit Dwelling	1949	15.2	554.6	18.0	54.1
1776 Norman Road	Single Unit Dwelling	1951	21.3	1,572.5	6.4	19.1
1719 Olive Road	Single Unit Dwelling	1955	12.2	397.5	25.2	75.5
1783 Olive Road	Single Unit Dwelling	1956	12.2	397.5	25.2	75.5
1763 Olive Road	Single Unit Dwelling	1956	12.7	412.4	24.2	72.7
1715 Olive Road	Single Unit Dwelling	1959	12.2	397.4	25.2	75.5
1752 Norman Road	Semi-Detached Dwelling Unit	1989	10.7	807.4	12.4	37.2
1754 Norman Road	Semi-Detached Dwelling Unit	1989	10.7	809.2	12.4	37.1
Average			14.3	587.9	19.9	59.7
Low						
High						

KINSMEN NORMAN ROAD PARK REDEVELOPMENT PLAN



THE CORPORATION OF THE CITY OF WINDSOR

PLANNING JUSTIFICATION REPORT

Kinsmen Norman Road Park Development
1730 Olive Road, Windsor

SEPTEMBER 2025 – 25-1503

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APPENDICES

Appendix A – Provincial Planning Statement 2024

Appendix B – City of Windsor Official Plan

1.0 INTRODUCTION

1.1 PURPOSE

Dillon Consulting Limited (Dillon) has been retained by The Corporation of the City of Windsor (hereinafter referred to as the “applicant”) to provide support for the proposed Zoning By-law Amendment and development of the southern portion of the municipally-owned Kinsmen Norman Road Park, located at 1730 Olive Road, within the City of Windsor, hereinafter referred to as the “subject site” (refer to **Figure 1: Location Map**).

The proposed amendment will permit the creation of six (6) single detached dwellings on six (6) individually subdivided lots. The remaining portion of the park, referred to as “parkland” will be retained as public open space (refer to **Figure 5: Zoning By-law Amendment – Subject Site** and **Figure 6: Conceptual Site Plan**). Proceeds from the sale of the subject site are intended to support enhancements to the retained parkland (refer to **Figure 7: Draft Park Improvement Plan**).

Figure 1: Location Map



Source: City of Windsor Online Mapping (2024)

1.2 PLANNING POLICIES

The subject site is designated accordingly in the City of Windsor Official Plan, & Zoning By-law 8600 as follows:

City of Windsor Official Plan – Schedule D: Land Use

- Residential

As indicated in **Figure 2**, the subject site is designated “Residential” under the City’s Official Plan (Windsor OP). As per S.6.2.1.2, Low Profile housing developments are buildings or structures generally no greater than three (3) storeys in height and are classified as small scale forms: single detached, semi-detached, duplex and row and multiplexes with up to 8 units(S.6.3.2.3.a). Further examination is provided in the Planning Analysis section of this report on how the proposed development meets the criteria under Policy 6.3 – ‘Residential’ and is consistent the intent of the Official Plan (OP).

City of Windsor Zoning By-law 8600 – Schedule A: Zoning District Map

- Green District 1.1 (GD1.1)

As shown in **Figure 3**, the subject site is currently zoned Green District 1.1 (GD1.1) in the City of Windsor Zoning By-law 8600. The current zoning does not permit residential uses nor does it appropriately reflect the proposed development. As such, the applicant is requesting that Council approve a Zoning By-law Amendment to the City of Windsor Zoning By-law 8600 to apply a Residential District 1.2 (RD1.2) to allow for future single detached dwellings to be constructed.

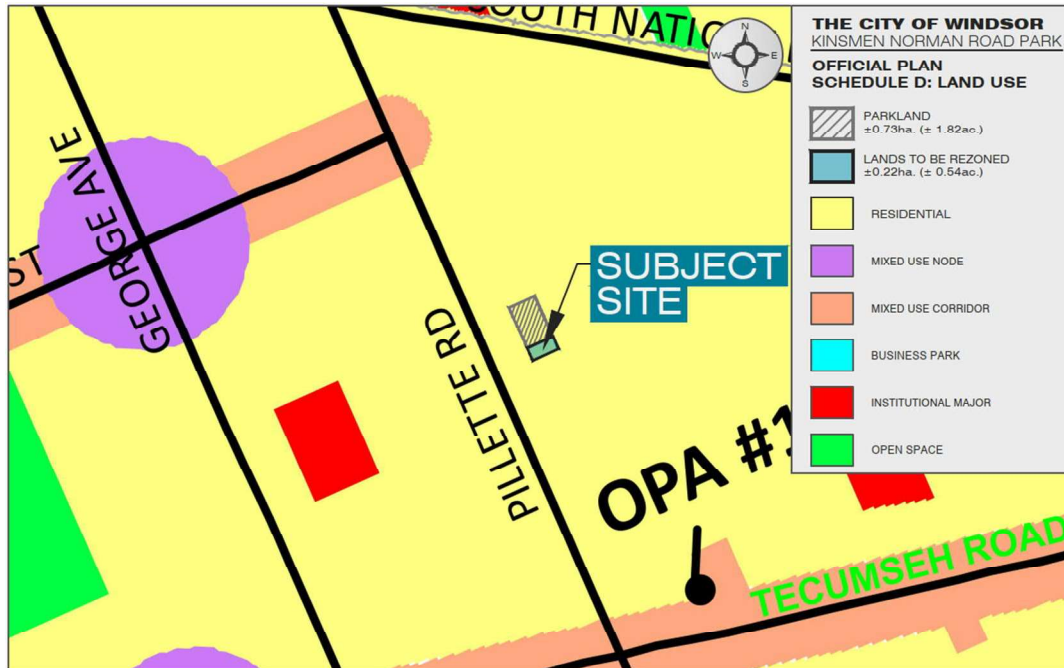
The development is complementary in massing, setback, and orientation to nearby homes, all of which are zoned RD1.2 and RD2.1 further. The proposal aligns with the prevailing low-profile residential form permitted under RD1.2 zoning: single detached dwellings, ensuring consistency in scale and use. It connects seamlessly to pedestrian and transit networks, encouraging landscaped front yards, porches, and pedestrian-scale features. This approach enhances walkability and reinforces the established neighbourhood character.

1.3 PROPOSED APPLICATION

An application for a Zoning By-law Amendment to the City of Windsor Zoning By-law 8600 is being requested to change the existing designation of the subject site from Green District (GD1.1) to Residential District 1.2 (RD1.2) to allow for future single detached dwellings to be constructed. As demonstrated in Section 3.0 of this report, the intensification of these lands for residential development is in keeping with pertinent local policies, provisions, and guidelines of the Provincial Planning Statement (2024), the City of Windsor Official Plan, and the City of Windsor Zoning By-law 8600. All other building requirements for single detached dwelling is in accordance with the Residential District 1.2 (RD1.2) Zone.

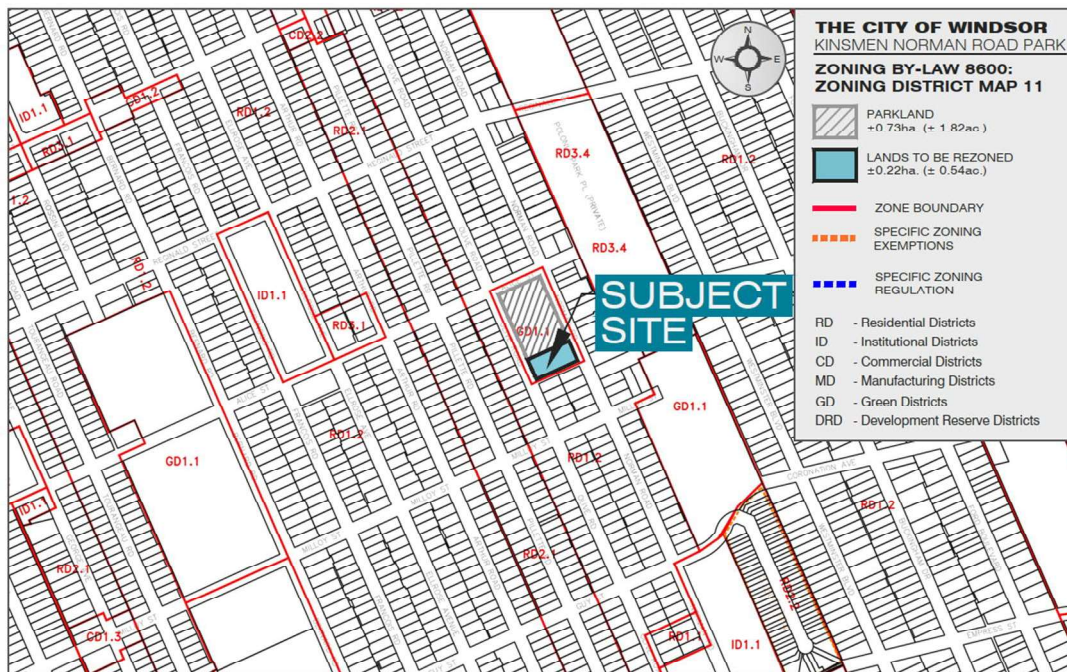
As per the Stage 1 Planning Consultation letter (PC-039/25) dated December 19, 2024, a Zoning By-law Amendment has been identified as a requirement for the proposed development. A Stage 1 site Plan Control Approval application will follow the approval of the Zoning By-law Amendment.

Figure 2: Existing City of Windsor Official Plan Designation



Source: City of Windsor Official Plan Schedule 'D,' Prepared by Counterpoint Land Development by Dillon Consulting Limited

Figure 3: Existing City of Windsor Zoning By-law 8600 Designation



Source: City of Windsor Zoning by-law 8600, Zoning District Map 11, Prepared by Counterpoint Land Development by Dillon Consulting Limited

1.4 PROPOSED DEVELOPMENT

The subject site forms the southern portion of the larger, municipally-owned Kinsmen Norman Road Park (9,826.0 m² / 105,766.2f t²), located at 1730 Olive Road, and is bounded by Olive Road to the west, Alice Road to the north and Norman Road to the east (refer to **Figure 1: Location Map**). The subject site is legally described as:

- Plan 1063; Lots 309 To 319; Lots 431 To 441; Pt Lots 320; 430 & Pt Closed Alley in The City of Windsor.

The total area of the subject site under application is 2,430.0 m² (26,156.3 ft²), approximately 24% of the existing park's site area, with ±36.0m (118.1ft.) of frontage on both Olive Road and Norman Road respectively, which are local roads within the existing low-density residential neighbourhood (refer to **Figure 5: Zoning By-law Amendment – Subject Site**).

1.4.1 Existing Conditions

Kinsmen Norman Road Park is a well-maintained but underutilized open green space featuring a gently sloping lawn with scattered mature trees and informal turf pathways, offering opportunities for passive recreation such as walking, dog-walking, and unstructured play. While the grass is regularly mowed and the trees cared for, the park lacks built amenities such as playground equipment, sports courts, seating areas, or lighting infrastructure. A public washroom facility that once served the site has been recently removed. The park currently functions more as a transitional or passive space rather than an active community hub (refer to **Figure 7: Draft Park Improvement Plan**).

The proposed rezoning would enable the sale of the subject site to a private developer with the intent to be subdivided into six (6) individual lots, each to accommodate a single detached dwelling. Proceeds from the sale are intended to support improvements to the remaining parkland, which will comprise a total area of 7,396.0 m² (79,609.8 ft²) and will continue to be retained as public open space. This strategy allows the City to retain and improve the remaining parkland while modestly increasing the housing supply through compatible infill.

1.4.2 Alley Closure

In support of the proposed development, The Corporation of the City of Windsor has submitted an application (File No. S 76/2025) to the Development & Heritage Standing Committee on July 7th, 2025, requesting the closure of the 7.62m-wide east/west alley located between Olive Road and Norman Road, directly south of the subject site. The application recommends that the north part of the alley be retained and consolidated with the subject site for the purpose of enlarging the proposed development area and avoiding the creation of a secluded area prone to illicit activities. The south half of the alley is proposed to be closed and conveyed, on an as-is condition and at cost, to the abutting property owners to the south. This alley closure and partial consolidation will enable a more efficient and secure site design, contributing to the compatibility and safety of the proposed residential development.

1.4.3 Future Parks Improvement

Importantly, implementation of these improvements is contingent upon the successful sale of the subject site. Proceeds from the residential development will be reinvested into the park to fund these upgrades. This strategy enables the City to both modestly increase local housing supply and reinvest in enhanced recreational amenities for the surrounding community, improving both functional amenities and year-round usability of the retained greenspace, and balancing development with public benefit.

1.4.5 Proposed Residential Development

The proposed development consists of the subdivision and development of the subject site into six (6) residential lots to each accommodate a single detached dwelling, permitted under the proposed Residential District 1.2 (RD1.2) zone in the City of Windsor Zoning By-law 8600. The lots are organized in a back-to-back configuration, with Lots 1-3 fronting Norman Road and Lots 4-6 fronting Olive Road, each with direct vehicular access to the adjacent public right-of-way (refer to **Figure 6: Conceptual Site Plan**).

Lots 1-3 are each 417.0 m² (4,488.5 ft²) and Lots 4-6 are each 392.0 m² (4,219.2 ft²), with an average lot frontage of 12.0m and lot depths of 34.8m (Lots 1-3) and 32.7m (Lots 4-6) respectively. The proposed lots meet the minimum lot size provisions of the RD1.2 zone as well as conform to all other applicable zoning provisions, including:

- Maximum lot coverage: 45%;
- Maximum building height: 10.0 metres; and
- Minimum setbacks: Front (6.0 m), Rear (7.5 m), Interior Side (1.2 m), and Exterior Side.

The development is intended to be compatible with the surrounding low-density residential character of the neighbourhood. The site's dual frontage provides flexibility for access and circulation while minimizing traffic impacts. No new public roads are proposed, and all required services (water, sanitary, storm, and utilities) are anticipated to be connected via existing infrastructure along Olive Road and Norman Road.

The subdivision of the subject site will be pursued through the appropriate planning approvals under the Planning Act, including consent or plan of subdivision, as required. Building permits for individual homes will be obtained following zoning approval and servicing confirmation.

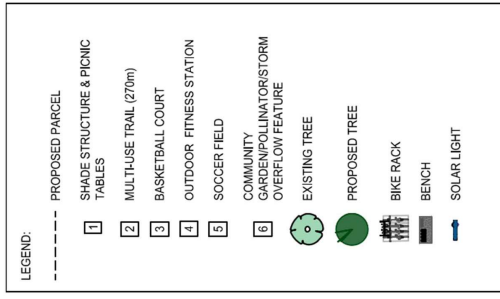
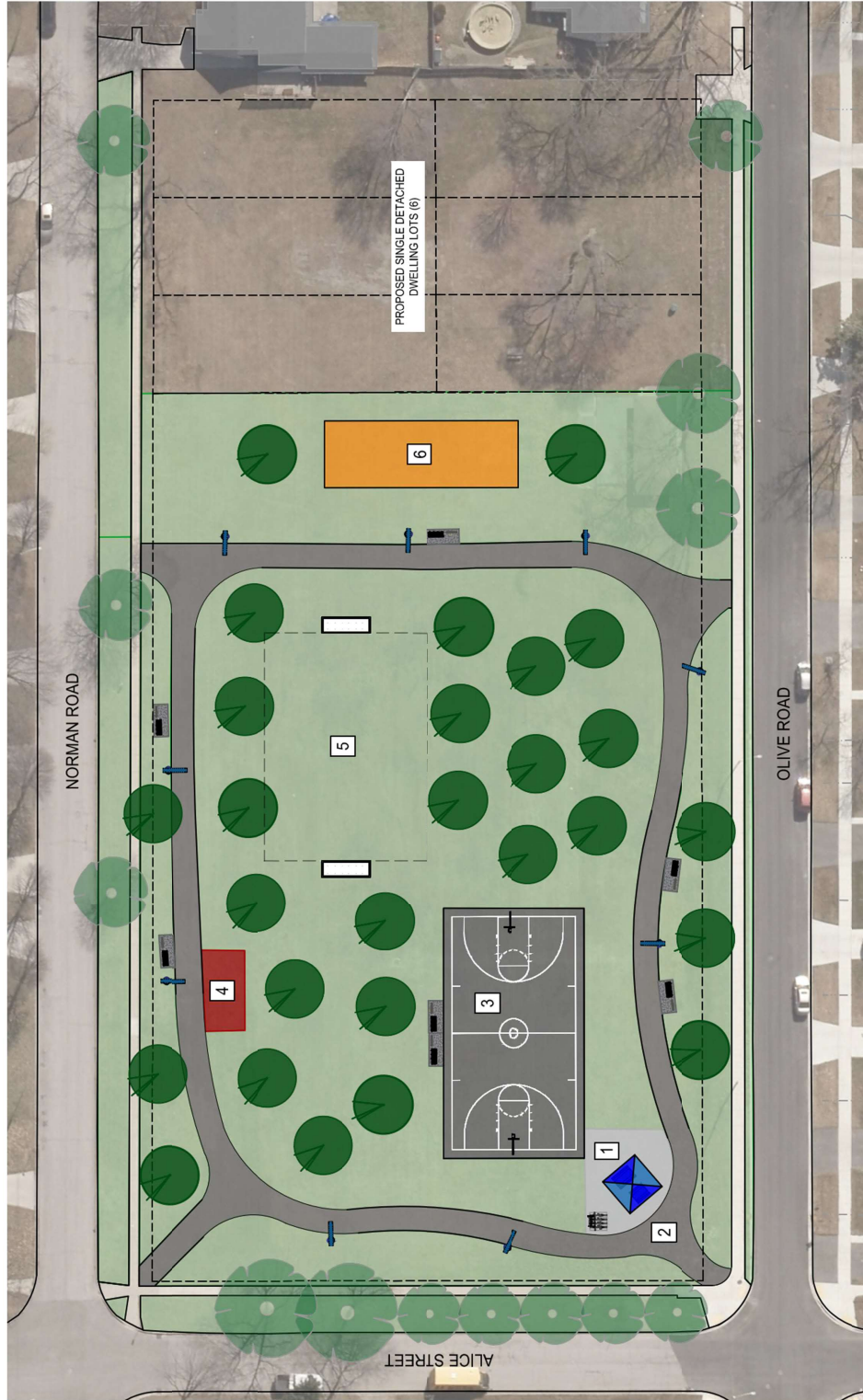
Figure 5: Zoning By-law Amendment – Subject Site



Figure 6: Conceptual Site Plan



Figure 7: Draft Park Improvement Plan



JUNE 2025



Prepared by: The City of Windsor – Parks Development

2.0 EXISTING LAND USES

2.1 SUBJECT SITE

The physical attributes of the subject site are as follows:

- A total site area of 0.24 hectares (0.60 ac);
- Rectangular shape;
- Frontage on both Olive Road and Norman Road;
- Vehicular Access is provided via both roads;
- Currently used as a Green Space and part of the Kinsmen Norman Road Park (24%);
- Transit route and stops located along Pillette Road, at both Alice Road and Milloy Road intersections; and
- Access to existing municipal services, including surcharge pond to the north within the parkland.

2.2 SURROUNDING LAND USES

The surrounding land uses are varied as shown in **Figure 12: Surrounding Land Uses** and are described as follows:

North

- **Existing Uses:** Low-rise residential built-forms (i.e. single detached dwellings)
- **Official Plan Designation(s):** Residential and Mixed Use Corridor (Pillette Rd & Seminole St.); and
- **Zone:** Residential District 1.1 (RD1.1);

East

- **Existing Uses:** Low-rise residential built-forms (i.e. single detached dwellings, semi-detached dwellings and townhouses) and Polonia Park;
- **Official Plan Designation(s):** Residential; and
- **Zone:** Residential District 1.1 (RD1.1); Residential District 3.4 (RD3.4) and Green District 1.1 (GD1.1).

South

- **Existing Uses:** Low-rise residential built-forms (i.e. single detached dwellings), Commercial Uses (Retail and Restaurants along Tecumseh Road East);
- **Official Plan Designation(s):** Residential and Mixed Use Corridor (Tecumseh Rd. E);
- **Zone:** Residential District 1.1 (RD1.1);

West

- **Existing Uses:** Low-rise residential built-forms (i.e. single detached dwellings);
- **Official Plan Designation(s):** Residential; and
- **Zone:** Residential District 1.1 (RD1.1) and Residential District 2.1 (RD2.1);

Existing Neighboring Parks:

- **Kinsmen Norman Road Park:** immediately north of subject site, upgrades to park intended.
- **Polonia Park:** ~2–3 minutes' walk east; offers a soccer field, baseball diamond, basketball key, walking trails, and an accessible playground.
- **AKO Park:** ~10 minutes' walk southeast; features open green space, mature trees, and informal recreational areas.
- **Thomson Park:** ~12 minutes' walk southwest; includes playgrounds, walking paths, and passive green space within a quiet residential setting.
- **Pykes Park:** ~12 minutes' walk northwest; a small neighbourhood park with benches, pathways, and landscaped areas.
- **Sommes Park:** ~13 minutes' walk south; includes playground equipment, shade trees, and seating areas for local residents.
- **Jackson Park:** ~3 km away (~30–35 minute walk or 5–7 minute drive); a major regional park featuring sunken gardens, war memorials, seasonal lighting displays, and extensive walking trails.
- **Mic Mac Park:** ~4–5 km away (~8–10 minute drive); a large urban park known for expansive open space, sports facilities, water play areas, and multi-use trails.

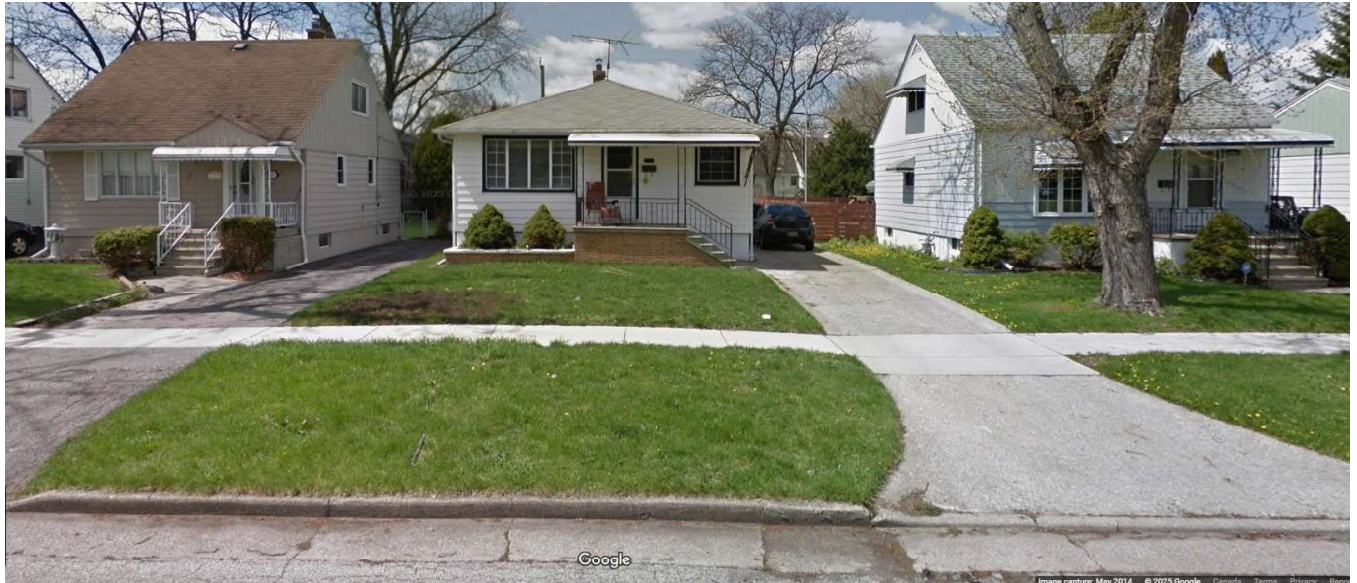
The subject site is exceptionally well-situated within a mature residential neighbourhood that is richly served by a network of parks and open spaces. With direct access to Kinsmen Norman Road Park and several neighbourhood parks including Polonia Park, AKO Park, Thomson Park, Pykes Park, and Sommes Park, all within a ~15-minute walk, the site offers future residents a high level of walkable recreational amenity. In addition, proximity to larger regional parks such as Jackson Park and Mic Mac Park further enhances the site's livability and reinforces its suitability for modest residential intensification. This well-connected green space context supports healthy, active living and aligns with broader city-building objectives related to complete communities and sustainable neighbourhood design.

Existing Schools:

- **David Maxwell Public School:** 0.25 km, about a 3-minute walk;
- **Ford City Public School:** 0.8 km, ~ 10-minute walk;
- **Coronation Public School:** 1.2 km, ~ 15-minute walk;
- **W.F. Herman Academy (Secondary School):** 1.5 km, ~ 18-minute walk; and
- **École élémentaire Catholique Sainte-Thérèse:** 1.6 km, ~ 20-minute walk.

The subject site is well-served by a range of educational institutions within the surrounding area, offering both English and French-language education options at the elementary and secondary levels, contributing to the area's suitability for families.

Figure 8: Existing Single Detached Dwellings immediately across Subject Site, along Olive Road



Source: 2025 Google Street View – Image Capture (Jan. 2021)

Figure 9: Existing Single Detached Dwellings south of Subject Site, along Norman Road



Source: 2025 Google Street View – Image Capture (May 2014)

Figure 10: Existing Single Detached Dwellings immediately across Subject Site, along Norman Road



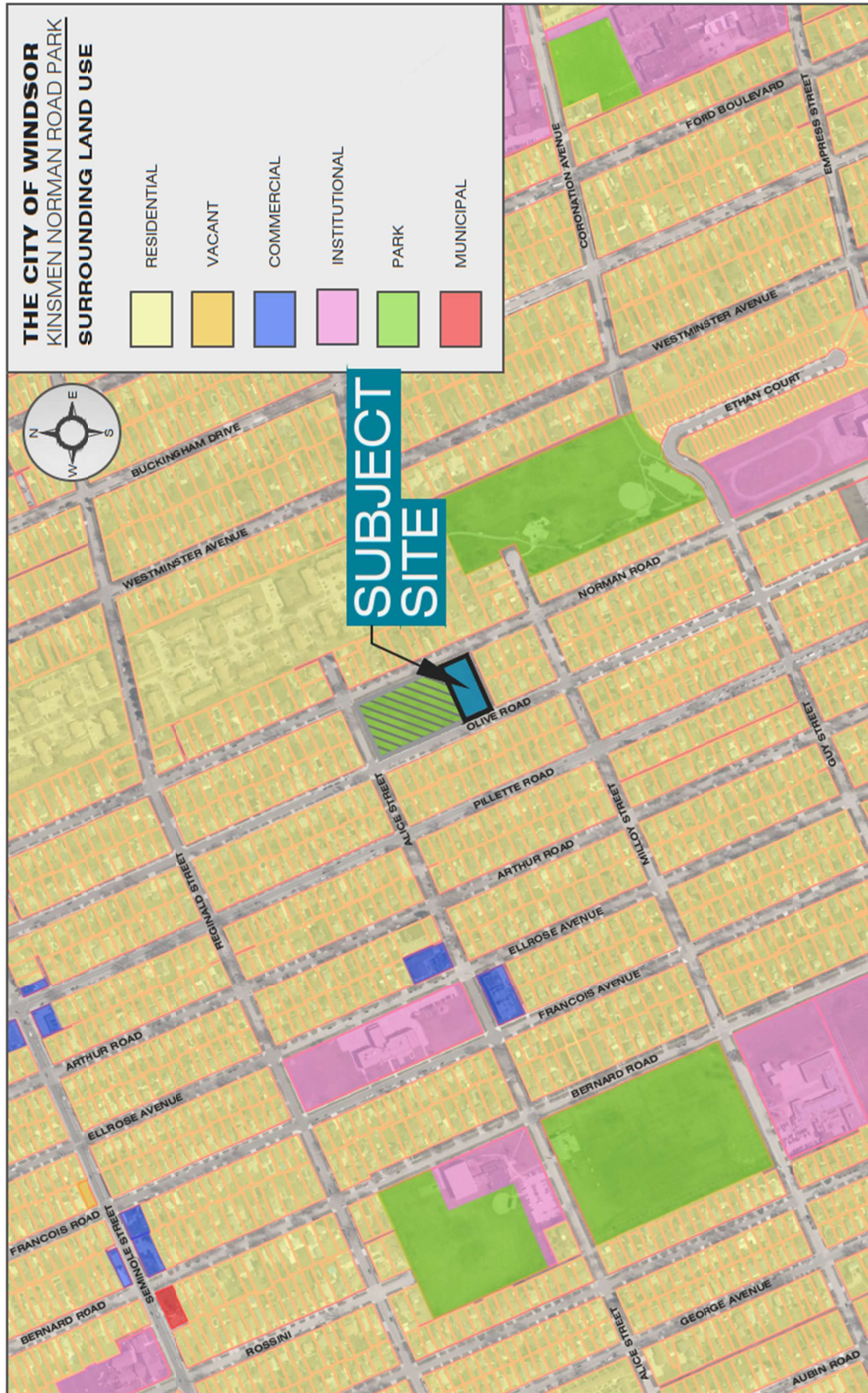
Source: 2025 Google Street View – Image Capture (Jan. 2021)

Figure 11: Existing Single Detached Dwellings south of Subject Site, along Norman Road



Source: 2025 Google Street View – Image Capture (Jan. 2021)

Figure 12: Surrounding Land Uses



3.0 PLANNING EVALUATION

To determine the feasibility and appropriateness of the proposed development, a comprehensive evaluation of the potential planning issues and impacts has been undertaken. The scope and level of detail of the planning evaluation has been based on:

- Provincial Planning Statement (2024);
- City of Windsor Official Plan policies and criteria;
- City of Windsor Zoning By-Law 8600 regulations; and
- Visual inspections of the site and surrounding lands.

Recognizing that overlaps exist between the various policies and criteria in the Official Plan, the approach used attempts to consolidate the relevant policies and criteria to identify and evaluate the compliance and/or potential planning and land use related issues associated with the proposed residential development.

3.1 PROVINCIAL PLANNING STATEMENT

The Provincial Planning Statement, 2024 (PPS 2024) is a consolidated land use planning policy framework that guides decision-making on growth, development, and resource management across the province. It emphasizes flexibility and local autonomy while maintaining provincial interests in building strong, healthy communities. Key priorities include increasing housing supply and affordability, supporting economic development, protecting the environment and agricultural lands, and ensuring infrastructure and public service facilities are available to meet current and future needs. The statement promotes a balanced approach to intensification, greenfield development, and rural growth, aligning planning decisions with long-term sustainability goals. All decisions affecting planning matters shall be consistent with the revised Provincial Planning Statement (refer to [Appendix A – Provincial Planning Statement](#)).

3.2 CITY OF WINDSOR OFFICIAL PLAN

The local policy context is provided in the City of Windsor Official Plan (OP) which contains a holistic set of goals, objectives, and policies to manage and direct growth in the municipality. The primary goals of the City of Windsor OP include:

- Fostering safe, inclusive communities by promoting diverse housing, vibrant neighborhood centers, and active citizen participation while preserving neighborhood character;
- Supporting sustainable economic growth through compact, residential developments, revitalization, and efficient infrastructure to strengthen the City Centre and employment hubs; and
- Promoting environmental sustainability by encouraging energy-efficient designs, expanding green spaces, and integrating sustainable transportation for a healthy urban environment.

In 2022, the City of Windsor adopted Official Plan Amendment No. 159 to provide further policy direction in regards to residential intensification and residential development. The purpose of this amendment is to encourage the production of affordable and attainable housing through residential intensification within the City of Windsor.

The subject site is currently designated as Residential in the Official Plan. The proposed development is in conformance with the basic land use policies outlined in the existing OP (refer to [Appendix B – City of Windsor Official Plan Policies](#)).

3.3 CITY OF WINDSOR ZONING BY-LAW 8600

The City of Windsor’s Zoning By-Law 8600 designates the subject lands Green District 1.1 (GD1.1). As mentioned in Section 2 of this report, the current zoning on the site does not permit the proposed residential development. As such, the applicant is requesting that Council approve a Zoning By-law Amendment to the City of Windsor Zoning By-law 8600 to apply a Residential District 1.2 (RD1.2).

The proposed development of the subject site is consistent with the Provincial Planning Statement 2024, and the City of Windsor Official Plan policies by encouraging opportunities for development, intensification, and revitalization of areas with sufficient existing and planned infrastructure as well as a full range of housing types and densities. The proposed development will support the future growth of the community and provide a greater diversity of uses in the area. The proposed development is compatible with the surrounding land uses.

3.4 ‘HOUSING SOLUTIONS MADE FOR WINDSOR’

‘Housing Solutions Made for Windsor’ is a comprehensive municipal strategy introduced in 2024 to accelerate housing development in response to rapid population growth and the broader provincial housing crisis. Since 2020, Windsor’s metropolitan area has grown by over 17,000 residents, intensifying demand for diverse and affordable housing options.

The initiative serves as the City’s localized response to Ontario’s More Homes Built Faster Act (Bill 23) and is tied to Windsor’s Municipal Housing Pledge to deliver 13,000 new homes by 2031; the city’s share of the province’s goal to construct 1.5 million homes across Ontario. To meet this target, the City has adopted a multi-faceted approach that includes unlocking surplus and underutilized municipal lands, streamlining approvals, incentivizing infill development, and supporting a full spectrum of housing types, including affordable, rental, and “missing middle” forms.

The proposed development of a portion of the Kinsmen Norman Road Park aligns with these objectives. It reflects the City’s strategy to leverage municipal assets for modest, context-sensitive intensification, while reinvesting proceeds from land sales into public amenities. In this case, the remaining parkland will be retained and improved, balancing the need for new housing with the preservation of open space. Overall, the initiative embodies a place-based approach to housing delivery—facilitating gentle density, respecting neighbourhood character, and contributing meaningfully to both provincial housing goals and local quality-of-life outcomes.

3.5 PLANNING ANALYSIS AND CONSIDERATIONS

The proposed development of the Subject Site is consistent with the Provincial Planning Statement 2024, and the City of Windsor Official Plan policies by encouraging opportunities for development, intensification, and revitalization of areas with sufficient existing and planned infrastructure as well as a full range of housing types and densities. The proposed development will support the future growth of the community and expand the existing built form of the area. The proposed development is compatible with the surrounding land uses.

3.5.1 Site Suitability

The subject site is ideally suited for the proposed residential development for the following reasons:

- The land area is sufficient to accommodate this intensified development allowing for additional low density residential units in the neighbourhood and ensuring compatibility (OP S.6.3.1.7; PPS S.2.1.4, S.2.2.1.c);
- The proposed single detached dwellings are consistent in built form, scale, and density with existing residential buildings, and will integrate seamlessly into the neighbourhood, maintaining the established character and supporting gradual, compatible infill (OP S.6.3.2.5.c, S.8.7.2.1.a, S.8.7.2.3);
- Kinsmen Norman Road Park directly abuts the site, and several other parks including Polonia Park, AKO Park, Thomson Park, Pykes Park and Sommes Park, amongst many others, are within a 15-minute walk. These are accessible through the existing network of sidewalks, offering recreational opportunities, and promoting active transportation (OP S.4.2.1.2, S.6.3.1.5, S.7.2.1.4, S.7.2.1.5, S.7.2.2.5, S.7.2.3.1; PPS S.2.1.6.a, S.2.2.1.c, S.3.9.1.a,b);
- The development expands housing options in an existing residential area. This contributes to an expansion of the housing stock, accommodating various demographics, including individuals, families, and seniors. This variety effectively meets the increasing demand for affordable and diverse housing solutions (OP S.3.2.1.2, S.4.2.3.4, S.6.3.1.1; PPS S.2.1.4, S.2.1.6.c, S.2.2.1.a,b, S.2.3.1.3);
- The location of the proposed development is appropriate in that it is in close proximity to commercial uses and recreational connections in the surrounding area along Tecumseh Road East and Seminole Road (OP S.6.3.1.5; PPS S.2.1.6.a); and
- The site's size and dual frontage enable a compact lot layout with direct street access, avoiding the need for new roads while optimizing underused parkland. The back-to-back configuration maximizes land efficiency and maintains neighbourhood rhythm (OP S.7.2.1.2, S.7.3.1.3; PPS S.2.2.1.c, S.2.3.1.2.a);
- The proposed alley closure and consolidation will mitigate safety concerns related to unused laneways, improve site security, and support a more coherent lot layout (PPS S.2.1.6, S.2.3.1.2.a);
- The proposal advances PPS 2024 and Official Plan goals for intensification, housing diversity, and complete communities by adding family-friendly, ground-oriented housing within an established urban fabric (PPS S.2.1.6, S.2.2.1.b, S.2.3.1.3, S.2.9.1.a; OP S.6.3.1.1, S.6.3.1.3).

In summary, the proposed development presents a thoughtful infill opportunity that modestly increases housing supply within an established, well-served neighbourhood (OP S.6.3.1.3; PPS S.2.3.1.1). By introducing six single detached dwellings that match the scale and character of surrounding homes, the project supports gentle density, optimizes existing infrastructure (OP S.6.3.2.5.c, S.8.7.2.1.a, S.8.7.2.3; PPS S.2.2.1.c, S.3.1.2.a), and enhances access to housing for a range of household types. Allowing more people to live in a connected, amenity-rich area aligns with principles of complete, compact communities and strengthens neighbourhood resilience over time (OP S.4.2.4.1, S.4.2.4.3; PPS S.2.1.6, S.2.9.1.a).

3.5.2 Intensification

The proposed development adheres to the City of Windsor Intensification Guidelines, offering guidance for the design of new developments that harmonize the use, scale, architecture, streetscapes, and neighbourhoods in Windsor. Specifically addressing new housing development, the City aims to:

- Manage the transition between new development and existing lower density residential neighborhoods;
- Control the scale and massing of new development, especially when considering a more intensified form; and
- Mitigate any adverse effects on adjacent built forms and the comfort and use of open spaces and streets.

The proposed development represents a sensitive and strategic form of intensification within a mature, low-density neighbourhood (OP S.6.3.1.3; PPS S.2.2.1.b.2). By introducing six single detached dwellings on underutilized municipal land, the project delivers new housing without altering the area's character or built form (OP S.6.3.2.5.c, S.8.7.2.1.a).

Consistent with the City of Windsor’s Intensification Guidelines, the proposal ensures a smooth transition in scale (OP S.6.3.2.5.f), protects adjacent residential amenity, and reinforces the existing streetscape (OP S.8.7.2.1.a, S.8.7.2.1.d, S.8.7.2.3). The dual frontage along Olive Road and Norman Road supports an active, pedestrian-oriented interface and enables efficient lot configuration without new road infrastructure (PPS S.2.2.1.c, S.2.3.1.2.a).

Importantly, this modest increase in population brings more residents into a well-served community with access to parks, schools, transit, and retail (OP S.6.3.1.5, S.7.2.3.1.b; PPS S.2.1.6.a). It enhances the viability of local amenities, supports complete community goals, and demonstrates how low-impact infill can contribute meaningfully to housing supply and neighbourhood vitality (PPS S.2.1.6, S.2.9.1.a).

3.5.3 Compatibility

The proposed single detached dwellings are functionally and contextually compatible with the surrounding low-density residential uses (OP S.6.3; PPS 2024) (OP S.6.3.1.1, S.6.3.2.5.c, S.8.7.2.1.a, S.8.7.2.3). The site is designated Residential in the Official Plan, which supports low-profile housing forms and encourages gentle intensification that respects neighbourhood character, without introducing land use conflicts or significant disruption (OP S.6.3.1.1, S.6.3.2.5.c, S.8.7.2.1.a, S.8.7.2.3).

Municipalities are directed to accommodate growth within serviced settlement areas through compatible development (PPS, S.2.1). This proposal does not introduce a new land use but reinforces the existing residential pattern, using existing infrastructure without generating land use conflict or adverse impact (PPS S.2.3.1.1, S.2.3.1.3, S.3.1.1.c, S.3.1.2.a; OP S.7.3.1.3).

By modestly increasing residential density within an established neighbourhood, the development supports walkability, infrastructure efficiency, and local amenity use, key components of complete communities as defined by both provincial and municipal policy frameworks (OP S.7.2.1.5; PPS S.2.1.6, S.2.2.1.c, S.2.3.1.2, S.3.1.1, S.3.9.1.a).

3.5.4 Natural Environment Impacts

The proposed development is not anticipated to result in any significant negative impacts to the natural environment. While some trees are proposed for removal to accommodate the development, the affected trees are not part of a significant woodland or natural heritage feature (PPS S.4.1.5.b). Some trees will be preserved, and additional efforts during the detailed design phase may allow for further tree retention and landscaping enhancements (OP S.8.5.2.8).

3.5.5 Municipal Service Impacts

The proposed development will be fully serviced by existing municipal infrastructure, including water, sanitary, and stormwater systems, without requiring major upgrades or extensions (OP S.7.3.1.3, S.7.3.2.3; PPS S.3.1.1, S.3.1.2.a, S.3.6.1, S.3.6.2). Each of the six single detached dwellings will connect directly to the existing storm and sanitary networks along Olive Road and Norman Road via private service connections.

A Stormwater Management Memo by Dillon Consulting Limited, dated July 2025, was prepared in support of the application, confirming that stormwater flows will be discharged unrestricted into the existing storm network. To mitigate potential impacts from increased runoff due to new impervious surfaces, a surcharge pond is proposed within the retained parkland north of the site. This facility will provide temporary storage during major storm events, including the 100-year storm and Urban Stress Test (UST), and relieve pressure on the municipal system, ensuring post-development conditions do not negatively impact downstream infrastructure (PPS S.2.9.1.b, S.3.6.8, S.3.6.8.b,c,d,f,g). Overall, the development demonstrates alignment with best practices in low-impact servicing and stormwater management, and is not anticipated to generate adverse impacts on municipal infrastructure capacity (PPS S.3.1.1.c).

3.5.6 Social and/or Economic Considerations

The proposed development offers substantial social and economic benefits by expanding the existing housing options that cater to various demographic needs, including individuals, families, and seniors, thereby promoting social diversity and inclusivity (OP S.3.2.1.2, S.4.2.3.4, S.6.3.1.1; PPS S.2.1.4, S.2.1.6.c, S.2.2.1.b). It also adds family-oriented housing within walking distance of parks, schools, and transit, helping meet the needs of a growing and diversifying population (OP S.4.2.1.2, S.6.3.1.5, S.7.2.3.1.b; PPS S.2.1.6.a, S.3.9.1.b). The development also supports inclusivity and affordability by increasing ground-oriented housing supply without requiring major public investment in new infrastructure or services (PPS S.2.2.1.a,

S.3.1.2.a). The project promotes more efficient use of existing municipal assets, generates property tax revenue, and supports local businesses through increased foot traffic and neighbourhood vitality (PPS S.3.1.1.a). This aligns with PPS 2024 objectives to optimize infrastructure, promote complete communities, and support long-term economic sustainability through compact urban growth (PPS S.2.1.6, S.2.3.1.2.b, S.2.9.1.a, S.3.1.1.a).

3.5.7 Housing

Housing is the primary rationale for this application and must be understood within the context of mounting pressures on local housing supply, affordability, and demographic change (PPS S.1 Vision, S.2.1.4, S.2.2.1). The proposed development directly supports the objectives of Section 6.3: Residential of the City of Windsor Official Plan, which calls for a range of housing types, tenures, and densities that respond to the needs of existing and future residents, while reinforcing the stability and identity of neighbourhoods (OP S.3.2.1.2, S.4.2.3.4, S.6.3.1.1).

The Official Plan encourages intensification in a manner that is compatible, context-sensitive, and designed to make efficient use of land and infrastructure (OP S.6.3.1.3, S.6.3.2.5.c, S.7.3.1.3). This development achieves precisely that with the introduction of six (6) single detached dwellings in an area that is already serviced, walkable, and proximate to community amenities, without requiring major public investment or disruptive land use change (OP S.6.3.1.5, S.7.2.3.1.b; PPS S.2.1.6.a, S.2.2.1.c, S.3.1.2.a). It responds to Windsor's growing demand for low-rise housing suitable for families, downsizing seniors, and moderate-income households, in a city where rental and ownership affordability have both declined sharply in recent years (PPS S.1 Vision, S.2.2.1.a).

Critically, this proposal addresses the persistent gap between policy intent and implementation. While the City has articulated strong policy support for housing diversity and intensification, actual delivery of infill housing at a neighbourhood scale has often been constrained. This application offers a rare opportunity to implement Official Plan policy in a tangible, modest, and strategically located way.

Moreover, the development aligns with broader provincial goals under PPS 2024, which prioritize housing supply, compact growth, and complete communities (PPS S.1 Vision, S.2.1.6, S.2.2.1, S.2.3.1.2, S.2.9.1.a). The PPS explicitly directs municipalities to support intensification that contributes to housing affordability and a range of dwelling types within existing built-up areas (PPS S.2.2.1.a,b, S.2.3.1.3). This proposal meets that directive without compromising neighbourhood character or environmental function. Importantly, the proposal reflects the City's Housing Solutions Made for Windsor (2024) strategy, which calls for unlocking underutilized municipal lands and streamlining approvals to meet its housing pledge under the More Homes Built Faster Act (Bill 23). This project contributes directly to Windsor's commitment to deliver 13,000 new homes by 2031.

In summary, the proposed development represents a necessary and well-considered response to Windsor's housing challenge, one that advances municipal and provincial policy goals, supports demographic and market needs, and reflects responsible land use planning in the public interest (PPS S.6.1.1).

3.5.8 Economic Development Innovation

The proposed development reflects an innovative municipal strategy to address the housing crisis and infrastructure reinvestment by selectively repurposing underutilized public land (PPS S.1 Vision, S.2.2.1.b.2, S.2.3.1.3, S.3.1.1.b). The decision to sell a small, non-programmed portion of Kinsmen Norman Road Park, while retaining the majority of the greenspace (over 75% of the park), is a deliberate, balanced approach that aligns with Windsor's 2024 'Housing Solutions Made for Windsor' strategy and broader goals under the More Homes Built Faster Act (Bill 23).

Rather than relying solely on external funding or property tax increases, the City is proactively leveraging its own land assets to simultaneously meet two critical objectives: delivering much-needed housing and reinvesting in public amenities (PPS S.3.1.1.b, S.3.9.1.b, S.2.2.1.b.2). This approach ensures that the proceeds from the land sale are directly reinvested into the retained parkland, enabling the development of new trails, sports courts, outdoor fitness infrastructure, and stormwater improvements that would otherwise remain unfunded (OP S.4.2.1.2, S.6.3.1.5; PPS S.2.9.1.b, S.3.6.8, S.3.9.1.b).

This model represents a shift in how municipalities manage land and infrastructure renewal in the context of constrained budgets and rising service demands. It demonstrates place-based, fiscally responsible planning, where growth and public benefit are integrated (PPS S.3.1.1.a, S.6.2.1.a). By monetizing a small, developable portion of a larger park while maintaining public access to open space, the City is advancing complete community outcomes without sacrificing the integrity of neighbourhood green infrastructure (PPS S.2.1.6.a, S.2.9.1.d, S.3.9.1.b).

This land disposition strategy may serve as a replicable model for other municipalities facing similar tensions between growth targets, funding limitations, and infrastructure renewal needs. It reflects a practical and policy-supported evolution in municipal land stewardship, balancing intensification, housing delivery, and community reinvestment in a single, integrated action. It transforms a land sale into a long-term public good, attracting new residents, enhancing quality of life, and setting a precedent for equitable, fiscally responsible urban growth (PPS S.1 Vision, S.6.1.1)

3.5.9 Transportation

The proposed development supports a transit-oriented and pedestrian-friendly design in line with the City of Windsor's Official Plan and Transit Master Plan (OP S.7.2.1.5, S.7.2.2.5; PPS S.2.2.1.d, S.2.3.1.2.d, S.2.9.1.a). The subject site is within ~250 metres of existing bus stops along Pillette Road, served by:

- Route 3 – Central (30-minute peak frequency);
- Route 1C – Transway (10-minute peak frequency); and
- Route 418X – Express (30-minute peak frequency) via nearby Tecumseh Road East.

These routes provide strong east-west and north-south connectivity, supporting reduced car dependency and promoting sustainable travel options (PPS S.3.2.1, S.3.2.2).

The proposed dwellings front onto Norman Road and Olive Road, reinforcing a walkable streetscape (OP S.7.2.3.1.c). Each lot includes one (1) off-street parking space in accordance with Zoning By-law 8600 (OP

S.7.2.6.18.b). Overall, the development leverages existing transit infrastructure and aligns with the City's broader goals for compact, connected, and mobility-oriented communities (OP S.7.2.1.2, S.7.2.1.4, S.7.2.1.5; PPS S.2.1.6.a, S.2.2.1.d, S.2.3.1.2.d, S.2.9.1.a, S.3.2.2, S.3.2.3).

3.5.10 Good Planning

The proposed development and rezoning represents good planning as it addresses the intent of the City of Windsor to promote a healthy, liveable community contributing to the guiding principles of the Provincial Planning Statement, City of Windsor Official Plan, its Zoning By-law 8600 and the 'Housing Solutions Made for Windsor' (2024) strategy (OP S.3.2; PPS S.6.1.1). Residential uses on the subject site represent an efficient development pattern that optimizes the use of land by developing an underutilized site (PPS S.2.2.1.c, S.2.3.1.2.a, S.3.1.2.a). The proposed development will blend well with the existing residential uses, while providing a natural extension of the residential built form in the area (OP S.6.3.2.5.c, S.8.7.2.1.a, S.8.7.2.3). It contributes to a high quality of life by fostering a "live, work, and play" environment where existing and future residents experience a strong sense of community and collective pride in their surroundings (OP S.4.2.4.1; PPS S.1 Vision, S.2.1.6.c). The proposed development is consistent with the policies outlined in the Provincial Planning Statement, City of Windsor Official Plan and Zoning By-Law, and represents good planning (OP S.3.2; PPS S.6.1.1).

3.6 BACKGROUND STUDIES

As identified through the Stage 1 Planning Consultation Letter dated April 28, 2025 (PC-039-025) to the City of Windsor, the following supporting background studies were identified as required for the Zoning By-law Amendment application.

3.6.1 Public Open House

A Public Open House was held on Thursday June 26th, 2025, at St. Theresa's catholic Church between 6:00pm to 7:30pm. The meeting was offered in a hybrid format, providing both in-person and virtual attendance options to ensure accessibility and encourage broader community participation in the discussion regarding the proposed rezoning and future development. The meeting was advertised in advance through public notices and on the City's webpage. The objective was to provide residents and stakeholders with an opportunity to learn more about the proposal, ask questions, and share feedback. A two-week comment period was made available following the Public Open House, in accordance with municipal engagement protocols. Comments could be submitted via email, mail and phone until July 11th, 2025.

An Engagement Summary Report has been prepared and is provided under separate cover which includes the Open House Notice and presentation panels, along with responses to comments received by residents. No major concerns were highlighted.

3.6.2 SWM Memo

The Storm Water Management Memo was prepared by Dillon Consulting Limited (September 2025) to support future residential development. The report confirms that flows from the proposed future single-

detached dwellings will be released unrestricted into Flows from the proposed single-detached dwellings will be released unrestricted into the receiving storm network on Norman Road and Olive Road through direct private storm connections and surface discharge to the streets. An underground surcharge storage chamber is proposed on the existing park's footprint, located directly north of the proposed dwellings within Kinsmen Park. This chamber will serve as a relief for the overall system, designed to compensate for the increase in runoff produced by the increased impervious surface due to development. Further geotechnical investigation is required for the detailed design of the chambers, including verification of groundwater conditions and bearing capacity assessment of the underlying soils.

3.6.3 Summary

The supporting background studies prepared for the Zoning By-law Amendment application demonstrated that the proposed development is technically feasible and appropriate for the site. The SWM Memo confirmed that the site can be fully serviced by existing municipal infrastructure without constraints. The Public Open House, both in person and virtually, generated some comments and feedback which were addressed in the Engagement Summary Report. No major concerns were highlighted. Collectively, these studies and reports provide a comprehensive planning and technical foundation to support the proposed residential development and rezoning.

4.0 CONCLUSION

The proposed Zoning By-law Amendment to rezone a portion of the municipally owned Kinsmen Norman Road Park at 1730 Olive Road from Green District 1.1 (GD1.1) to Residential District 1.2 (RD1.2) is appropriate, justified, and represents good planning. The amendment will facilitate the development of six (6) single detached dwellings within an established, well-serviced neighbourhood, delivering modest, ground-oriented housing that aligns with the City's goals for intensification, housing choice, and community reinvestment. The proposed application should be approved by the City of Windsor Council as it:

- Is consistent with the PPS 2024; supporting compact growth, housing supply, infrastructure optimization, and complete communities;
- Meets the intent and purpose of the City of Windsor Official Plan; particularly Section 6.3: Residential, by promoting compatible, low-profile residential infill;
- Is a site that is physically suitable; with full municipal servicing and no need for infrastructure upgrades;
- Will contribute to the surrounding residential uses and enjoyment of area residents;
- Respects the character and function of surrounding land uses, introducing gentle density without adverse impacts on neighbouring properties;
- Will have favourable positive impacts for the City of Windsor providing for additional residential opportunities;
- Will be integrated into the local multimodal transportation network; being located within walking distance of existing transit routes and embedded within a walkable street grid;
- Will not have negative impacts on municipal services;
- Will not have negative natural environmental impacts;
- Will generate positive social and economic outcomes, adding family-oriented housing, reinvesting in public parkland, and supporting local vitality;
- Reflects the objectives of the City's 'Housing Solutions Made for Windsor' strategy, including unlocking surplus land and streamlining approvals to meet local housing pledges under Bill 23; and
- Is fiscally responsible, reinvesting public value for public benefit through planned upgrades to the retained parkland.

For the above reasons, it would be appropriate for the City of Windsor Council to provide support for the Zoning by Law Amendment of the subject site to allow for the proposed residential development. This report demonstrates that the proposal is suitable and will not be impacted by or negatively impact surrounding uses but rather contribute to the neighbour's vitality, is consistent with the PPS, conforms to the intent and purpose of the City of Windsor Official Plan, and represents good planning.



Amy Farkas, MCIP, RPP
Associate & Planner

APPENDIX A

PROVINCIAL PLANNING STATEMENT POLICIES (PPS 2024)

PROVINCIAL PLANNING STATEMENT (PPS 2024)

The Provincial Planning Statement (2024) is a consolidated statement of Ontario’s policies on land use planning. It gives provincial policy direction on key land use planning issues that affect communities, such as:

- Efficient use and management of land and infrastructure;
- The provision of sufficient housing to meet changing needs, including affordable housing;
- The protection of the environment and resources including farmland, natural resources (for example, wetlands and woodlands) and water;
- Opportunities for economic development and job creation;
- The appropriate transportation, water, sewer, and other infrastructure needed to accommodate current and future needs; and
- The protection of people, property, and community resources by directing development away from natural or human-made hazards, such as flood prone areas.

Essentially, the PPS (2024) supports improved land use planning and management, which contributes to a more effective and efficient land use planning system. The following table provides a summary of the key policy considerations of the PPS (2024), as it pertains to the proposed re-use of the subject site.

Our analysis concludes that the proposed development is consistent with the policies included below:

PPS POLICY	POLICY	RESPONSE
2.0 Building Homes, Sustaining Strong and Competitive Communities		
2.1 Planning for People and Homes		
2.1.4	To provide for an appropriate range and mix of housing options and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall: a) maintain at all times the ability to accommodate residential growth for a minimum of 15 years through lands which are designated and available for residential development; and b) maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned, including units in draft approved or registered plans.	The development introduces six (6) Single Detached Dwelling, expanding the existing housing mix in the area and utilizing designated land efficiently to meet long-term growth needs. Full municipal services ensure capacity to support residential growth. The proposed development contributes to both long- and short-term housing supply goals by utilizing municipally owned land that is designated for residential use and fully serviced. Through the Zoning By-law Amendment, the site will support immediate development of low-density housing, helping Windsor meet growth targets and accommodate projected residential demand.

PPS POLICY	POLICY	RESPONSE
2.1.6	Planning authorities should support the achievement of complete communities by: a) accommodating an appropriate range and mix of land uses, housing options, transportation options with multimodal access, employment, public service facilities and other institutional uses (including, schools and associated child care facilities, long-term care facilities, places of worship and cemeteries), recreation, parks and open space, and other uses to meet long-term needs; b) improving accessibility for people of all ages and abilities by addressing land use barriers which restrict their full participation in society; and c) improving social equity and overall quality of life for people of all ages, abilities, and incomes, including equity-deserving groups.	The proposed development aligns with complete community principles by introducing low density housing in a walkable, transit-accessible area with proximity to parks, schools, and commercial amenities. It addresses housing needs for a range of demographics and income levels, promotes inclusive design through compact lot configuration, and reinvests in park infrastructure to improve accessibility and social inclusion for all residents.
2.2 Housing		
2.2.1	Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected needs of current and future residents of the regional market area by: a) establishing and implementing minimum targets for the provision of housing that is affordable to low- and moderate-income households, and coordinating land use planning and planning for housing with Service Managers to address the full range of housing options including affordable housing needs; b) permitting and facilitating: 1. all housing options required to meet the social, health, economic and well-being requirements of current and future residents, including additional needs housing and needs arising from demographic changes and employment opportunities; and 2. all types of residential intensification, including the conversion of existing commercial and institutional	The development supports affordable housing goals by introducing a medium-density unit development. Residential uses represent an efficient and sustainable development pattern by transforming previously developed land into a viable, vibrant community. Through redevelopment, the project facilitates residential intensification, optimizing land use and infrastructure while contributing to a residential environment. Its compact design efficiently utilizes resources and encourages active transportation, minimizing environmental impact. The location within a transit-supportive area further aligns with sustainable urban growth objectives, promoting reduced vehicle dependency and future transit-oriented development.

PPS POLICY	POLICY	RESPONSE
	buildings for residential use, development, and introduction of new housing options within previously developed areas, and redevelopment which results in a net increase in residential units in accordance with policy 2.3.3; c) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation; and d) requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations.	
2.3 Settlement Areas and Settlement Area Boundary Expansions		
2.3.1 General Policies for Settlement Areas		
2.3.1.1	Settlement areas shall be the focus of growth and development. Within settlement areas, growth should be focused in, where applicable, strategic growth areas, including major transit station areas.	The subject site is located within a designated Settlement Area per Schedule “A” of the City of Windsor Official Plan, and represents appropriate, modest intensification on underutilized land within an established neighbourhood.
2.3.1.2	Land use patterns within settlement areas should be based on densities and a mix of land uses which: a) efficiently use land and resources; b) optimize existing and planned infrastructure and public service facilities; c) support active transportation; d) are transit-supportive, as appropriate; and e) are freight-supportive.	The proposed low-density infill development is compact and utilizes existing servicing and transportation infrastructure without expansion. It supports walkability through sidewalk access to nearby parks and schools and is within walking distance of multiple bus routes. While not freight-oriented, the proposal is appropriately scaled to the residential context and aligns with complete community and infrastructure optimization goals.
2.9 Energy Conservation, Air Quality and Climate Change		
2.9.1	Planning authorities shall plan to reduce greenhouse gas emissions and prepare for the <i>impacts of a changing climate</i> through approaches that: a) support the achievement of compact, <i>transit-supportive</i>, and <i>complete communities</i>;	The proposal supports compact, infill development within a well-serviced urban neighbourhood, reducing sprawl and vehicle dependence. It is within walking distance of parks, schools, and transit, aligning with climate-conscious

PPS POLICY	POLICY	RESPONSE
	b) incorporate climate change considerations in planning for and the development of <i>infrastructure</i>, including stormwater management systems, and <i>public service facilities</i>; c) support energy conservation and efficiency; d) promote <i>green infrastructure</i>, <i>low impact development</i>, and <i>active transportation</i>, protect the environment, and improve air quality; and e) take into consideration any additional approaches that help reduce greenhouse gas emissions and build community resilience to the <i>impacts of a changing climate</i>.	growth objectives. A stormwater surcharge pond is proposed to manage runoff from new impervious surfaces, mitigating flood risk and adapting to extreme weather. The modest scale and efficient use of existing infrastructure further align with energy and emissions reduction goals.

3.0 Infrastructure and Facilities

3.1 General Policies for Infrastructure and Public Service Facilities

3.1.1	<i>Infrastructure and public service facilities</i> shall be provided in an efficient manner while accommodating projected needs. Planning for <i>infrastructure</i> and <i>public service facilities</i> shall be coordinated and integrated with land use planning and growth management so that they: a) are financially viable over their life cycle, which may be demonstrated through asset management planning; b) leverage the capacity of development proponents, where appropriate; and c) are available to meet current and projected needs.	The proposed development uses existing water, sanitary, and stormwater infrastructure, requiring no major upgrades. A stormwater management memo confirms capacity and outlines a surcharge pond on adjacent parkland to mitigate runoff, ensuring financial and functional efficiency aligned with growth management.
3.1.2	Before consideration is given to developing new <i>infrastructure</i> and <i>public service facilities</i>: a) the use of existing infrastructure and public service facilities should be optimized; and b) opportunities for adaptive re-use should be considered, wherever feasible.	The project avoids new infrastructure construction by optimizing existing municipal systems and access roads (Norman and Olive), reflecting responsible infill planning.
3.1.3	<i>Infrastructure and public service facilities</i> should be strategically located to support the effective and efficient delivery of emergency management services, and to ensure the protection of public health and safety in accordance with the policies in Chapter 5: Protecting Public Health and Safety.	The development is in a fully serviced, accessible area, with adequate street access for emergency vehicles, thereby supporting public safety and operational readiness.

PPS POLICY	POLICY	RESPONSE
	consider cumulative impacts of stormwater from development on a <i>watershed scale</i> .	
3.9 Public Spaces, Recreation, Parks, Trails and Open Space		
3.9.1	Healthy, active, and inclusive communities should be promoted by: a) planning public streets, spaces, and facilities to be safe, meet the needs of persons of all ages and abilities, including pedestrians, foster social interaction and facilitate <i>active transportation</i> and community connectivity; b) planning and providing for the needs of persons of all ages and abilities in the distribution of a full range of publicly-accessible built and natural settings for recreation, including facilities, parklands, public spaces, open space areas, trails, and linkages, and, where practical, water-based resources; c) providing opportunities for public access to shorelines; and d) recognizing provincial parks, conservation reserves, and other protected areas, and minimizing negative impacts on these areas.	The development supports healthy, inclusive communities by enhancing walkability, fronting onto public streets, and providing access to nearby parks and open spaces. It fosters active transportation and social interaction within an age-inclusive, connected neighbourhood.
6.0 Implementation and Interpretation		
6.1 General Policies for Implementation and Interpretation		
6.1.1	The Provincial Planning Statement (PPS) shall be read in its entirety and all relevant policies are to be applied to each situation.	The proposed development adopts a comprehensive and coordinated approach to land use planning, infrastructure provision, and public service facilities. It adheres to the principles of sustainable growth, promoting accessibility, inclusivity, and the protection of public health and safety. The project is aligned with the long-term objectives of the PPS, balancing environmental stewardship with economic and social considerations, while ensuring compliance with all relevant regional and provincial land use policies.
6.1.2	The PPS shall be implemented in a manner that is consistent with the recognition and affirmation of existing Aboriginal and treaty rights in section 35 of the <i>Constitution Act</i> , 1982.	
6.1.3	This PPS shall be implemented in a manner that is consistent with <i>Ontario Human Rights Code</i> and the <i>Canadian Charter of Rights and Freedoms</i> .	
6.1.4	When implementing the PPS, the Minister of Municipal Affairs and Housing may make decisions that take into account other considerations to balance government priorities.	
6.1.9	To assess progress on implementation of this PPS, the Province may: a) identify key indicators to measure the outcomes, relevance, and efficiency of	

PPS POLICY	POLICY	RESPONSE
	the policies in this PPS in consultation with municipalities, Indigenous communities, other public bodies, and stakeholders; b) monitor and assess the implementation of this PPS through the collection and analysis of data under each indicator; and c) consider the resulting assessment in each review of this PPS.	

APPENDIX B

CITY OF WINDSOR OFFICIAL PLAN

CITY OF WINDSOR OFFICIAL PLAN

Our analysis concludes that the proposed development is consistent with the policies included below:

	OP POLICY	RESPONSE
3.0 Development Strategy		
3.2	The policies of this Plan are directed toward accommodating the projected growth through practical and efficient land use management strategies that promote a compact pattern of development and balanced transportation system. Compatible residential, commercial and employment growth will be directed to appropriate locations within existing and planned neighbourhoods to reduce development and infrastructure costs and provide opportunities to live, work and shop in close proximity. Mixed use developments will be encouraged with strong pedestrian orientations and to support public transit. This concept will enable Windsor to continue its growth and foster a vibrant economy, while ensuring a safe, caring and diverse community and a sustainable, healthy environment. In order to manage growth consistent with the community vision, the following key policy directions are provided for in the other chapters of this Plan.	The proposed development supports compact, efficient growth by adding residential units within an established neighbourhood, optimizing land use and existing infrastructure. Its walkable design and proximity to transit, commercial areas, and parkland contribute to a complete community model, aligned with the City's growth vision.
3.2.1.2	Encouraging a range of housing types will ensure that people have an opportunity to live in their neighbourhoods as they pass through the various stages of their lives. Residents will have a voice in how this new housing fits within their neighbourhood. As the city grows, more housing opportunities will mean less sprawl onto agricultural and natural lands.	By introducing new single detached dwellings in an existing low density neighbourhood, the development adds options for individuals and families across life stages, helping to meet demographic needs and reduce pressure on undeveloped land at the urban fringe. This aligns with the City's goals of intensification and neighbourhood sustainability.
4.0 Healthy Community		
4.2.1.2	To provide for activities and facilities which will foster an active lifestyle to improve community health.	The development supports active lifestyles by situating new housing within walking distance of existing and upgraded parks,

	OP POLICY	RESPONSE
		trails, and community amenities, fostering health and well-being.
4.2.1.5	To encourage a mix of housing types and services to allow people to remain in their neighbourhoods as they age.	By offering accessible, ground-oriented single detached dwellings, the proposal enables aging-in-place and supports long-term neighbourhood stability.
4.2.3.4	To accommodate the appropriate range and mix of housing.	The project contributes to housing diversity by introducing low-density infill that broadens housing choices within the community.
4.2.4.1	To encourage development that fosters social interaction	The compact lot design and orientation toward public streets promote neighbourly interaction and a socially cohesive streetscape.
4.2.4.3	To encourage developments that adapt to changing resident needs.	The flexible unit form can accommodate diverse households, including growing families and downsizing seniors, reflecting changing demographic needs.
4.2.7.3	To encourage emergency services in close proximity to where people live	The site is in close proximity to emergency services, schools, and transit, ensuring accessible safety and public services for new residents.
6.0 Land Use		
6.2.1.2	Low Profile developments are buildings or structures generally no greater than three (3) storeys in height.	The proposed development will adhere to the all the provisions of the proposed zoning: Residential District 1.2 (RD1.2) which limits building height to 9.0m.
6.3. Residential		
6.3.1 Objectives		
6.3.1.1	To support a complementary range of housing forms and tenures in all neighbourhoods.	The proposed development introduces low-density, single detached dwellings that contribute to a more diverse and compact neighbourhood form. It promotes strategic infill within a serviced area, supports active and transit-oriented transportation, and enhances access to nearby parks, schools, and commercial amenities. The dwellings are compatible with home-based occupations and help address long-term housing demand by efficiently utilizing underused municipal land. The development leverages municipal land to generate funding for significant park upgrades, providing enhanced amenities that would not otherwise be delivered. Overall,
6.3.1.2	To promote compact neighbourhoods which encourage a balanced transportation system.	
6.3.1.3	To promote residential redevelopment, infill and intensification initiatives in locations in accordance with this plan.	
6.3.1.4	To ensure that the existing housing stock is maintained and rehabilitated.	
6.3.1.5	To provide for complementary services and amenities which enhance the quality of residential areas.	
6.3.1.6	To accommodate home based occupations.	

	OP POLICY	RESPONSE
6.3.1.7	To ensure that a sufficient land supply for residential and ancillary land uses is available to accommodate market demands over the 20 year period of this Plan.	the development reinforces neighbourhood vitality and aligns with Windsor's goals for complete, resilient residential communities.
6.3.2 Policies		
6.3.2.1	Uses permitted in the Residential land use designation identified on Schedule D: Land Use include Low Profile, and Medium Profile dwelling units. High Profile Residential Buildings shall be directed to locate in the City Centre, Mixed Use Centres and Mixed Use Corridors. (Added by OPA #159 – APPROVED July 11, 2022 , B/L# 100-2022)	The proposed single detached dwellings are a permitted Low Profile residential use under the Residential land use designation.
6.3.2.3.a	For the purposes of this Plan, Low Profile housing development is further classified as follows: small scale forms: single detached, semi-detached, duplex and row and multiplexes with up to 8 units. Large scale forms: buildings with more than 8 units	The proposed development consists of six small scale Low Profile dwelling forms (single detached homes), consistent with the OP definition. These are appropriate for the surrounding context and maintain the existing neighbourhood character.
6.3.2.5.c	At the time of submission, the proponent shall demonstrate to the satisfaction of the Municipality that a proposed residential development within an area having a Neighbourhood development pattern is: (c) compatible with the surrounding area in terms of character, scale, appearance and design features, including height, massing, building materials, landscape character and setback between the buildings and streets;	The design, massing, setbacks, and overall built form of the proposed homes are compatible with the surrounding neighbourhood. The development integrates seamlessly in terms of height, architectural form, and landscaping, ensuring visual continuity and neighbourhood cohesion.
7.0 Infrastructure		
7.1.2.3	Optimal use of existing infrastructure; An accessible, affordable and available transportation system	The proposed development optimizes existing infrastructure and promotes a compact form of development. Its location along established roads with nearby transit routes supports accessible, affordable, and available transportation options.
7.2.1.2	To make efficient use of existing and planned transportation infrastructure.	The site is already serviced by a well-developed transportation network. No new road extensions are needed, demonstrating efficient use of existing and planned transportation infrastructure.

	OP POLICY	RESPONSE
7.2.1.4	To provide a system with functional connectivity and interconnectedness	The proposal benefits from existing street connectivity (Olive Road and Norman Road) and sidewalk networks, contributing to a functionally connected neighbourhood that supports all modes of travel.
7.2.1.5	To promote a land use pattern, density and mix of uses that reduces vehicle trips and supports alternative transportation modes including public transit.	The proposed development adds housing in a walkable, well-connected neighbourhood near parks, schools, and transit, thereby reducing reliance on personal vehicles and supporting alternative transportation modes.
7.2.2.5	Council shall promote development patterns that support an increase in walking, cycling and public transportation in accordance with the Land Use and Urban Design chapters of this Plan.	Its compact lot layout, pedestrian-oriented design, and integration with existing sidewalks promote safe, barrier-free, and convenient access for people of all ages and abilities, advancing Council's goals for sustainable, transit-supportive, and active communities.
7.2.3.1	Council shall require all proposed developments and infrastructure undertakings to provide facilities for pedestrian movements wherever appropriate by: (a) Requiring safe, barrier free, convenient and direct walking conditions for persons of all ages and abilities; (b) Ensuring that all residents have access to basic community amenities and services and public transit facilities without dependence on car ownership; and (c) Providing a walking environment within public rights-of-ways that encourages people to walk to work or school, for travel, exercise, recreation and social interaction.	
7.2.6.18	Council shall encourage the development of a peaceful atmosphere in residential areas by: (a) Maintaining an adequate system of Collector and Arterial roads to serve residential areas; (b) Requiring the provision of appropriate noise control measures and off-street parking; (c) Locating medium and high density residential development along Collector and Arterial roads; and (d) Requiring the construction of sidewalks in accordance with policy 7.2.3.2; (e) Requiring that traffic calming devices be included in the road design of any new residential subdivision in accordance with the Traffic Calming Policy; (f) Applying the principles of the Traffic	The proposal maintains the residential atmosphere by providing off-street parking, fronting homes on quiet local roads, and aligning with Windsor's sidewalk and traffic calming policies to ensure safety and quality of life.

	OP POLICY	RESPONSE
	Calming Policy to existing roads in residential areas by ensuring that traffic calming is considered as part of any neighbourhood management plan and that traffic calming devices are placed where deemed to be necessary; and (g) Requiring that the impact of noise and the feasibility of achieving noise control objectives must be considered as factors in determining the design of residential areas and that noise barriers be considered only when it has been demonstrated that alternate noise abatement solutions are not feasible.	
7.3.1.1	To provide infrastructure in a coordinated, efficient and cost-effective manner to accommodate projected needs.	The proposed development demonstrates smart growth by maximizing the use of existing municipal infrastructure, roads, water, stormwater, and sanitary services, without requiring off-site upgrades. This aligns with policy objectives for coordinated, cost-effective infrastructure delivery and reinforces the efficient use of public resources while supporting compact, serviceable growth.
7.3.1.3	To maximize and optimize the use of existing infrastructure and corridors prior to the extension and creation of new ones.	
7.3.2.3	Council shall require all new developments to have full municipal infrastructure available, or agreements in place to provide such infrastructure, as a condition of approving a development proposal.	
8.0 Urban Design		
8.3.1	To achieve maximum user comfort in the design of new development; To foster development that provides a pedestrian scale; and To foster a sense of place within Windsor and its neighbourhoods.	The proposed development fosters a strong sense of place by introducing modest-scale infill that respects neighbourhood character and supports pedestrian-oriented design. By aligning building setbacks and scale with surrounding homes, the development enhances user comfort and contributes to a coherent, walkable streetscape.
8.5.2.8	Council will encourage a proposed development or infrastructure undertaking to retain and incorporate natural features and functions with regard to, but not limited to, the following: (a) its function as part of a larger vegetated area; (b) its potential to adapt to post-construction conditions; and (c) its contribution to shading and screening on site and for adjacent properties.	Where feasible, the development will retain mature trees and integrates landscaping that supports local shading and screening functions, contributing to site aesthetics and ecological resilience.

THE CORPORATION OF THE CITY OF WINDSOR

PUBLIC OPEN HOUSE - ENGAGEMENT SUMMARY

Kinsmen Norman Road Park Development
1730 Olive Road, Windsor

JULY 2025 – 25-1503

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1.0 THE PUBLIC OPEN HOUSE

A Public Open House was held by Dillon Consulting Limited, on behalf of The Corporation City of Windsor (hereinafter referred to as “the applicant”), to provide support for the proposed Zoning By-law Amendment and redevelopment of the southern portion of the municipally-owned Kinsmen Norman Road Park, located at 1730 Olive Road, within the City of Windsor, hereinafter referred to as the “subject site”.

The Public Open House was held in advance of the statutory public meeting to provide early engagement on the proposed amendment that seeks to rezone the site from Green District 1.1 (GD1.1) to Residential District 1.2 (RD1.2) zone. This will allow the creation of six (6) single detached dwellings on six (6) individual lots. The remaining portion of the park will be retained as public open space. Proceeds from the sale of the subject site are intended to support enhancements to the retained parkland.

The proposed intensification of these lands for residential development is in keeping with pertinent local policies, provisions, and guidelines of the Provincial Planning Statement (2024), the City of Windsor Official Plan, and the City of Windsor Zoning By-law 8600. All other building requirements for a single detached dwelling will be in accordance with the Residential District 1.2 (RD1.2) Zone.

1.1 NOTICE

In accordance with the Planning Act, notices of the Public Open House were hand delivered at least 15 days before the date of the event, to every owner of land within 120 metres of the subject land, using the mailing list provided by the City of Windsor (refer to [Appendix A – Mailing Notice of Public Open House](#)).

In addition, the display materials used at the Public Open House on June 26, 2025, including the notice and concept plans, were posted online via the City’s dedicated project webpage. This ensured that residents who could not attend the in-person or virtual events still had full access to the presentation materials. Written comments could be submitted via mail or email until July 11th, 2025 (refer to [Appendix B – Webpage Notice of Public Open House](#)).

1.2 FORMAT

In-Person Public Open House

An in-person Public Open House was held on Thursday June 26th, 2025, between 6:00 pm and 7:30pm at St. Theresa’s Catholic Church. It was conducted in an open house, drop-in format, with display materials available for viewing. Project representatives were on hand to gather feedback and respond to questions regarding the proposed development (refer to [Appendix C – In Person Public Open House Presentation Boards](#)).

Virtual Public Open House

A virtual Public Information Centre was conducted concurrently on Thursday June 26th, 2025, between 6:00pm to 7:30pm. Participants were required to register in advance by submitting a request to KNRparkdev@dillon.ca by 12:00pm on the day of the Public Open House. A virtual Public Open House link was subsequently provided to registrants via email. The virtual Public Open House included a brief presentation of project materials followed by a live question-and-answer session (refer to **Appendix D – Virtual Public Open House Presentation Slides**).

Following both Public Open House sessions, written comments were accepted via mail, email, or phone until **July 11th, 2025**. All questions and concerns received during and after the sessions were considered as part of the public engagement process. Responses to the key themes raised are provided within this summary report.

1.3 ATTENDANCE

In Person Public Open House

A total of six (6) residents attended the in-person Public Information Centre, as documented in the official record of attendance. Refer to **Appendix E – In Person Record of Attendance**, for a redacted version of the in person attendance sheet.

Virtual Public Open House

A total of three (3) individuals registered to attend the virtual Public Open House session. Of those, only one (1) participant attended and remained present for a portion of the session. Refer to **Appendix F – Virtual Record of Attendance**, for a redacted version of the in person attendance sheet.

2.0 FEEDBACK SUMMARY

2.1 COMMENTS

Several comments were recorded from conversations with attendees of the in-person Public Open House and the virtual Public Open House. One (1) written submission was received via email to Parks Development staff from the City of Windsor, and a few questions were received through the chat at the virtual open house. No email comments were received through the dedicated project email address as part of the public consultation process. These comments have been reviewed and considered in the preparation of this engagement summary. Below is a table presenting the comments received, along with a summary of resident feedback and corresponding responses. Additional information has been put forth, organized by key themes to support the responses (refer to [Appendix G – Email Correspondence](#) and [Appendix H – Chat Records](#)).

The feedback gathered demonstrates a mix of support, practical concerns, and constructive suggestions. While some residents voiced concerns about traffic and land use priorities, the majority of comments expressed optimism for both the housing proposal and the park improvements. The engagement process also revealed a strong desire for community-oriented amenities and safety features. By integrating public input with planning policy and a clear implementation process, the proposal aims to deliver both much-needed housing and enhanced public spaces, setting a precedent for sustainable, community-driven development.

2.2 RESPONSES – SUBJECT SITE

COMMENTS	RESPONSES
2.2.1 Planning Policy & Process	
Comments related to Windsor’s housing needs and intensification	- The proposed development aligns with the key objectives of the City of Windsor Official Plan and the Provincial Planning Statement (PPS, 2024), which prioritize compact, complete communities, intensification within built-up areas, and the efficient use of existing infrastructure. It supports Windsor’s ‘Housing Solutions Made for Windsor’ strategy, which encourages the development of a diverse range of housing types and the unlocking of municipally-owned lands to address the housing crisis. The project contributes directly to Windsor’s pledge to deliver 13,000 new homes under Ontario’s More Homes Built Faster Act. - The City of Windsor’s Zoning By-law Amendment process includes formal notice to nearby property owners, a public meeting at the Development & Heritage Standing Committee (DHSC), and a final decision by City Council. Public input gathered through this process is considered alongside planning policies and technical studies in evaluating the application’s merits. This transparent and consultative

COMMENTS	RESPONSES
	process ensures that proposed changes to the zoning by-law are evaluated against local and provincial planning objectives and the interests of the surrounding community.
2.2.2 Access, Traffic & Parking	
Comments related to existing parking pressures, traffic along both Norman Road and Olive Road, and vehicular accesses.	The proposed development is designed to minimize traffic impact by orienting all six (6) dwellings along existing local roads, without introducing new road infrastructure. Each lot will include off-street parking to accommodate two vehicles and will be consistent with the parking requirements in Zoning By-law 8600. The dual frontage layout supports safe access and improved circulation, while maintaining pedestrian-friendly streetscapes. The development leverages the existing sidewalk network, and no significant traffic volume increases are anticipated given the modest scale of the project.
2.2.3 Scale & Building Height	
Comments related size and scale of proposed development, and neighbourhood compatibility.	The proposed six single detached dwellings are intended to be low-profile housing that mirrors the height, massing, and built form of surrounding RD1.2-zoned residential properties. The dwellings will maintain consistent front yard setbacks, and lot sizes, to ensure visual and functional compatibility with the established neighbourhood character. This gentle density increase introduces new housing while respecting the scale and rhythm of the existing streetscape.
2.2.4 Development Replication & Alternatives	
Can this proposed plan be completed at Polonia Park ? And use that money that will be made from selling the land to development Kinsmen-Norman Park ? The park there is spacious.	While Polonia Park is larger, this proposal is rooted in site-specific conditions at Kinsmen-Norman Park, including underused open space. Selling a portion of this land generates revenue directly tied to reinvestment in the same park, enabling targeted improvements such as new play areas, lighting, gardens, and gathering spaces. While this reinvestment model would not directly apply to Polonia Park, it is intended to be used as a precedent for future park reinvestment discussion.
Because there is limited parking space on Norman road. Using the proposed house land for parking instead of building homes there would be a benefit.	While parking needs have been noted and is an important consideration, land use must also respond to broader housing needs and policy directions. The six (6) proposed single detached dwellings reflect gentle intensification, aligned with the City's housing and infill objectives. Moreover, existing park improvements are designed to encourage walkability, reduce parking demand, and maintain neighbourhood character.
Last question, the proposed land, will that be available for people to buy or will it be offered to contractors?	If the rezoning is approved, the surplus land will be sold through a municipal disposition process. This typically involves offering the land to the open market through a request for offers or sale, which may include individuals or developers. Final conditions of sale, including

COMMENTS	RESPONSES
	design criteria or subdivision requirements, will be subject to City approval.

2.3 RESPONSES - PARK IMPROVEMENT PLAN

Several resident comments received during the public engagement process related specifically to park amenities and the Parks Improvement Plan. These comments (refer to [Table 1: Comments Related to the Park Improvement Plan](#)), included requests for additional garbage bins near the soccer field, improved lighting to deter nighttime loitering, and the inclusion of community gardens to foster social interaction. While these concerns fall outside the scope of the current Zoning By-law Amendment application related to the subject site, they are important and have been documented and forwarded to the Parks Development team for review. These comments will be considered as part of the City's Parks Improvement Plan for Kinsmen Norman Road Park, which is a separate process from the Zoning By-law Amendment application. This parallel planning process ensures that enhancements to public space can proceed in coordination with, but independent from, the proposed residential development. Based on this feedback, revisions have been made to the Parks Improvement Plan (refer to [Appendix I – Draft Park Improvement Plan](#)).

Table 1: Comments Related to the Park Improvement Plan

RESIDENT COMMENTS	RESPONSES
I would like to see a garbage can closer to the soccer field. I really am not sure if it will help. They tend to dump the can, turn it upside down and sit on it. Any thoughts?	Waste receptacles are important for park cleanliness. The Park Development staff will explore options to install durable, vandal-resistant bins in visible locations near the soccer field.
I strongly feel another solar light is needed here. This will be a dark area of the park. With or without the community garden this will be a place kids will gather after dark (they do now up against the fence of existing home).	Lighting concerns are valid, especially where park edges abut residential areas. The final design will include review of Crime Prevention Through Environmental Design (CPTED) principles. Additional solar lighting can be considered along key pathways or community gathering areas to enhance visibility and safety without contributing to light pollution.
I know we can call the Police when something is going on, but realistically they are busy with so many other things a drive by is not a priority especially at night.	Lighting, visibility, and community presence (e.g., a community garden) are intended to create a sense of ownership and passive surveillance. These will be considered and explored further by the Park Development Staff.
I like the proposal, I feel we have a great plan here to go forward. I love the idea of the community garden, as I feel it will help bring more of the neighbours together. A feeling of	This feedback is appreciated. The park improvements aim to foster social connection, belonging, and inclusive public space. Events like the Public Open House are part of ongoing engagement,

RESIDENT COMMENTS	RESPONSES
accomplishment and even a reason to socialize. Thank you for putting this together.	and future design and programming stages will continue to welcome input from residents.
I know we will always have people against change, unfortunately, things have to move forward at some point. It is time for this property	Change in established neighbourhoods can be challenging, but it also presents opportunity. This proposal reflects a balanced and context-sensitive approach retaining most of the park, improving its features, and adding needed housing. Constructive dialogue remains central to ensuring change benefits both current and future residents.

3.0 NEXT STEPS

Following the Public Open House, a summary of all public feedback received will be compiled and submitted to the City of Windsor for review and consideration as part of the final development application process.

Subsequently, in accordance with the Planning Act, the City of Windsor will schedule a statutory public meeting for the proposed Zoning By-law Amendment application. Formal notice of this meeting will be provided to all landowners within 120 metres of the subject property. At that time, all supporting documentation, including technical studies, plans, and summaries of public engagement, will be made available for public review in advance of Council's deliberation on the proposed Zoning By-law Amendment.

4.0 SUMMARY

Dillon Consulting Limited, on behalf of The Corporation of The City of Windsor, hosted a Public Open House with regards to the proposed Zoning By-law Amendment and redevelopment of the southern portion of the municipally-owned Kinsmen Norman Road Park, located at 1730 Olive Road, within the City of Windsor.

The Public Open House was held on Thursday June 26th, 2025, at St. Theresa's Catholic Church between 6:00pm to 7:30pm. It was conducted in an open house, drop-in format, with display materials available for viewing and project representatives on hand for discussion. It was also offered virtually at the same time. In accordance with the Planning Act, notice of the Public Open House was provided by hand delivery to landowners within 120 metres of the subject site, 20 days in advance of the meeting.

The proposal seeks to rezone the site from Green District 1.1 (GD1.1) to Residential District 1.2 (RD1.2) zone to allow the creation of six (6) single detached dwellings on six (6) individually subdivided lots. The remaining portion of the park will be retained as public open space. Proceeds from the sale of the subject site are intended to support enhancements to the retained parkland.

Several comments were recorded from conversations with attendees of the in-person Public Open House and the virtual meeting. One (1) written submission was received via email to Parks Development staff from the City of Windsor, and a few questions were received through the chat at the virtual open house. No email comments were received through the dedicated project email address as part of the public consultation process. Feedback focused on the appreciation of the benefits of the improved parks and amenities, including the contribution to the community's social life. Some questions related to scale and height, compatibility, traffic and parking impacts, infrastructure strain and privacy loss were recorded. In response, the project team emphasized policy alignment with the Provincial Planning Statement (2024), the City's Official Plan and intensification guidelines, and outlined mitigation strategies including building articulation, setbacks, privacy buffers, and servicing adequacy. A summary of resident concerns and corresponding responses has been provided in this report.

All feedback received will be submitted to the City of Windsor as part of the application package. A formal statutory public meeting will be scheduled by the City, at which point supporting documentation will be made available for public review in advance of Council's consideration.

Based on a review of the planning policy framework and supplementary technical reports, this development is consistent with good planning principles. The proposed development has regard for the Provincial Planning Statement as it encourages the use of underutilized lands by proposing an intensification that exists in harmony with the surrounding land uses, while making efficient use of the existing municipal infrastructure. The proposed development conforms with the general intent of the City of Windsor Official Plan, as it promotes compact form which will diversify the housing options currently available in the area.

APPENDIX A

MAILING NOTICE OF PUBLIC OPEN HOUSE

PUBLIC INFORMATION CENTRE (PIC)

Proposed Redevelopment for Part of Kinsmen Norman Road Park

Dillon Consulting Limited, on behalf of The Corporation of the City of Windsor, is hosting both in-person and virtual Public Information Centre (PIC) sessions to present a proposed Zoning By-law Amendment for a portion of Kinsmen Norman Road Park.

PURPOSE OF THE APPLICATION:

- To rezone the southern portion of the subject site from Green District 1.1 (GD1.1) zone to Residential District 1.2 (RD1.2) zone; and
- To create six (6) Single Unit Dwelling lots.

WHAT TO EXPECT:

During these sessions, guests will be able to:

- Meet the project team (in-person or virtually); and
- Discuss comments and questions relating to the proposed developments.

This meeting forms part of the ongoing Planning process in advance of the statutory public meeting required under the Planning Act to obtain approvals. The City of Windsor will be inviting all residents and landowners within 120m of the subject site about additional meetings in the near future.

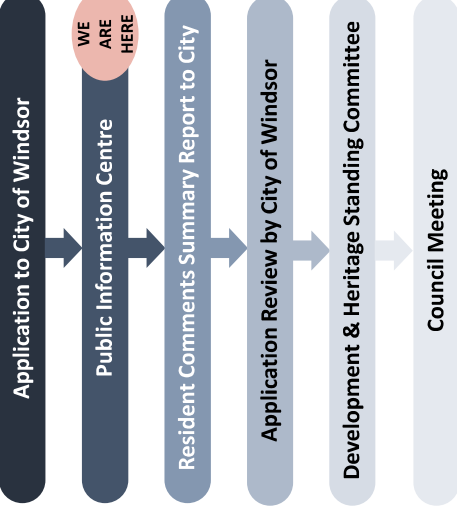
SUBMISSION FORMATS (simultaneous):

- In-person PIC:** Drop-in format with displays and project team available to discuss; and
 - Virtual PIC:** Drop-in format with project materials available for review along with a live Q&A session.
- We are looking forward to your input and comment.

Project Email: KNRparkdev@dillon.ca



THE PLANNING PROCESS:



Thursday June 26th, 2025

6:00 pm to 7:30pm

In Person PIC Venue:

St. Theresa's Catholic Church
(1991 Norman Road, Windsor, ON, N8T 1S1)

Virtual Pic Registration

Please email your registration request to:
KNRparkdev@dillon.ca by 12:00 PM on June 26th, 2025.
A meeting link will be sent to you via email upon registration.

Submit written comments via mail or email by July 11th, 2025.

Please refer to the City of Windsor's Webpage for additional information:

<https://www.citywindsor.ca/residents/parks-and-forestry/parks-development/park-improvement-open-houses/kinsmen-norman-road-park-redevelopment-open-house>

Contact

Amy Farkas, MCIP RPP
Associate & Project Manager
Dillon Consulting Limited
1 Riverside Drive, Windsor, N8W 5K8
T – 519.948.5000 ext. 3205

COUNTERPOINT LAND DEVELOPMENT BY DILLON CONSULTING



THE CITY OF WINDSOR
KINSMEN NORMAN ROAD PARK
(1730 OLIVE ROAD)

ZONING BY-LAW AMENDMENT
FIGURE 1.0

FILE LOCATION: directory/projects/2025/dillon_3/1730olive/1553/25-1004-kinmen-park-concept-plan.dwg
June 11, 2025 2:04 PM

SOURCE: THE COUNTY OF ESSEX INTERACTIVE MAPPING (2024)

CREATED BY: ESB
CHECKED BY: ESB
DESIGNED BY: ESB

MAP DRAWING INFORMATION:
THIS DRAWING IS FOR INFORMATION PURPOSES ONLY.
ALL DIMENSIONS AND BOUNDARY INFORMATION SHOULD
BE VERIFIED BY AN OLS PRIOR TO CONSTRUCTION.

SCALE: 1:750 (11x17)

COUNTERPOINT
LAND DEVELOPMENT BY

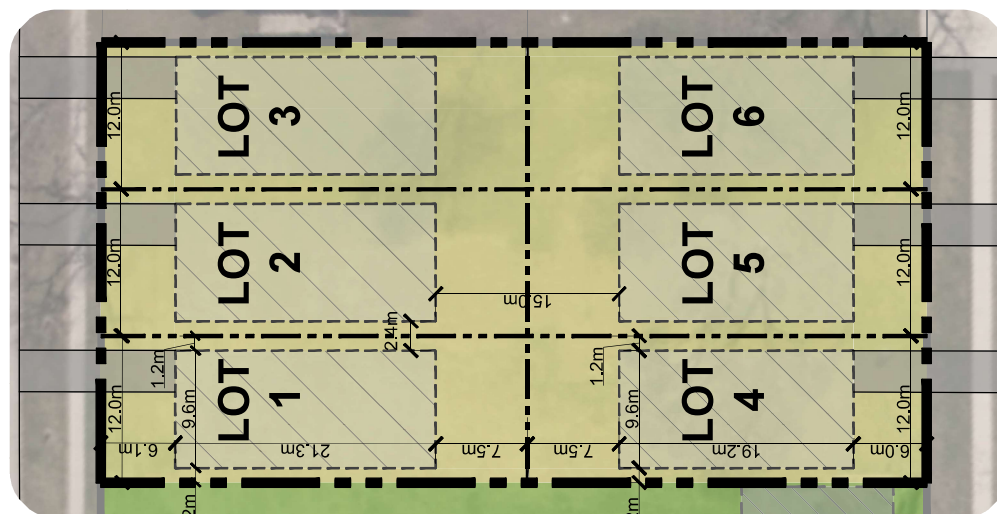
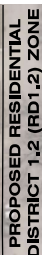
DILLON
CONSULTING

PROJECT: 25-1003
STATUS: DRAFT
DATE: 06/11/2025

EXISTING KINSMEN NORMAN ROAD PARK
(±5,926m²)

PARKLAND (GD1:1)
(±7,396.0 sq.m)

SUBJECT SITE (RD1:2)
(±2,430.0 sq.m)



SITE DATA MATRIX		
RESIDENTIAL DISTRICT 1.2 (RD1.2) PROVISIONS		
SUBJECT SITE AREA	2,430.0 sq.m	
LOT WIDTH	12.0 m	
MIN. LOT COVER	360.0 sq.m	
MAX. LOT COVERAGE	45.0 %	
MAX. BUILDING HEIGHT	9.0 m	
MIN. FRONT YARD DEPTH	6.0 m	
MIN. REAR YARD DEPTH	7.5 m	
MIN. SIDE YARD DEPTH	1.2 m	
MAX. GROSS FLOOR AREA	400.0 sq.m	
LOTS 1-3		
LOT DEPTH	34.8 m	
LOT WIDTH	12.0 m	
LOT AREA	417m ²	
MAX. LOT COVERAGE	45.0% / 187.7m ²	
LOTS 4-6		
LOT DEPTH	34.8 m	
LOT WIDTH	12.0 m	
LOT AREA	417m ²	
MAX. LOT COVERAGE	45.0% / 176.4m ²	



MAP DRAWING INFORMATION:
THE DRAWING IS FOR INFORMATION PURPOSES ONLY.
ALL DIMENSIONS AND BOUNDARY INFORMATION SHOULD
BE VERIFIED BY AN O.G.S. PRIOR TO CONSTRUCTION.
CREATED BY: ESB
DESIGNED BY: ESB



SCALE: 1:400 (11x17)

COUNTERPOINT
LAND DEVELOPMENT BY
DILLON
CONSULTING

APPENDIX B

WEBPAGE NOTICE OF PUBLIC OPEN HOUSE

Home > Residents > Parks and Forestry > Parks Development > Park Improvement Open Houses
> Kinsmen Norman Road Park Redevelopment Open House

Park Improvement Open Houses

Kinsmen Norman Road Park Redevelopment Open House

Lanspeary Park Redevelopment

Sandpoint Beach Park Master Plan and Environmental Assessment

Kinsmen Norman Road Park Redevelopment Open House

The City of Windsor is moving forward with plans to redevelop Kinsmen Norman Road Park, with proposed upgrades designed to enhance year-round use, improve accessibility, and elevate the overall quality of this neighbourhood park. Initial work is underway, including the removal of an outdated washroom building. This project aligns with the Rediscover Our Parks Master Plan and broader city planning initiatives, including Planning for Windsor's Growth and Housing Solutions Made for Windsor.

Public Consultation on Proposed Zoning By-law Amendment

A public information centre (PIC) was held on June 26, 2025, as part of the proposed Zoning By-law amendment for a portion of Kinsmen Norman Road Park to support future residential development, which will help fund the planned park improvements. The amendment would rezone the southern portion of the park from Green District 1.1 (GD1.1) to Residential District 1.2 (RD1.2), allowing for the creation of six single-unit dwelling lots. This meeting forms part of the ongoing Planning process in advance of the statutory public meeting required under the Planning Act.

Written comments on the proposal were accepted until July 11, 2025, and the comment period is now closed. The project team will review all input received and present recommendations to City Council.

- Click to view the [PIC notice, proposed Zoning By-law amendment plan, and concept plan](#).
- Click to view the [PIC display boards](#).

Previous Public Engagement on Park Design

The redevelopment process began with a public open house in 2022 to gather community input on priorities and ideas for park improvements. In 2024, a concept plan was presented to the public at an open house on December 2, 2024, along with an online survey that received 117 responses. Feedback showed strong support for upgrades such as new pathways, lighting, benches, and trees, with residents emphasizing safety, accessibility, and inclusive outdoor spaces. The display boards from the open house in December 2024 are available below:

- [December 2024 Open House Display Boards](#)

The existing park conditions and proposed park redevelopment concept plan are available below:

APPENDIX C

IN PERSON PUBLIC OPEN HOUSE PRESENTATION BOARDS

Welcome!
Thank you for joining us.

Kinsmen Norman Road Park Public Open House

Zoning By-Law Amendment

June 26th, 2025
6:00 pm – 7:30 pm
St. Theresa's Catholic Church

PUBLIC OPEN HOUSE

KINSMEN NORMAN ROAD PARK (1730 OLIVE ROAD)

The Public Open House is being hosted to rezone the southern portion of the Park from Green District 1.1 (GD1.1) zone to Residential District 1.2 (RD1.2) zone; and to create six (6) Single Unit Dwelling lots. A Zoning By-Law Amendment is required to facilitate the proposed development.

The Public Open House is being hosted in a “drop-in” format with representatives available to take comments and answer questions.

The goals of the Public Open House are as follows:

1. Provide Information on the planning process and development approvals process;
2. Provide information on the proposed development;
3. Provide details on anticipated timelines;
4. Collect comments and feedback from attendees to help guide the proposed development as it continues through the approvals process.

Collecting Feedback:

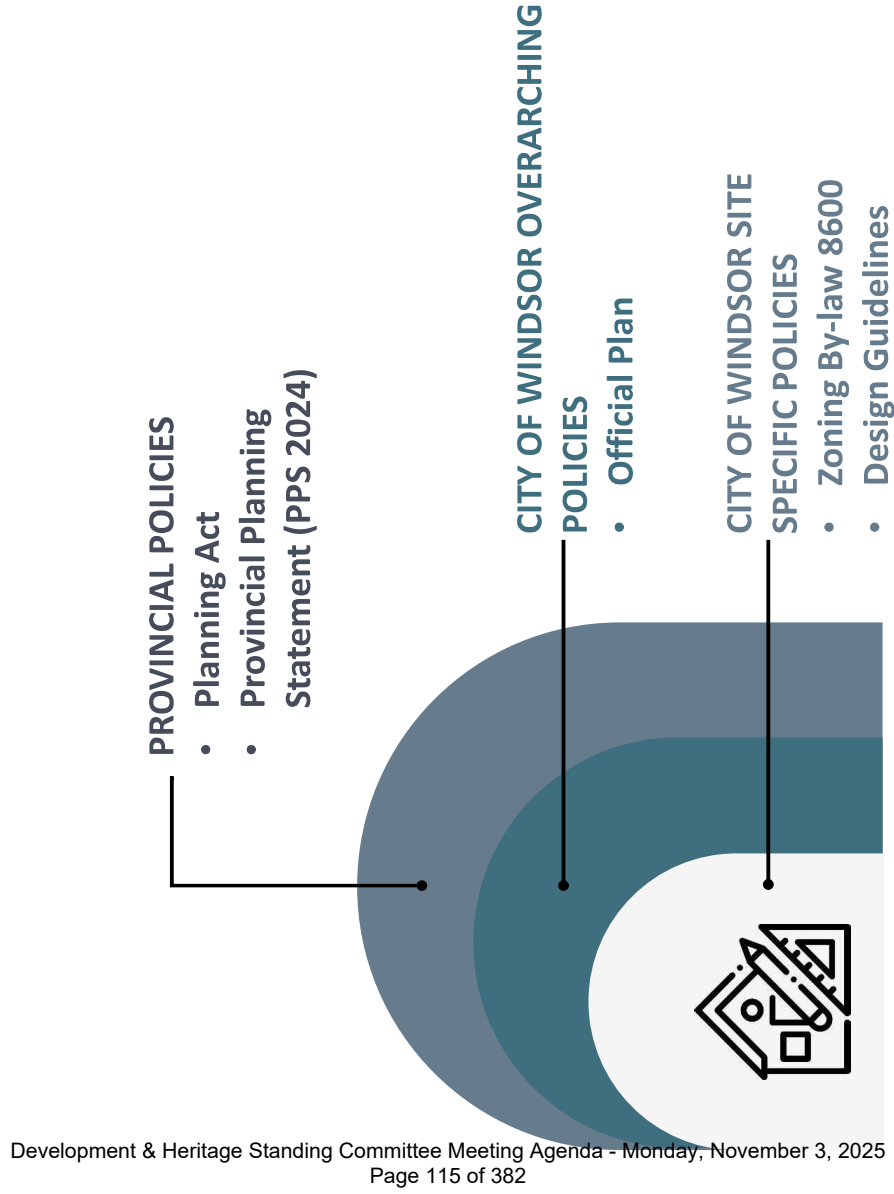
1. Comment forms are available for attendees to complete;
2. Comments may also be emailed to KNRparkdev@dillon.ca
3. All comments will be compiled at the end of the meeting and will be included in a report summarizing the meeting;
4. All comments will be accepted until July 11th, 2025

To Submit Comments: KNRparkdev@dillon.ca

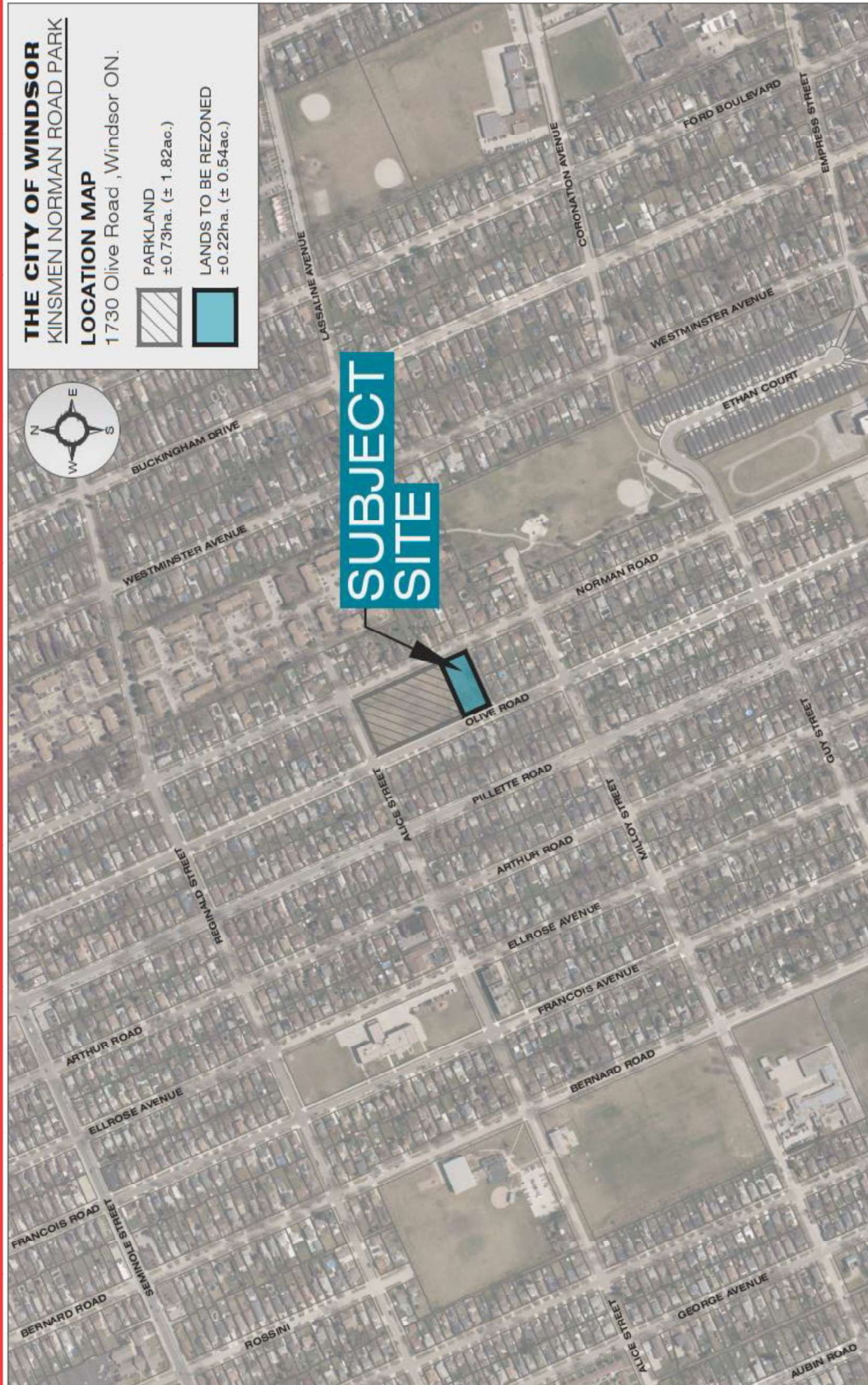


COUNTERPOINT
LAND DEVELOPMENT BY
DILLON
CONSULTING

PLANNING PROCESS & OVERVIEW



PROJECT LOCATION



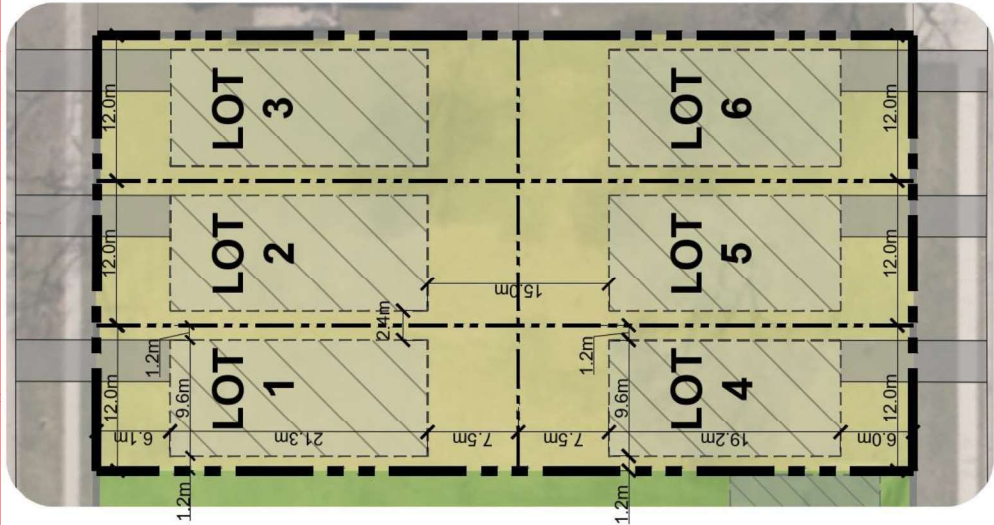
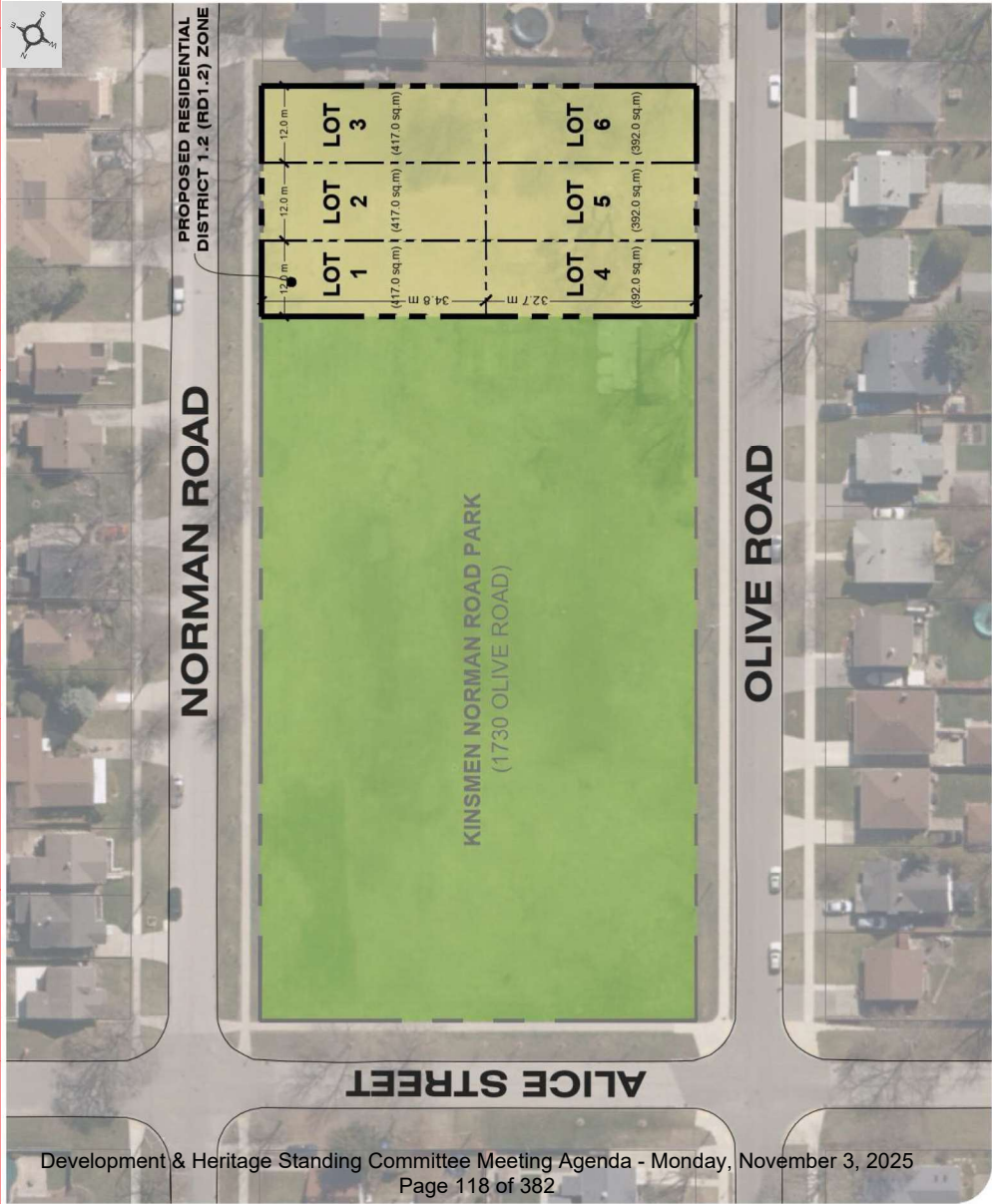
ZONING BY-LAW AMENDMENT PLAN



ZONING BY-LAW AMENDMENT

- TO REZONE THE SOUTHERN PORTION OF THE PARK FROM GREEN DISTRICT 1.1 (GD1.1) ZONE TO RESIDENTIAL DISTRICT 1.2 (RD1.2) ZONE;
- TO CREATE SIX (6) SINGLE UNIT DWELLING LOTS

CONCEPTUAL DEVELOPMENT PLAN



SITE DATA MATRIX	
RESIDENTIAL DISTRICT 1.2 (RD1.2) PROVISIONS	
SUBJECT SITE AREA	2,430.0 sq.m
LOT WIDTH	12.0 m
MIN. LOT AREA	360.0 sq.m
MAX. LOT COVERAGE	45.00%
MAX. BUILDING HEIGHT	9.0 m
MIN. FRONT YARD DEPTH	6.0 m
MIN. REAR YARD DEPTH	7.5 m
MIN. SIDE YARD DEPTH	1.2 m
MAX. GROSS FLOOR AREA	400.0 sq.m
LOTS 1-3	
LOT DEPTH	34.8m
LOT WIDTH	12.0m
LOT AREA	417.0sq.m
MAX. LOT COVERAGE	45% 187.7sq.m
LOTS 4-6	
LOT DEPTH	32.7m
LOT WIDTH	12.0m
LOT AREA	392.0sq.m
MAX. LOT COVERAGE	45% 176.4sq.m

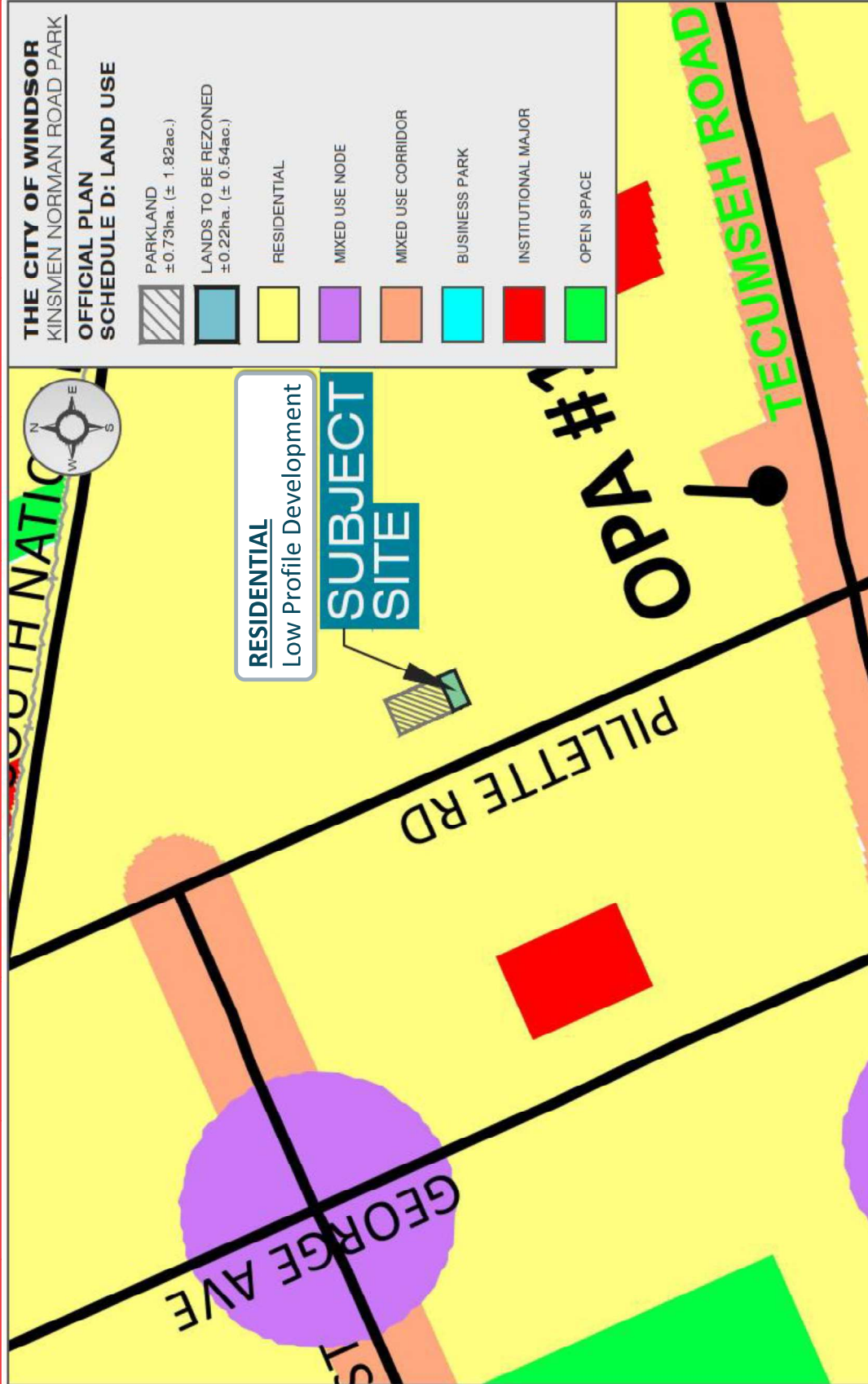


Kinsmen Norman Road Park Redevelopment – Zoning By-law Amendment



COUNTERPOINT
LAND DEVELOPMENT BY
DILLON
CONSULTING

OFFICIAL PLAN DESIGNATIONS



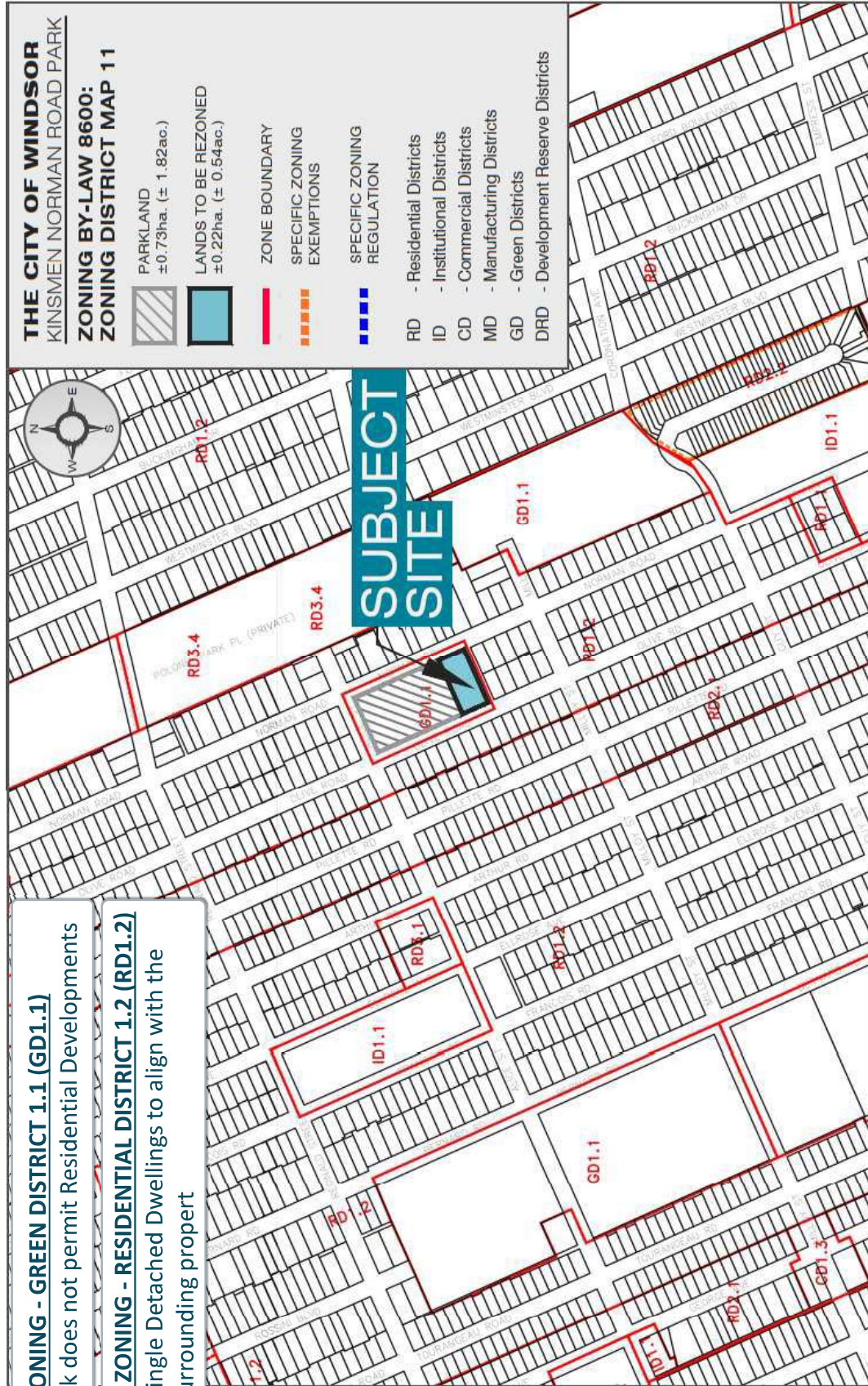
ZONING BY-LAW DESIGNATIONS

EXISTING ZONING - GREEN DISTRICT 1.1 (GD1.1)

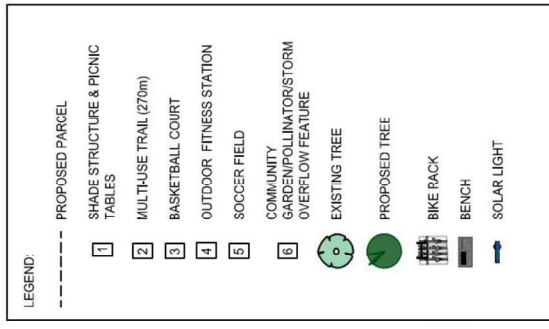
Existing Park does not permit Residential Developments

PROPOSED ZONING - RESIDENTIAL DISTRICT 1.2 (RD1.2)

It permit Single Detached Dwellings to align with the zoning of surrounding property



KINSMEN NORMAN ROAD PARK REDEVELOPMENT PLAN



NEXT STEPS



1. Finalize background reports
2. Submission of Planning Consultation Stage 2 Application to the City of Windsor
3. Submission of Zoning By-law Amendment Application to the City of Windsor
4. Statutory Public Meeting / Development and Heritage Standing Committee
5. Council Meeting

Stay Involved



Ask questions today and provide your feedback

Contact Us



Amy Farkas, MCIP RPP, Associate
Dillon Consulting Limited
KNRparkdev@dillon.ca

APPENDIX D

VIRTUAL PUBLIC OPEN HOUSE PRESENTATION SLIDES

Welcome!
Thank you for joining us.

Kinsmen Norman Road Park Public Open House

Zoning By-Law Amendment

June 26th, 2025

6:00 pm – 7:30 pm

St. Theresa's Catholic Church

HOUSEKEEPING

- This meeting is being recorded;
- The purpose of this meeting is:
 - To provide an overview of the proposed development and it's planning process;
 - To listen and to answer participant questions & collect feedback and input of the proposed development.
- Questions and comments will be addressed at the end of the presentation;

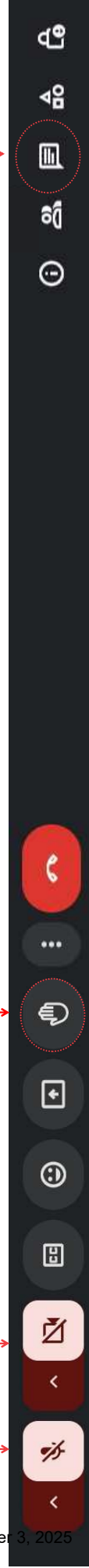
- Use the CHAT function for questions / comments;
- Use RAISE HAND to speak at the end.

TURN
ON / OFF
CAMERA

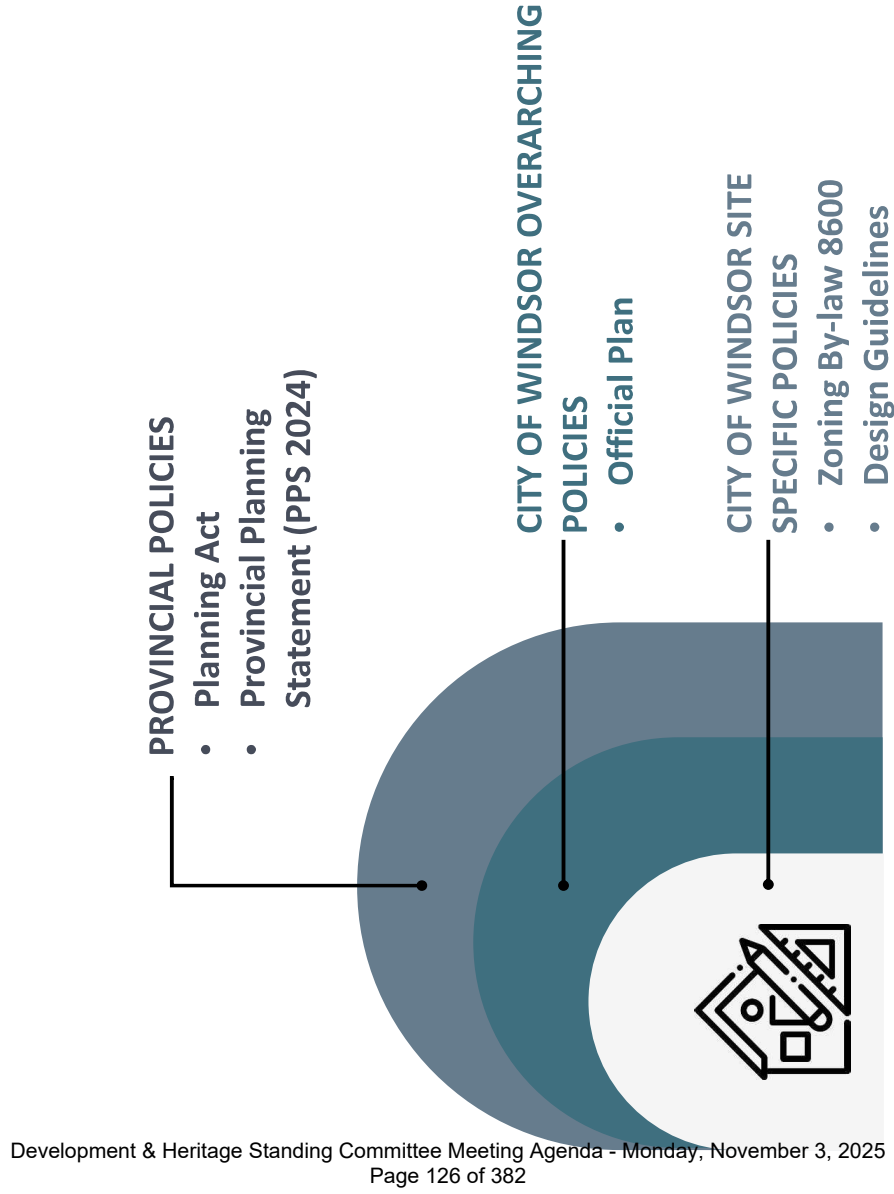
MUTE

RAISE YOUR
HAND TO
SPEAK

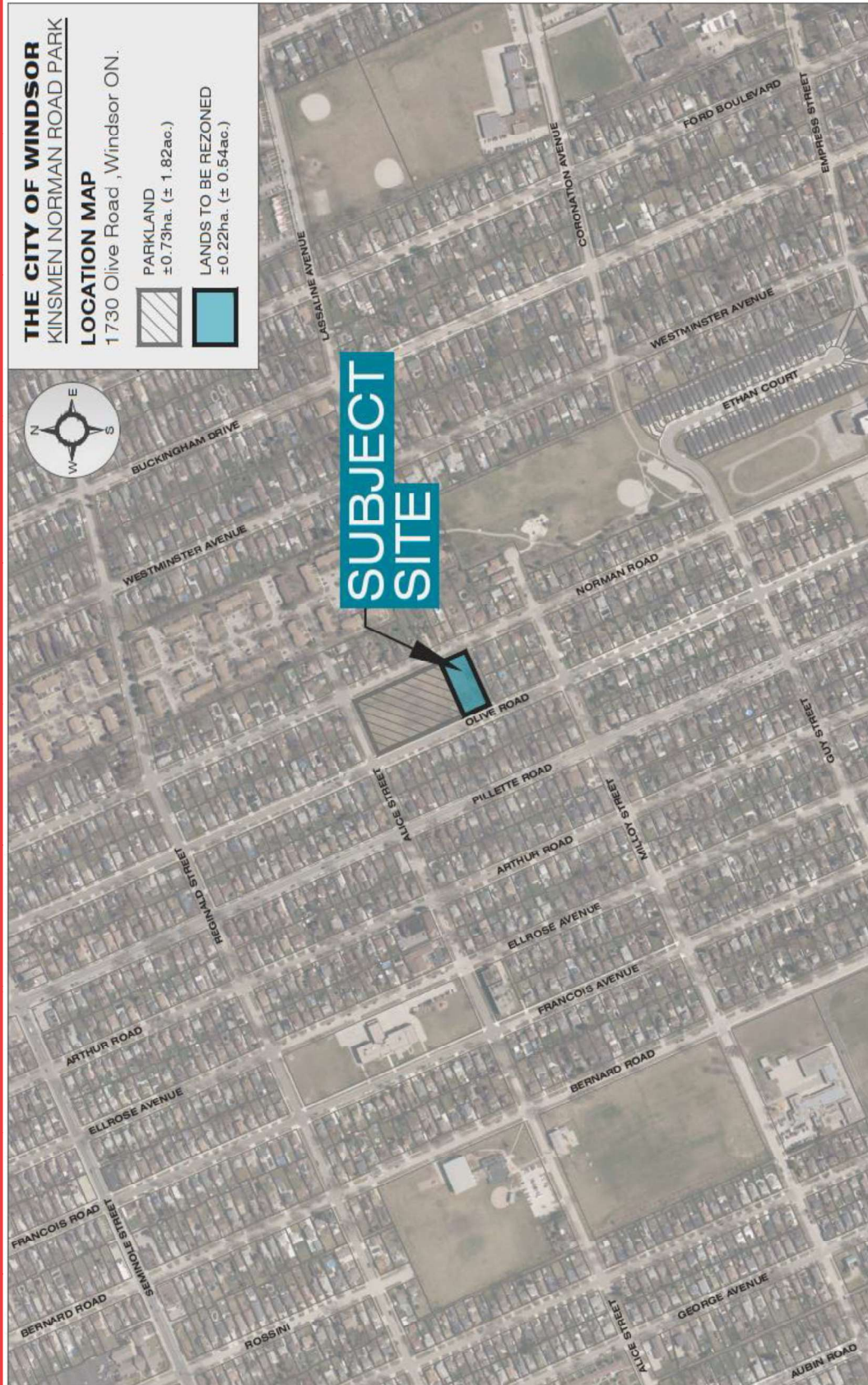
QUESTIONS
IN THE CHAT



PLANNING PROCESS & OVERVIEW



PROJECT LOCATION



ZONING BY-LAW AMENDMENT PLAN

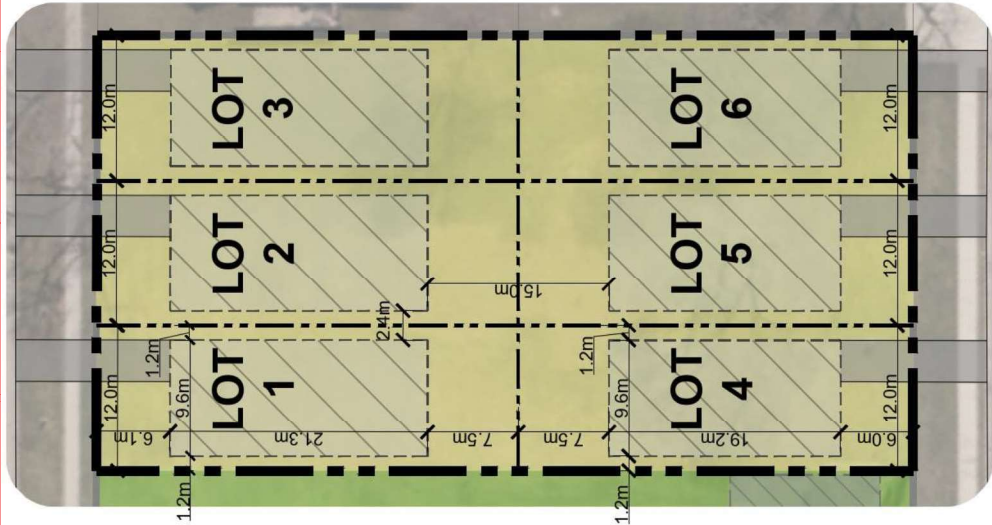


ZONING BY-LAW AMENDMENT

- TO REZONE THE SOUTHERN PORTION OF THE PARK FROM GREEN DISTRICT 1.1 (GD1.1) ZONE TO RESIDENTIAL DISTRICT 1.2 (RD1.2) ZONE;
- TO CREATE SIX (6) SINGLE UNIT DWELLING LOTS



CONCEPTUAL DEVELOPMENT PLAN



SITE DATA MATRIX	
RESIDENTIAL DISTRICT 1.2 (RD1.2) PROVISIONS	
SUBJECT SITE AREA	2,430.0 sq.m
LOT WIDTH	12.0 m
MIN. LOT AREA	360.0 sq.m
MAX. LOT COVERAGE	45.00%
MAX. BUILDING HEIGHT	9.0 m
MIN. FRONT YARD DEPTH	6.0 m
MIN. REAR YARD DEPTH	7.5 m
MIN. SIDE YARD DEPTH	1.2 m
MAX. GROSS FLOOR AREA	400.0 sq.m
LOTS 1-3	
LOT DEPTH	34.8m
LOT WIDTH	12.0m
LOT AREA	417.0sq.m
MAX. LOT COVERAGE	45% 187.7sq.m
LOTS 4-6	
LOT DEPTH	32.7m
LOT WIDTH	12.0m
LOT AREA	392.0sq.m
MAX. LOT COVERAGE	45% 176.4sq.m

EXISTING KINSMEN NORMAN ROAD PARK (±19,626m²)

SUBJECT SITE (RD1.2) (±2,430.0 sq.m)

PROPOSED SINGLE DETACHED DWELLING LOTS (6)

BUILDING SETBACKS

PARKLAND (GD1.1) (±7,396.0 sq.m)

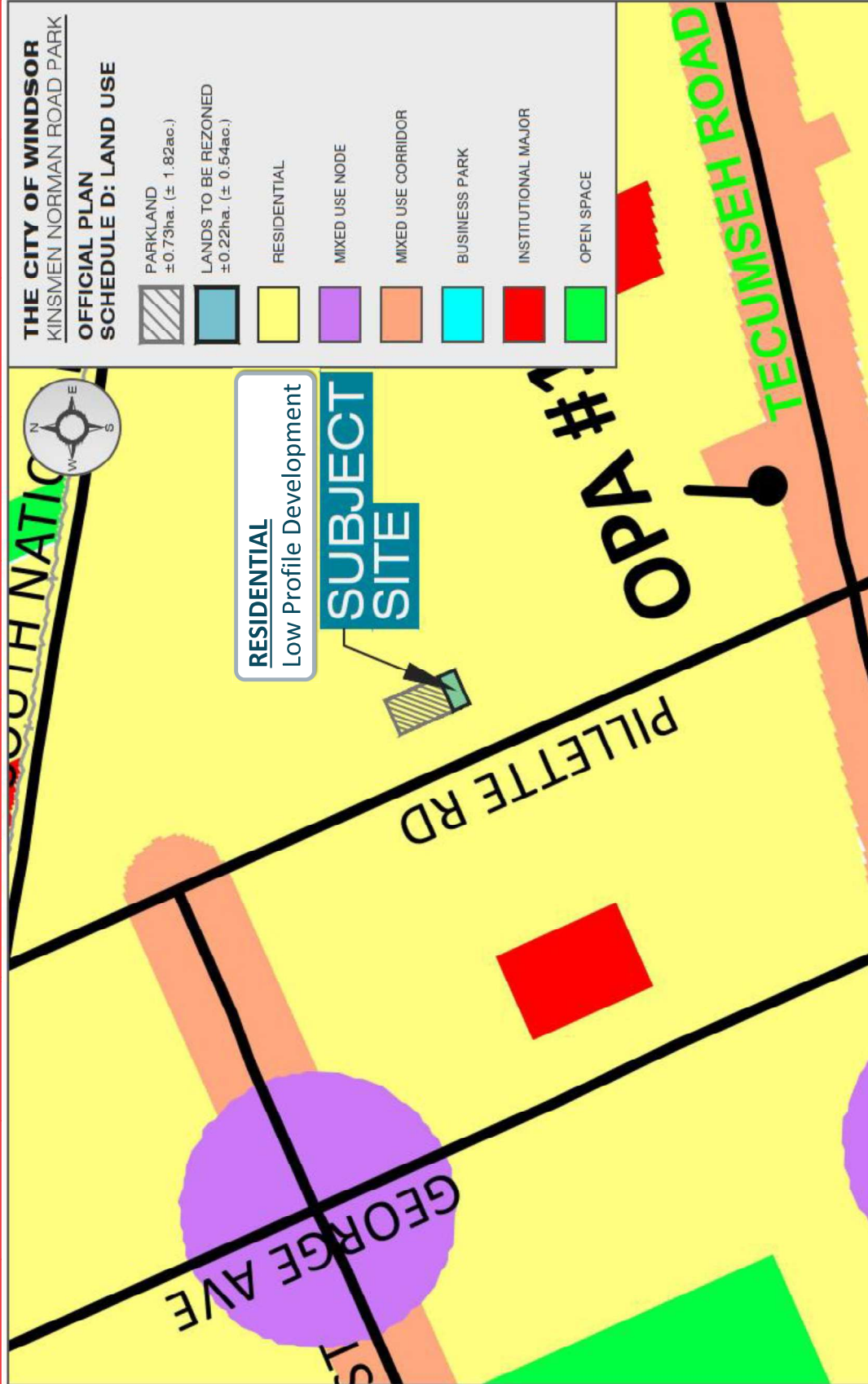
MAX. DEVELOPEABLE AREA

Kinsmen Norman Road Park Redevelopment – Zoning By-law Amendment



COUNTERPOINT
LAND DEVELOPMENT BY
DILLON
CONSULTING

OFFICIAL PLAN DESIGNATIONS



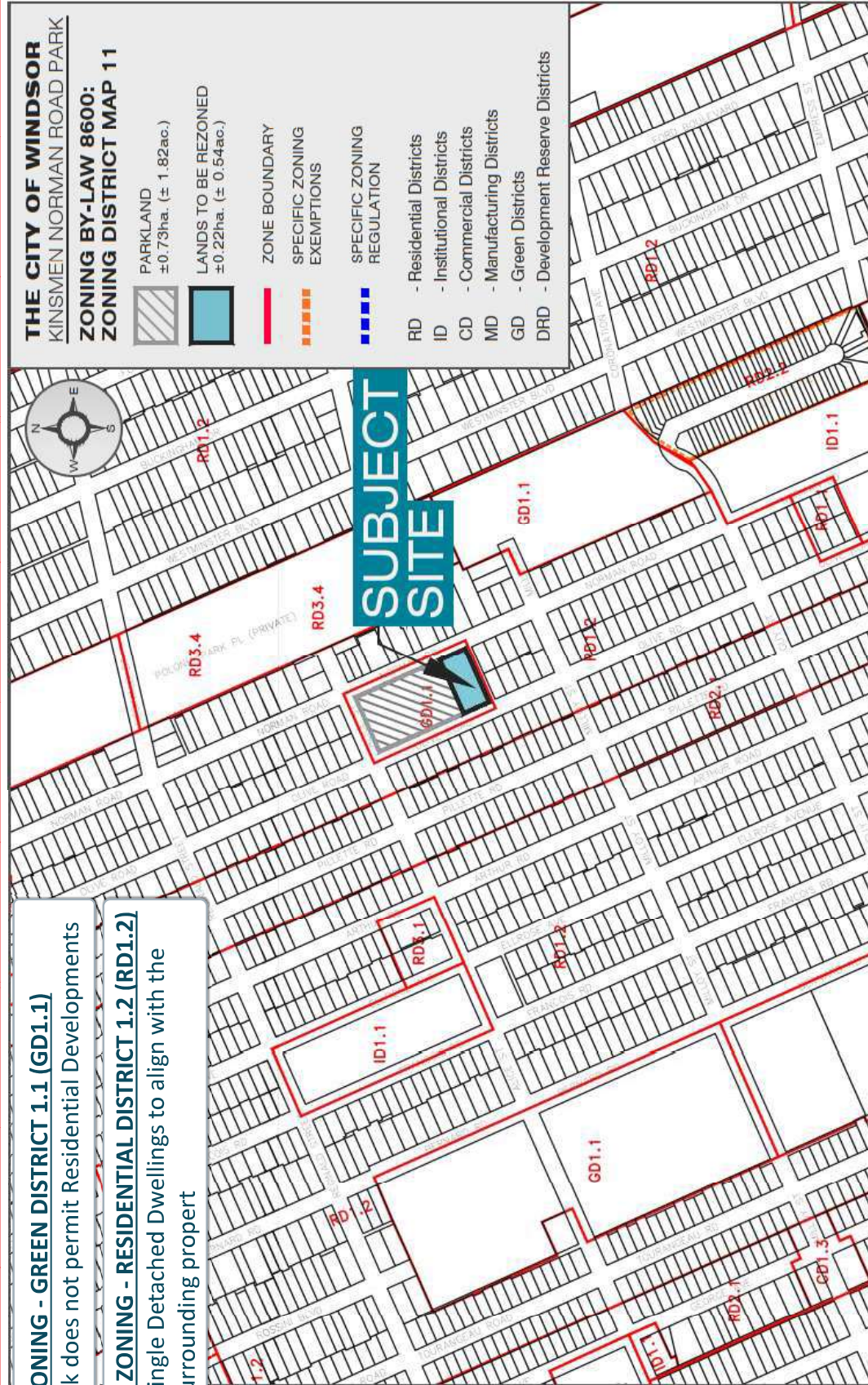
ZONING BY-LAW DESIGNATIONS

EXISTING ZONING - GREEN DISTRICT 1.1 (GD1.1)

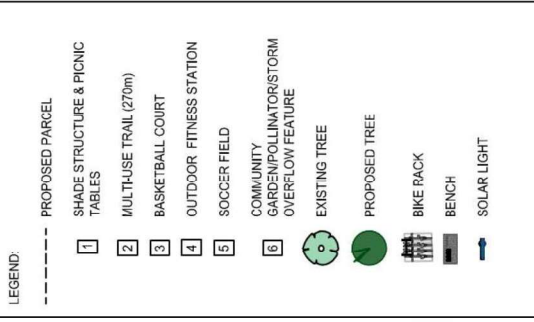
Existing Park does not permit Residential Developments

PROPOSED ZONING - RESIDENTIAL DISTRICT 1.2 (RD1.2)

It permit Single Detached Dwellings to align with the zoning of surrounding property



KINSMEN NORMAN ROAD PARK REDEVELOPMENT PLAN



NEXT STEPS



1. Finalize background reports
2. Submission of Planning Consultation Stage 2 Application to the City of Windsor
3. Submission of Zoning By-law Amendment Application to the City of Windsor
4. Statutory Public Meeting / Development and Heritage Standing Committee
5. Council Meeting

Stay Involved



Ask questions today and provide your feedback

Contact Us



Amy Farkas, MCIP RPP, Associate
Dillon Consulting Limited
KNRparkdev@dillon.ca

APPENDIX E

IN PERSON RECORD OF ATTENDANCE

RECORD OF ATTENDANCE

Kinsmen Norman Road Park Development (1730 Olive Road)

Public Information Centre – Thursday June 26th, 2025

By signing this form and attending this Open House / Public Information Centre, I hereby acknowledge that the information I've provided will become part of the record on file. Information provided might be used by the City of Windsor, it's third party contractors and/or other levels of government and elected officials in accordance with the Ontario's *Municipal Freedom of Information and Protection of Privacy Act*. Questions about the collection of personal information contained on this form should be directed to the Manager, Records and Elections City of Windsor, 350 City Hall Square West – Suite 530, Windsor, On N9A 6S1, Phone: 519-255-6100, ext. 6578.

Name	Mailing Address
[REDACTED]	[REDACTED]
[REDACTED]	[REDACTED]
[REDACTED]	[REDACTED]
[REDACTED]	[REDACTED]
[REDACTED]	[REDACTED]
[REDACTED]	[REDACTED]

APPENDIX F

VIRTUAL RECORD OF ATTENDANCE

First name	Last name	Email	Duration	Time joined	Time exited
			33 min	5:50 PM	6:23 PM
Sophia	Symons	ssymons@dillon.ca	1 hr 52 min	5:28 PM	7:20 PM
Sophia	Symons	symo*****@***.com	1 hr 48 min	5:32 PM	7:20 PM

APPENDIX G

EMAIL CORRESPONDENCE

Subject: RE: Kinsmen Norman Road Park



Kameli, Hoda <hkameli@citywindsor.ca>

to Al-Yassiri, Wadah, Lisa GN, Ash, Laura, Yilmaz, Canberk

Hi Wadah,

Will do, thank you.

Best,

Hoda Kameli BSc, MA, MLA,, AAPQ, CSLA

Landscape Designer, OALA | Landscape Architect, AAPQ, CSLA



Parks Development | 2450 McDougall St Windsor, ON., N8X 3N6

Phone (519) 253-2300x2701 | (519) 818-2720

hkameli@citywindsor.ca

From: Al-Yassiri, Wadah

Sent: June 26, 2025 8:13 PM

To: [REDACTED]

Cc: Ash, Laura ; Yilmaz, Canberk

Subject: Re: Kinsmen Norman Road Park

Good evening [REDACTED]

Thanks for your email and follow up. Nice meeting you again at the open house.

Looping our Parks landscape architect for awareness.

Hi Hoda,

Please push the tow led lights to the east and west and add a third one in the middle to light up the whole southern side of the park better. Thanks!

Best regards,

Wadah

Regards, Wadah Al-Yassiri

Manager Parks Development

Get [Outlook for Android](#)

From: [REDACTED]

Sent: Thursday, June 26, 2025 7:39:25 PM

To: Al-Yassiri, Wadah <walyassiri@citywindsor.ca>

Subject: Re: Kinsmen Norman Road Park

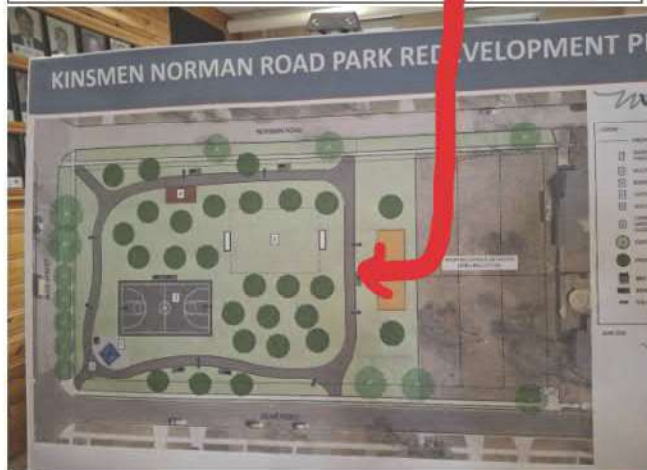
You don't often get email from [REDACTED] [Learn why this is important](#)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good Evening Wadah

Thank you for speaking with me again this evening. I appreciate that you take our concerns seriously.

As we discussed at the Public Information on June 26 – add a 3rd solar light on the south side of the park closest to the homes to hopefully deter homeless people from bedding down in this area. Move the planned 2 solar lights out to accommodate the 3rd.



Thank you

On Tue, Dec 3, 2024 at 3:00 PM Al-Yassiri, Wadah <walyassiri@citywindsor.ca> wrote:

Good afternoon,

Thank you [REDACTED] attending the open house yesterday and your follow-up e-mail today.

Your feedback is duly noted and will be discussed/considered with my team during the final design stage of the park plans.

If you have any question or like to share more comments, please do and thank you!

Best Regards,

Wadah Al-Yassiri, P. Eng., CET. | Manager, Parks Development

Parks Department | 2450 McDougall St. | Windsor, ON., N8X 3N6 |

519. 253. 2300 Ext. 2740 | 519. 562. 8525 | walyassiri@citywindsor.ca



www.citywindsor.ca

IMPORTANT NOTICE:

This email is confidential and intended for the addressee only. If you are not the intended person, please notify me and destroy this and all copies immediately. Thank you!

From: [REDACTED]
Sent: Tuesday, December 3, 2024 12:12 PM
To: Kaschak, Gary <gkaschak@citywindsor.ca>; Al-Yassiri, Wadah <walyassiri@citywindsor.ca>
Subject: Kinsmen Norman Road Park

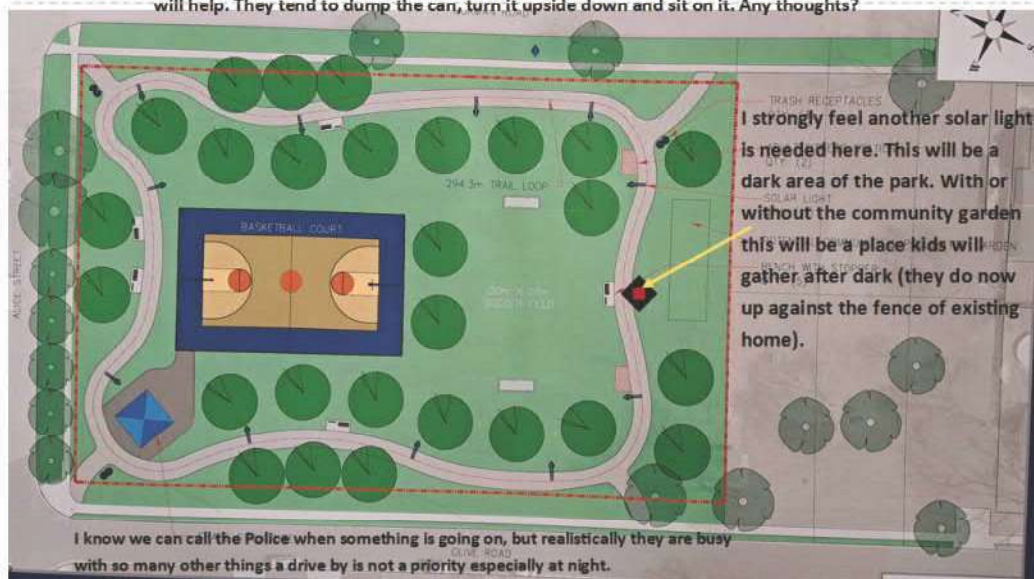
You don't often get email from [REDACTED]

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good Afternoon Wadah and Gary

I've attached a picture with my thoughts that I discussed with Wadah last evening.

I would like to see a garbage can closer to the soccer field. I really am not sure if it will help. They tend to dump the can, turn it upside down and sit on it. Any thoughts?



I like the proposal, I feel we have a great plan here to go forward. I love the idea of the community garden, as I feel it will help bring more of the neighbours together. A feeling of

Thank you for putting this together. Sorry I was late, I thought it was a just pop-in sort of event.

I know we will always have people against change, unfortunately, things have to move forward at some point. It is time for this property.

Thank you for your time.

--

[REDACTED]

[REDACTED]

APPENDIX H

CHAT RECORDS

00:46:09.029,00:46:12.029

[REDACTED]: If you have any questions, comments or concerns, feel free to unmute, type in the chat box here, or send an email to KNRparkdev@dillon.ca! Comments received before July 11th, 2025 will be included in an engagement summary that will be reviewed by City Staff, and will be circulated to City Council.

00:48:11.073,00:48:14.073

Rita E: Can this proposed plan be completed at Polonia Park ? And use that money that will be made from selling the land to development Kinsmen-Norman Park ? The park there is spacious.

00:51:33.335,00:51:36.335

Rita E: okay thank you - because there is limited parking space on Norman road. Using the proposed house land for parking instead of building homes there would be a benefit.

00:52:01.813,00:52:04.813

Rita E: last question, the proposed land, will that be available for people to buy or will it be offered to contractors ?

00:54:04.669,00:54:07.669

Rita E: okay thank you so much for your time. Have a great evening!

APPENDIX I

DRAFT PARK

IMPROVEMENT PLAN

KINSMEN NORMAN ROAD PARK REDEVELOPMENT PLAN



LEGEND:

- PROPOSED PARCEL
- 1 SHADE STRUCTURE & PICNIC TABLES
- 2 MULTI-USE TRAIL (270m)
- 3 BASKETBALL COURT
- 4 OUTDOOR FITNESS STATION
- 5 SOCCER FIELD
- 6 COMMUNITY GARDEN/POLLINATOR/STORM OVERFLOW FEATURE
- EXISTING TREE
- PROPOSED TREE
- BIKE RACK
- BENCH
- SOLAR LIGHT

JUNE 2025



SERVICING AND COMPOSITE
UTILITY PLAN
FIGURE 1.0

- SUBJECT SITE
- EXISTING WATERMAIN
- EXISTING SANITARY SEWER
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- PROPOSED STORM SERVICE
- PROPOSED WATER SERVICE
- PROPOSED SANITARY SERVICE
- EXISTING GAS MAIN
- EXISTING HYDRO POLE
- PROPOSED REAR YARD CATCH BASIN

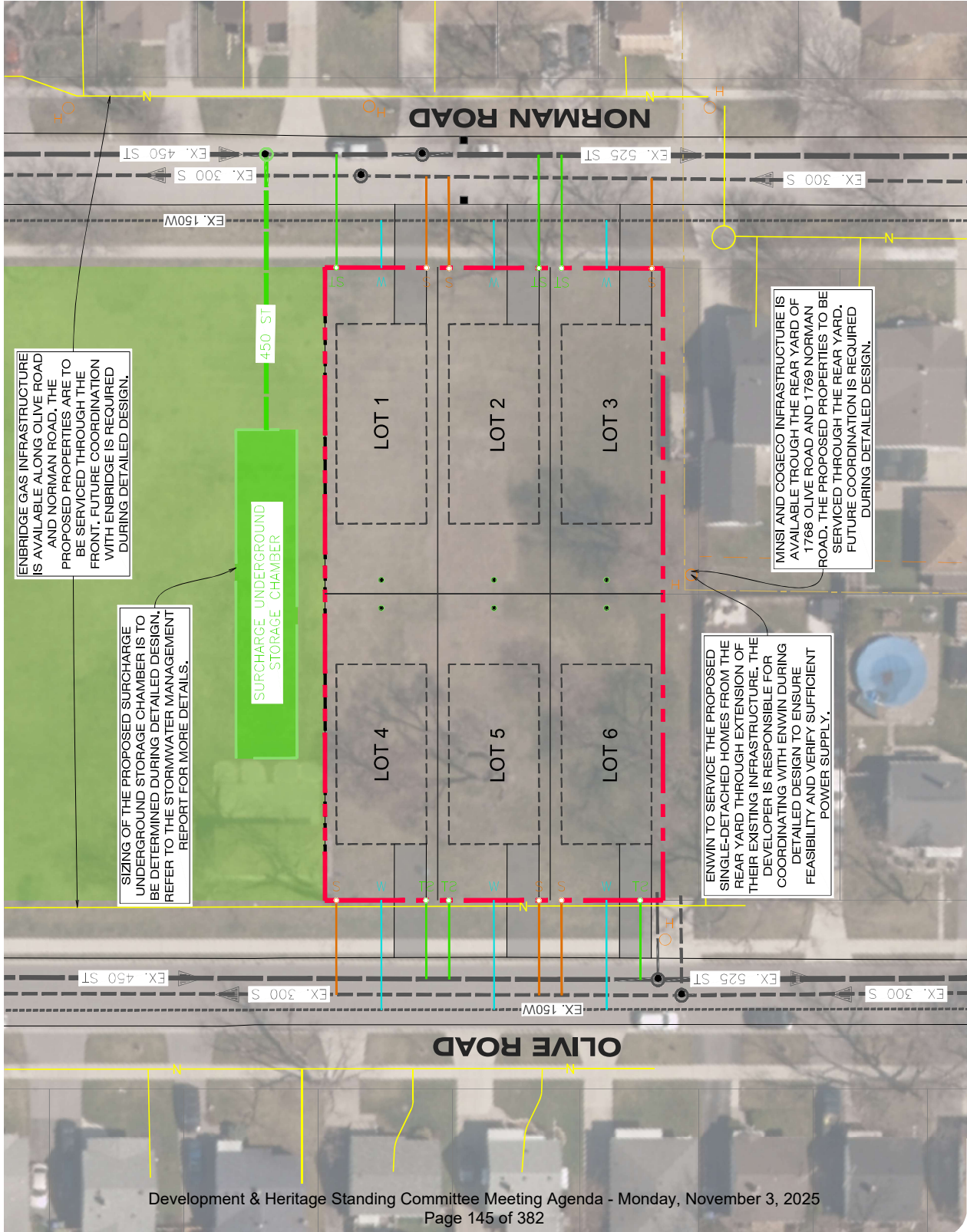
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CHECKED BY: SEF
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PHOTOGRAPHY (2024)

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September 16, 2025 11:13 PM



PROJECT: 25-1503
STATUS: DRAFT
DATE: 09/12/2025



The Corporation of the City of Windsor

STORMWATER MANAGEMENT REPORT

KINSMEN NORMAN ROAD PARK DEVELOPMENT

Zoning By-Law Amendment Application

SEPT 2025 – 25-1503

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Appendix B	Water Quantity Control Calculations
Appendix C	Figures
Appendix D	Olive Road Geotechnical Report

1.0 INTRODUCTION

Dillon Consulting Limited (Dillon) was retained by the Corporation of the City of Windsor to prepare a Stormwater Management (SWM) strategy for the proposed development located at 1730 Olive Road, Windsor, Ontario. More specifically, this stormwater management strategy is intended to provide mitigation measures for the anticipated increase in runoff as a result of the development. This technical design brief supports a site-specific Zoning By-law Amendment (ZBA) application.

The proposed development is approximately 0.24 ha (0.60 acres) and, when fully developed, will feature six private single-detached homes. Refer to **Figure 1** for the proposed location.



Figure 1: Proposed Site Location

The site is bounded by parkland to the north, Norman Road to the east, single detached dwellings to the south, and Olive Road to the west. The property currently consists of parkland and falls under the 'Green District' (GD1.1) zoning.

1.1 STORMWATER MANAGEMENT DESIGN REQUIREMENTS

The stormwater management design for the Site is based on the Windsor/Essex Region Stormwater Management Manual (June 2024) [WERSM]. The following sections outline the SWM design requirements for the Site.

1.1.1 Quantity Control

In the City of Windsor, quantity control criteria require that peak release rates from proposed developments for return period storms (2-year to 100-year inclusive) be restricted to their pre-development rates. Due to the nature of this development, however, it is not feasible to collect all runoff into a single feature to achieve this control.

In consultation with the City of Windsor, flows from the proposed single-detached dwellings are to be released unrestricted into the receiving storm network on Norman Road and Olive Road. To address this unrestricted release, the site's quantity control criteria will compensate by providing an equivalent volume to what would typically be required.

2.0 EXISTING CONDITIONS

Under existing conditions, the Site's runoff is directed towards a catch basin located in the middle of the Kinsmen Park site area. Runoff from the site ultimately outlets into the storm sewer along Norman Road. The Site primarily consists of grassed surfaces.

2.1 ALLOWABLE RELEASE RATE

A 0.24 ha site catchment area was used for the pre-development condition analysis. ERCA public interactive mapping shows that existing soil conditions consist of Brookston Clay Loam, classified as Hydrologic Soil Group (HSG) D.

In consultation with the City of Windsor, it was determined that the allowable release rate will be restricted to the 5-year pre-development conditions. Refer to **Appendix A** for communication with the City of Windsor.

Pre-development runoff parameters for the Site are summarized in **Table 1**:

Table 1: Pre-Development Parameters		
Site Area (Ha)	Runoff Coefficient	5-Year Pre-Development Flow (L/s)
0.24	0.20	8.8

The existing conditions design inputs and calculations are presented in **Appendix B**.

3.0 POST DEVELOPMENT CONDITIONS

Under post-development conditions, the site will consist of six single-detached dwellings. A runoff coefficient of 0.70 has been used for the analysis.

Post-development calculations for the site are provided in **Appendix B**.

Table 2: Post-Development Parameters

Drainage Area (ha)	Runoff Coefficient
0.24 ha	0.70

3.1 STORMWATER MANAGEMENT DESIGN

The SWM design under proposed conditions is to include water quantity control compensation for the Site. A preliminary composite servicing plan is provided in **Appendix C**.

3.1.1 Water Quantity Control

As established in Section 1.1.1, quantity control is proposed by providing compensation for the unrestricted flows produced by the site. More specifically, a surcharge underground storage chamber located directly north of the development, within Kinsmen Park, will be used to store an equivalent quantity control detention volume. Although the site will not contribute directly to this storage chamber, calculations have been provided under the assumption that the site will contribute directly to this storage system while being restricted to the allowable release rate in order to establish the required volume.

Following the procedures established in the WERSM Appendix D, backwater conditions have been considered as part of the required volume estimate. Through a review of existing downstream conditions, it was estimated that the system experiences no release for a duration of approximately 20 minutes due to tailwater conditions in the receiving sewer.

Based on an allowable release rate of 8.8 L/s and the backflow conditions assessment, the following storage requirements have been estimated for the proposed redevelopment.

Table 3: Water Quantity Control Volume Requirements

Design Storm	Storage Requirements (m ³)
100-Year	98
UST	103
Backwater Conditions – No Release	109

Detailed storage calculations for the Site to meet the water quantity control requirements are provided in **Appendix B**.

The provided on-site storage and outlet design should be further refined to be consistent with the Site's final grading and servicing strategy.

3.2 UNDERGROUND STORAGE CHAMBER FEASIBILITY

To assess the feasibility and potential cost of this solution, an ADS StormTech MC-3500 storage chamber will be used as a reference. Based on a review of a geotechnical report provided by the City for a nearby project on Olive Road completed in 2021, the site appears to be suitable for an underground storage chamber based on their estimated long-term groundwater table of 3.7 to 4.4 m below ground surface.

It is important to note that this groundwater table was estimated based on borehole observations only, as no long-term monitoring was provided. Therefore, further geotechnical involvement will be required to confirm the seasonally high groundwater level at the chamber location. If it is found that the seasonally high groundwater table encroaches on the storage system's invert, alternative products will be required.

Additionally, a geotechnical engineer will need to assess the bearing capacity of the underlying soils as part of the chamber's final design.

The geotechnical report for the nearby project is provided in **Appendix D**. Although this report provides some context as to what can be anticipated on this site, it was not prepared in support of this development and includes a disclaimer that it should not be used for purposes outside of its original intent.

3.3 UNDERGROUND STORAGE CHAMBER COST ESTIMATE

Based on a review of recent project bids for a similar City of Windsor project involving the same MC-3500 StormTech System and similar volumes, as well as direct communication with a supplier, the approximate cost for a storage chamber is estimated to range between \$600 to \$750 per m³, which includes both stone and labor.

Overall, it is recommended to use larger storage chambers, as the price per cubic meter is more cost-effective. This was confirmed by the supplier and through our review of recent project bids.

4.0 CONCLUSIONS AND RECOMMENDATIONS

The Stormwater Management (SWM) strategy proposed for the development of the six single-detached dwellings at 1730 Olive Road is as follows. This design satisfies the site-specific Zoning By-law Amendment (ZBA) application requirements at this time.

- Flows from the proposed single-detached dwellings will be released unrestricted into the receiving storm network on Norman Road and Olive Road through direct private storm connections and surface discharge to the streets.
- An underground surcharge storage chamber is proposed on the existing park's footprint, located directly north of the proposed dwellings within Kinsmen Park. This chamber will serve as a relief for the overall system, designed to compensate for the increase in runoff produced by the increased impervious surface due to development.
- Further geotechnical investigation is required for the detailed design of the chambers, including verification of groundwater conditions and bearing capacity assessment of the underlying soils.

This report is respectfully submitted for review and approval. Should you have any questions, we would be pleased to discuss the results of our evaluation in further detail.

Yours sincerely,

DILLON CONSULTING LIMITED

James Michener, P. Eng
Water Resources Engineer

Jaidy Wilson, EIT
Civil Designer

APPENDIX A

Email Communications with City of Windsor – Allowable Release Rate



Wilson, Jaidy <jdwilson@dillon.ca>

Fwd: FW: Outline of Services - Kinsmen Norman Road Park (1730 Olive Road)

Lopez, Daniel <DLopez@citywindsor.ca>

Mon, Jun 9, 2025 at 3:22 PM

To: "Wilson, Jaidy" <jdwilson@dillon.ca>

Hi Jaidy,

I sent over what I have for Norman Road As built.

For SWM: See attached "2025 – KINSMEN NORMAN ROAD PARK DISPOSITION". I am proposing using some park land for stormwater mitigation measures. We should be restricting to 5 year release rate, as both Olive and Norman have storm sewers.

Survey CAD and PDF attached – please note we don't have legal survey or the ability to create R plans, this is only for servicing and design of the lots and parkland.

Thank you,

Daniel

From: Wilson, Jaidy <jdwilson@dillon.ca>**Sent:** June 9, 2025 2:30 PM**To:** Lopez, Daniel <DLopez@citywindsor.ca>**Subject:** Re: FW: Outline of Services - Kinsmen Norman Road Park (1730 Olive Road)

You don't often get email from jdwilson@dillon.ca. [Learn why this is important](#)

[Quoted text hidden]

4 attachments**S-1605-ALL.pdf**
1789K**KINSMEN PARK.dwg**
334K**KINSMEN PARK.pdf**
348K**2025 - KINSMEN NORMAN ROAD PARK DISPOSITION - 10APR2025.pdf**
1841K

APPENDIX B

Water Quantity Control Calculations

	Stormwater Management Calculations	Project: Kinsmen Norman Road Park Dev.	No.: 25-1503	
	Rational Method Calculations	By: JW	Date: 9/12/2025	Page:
		Checked: JVM	Scenario: Existing	1

Calculation of existing runoff rate is undertaken using the Rational Method:

$$Q = CIA / 360$$

Where: Q = Peak flow rate (litres/second)

C = Runoff coefficient

I = Rainfall intensity (mm/hour)

A = Catchment area (hectares)

Project Area **0.24** hectares

Composite Runoff Coefficient		
Land Use	Area (m ²)	C
Open Area	2,427	0.20
Composite Runoff Coefficient	2,427	0.20

Time of Concentration							
Method	Up EL (m)	Down EL (m)	Length (m)	Slope (%)	Area (ha)	C	Min Inlet Time (min)
	184.57	184.48	32.60	0.28	0.24	0.20	15
Bransby Williams						t _c (min) =	N/A
Airport						t _c (min) =	25.5

Rainfall intensity calculated in accordance with City of Windsor IDF Parameters:

(if only two paramters are provided, enter B as "0" and C as positive number)

Where: A, B, and C = IDF Parameters From Local Municipality Guidelines

I = Rainfall intensity (mm/hour)

T = Time of concentration (hours)

$$I = \frac{A}{(B + t_c)^C}$$

Return Period (Years)	2	5	10	25	50	100
A	854.0	1259.0	1511.0	1851.0	2114.0	2375.0
B	7.0	8.8	9.5	10.2	10.6	11.0
C	0.818	0.838	0.845	0.852	0.858	0.861
T (mins) **	25.5	25.5	25.5	25.5	25.5	25.5
I (mm/hr)	49.5	65.0	74.9	88.0	97.4	107.2
Q (L/s)	6.7	8.8	10.2	12.0	13.2	14.6
Q (m ₃ /s)	0.007	0.009	0.010	0.012	0.013	0.015



Stormwater Management Calculations

Project: Kinsmen Norman Road Park Dev.

No.: 25-1503

Required Storage Volume

By: JW

Date: 9/12/2025

Page: 2

Checked: JVM

Scenario: Proposed

Calculation of existing runoff rate is undertaken using the Rational Method: $Q = CIA / 360$

Where: Q = Peak flow rate (litres/second)
C = Runoff coefficient
I = Rainfall intensity (mm/hour)
A = Catchment area (hectares)

Project Area hectares

Composite Runoff Coefficient		
Land Use	Area (m ²)	C
Single Detached	2,426.98	0.70
Composite Runoff Coefficient	2,426.98	0.70
100 - Year C Value		0.88

100 - year C Value = $\frac{\text{Storage Depth (from Section 3.3.2.1 WERSMSM)}}{108\text{mm (100 Year 24 hour Rainfall)}}$ [1, Eq. 3.3.2.2]

Storage Depth = $72 + 0.33x$ (Hydrological Soil Group D) [1, Eq. 3.3.2.1d]

Storage Depth = 95.10
100-yr C = 0.881

Catchment ID

1:100 Year Modified Rational

Catchment Parameters

Area

0.24 ha

Runoff C

0.88

AxC

0.21

target

8.84 L/s

Sreq'd

98 m³

Catchment ID

UST

Catchment Parameters

Area

0.24 ha

MRM Critical Duration

1.17 hr

Additional Volume

4.96 m³

Sreq'd

103 m³

Catchment ID

Backwater Conditions - No Release

Catchment Parameters

Allowable Release Rate

0.009 m³/s

No Outflow Period

1200 sec

Additional Volume

10.61 m³

Sreq'd

109 m³

start time

10

increment

10

time (min)	Intensity (mm/hr)	Flow (m ³ /s)	Vol in (m ³)	target (m ³ /s)	vol out (m ³)	net (m ³)
10	172.68	0.103	61.55	0.009	5	56.25
20	123.48	0.073	88.03	0.009	11	77.43
30	97.06	0.058	103.80	0.009	16	87.89
40	80.44	0.048	114.69	0.009	21	93.47
50	68.94	0.041	122.88	0.009	27	96.36
60	60.50	0.036	129.39	0.009	32	97.57
70	54.01	0.032	134.76	0.009	37	97.64
80	48.86	0.029	139.33	0.009	42	96.90
90	44.66	0.027	143.29	0.009	48	95.55
100	41.18	0.024	146.78	0.009	53	93.74
110	38.23	0.023	149.90	0.009	58	91.56

<< MAX. STORAGE

Stress Test Additional Runoff Volume (m³) = $1.75 \frac{\text{mm}}{\text{hr}} \times \text{MRM Critical Duration (hrs)} \times \text{Area (ha)} \times 10$ [1, Eq. 3.3.2.2b]

Additional Storage Volume due to Backwater (m³) = Allowable Release Rate $\left(\frac{\text{m}^3}{\text{s}}\right) \times \text{No Outflow Period(s)}$ [1, D3.3.2.1]

References:

[1] Windsor/Essex Region Stormwater Manual (June, 2024)

APPENDIX C

Figures

**SERVICING AND COMPOSITE
UTILITY PLAN**
FIGURE 1.0

- SUBJECT SITE
- EXISTING WATERMAIN
- EXISTING SANITARY SEWER
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- PROPOSED STORM SERVICE
- PROPOSED WATER SERVICE
- PROPOSED SANITARY SERVICE
- EXISTING GAS MAIN
- EXISTING HYDRO POLE
- PROPOSED REAR YARD CATCH BASIN

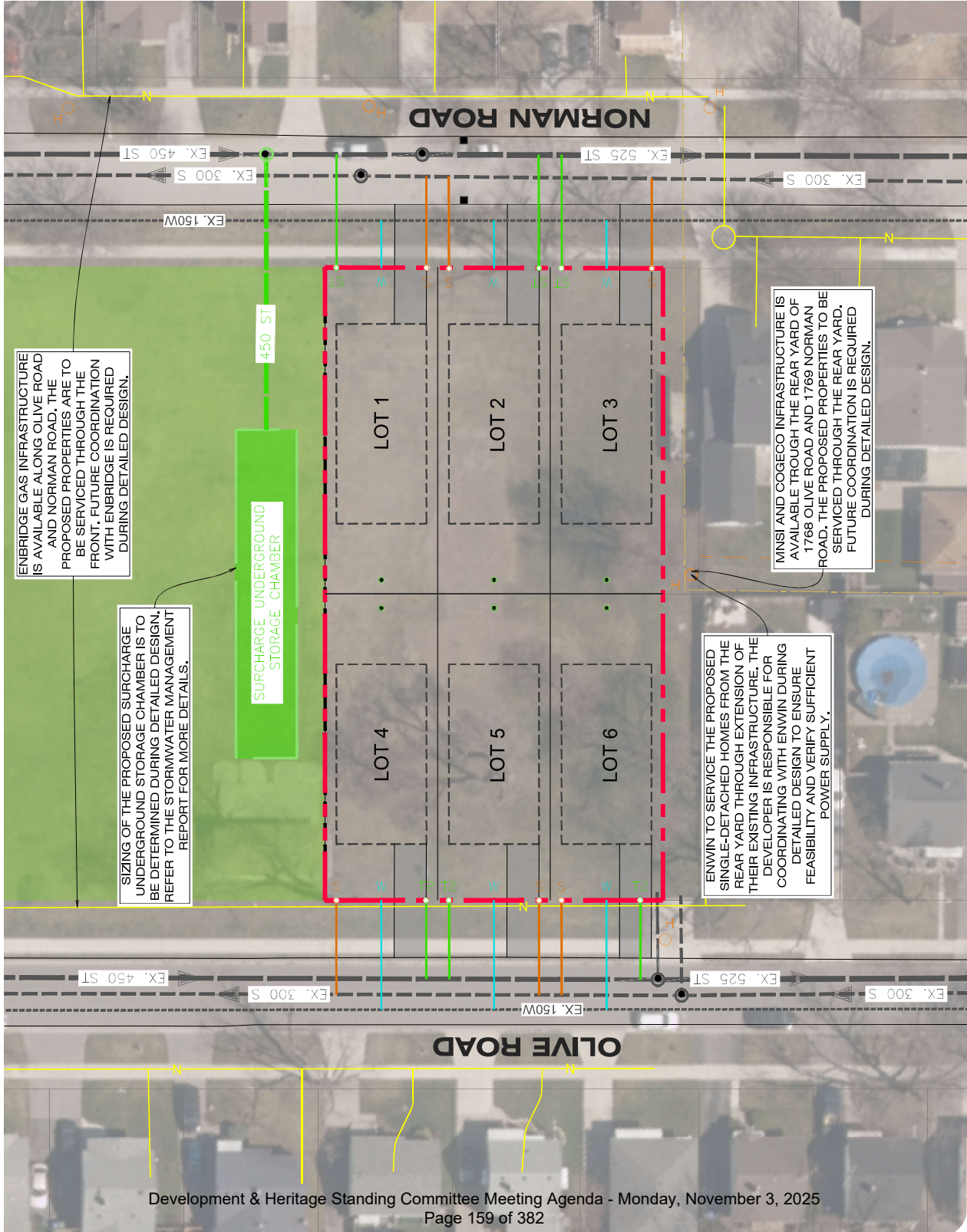
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PROJECT: 25-1503
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DATE: 09/12/2025



APPENDIX D

Olive Road Geotechnical Report



Geotechnical Investigation

The Corporation of the City of Windsor

Project Name:

Proposed Sewer Upgrading and Road Reconstruction
Olive Road
Windsor, Ontario

Project Number:

LON-21000835-A0

Prepared By:

EXP Services Inc.
2199 Blackacre Drive, Suite 600
Oldcastle, Ontario, N0R 1L0
Canada
t: +1.519.737.0588
f: +1.519.737.0751

Date Submitted:

March 19, 2021

Geotechnical Investigation

The Corporation of the City of Windsor

Type of Document:

Final Report

Project Name:

Proposed Sewer Upgrading and Road Reconstruction
Olive Road
Windsor, Ontario

Project Number:

LON-21000835-A0

Prepared By:

EXP Services Inc.
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Brent Gusba, P. Eng., PMP
Geotechnical Engineer



Geordy Fournier, P. Eng.
Geotechnical Engineer

Date Submitted:

March 19, 2021

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1. Introduction and Background

1.1 Introduction

EXP Services Inc. (EXP) was retained by the Corporation of the City of Windsor (City) to conduct a Geotechnical Investigation for the proposed sewer upgrading and reconstruction of Olive Road, in the City of Windsor, Ontario.

Authorization for EXP to proceed with the Geotechnical Investigation was provided through Contract Purchase Order CO6095 dated January 28, 2021.

It is understood that it is proposed to install new sewers on Olive Road between Tecumseh Road East and Milloy Street in the City of Windsor. The existing sewers range in depth from about 1.0 to 3.0 metres (m) below grade and it is understood that the new sewers will be installed in the same range of depth. Sewer upgrading in the two alleys on either side of Olive Road, just north of Tecumseh Road East, is also proposed.

Following the installation of the sewers, the roadway will be reconstructed using a flexible pavement structure.

1.2 Terms of Reference

The purpose of the investigation was to examine the subsurface soil and groundwater conditions at the site by advancing seven (7) boreholes at the locations illustrated on the attached Borehole Location Plan, **Drawing 1**.

The investigation and preparation of this report were carried out in general accordance with our proposal P20-426, dated January 28, 2021. Authorization to proceed with the investigation was provided by the City through Contract Purchase Order CO6095.

The objective of the Geotechnical Report is to summarize the results of the investigation and provide geotechnical engineering guidelines to assist with the design and construction of the proposed work. More specifically, this report provides comments on excavation, groundwater control, backfilling and pavement design.

This report is provided on the basis of the Terms of Reference presented above and with the assumption that the design will be in accordance with applicable codes and standards. If there are any changes in the design features relevant to the geotechnical analyses, or if any questions arise concerning the geotechnical aspects of the codes and standards, this office should be contacted to review the design.

Reference is made to **Appendix B** of this report, which contains further information necessary for the proper interpretation and use of this report.

2. Methodology

The fieldwork was conducted on February 15, 2021 and consisted of advancing seven (7) boreholes at the approximate locations shown on **Drawing 1**. These boreholes are designated as BH 1 through BH 7.

The boreholes were completed by a specialist drilling subcontractor under the full-time supervision of EXP geotechnical staff. The boreholes were advanced utilizing a truck-mounted drill rig equipped with continuous flight solid and hollow stem augers, soil sampling and soil testing equipment. In the boreholes, soil samples were recovered at regular intervals of depth using conventional split spoon sampling equipment or auger methods. The boreholes drilled at the site were terminated at depths of between 1.2 and 5.0 m below the ground surface.

Within the boreholes, Standard Penetration Tests (SPTs) were performed to assess the compactness or consistency of the underlying soils and to obtain representative samples. During the drilling, the soil samples obtained from the boreholes were examined and logged in the field by EXP geotechnical personnel.

Soil stratigraphy and observations pertaining to groundwater seepage into the boreholes are recorded in the borehole logs found in **Appendix A**. Following the drilling, the boreholes were backfilled with the excavated materials and bentonite, to satisfy the requirements of Ontario Regulation 903.

Representative samples of the various soil strata encountered at the borehole locations were taken to our laboratory in Windsor for further examination by a geotechnical engineer. Laboratory testing for this investigation comprised of routine moisture content determinations. The results are presented on the borehole logs found in **Appendix A**.

Samples remaining after laboratory testing will be stored for a period of three months following the issuance of the report. After this time, they will be discarded unless prior arrangements have been made for longer storage.

The ground surface elevations at the borehole locations have been interpolated from elevations noted on project drawings provided by the City of Windsor. It is understood that the elevations are referenced to geodetic datum.

3. Site and Subsurface Conditions

3.1 Site Description

The subject site is Olive Road between Tecumseh Road East and Milloy Street, in the City of Windsor, Ontario. The roads are in an urban cross-section and the road profiles generally decrease in elevation going northwards. The site also includes two alleys on either side of Olive Road, just north of Tecumseh Road East.

3.2 Soil Stratigraphy

The detailed stratigraphy encountered in each borehole is shown on the borehole logs found in **Appendix A** and summarized in the following paragraphs. It must be noted that the boundaries of the soil indicated on the borehole logs are inferred from non-continuous sampling and observations during drilling. These boundaries are intended to reflect transition zones for geotechnical design and should not be interpreted as exact planes of geological change.

The pavement structure and subsurface soil conditions encountered in the boreholes advanced at the site are summarized as follows.

Pavement Structure

Boreholes BH 1 to BH 5 were drilled on Olive Road where a pavement structure comprised of 100 to 130 millimetres (mm) of asphalt overlying about 100 to 630 mm of grey crushed granular material was encountered.

Boreholes BH 6 and BH 7 were drilled in the alleys and encountered about 250 mm of brown and grey gravel at the surface.

Fill Material

Beneath the surface gravel in BH 6, blackish brown sand and gravel material was encountered. The thickness of the sand and gravel was about 510 mm at the borehole location.

Underlying the granular road base material in BH 1, the sand and gravel in BH 6 and the surface gravel in BH 7, brown sand fill material was encountered. The thickness of the sand fill was about 1.4 m at the location of BH 6 and BH 7. Borehole BH 1 was terminated in this material. Measured 'N' values obtained from standard penetration testing carried out in the sand fill ranged from 5 to 17 blows per 0.3 m. The moisture content of the samples of the sand fill material obtained varied from about 9 to 22 percent.

Silty Clay Till

Beneath the pavement structure in BH 2 to BH 5, an extensive stratum of silty clay till was encountered. The upper portion of the glacial till was weathered to a mottled brown and grey colour and was about 0.6 to 1.5 m in thickness at the borehole locations. The measured 'N' values obtained in the mottled silty clay till ranged from 7 to 10 blows per 0.3 m, indicating a firm to stiff consistency. The moisture content of the samples of mottled silty clay till obtained varied from about 18 to 20 percent.

Underlying the mottled silty clay till in BH 2 to BH 5, stiff to hard, brown silty clay till was encountered. The brown crust ranged in thickness from about 2.3 to 3.1 m at the borehole locations. Measured 'N' values in the brown silty clay till ranged from 10 to 38 blows per 0.3 m. The moisture content of the samples of brown silty clay till obtained varied from about 12 to 16 percent.

Beneath the brown silty clay till in BH 2 to BH 5, and the fill materials in BH 6 and BH 7, firm to stiff, grey silty clay till was encountered to the termination depth of the boreholes. Measured 'N' values in the grey silty clay till ranged from 6 to 17 blows per 0.3 m. The moisture content of the samples of grey silty clay till obtained varied from about 11 to 14 percent.

3.3 Groundwater Conditions

During drilling, no groundwater seepage was encountered in the boreholes.

It is noted that the depth to the groundwater table may vary in response to climatic or seasonal conditions, and, as such, may differ at the time of construction, with higher levels in wet seasons.

The brown/grey colour interface within the silty clay material between a depth of about 3.7 and 4.4 m below grade is typically indicative of the long-term groundwater level.

4. Discussion and Recommendations

4.1 General

As indicated previously, it is proposed to install new sewers on Olive Road between Tecumseh Road East and Milloy Street in the City of Windsor. The existing sewers range in depth from about 1.0 to 3.0 m below grade and it is understood that the new sewers will be installed in the same range of depth. Sewer upgrading in the two alleys on either side of Olive Road, just north of Tecumseh Road East, is also proposed.

Following the installation of the sewers, the roadway will be reconstructed using a flexible pavement structure.

The following sections of this report provide discussion and recommendations pertaining to the geotechnical aspects for excavation, groundwater control, backfilling and pavement design.

4.2 Excavations

At the start of construction, the pavement at the project limits should be saw-cut to avoid disturbance to the adjacent sections of the roadway and to facilitate removal of the existing asphalt. During excavation, care should be taken to completely remove the sections of existing pipe and all deleterious materials, so that a suitable founding stratum is exposed to receive the new pipe(s).

Based on the results of this investigation, the subsurface soil conditions are generally comprised of the existing pavement structure overlying an extensive stratum of firm to hard silty clay till.

Excavations at the site must be carried out in accordance with Regulation 213/91 'Construction Projects' of the Occupational Health and Safety Act & Regulations of Ontario (Act). Based on the type and consistency of the subsurface soils encountered at the site, they may be classified as 'Type 3' soils under the Act.

It is anticipated that vertically cut excavations utilizing a pre-fabricated support system (trench liner box) will be used for sewer installations at the site. The support system should be designed and installed in a manner that not only provides protection for the workers, but also provides adequate support for the sides of the excavation.

Where applicable, the lateral earth pressure acting on the excavation shoring walls may be calculated from the following equation:

$$P = K (\gamma h + q)$$

where, P = lateral earth pressure in kPa acting at depth h;

γ = natural unit weight, a value of 21.0 kN/m³ may be assumed;

h = depth of point of interest in m;

q = equivalent value of any surcharge on the ground surface in kPa;

K = coefficient of earth pressure = 0.3.

Surcharge loading adjacent to the excavation should be minimized. This can be accomplished by maintaining a level area extending at least one metre outward from the upper edge of each wall of the excavation and keeping this area clear of equipment, excavated soil and construction materials.

For relatively shallow installations, conventional open cut excavations with side slopes of 1 horizontal to 1 vertical or flatter could be considered.

If the sewers are to be installed in a common trench, the deeper sewer should be constructed first and backfilled to the invert depth of the shallower sewer.

Problems with groundwater control during construction are generally not anticipated. However, some water seepage emanating from the mottled silty clay till may be present during construction in seasonally wetter conditions and/or after periods of heavy rainfall. Localized sumps installed at the base of the excavation should be adequate to control the anticipated seepage volumes. To reduce the potential for water seepage into the excavations, adequate provisions should be made for the control of surface water adjacent to the open utility trenches.

4.3 Protection of Existing Structures

Several existing utility lines will likely cross the proposed sewer alignments. It is recommended that the alignment and depth of existing utilities be checked relative to the proposed excavations. Where existing utilities are exposed during the excavation, suitable temporary or permanent support of these utilities should be provided consistent with the requirements of the respective utility company.

Adjacent to a vertically sided supported excavation, there are several zones in which movements of the surrounding soil may take place, as noted in **Drawing 2**. Within a zone defined by the vertical wall of the excavation and a line sloped at 1 horizontal to 2 vertical, upward and outward from 0.6 metres below the base of the excavation, substantial movements may be anticipated. Structures within this zone should be protected by underpinning. Within a zone bounded by lines sloping at 1H to 2V and 1H to 1V from a point located 0.6 metres below the base of the excavation, some settlements may be expected.

Longitudinal open sections of the trench(s) should be kept to a minimum and backfilling of the trench(s) should be carried out immediately behind the support system. Any utilities along the proposed route of the new services that fall within the zones described above should be continuously monitored during construction so that corrective action can be taken if significant ground movement is observed.

4.4 Pipe Bedding and Cover

The subgrade soils beneath the sewers are generally expected to consist of undisturbed silty clay till. Localized improvement to the base soils may be required if work is carried out in wet weather.

The bedding and cover for the new services should be placed in accordance with the Ontario Provincial Standard Drawings (OPSD 802.010 and 802.031). In general, the minimum bedding depth underneath the pipe should be 0.15 times the inside diameter of the pipe, but should not be less than 150 mm or greater than 300 mm in thickness. The aggregate material should be placed around the pipe to at least 300 mm above the pipe. The bedding and cover should be compacted to a minimum of 95 percent of Standard Proctor Maximum Dry Density (SPMDD).

Granular 'A' material may be used as bedding and cover for the proposed sewers. Below the spring line of the pipe, clear stone may be used in lieu of Granular 'A'. If clear stone is utilized, a geotextile should be wrapped around the stone bedding to minimize the migration of fines into the stone material. As an alternative to clear stone and Granular 'A', fine sand material with trace gravel meeting the gradation requirements of OPSS Granular 'B', Type I could be considered for the bedding and cover.

4.5 General Trench Backfill

The existing pavements, and any deleterious fill materials and/or topsoil (should they be encountered) are not considered suitable for trench backfill, and once excavated, should be removed from the site and wasted.

Based on the results of this investigation, most of the natural silty clay soils may be used for general trench backfill, provided that reasonable care is exercised in the handling and placement of the material, and that the material is of the appropriate moisture content.

The trench backfill over the existing sewers may also have the potential to be re-used as general trench backfill, provided the fill is clean and similar in composition to the native silty clay excavated. It is imperative that any excavated soils proposed for re-use as general trench backfill be examined by the geotechnical engineer.

In order to achieve the required levels of compaction, the material should have a moisture content within 3 percent of the optimum moisture content as determined by the Standard Proctor density test. The material should be placed in maximum 300 mm thick loose lifts and uniformly compacted to at least 95 percent of SPMDD.

Where the upper one metre of the trench backfill forms a roadway subgrade, it should be uniformly compacted to at least 98 percent of SPMDD.

With the backfill recommendations outlined above and the maximum anticipated excavation depths, it is estimated that the long-term settlement of the trench backfill will be in the order of about 30 to 40 mm. A program of in-situ density testing should be carried out on the trench backfill to monitor the degree and uniformity of compaction levels achieved.

Imported granular material such as Granular 'B', Type I may be used in the upper portion of the backfill to reduce the amount of post-construction settlement, or if sufficient native borrow is not available.

Disposal of excavated materials off site should conform to current Ministry of Environment guidelines.

4.6 Pavement Design

Following completion of the sewer installations, the reconstruction of the roadway may commence. Prior to constructing any new pavement structures at the site, existing pavements and all softened, loosened and/or otherwise deleterious materials should be removed from within the limits of the proposed roadway. The exposed subgrade should then be heavily proof-rolled with a non-vibratory steel wheel roller under the supervision of the geotechnical engineer. Any excessively softened areas identified during this operation should be sub-excavated and backfilled with approved granular material placed in maximum 300 mm thick loose lifts, uniformly compacted to 98 percent of SPMDD.

The proposed roadway at the site may be classified as a local roadway. For such a classification, a flexible pavement design consisting of 90 mm of asphalt and 350 mm of Granular 'A' base is recommended.

Table 1 – Recommended Pavement Structure Thicknesses

Pavement Layer	Pavement Structure	Compaction Requirements
Asphaltic Concrete	40 mm (surface) 50 mm (binder)	92% MRD
Granular 'A' (Base)	350 mm	100% SPMDD
Notes: 1) SPMDD denotes Standard Proctor Maximum Dry Density, MRD denotes Maximum Relative Density. 2) In-situ density testing should be carried out to confirm compaction levels. 3) The above recommendations are minimum requirements.		

These recommendations on thickness design are not intended to support heavy and concentrated construction traffic, particularly where only a portion of the pavement section is completed.

The flexible pavement should consist of HL 3 or HL 4 surface course asphalt and HL 4 or HL 8 binder course asphalt. The asphalt should be produced and placed in accordance with current OPSS requirements.

The Granular 'A' base material should be placed in maximum 250 mm thick loose lifts and uniformly compacted to 100 percent of SPMDD.

If construction is undertaken under adverse weather conditions (i.e., wet or freezing conditions) subgrade preparation and granular base requirements should be reviewed by the geotechnical engineer.

Effective drainage is an important factor in the life expectancy and performance of the completed pavement structure. In this regard, it is recommended that continuous perforated subdrains be installed along both sides of the roadways. The subdrains should be placed just below the subgrade level, bedded in properly graded granular material and connected to the roadside catch-basins. The pavement subgrade should be properly shaped and graded to provide adequate cross falls. Any water which finds its way into the granular base would then be directed to the subdrains.

Construction of the surface course asphalt should be delayed approximately 9 months to allow for settlement of the trench backfill material. However, delaying the final surface paving more than 12 months could increase the potential for distress to the binder course layer.

Where the new pavement joins an existing pavement, a milled joint at least 500 mm wide, should be made in the existing surface layer to receive the new asphaltic material.

It is understood that once sewer installation is complete in the alleys, they will be surfaced with a crushed granular material. The thickness of the granular material should be at least 450 mm.

4.7 Concrete Sidewalks and Curbs

Exterior concrete for sidewalks and curbs should be comprised of 32 MPa concrete with 6% air entrainment. During cold weather, the freshly placed concrete should be covered with insulating blankets to protect against freezing.

The concrete for use at the site should be proportioned and mixed in accordance with the requirements of OPSS 1350, Material Specification for Concrete. The concrete should be placed and cured in accordance with OPSS 351, Construction Specification for Concrete Sidewalk and OPSS 353, Construction Specification for Concrete Curb and Gutter Systems.

4.8 Inspection and Testing Requirements

An effective inspection and testing program is an essential part of construction monitoring. To ensure that construction is carried out in a manner consistent with the recommendations of this report and the various material and project specifications, the following should be implemented during the construction phase:

- In-situ density testing of backfill materials and granular base materials to determine if appropriate levels of compaction are achieved;
- Laboratory testing (grain size distribution analyses and Standard Proctor density tests) on the various granular materials placed at the site;
- Subgrade (proof-rolling) inspections to monitor the performance of the subgrade soil prior to construction of the pavement structure at the site;
- Concrete testing (slump, air content, temperature and casting of test specimens) for the curb and gutter to determine conformance to the project specifications;
- Inspection and in-situ density testing for the binder and surface course asphalt, including laboratory testing on asphalt samples to assess conformance to project specifications and OPSS requirements.

EXP would be pleased to prepare an inspection and testing work program prior to construction, incorporating the above items.

5. General Comments

The information presented in this report is based on a limited investigation designed to provide information to support an assessment of the current geotechnical conditions within the subject property. The conclusions and recommendations presented in this report reflect site conditions existing at the time of the investigation. Consequently, during the future development of the property, conditions not observed during this investigation may become apparent. Should this occur, EXP Services Inc. should be contacted to assess the situation, and the need for additional testing and reporting. EXP has qualified personnel to provide assistance in regards to any future geotechnical and environmental issues related to this property.

Our undertaking at EXP, therefore, is to perform our work within limits prescribed by our clients, with the usual thoroughness and competence of the engineering profession.

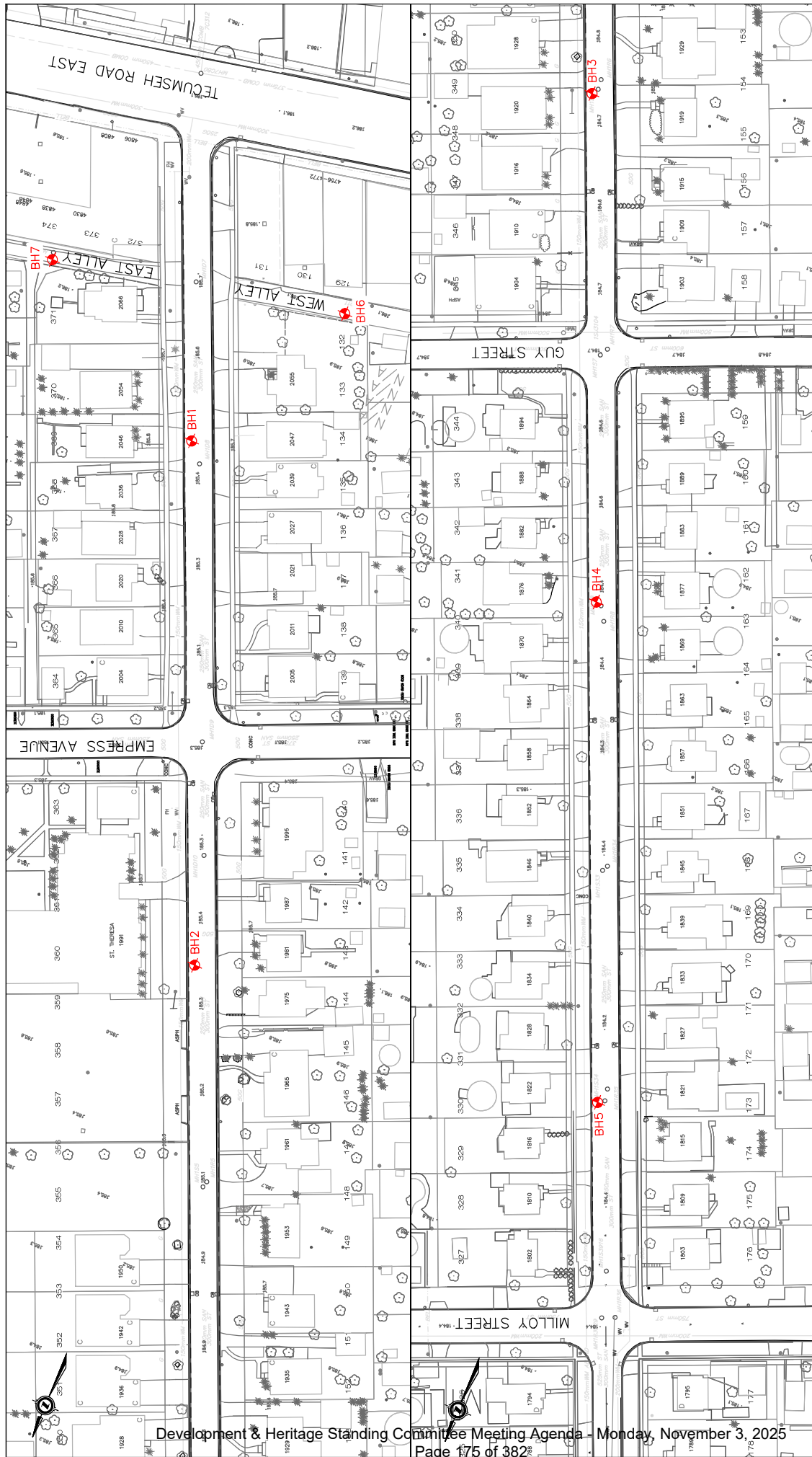
The comments given in this report are intended only for the guidance of design engineers. The number of test holes required to determine the localized underground conditions between test holes affecting construction costs, techniques, sequencing, equipment, scheduling, etc. would be much greater than has been carried out for design purposes. Contractors bidding on or undertaking the works should in this light, decide on their own investigations, as well as their own interpretations of the factual borehole results, so that they may draw their own conclusions as to how the subsurface conditions may affect them.

EXP Services Inc. should be retained for a general review of the final design and specifications to verify that this report has been properly interpreted and implemented. If not afforded the privilege of making this review, EXP Services Inc. will assume no responsibility for interpretation of the recommendations in this report.

This report was prepared for the exclusive use of **The Corporation of the City of Windsor** and may not be reproduced in whole or in part, without the prior written consent of EXP, or used or relied upon in whole or in part by other parties for any purposes whatsoever. Any use which a third party makes of this report, or any part thereof, or any reliance on or decisions to be made based on it, are the responsibility of such third parties. EXP Services Inc. accepts no responsibility for damages, if any, suffered by any third party as a result of decisions made or actions based on this report.

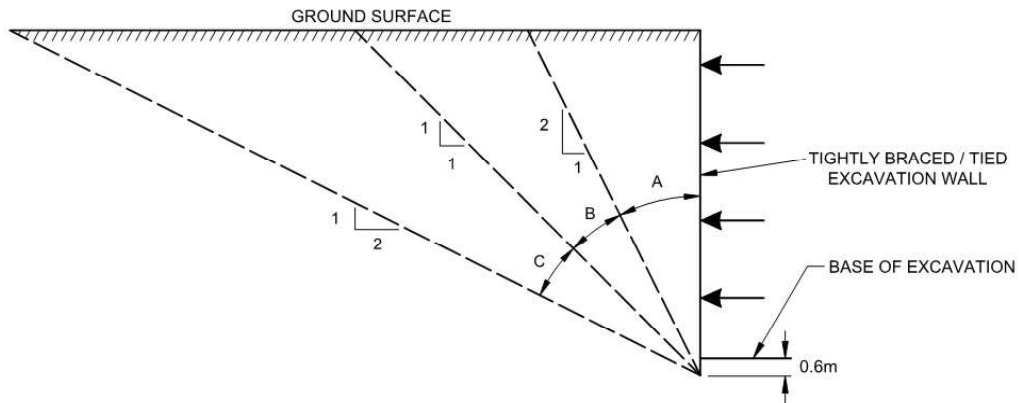
We trust this report is satisfactory for your purposes. Should you have any questions, please do not hesitate to contact this office.

Drawings



Geotechnical Investigation		CLIENT The Corporation of the City of Windsor	
Approximate Borehole Location		TITLE Borehole Location Plan	
NOTES: 1. The boundaries and soil types have been established only at test hole locations. Between test holes they are assumed and may be subject to considerable error. 2. Soil samples will be retained in storage for 3 months and then destroyed unless client advises that an extended time period is required. LON-2100835-40.		DRAWN BY: M.B. REVIEWED BY: B.G.	
LEGEND: BH1 Approximate Borehole Location		exp. EXP Services Inc. 15701 Robin's Hill Road, London, ON, N5V 0A5	
Windsor, Ontario		DATE MARCH 2021 SCALE NTS PROJECT NO. LON-2100835-40 DWG. 1	

DRAWING 2 – SUPPORT FOR ADJACENT STRUCTURES



NOTES:

Zone A:

Substantial movements in this zone should be anticipated. Structures in this zone should be protected by underpinning.

Zone B:

Some movement in this zone should be anticipated.

Zone C:

Slight movement in this zone may be anticipated.

Appendix A – Borehole Logs



BOREHOLE LOG

BH1

Sheet 1 of 1

CLIENT The Corporation of the City of Windsor PROJECT NO. LON-21000835-A0
PROJECT Olive Road Sewer Upgrading DATUM Geodetic
LOCATION Windsor, ON DATES: Completed February 15, 2021 Water Level _____

DEPTH (m bgs)	ELEVATION (~m)	STRATA DESCRIPTION	STRATA PLOT	WELL LOG	SAMPLES				MOISTURE CONTENT (%)	SHEAR STRENGTH	
					TYPE	NUMBER	RECOVERY (mm)	N VALUE (blows)		◆ S Field Vane Test (#=Sensitivity) ▲ Penetrometer ■ Torvane	Atterberg Limits and Moisture W _P W W _L ● SPT N Value × Dynamic Cone
0	185.50										
	185.37	ASPHALT - 130 mm			AS	S1A			6		
	185.17	FILL - Grey Crushed Granular Material, 200 mm			AS	S1B			18		
		FILL - sand, brown, fine grained, compact, moist			SS	S2	330	17	21		
1	184.28	End of Borehole at 1.2 m bgs.									
2											
3											
4											
5											
6											
7											

NOTES

- 1) Borehole Log interpretation requires assistance by EXP before use by others and must be read in conjunction with EXP Report LON-21000835-A0.
- 2) Borehole open and dry upon completion of drilling.
- 3) bgs denotes below ground surface.

SAMPLE LEGEND

☒ AS Auger Sample ☒ SS Split Spoon ■ ST Shelby Tube
☒ Rock Core (eg. BQ, NQ, etc.) ☒ VN Vane Sample

OTHER TESTS

G Specific Gravity C Consolidation
H Hydrometer CD Consolidated Drained Triaxial
S Sieve Analysis CU Consolidated Undrained Triaxial
γ Unit Weight UU Unconsolidated Undrained Triaxial
P Field Permeability UC Unconfined Compression
K Lab Permeability DS Direct Shear

WATER LEVELS

▽ Apparent ▼ Measured ▲ Artesian (see Notes)



BOREHOLE LOG

BH2

Sheet 1 of 1

CLIENT **The Corporation of the City of Windsor**PROJECT NO. **LON-21000835-A0**PROJECT **Olive Road Sewer Upgrading**DATUM **Geodetic**LOCATION **Windsor, ON**DATES: Completed **February 15, 2021**

Water Level _____

DEPTH (m bgs)	ELEVATION (~ m)	STRATA DESCRIPTION	STRATA PLOT	WELL LOG	SAMPLES				MOISTURE CONTENT (%)	SHEAR STRENGTH				
					TYPE	NUMBER	RECOVERY (mm)	N VALUE (blows)		◆ S Field Vane Test (#=Sensitivity)				
										▲ Penetrometer ■ Torvane				
										Atterberg Limits and Moisture				
										100	200 kPa			
											W _P W W _L			
											● SPT N Value	× Dynamic Cone		
											10	20	30	40
0	185.30													
	185.17	ASPHALT - 130 mm			AS	S1A								
	184.97	FILL - Grey Crushed Granular Material, 200 mm			AS	S1B			18					
		SILTY CLAY TILL - mottled brown and grey, some sand, trace gravel, stiff, moist			SS	S2	250	10	19					
1														
	183.93	SILTY CLAY TILL - brown, some sand, trace gravel, very stiff, moist			SS	S3	460	21	14					
2														
					SS	S4	430	28	12					
3														
					SS	S5	430	25	13					
4														
					SS	S6	430	24	13					
	180.88	SILTY CLAY TILL - grey, some sand, trace gravel, stiff, moist												
					SS	S7	460	12	13					
5	180.27	End of Borehole at 5.0 m bgs.												
6														
7														

NOTES

- 1) Borehole Log interpretation requires assistance by EXP before use by others and must be read in conjunction with EXP Report LON-21000835-A0.
- 2) Borehole open and dry upon completion of drilling.
- 3) bgs denotes below ground surface.

SAMPLE LEGEND

- AS Auger Sample SS Split Spoon ST Shelby Tube
Rock Core (eg. BQ, NQ, etc.) VN Vane Sample

OTHER TESTS

- G Specific Gravity C Consolidation
H Hydrometer CD Consolidated Drained Triaxial
S Sieve Analysis CU Consolidated Undrained Triaxial
γ Unit Weight UU Unconsolidated Undrained Triaxial
P Field Permeability UC Unconfined Compression
K Lab Permeability DS Direct Shear

WATER LEVELS

- ▽ Apparent ▼ Measured ▲ Artesian (see Notes)



BOREHOLE LOG

BH3

Sheet 1 of 1

CLIENT **The Corporation of the City of Windsor**PROJECT NO. **LON-21000835-A0**PROJECT **Olive Road Sewer Upgrading**DATUM **Geodetic**LOCATION **Windsor, ON**DATES: Completed **February 15, 2021**

Water Level _____

DEPTH (m bgs)	ELEVATION (~m)	STRATA DESCRIPTION	STRATA PLOT	WELL LOG	SAMPLES				MOISTURE CONTENT (%)	SHEAR STRENGTH	
					TYPE	NUMBER	RECOVERY (mm)	N VALUE (blows)		◆ S Field Vane Test (#=Sensitivity) ▲ Penetrometer ■ Torvane	Atterberg Limits and Moisture W _P W W _L ● SPT N Value 10 20 × Dynamic Cone 30 40
0	184.80	ASPHALT - 130 mm			AS	S1			10		
	184.67	FILL - Grey Crushed Granular Material, 630 mm									
	184.04				SS	S2	410	7	20		
-1		SILTY CLAY TILL - mottled brown and grey, some sand, trace gravel, firm, moist									
	183.43				SS	S3	460	25	13		
-2		SILTY CLAY TILL - brown, some sand, trace gravel, very stiff, moist									
					SS	S4	460	29	13		
-3											
					SS	S5	460	23	13		
-4	180.84										
		SILTY CLAY TILL - grey, some sand, trace gravel, stiff, moist			SS	S6	460	14	13		
-5	179.77										
					SS	S7	460	8	14		
-5		End of Borehole at 5.0 m bgs.									
-6											
-7											

NOTES

- 1) Borehole Log interpretation requires assistance by EXP before use by others and must be read in conjunction with EXP Report LON-21000835-A0.
- 2) Borehole open and dry upon completion of drilling.
- 3) bgs denotes below ground surface.

SAMPLE LEGEND

- ☒ AS Auger Sample ☒ SS Split Spoon ■ ST Shelby Tube
☒ Rock Core (eg. BQ, NQ, etc.) ☒ VN Vane Sample

OTHER TESTS

- G Specific Gravity C Consolidation
H Hydrometer CD Consolidated Drained Triaxial
S Sieve Analysis CU Consolidated Undrained Triaxial
γ Unit Weight UU Unconsolidated Undrained Triaxial
P Field Permeability UC Unconfined Compression
K Lab Permeability DS Direct Shear

WATER LEVELS

- ▽ Apparent ▼ Measured ▲ Artesian (see Notes)



BOREHOLE LOG

BH4

Sheet 1 of 1

CLIENT **The Corporation of the City of Windsor**PROJECT NO. **LON-21000835-A0**PROJECT **Olive Road Sewer Upgrading**DATUM **Geodetic**LOCATION **Windsor, ON**DATES: Completed **February 15, 2021**

Water Level _____

DEPTH (m bgs)	ELEVATION (~ m)	STRATA DESCRIPTION	STRATA PLOT	WELL LOG	SAMPLES				MOISTURE CONTENT (%)	SHEAR STRENGTH			
					TYPE	NUMBER	RECOVERY (mm)	N VALUE (blows)		◆ S Field Vane Test (#=Sensitivity)			
										▲ Penetrometer ■ Torvane			
										Atterberg Limits and Moisture W _P W W _L			
● SPT N Value × Dynamic Cone													
	184.50									100	200 kPa		
0	184.37	ASPHALT - 130 mm			AS	S1A			11				
	184.27	FILL - Grey Crushed Granular Material, 100 mm			AS	S1B			18				
		SILTY CLAY TILL - mottled brown and grey, some sand, trace gravel, stiff, moist											
1					SS	S2	380	10	18				
	182.82				SS	S3	430	10	13				
2		SILTY CLAY TILL - brown, some sand, trace gravel, hard to very stiff, moist											
					SS	S4	460	38	13				
3													
					SS	S5	460	21	13				
4	180.46												
		SILTY CLAY TILL - grey, some sand, trace gravel, stiff, moist											
5	179.47				SS	S6	330	11	12				
		End of Borehole at 5.0 m bgs.											
6													
7													

NOTES

- 1) Borehole Log interpretation requires assistance by EXP before use by others and must be read in conjunction with EXP Report LON-21000835-A0.
- 2) Borehole open and dry upon completion of drilling.
- 3) bgs denotes below ground surface.

SAMPLE LEGEND

AS Auger Sample SS Split Spoon ST Shelby Tube
Rock Core (eg. BQ, NQ, etc.) VN Vane Sample

OTHER TESTS

G Specific Gravity C Consolidation
H Hydrometer CD Consolidated Drained Triaxial
S Sieve Analysis CU Consolidated Undrained Triaxial
γ Unit Weight UU Unconsolidated Undrained Triaxial
P Field Permeability UC Unconfined Compression
K Lab Permeability DS Direct Shear

WATER LEVELS

▽ Apparent ▼ Measured ▲ Artesian (see Notes)



BOREHOLE LOG

BH5

Sheet 1 of 1

CLIENT **The Corporation of the City of Windsor**PROJECT NO. **LON-21000835-A0**PROJECT **Olive Road Sewer Upgrading**DATUM **Geodetic**LOCATION **Windsor, ON**DATES: Completed **February 15, 2021**

Water Level _____

DEPTH (m bgs)	ELEVATION (~ m)	STRATA DESCRIPTION	STRATA PLOT	WELL LOG	SAMPLES				MOISTURE CONTENT (%)	SHEAR STRENGTH				
					TYPE	NUMBER	RECOVERY (mm)	N VALUE (blows)		◆ S Field Vane Test (#=Sensitivity)				
										▲ Penetrometer ■ Torvane				
										Atterberg Limits and Moisture				
										100	200 kPa			
											W _P	W	W _L	
											● SPT N Value	× Dynamic Cone		
											10	20	30	40
0	184.30	ASPHALT - 100 mm			AS	S1A			10					
184.20		FILL - Grey Crushed Granular Material, 130 mm			AS	S1B			20					
184.07		SILTY CLAY TILL - mottled brown and grey, some sand, trace gravel, stiff, moist												
					SS	S2	360	8	20					
1														
	182.93	SILTY CLAY TILL - brown, some sand, trace gravel, very stiff to hard, moist			SS	S3	460	24	13					
2														
					SS	S4	460	34	12					
3														
					SS	S5	460	23	14					
4	180.64	SILTY CLAY TILL - grey, some sand, trace gravel, stiff to firm, moist			SS	S6	410	13	14					
					SS	S7	380	6	14					
5	179.27	End of Borehole at 5.0 m bgs.												
6														
7														

NOTES

- 1) Borehole Log interpretation requires assistance by EXP before use by others and must be read in conjunction with EXP Report LON-21000835-A0.
- 2) Borehole open and dry upon completion of drilling.
- 3) bgs denotes below ground surface.

SAMPLE LEGEND

- AS Auger Sample SS Split Spoon ST Shelby Tube
Rock Core (eg. BQ, NQ, etc.) VN Vane Sample

OTHER TESTS

- G Specific Gravity C Consolidation
H Hydrometer CD Consolidated Drained Triaxial
S Sieve Analysis CU Consolidated Undrained Triaxial
γ Unit Weight UU Unconsolidated Undrained Triaxial
P Field Permeability UC Unconfined Compression
K Lab Permeability DS Direct Shear

WATER LEVELS

- ▽ Apparent ▼ Measured ▲ Artesian (see Notes)



BOREHOLE LOG

BH6

Sheet 1 of 1

CLIENT **The Corporation of the City of Windsor**PROJECT NO. **LON-21000835-A0**PROJECT **Olive Road Sewer Upgrading**DATUM **Geodetic**LOCATION **Windsor, ON**DATES: Completed **February 15, 2021**

Water Level _____

DEPTH (m bgs)	ELEVATION (~m)	STRATA DESCRIPTION	STRATA PLOT	WELL LOG	SAMPLES				MOISTURE CONTENT (%)	SHEAR STRENGTH	
					TYPE	NUMBER	RECOVERY (mm)	N VALUE (blows)		• S Field Vane Test (#=Sensitivity) ▲ Penetrometer ■ Torvane	Atterberg Limits and Moisture W _p W W _L ● SPT N Value 10 20 × Dynamic Cone 30 40
0	186.20	FILL - gravel, brown and grey, trace sand, 250 mm									
	185.95	FILL - sand and gravel, blackish brown, dry, 510 mm			AS	S1			11		
	185.44	FILL - sand, brown, fine grained, loose, moist			SS	S2	230	5	9		
1					SS	S3	200	6	22		
2	184.07	SILTY CLAY TILL - brown becoming grey, some sand, trace gravel, very stiff, moist			SS	S4	460	16	16		
3					SS	S5	410	17	14		
	182.69	End of Borehole at 3.5 m bgs.									
4											
5											
6											
7											

NOTES

- 1) Borehole Log interpretation requires assistance by EXP before use by others and must be read in conjunction with EXP Report LON-21000835-A0.
- 2) Borehole open and dry upon completion of drilling.
- 3) bgs denotes below ground surface.

SAMPLE LEGEND

AS Auger Sample SS Split Spoon ST Shelby Tube
Rock Core (eg. BQ, NQ, etc.) VN Vane Sample

OTHER TESTS

G Specific Gravity C Consolidation
H Hydrometer CD Consolidated Drained Triaxial
S Sieve Analysis CU Consolidated Undrained Triaxial
γ Unit Weight UU Unconsolidated Undrained Triaxial
P Field Permeability UC Unconfined Compression
K Lab Permeability DS Direct Shear

WATER LEVELS

▽ Apparent ▼ Measured ▲ Artesian (see Notes)



BOREHOLE LOG

BH7

Sheet 1 of 1

CLIENT The Corporation of the City of WindsorPROJECT NO. LON-21000835-A0PROJECT Olive Road Sewer UpgradingDATUM GeodeticLOCATION Windsor, ONDATES: Completed February 15, 2021

Water Level _____

DEPTH (m bgs)	ELEVATION (~m)	STRATA DESCRIPTION	STRATA PLOT	WELL LOG	SAMPLES				MOISTURE CONTENT (%)	SHEAR STRENGTH	
					TYPE	NUMBER	RECOVERY (mm)	N VALUE (blows)		◆ S Field Vane Test (#=Sensitivity) ▲ Penetrometer ■ Torvane	Atterberg Limits and Moisture W _p W W _L ● SPT N Value 10 20 × Dynamic Cone 30 40
0	185.90	FILL - gravel, brown and grey, trace sand, 250 mm			AS	S1			22		○
	185.65	FILL - sand, brown, fine grained, loose, moist			SS	S2	280	9	11	●	○
1					SS	S3	380	18	16		○ ●
	184.38	SILTY CLAY TILL - brown becoming grey, some sand, trace gravel, very stiff to stiff, moist			SS	S4	410	14	11	○ ●	
2					SS	S5	430	14	12	○ ●	
3											
4		End of Borehole at 3.5 m bgs.									
5											
6											
7											

NOTES

- 1) Borehole Log interpretation requires assistance by EXP before use by others and must be read in conjunction with EXP Report LON-21000835-A0.
- 2) Borehole open and dry upon completion of drilling.
- 3) bgs denotes below ground surface.

SAMPLE LEGEND

- ▣ AS Auger Sample ▣ SS Split Spoon ■ ST Shelby Tube
▣ Rock Core (eg. BQ, NQ, etc.) ▣ VN Vane Sample

OTHER TESTS

- G Specific Gravity C Consolidation
H Hydrometer CD Consolidated Drained Triaxial
S Sieve Analysis CU Consolidated Undrained Triaxial
γ Unit Weight UU Unconsolidated Undrained Triaxial
P Field Permeability UC Unconfined Compression
K Lab Permeability DS Direct Shear

WATER LEVELS

- ▽ Apparent ▼ Measured ▲ Artesian (see Notes)

Appendix B – Limitations and Use of Report

LIMITATIONS AND USE OF REPORT

BASIS OF REPORT

This report ("Report") is based on site conditions known or inferred by the geotechnical investigation undertaken as of the date of the Report. Should changes occur which potentially impact the geotechnical condition of the site, or if construction is implemented more than one year following the date of the Report, the recommendations of EXP may require re-evaluation.

The Report is provided solely for the guidance of design engineers and on the assumption that the design will be in accordance with applicable codes and standards. Any changes in the design features which potentially impact the geotechnical analyses or issues concerning the geotechnical aspects of applicable codes and standards will necessitate a review of the design by EXP. Additional field work and reporting may also be required.

Where applicable, recommended field services are the minimum necessary to ascertain that construction is being carried out in general conformity with building code guidelines, generally accepted practices and EXP's recommendations. Any reduction in the level of services recommended will result in EXP providing qualified opinions regarding the adequacy of the work. EXP can assist design professionals or contractors retained by the Client to review applicable plans, drawings, and specifications as they relate to the Report or to conduct field reviews during construction.

Contractors contemplating work on the site are responsible for conducting an independent investigation and interpretation of the borehole results contained in the Report. The number of boreholes necessary to determine the localized underground conditions as they impact construction costs, techniques, sequencing, equipment and scheduling may be greater than those carried out for the purpose of the Report.

Classification and identification of soils, rocks, geological units, contaminant materials, building envelopment assessments, and engineering estimates are based on investigations performed in accordance with the standard of care set out below and require the exercise of judgment. As a result, even comprehensive sampling and testing programs implemented with the appropriate equipment by experienced personnel may fail to locate some conditions. All investigations or building envelope descriptions involve an inherent risk that some conditions will not be detected. All documents or records summarizing investigations are based on assumptions of what exists between the actual points sampled. Actual conditions may vary significantly between the points investigated. Some conditions are subject to change over time. The Report presents the conditions at the sampled points at the time of sampling. Where special concerns exist, or the Client has special considerations or requirements, these should be disclosed to EXP to allow for additional or special investigations to be undertaken not otherwise within the scope of investigation conducted for the purpose of the Report.

RELIANCE ON INFORMATION PROVIDED

The evaluation and conclusions contained in the Report are based on conditions in evidence at the time of site inspections and information provided to EXP by the Client and others. The Report has been prepared for the specific site, development, building, design or building assessment objectives and purpose as communicated by the Client. EXP has relied in good faith upon such representations, information and instructions and accepts no responsibility for any deficiency, misstatement or inaccuracy contained in the Report as a result of any misstatements, omissions, misrepresentation or fraudulent acts of persons providing information. Unless specifically stated otherwise, the applicability and reliability of the findings, recommendations, suggestions or opinions expressed in the Report are only valid to the extent that there has been no material alteration to or variation from any of the information provided to EXP.

STANDARD OF CARE

The Report has been prepared in a manner consistent with the degree of care and skill exercised by engineering consultants currently practicing under similar circumstances and locale. No other warranty, expressed or implied, is made. Unless specifically stated otherwise, the Report does not contain environmental consulting advice.

COMPLETE REPORT

All documents, records, data and files, whether electronic or otherwise, generated as part of this assignment form part of the Report. This material includes, but is not limited to, the terms of reference given to EXP by its client ("Client"), communications between EXP and the Client, other reports, proposals or documents prepared by EXP for the Client in connection with the site described in the Report. In order to properly understand the suggestions, recommendations and opinions expressed in the Report, reference must be made to the Report in its entirety. EXP is not responsible for use by any party of portions of the Report.

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REPORT FORMAT

Where EXP has submitted both electronic file and a hard copy of the Report, or any document forming part of the Report, only the signed and sealed hard copy shall be the original documents for record and working purposes. In the event of a dispute or discrepancy, the hard copy shall govern. Electronic files transmitted by EXP have utilized specific software and hardware systems. EXP makes no representation about the compatibility of these files with the Client's current or future software and hardware systems. Regardless of format, the documents described herein are EXP's instruments of professional service and shall not be altered without the written consent of EXP.

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Subject: Rezoning – 0 Spitfire Way – Z-020/25 ZNG/7318 – Ward 6

Reference:

Date to Council: November 3, 2025
Author: Adam Szymczak, MCIP, RPP
Senior Planner - Development
519-255-6543 x6250
aszymczak@citywindsor.ca

Planning & Building Services
Report Date: 9/29/2025
Clerk's File #: Z/15050

To: Mayor and Members of City Council

Recommendation:

1. THAT Zoning By-law 8600 **BE AMENDED** by deleting and replacing clause 439 in Section 20(1) as follows:
 439. **EAST SIDE OF LAUZON ROAD BETWEEN MCHUGH STREET AND THE VIA RAIL RAILWAY RIGHT-OF-WAY**
 1. For the lands comprising Parts 1 and 2, Plan 12R-29424 (Part of Block 42, Plan 12M-678; PIN 015962727 & 015962728), situated on the east side of Lauzon Road and the west side of Bowler Drive, north of Spitfire Way, the following additional provisions shall apply:
 - a) *Building Height: Main Building: maximum:*
 1. 56% of the *main building* footprint: *maximum* 31.0 m
 2. Remainder of *building* footprint: *maximum* 21.0 m
 - b) *Lot Area: minimum* 63.75 m² per *dwelling unit*
 - c) *Required Parking Spaces: minimum:* 1.24 for each *dwelling unit*
 - d) *Required Yard:* from Bowler Drive: *minimum* 23.0 m
 - e) *Landscaped Area: minimum* 31.0% of *lot area*
 - f) Section 24.26.5 shall not apply
 - g) Section 24.28.1.1 shall not apply
 - h) Section 24.40.20.3a) shall not apply

2. For the land comprising Part of Part 1, Plan 12-28651 (Part Lot 128, Concession 1 (McNiff's Survey); PIN 015962291), situated at the southeast corner of Lauzon Road and Spitfire Way the following additional provisions shall apply:

- a) A *Multiple Dwelling* shall be an additional permitted *main use*
- b) *Building Height: Main Building: maximum: 28.0 m*
- c) For a *Combined Use Building*, a *dwelling unit* shall be permitted on the ground floor
- d) A *Public Parking Area* may be designated exclusively for the *Multiple Dwellings* located on the lands described in Clause 1 above

[ZDM 14; ZNG/6590; ZNG/7318]

(B/L 78-2022 May 30/2022)

2. THAT Zoning By-law 8600 **BE FURTHER AMENDED** by changing the zoning of Part of Part 1, Plan 12-28651 (Part Lot 128, Concession 1 (McNiff's Survey); PIN 015962291), situated at the southeast corner of Lauzon Road and Spitfire Way (0 Spitfires Way; Roll No. 070-720-00107) by extending zoning exception 20(1)439 to include the subject parcel.
3. THAT, when Site Plan Control is applicable:
 - A. The following additional materials **BE SUBMITTED** with an application for site plan approval:
 - 1) A Noise Study to the satisfaction of the City Planner, Deputy City Planner, or Site Plan Approval Officer.
 - 2) A Vibration Study to the satisfaction of the City Planner, Deputy City Planner, or Site Plan Approval Officer.
 - 3) A Stage 1 Archaeological Assessment and any further recommended assessments are required to be entered into the Ontario Public Register of Archaeological Reports to the satisfaction of the City of Windsor and the Ontario Ministry of Citizenship and Multiculturalism, prior to any additional land disturbances. A final copy of these archaeological reports and the GIS study area must be submitted to the City of Windsor.
 - 4) Any other documents, materials, or studies identified in Appendix B to Report S 120/2025, to the satisfaction of the municipal department or external agency requesting them.
 - B. The Site Plan Approval Officer **BE DIRECTED** to incorporate the following into an approved site plan and an executed and registered site plan agreement:
 - 1) Any mitigation measures identified in a Noise Study and Vibration Study, subject to the satisfaction of the City Planner, Deputy City Planner, or Site Plan Approval Officer.
 - 2) Any requirements identified in Appendix B to Report S 120/2025, to the satisfaction of the municipal department or external agency requesting them.

C. The Site Plan Approval Officer **CONSIDER** the following items in an approved site plan and an executed and registered site plan agreement:

- 1) Provision of a controlled crossing for pedestrians at the intersection of Bowler Drive and Spitfire Way, to the satisfaction of the City Engineer.

Executive Summary:

N/A.

Background:

Application Information

Location: 0 Spitfire Way (Southeast corner of Lauzon Road & Spitfire Way); PIN 01596-2291; Roll No. 070-720-00107

Ward: 6 **Planning District:** Riverside **Zoning District Map:** 14

Applicant: Farhi Holdings Corporation (Jim Bujouves)

Owner: Farhi Holdings Corporation

Agent: Dillon Consulting Limited (Amy Farkas)

Proposal: The Applicant is requesting an amendment to Zoning By-law 8600 by adding a Zoning Exception to allow a Multiple Dwelling as an additional permitted use, to allow dwelling units on a ground floor in a combined use building, and allow a maximum building height of 28 m. The development proposal includes two buildings consisting of 6 storeys with a total of 122 dwelling units, 180 m² of commercial space, and a total of 192 parking spaces. The development will also include a controlled-access satellite parking area for Multiple Dwellings to the north (identified as Buildings E and F) with a total of 181 satellite parking spaces.

Submissions: All documents are available [online](#) or aszymczak@citywindsor.ca.

Attached to Report S 120/2025 as an Appendix: Conceptual Site Plan (Appendix A)

Not attached to Report S 120/2025 but available [online](#):

Zoning By-law Amendment Application; Cover Letter; Planning Justification Report (PJR); Servicing Strategy / Memo; Transportation Impact Study

Site Information:

OFFICIAL PLAN	ZONING	CURRENT USE	PREVIOUS USE
Mixed Use Corridor	Commercial District 2.2 (CD2.2)	Vacant Land	Unknown
LOT FRONTAGE LAUZON ROAD	LOT FRONTAGE SPITFIRE WAY	LOT AREA	LOT SHAPE
~100 m	~ 164 m	1.98 hectares	Irregular

Neighbourhood: Section 2.0 in the PJR provides an overview of the site and the surrounding land uses. There are Multiple Dwellings along Lauzon Road and McHugh Street. A recently constructed subdivision consisting of single unit dwellings is located to the north.

The WFCU Centre located to the east, is a multipurpose recreation, community centre, and entertainment complex. To the south and southwest is a major commercial area, consisting of Tecumseh Mall, big-box retail, and a mix of other retailers, restaurants, gas stations and commercial uses. Numerous parks and recreational trails are located in the surrounding area.

Nearby schools include Riverside Secondary School, St. Joseph's Catholic High School, Forest Glade Public School, Hetherington Public School, St. John Vianney Catholic Elementary School, and St. Rose Catholic Elementary School.

Lauzon Road and McHugh Street are classified as a Class II Arterial Road with a four-lane cross section, no on-street parking and sidewalks/multi-use trails. Spitfire Way is a Local Road with a two lane cross-section, a sidewalk on the north side, and a multi-use trail on the south side. Nearby Lauzon Parkway provides access to the EC Row Expressway.

The closest transit route is the Lauzon 10. The Lauzon 10 has an existing weekday peak frequency of 35 minutes. Per Transit Windsor's 2023 Service Plan, the Lauzon 10 is being replaced with two local routes. Both routes will have peak weekday frequencies of 30 minutes and will operate with two way conventional service loop bringing an improvement in transit service to this area. These routes are proposed to be implemented in conjunction with the completion of Transit Windsor's new East End Terminal in 2026.

The closest existing bus stop is adjacent to this property on Spitfires Way at Bowler southeast corner providing direct transit access to this development. Transit Windsor has no plans to relocate this bus stop, and this will remain unchanged with the new local route.

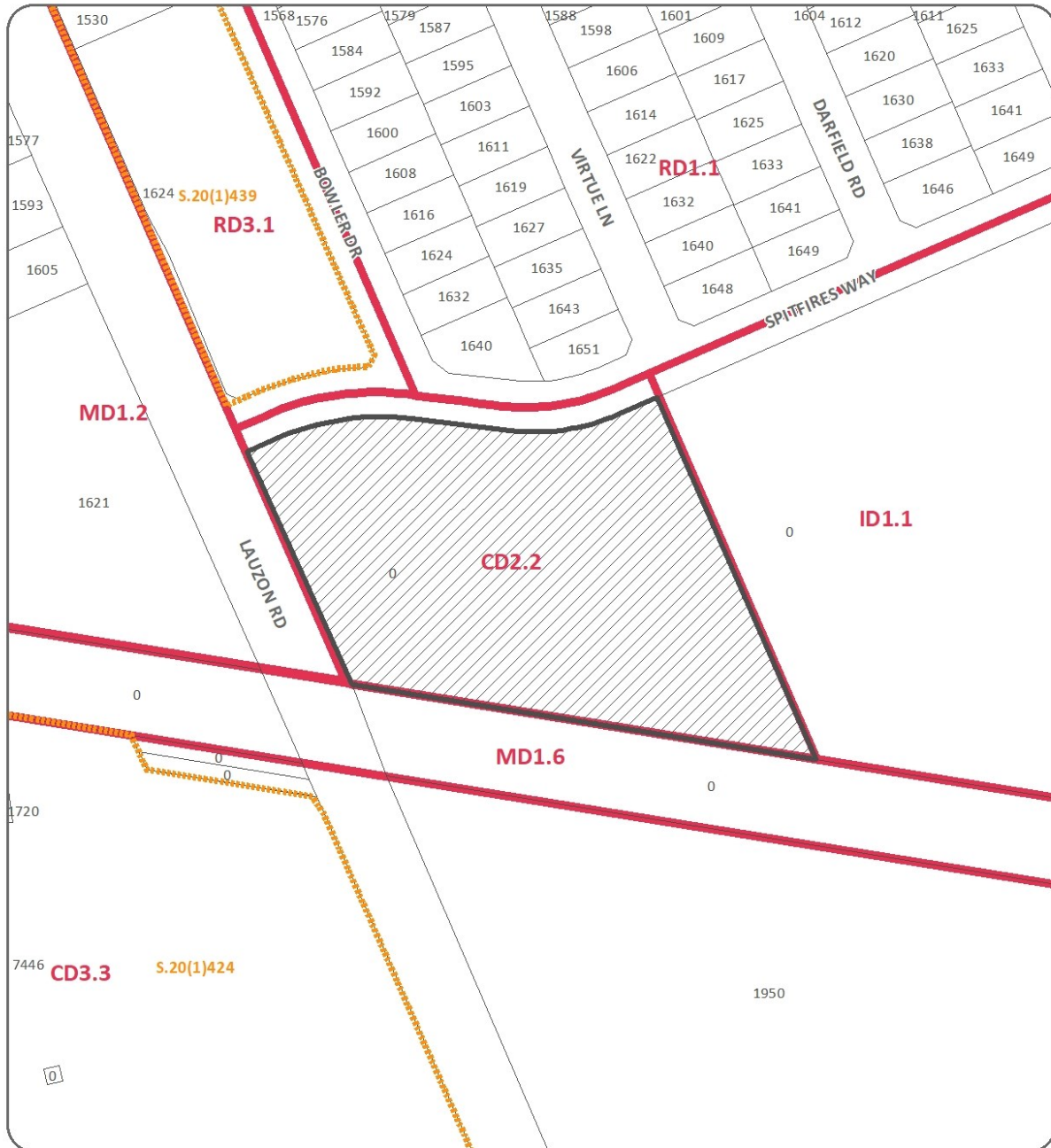
Figure 1: Key Map



KEY MAP - Z-020/25 ZNG/7318

● SUBJECT LANDS

Figure 2: Subject Parcel - Rezoning



PART OF ZONING DISTRICT MAP 14

N.T.S.

REQUESTED ZONING AMENDMENT

Applicant: Farhi Holdings Corp.



SUBJECT LANDS

PLANNING & BUILDING DEPARTMENT



DATE : SEPTEMBER, 2025
FILE NO. : Z-020/25, ZNG/7318

Figure 3: Neighborhood Map



NEIGHBOURHOOD MAP - Z-020/25, ZNG/7318



SUBJECT LANDS

Discussion:

Planning Justification Report (Counterpoint Land Development by Dillon Consulting, July 2025)

Section 3.5 in the Planning Justification Report (PJR) provides a comprehensive planning analysis and considerations of the proposed development and Zoning By-law Amendment application. The report notes that a Noise and Vibration study is currently underway and will be submitted during the Site Plan Control process.

The report concludes that the *“proposed mixed-use development is consistent with the intent of the Provincial Planning Statement and the City of Windsor’s Official Plan”*. It notes that the development is a compact form of residential infill, will make efficient use of existing services, and offer an alternative form of housing in the area.

The PJR states that the development *“will add to the vitality of the area”*, supports the *“efficient use of land and existing infrastructure”*, and is *“compatible with the surrounding land uses”*, and that the site is *“physically suitable and strategically located”*.

City of Windsor Planning and Development Services concurs with the analysis, summary, and conclusions in the PJR.

Servicing Strategy / Memo (Counterpoint Land Development by Dillon Consulting, 23 July 2025)

The Servicing Strategy states that the *“existing sanitary sewers have adequate capacity to accommodate flows from the proposed development”*. The existing stormwater pond *“has adequate capacity to accommodate the runoff from the subject site. The required on-site stormwater storage can be achieved through the parking (surface storage), underground storage, and flow control devices”*.

At the request of City of Windsor – Engineering – Development, the Applicant submitted a revised servicing memo that justifies the use of two persons per dwelling unit. City of Windsor Engineering deems the Servicing Strategy as acceptable. Stormwater management will be finalized during Site Plan Control.

Transportation Impact Study (Counterpoint Land Development by Dillon Consulting, August 2025)

The Transportation Impact Study (TIS) states that:

“The proposed development is anticipated to generate 128 vehicle trips (58 inbound, 70 outbound) during the AM peak hour, 96 vehicle trips (54 inbound, 42 outbound) during the PM peak hour, 111 vehicle trips (56 inbound, 55 outbound) during the Saturday peak hour, and 116 (63 inbound, 53 outbound) during the Sunday peak hour.” (Section 7.0, page 21)

The Lauzon Road and Spitfire Way intersection is expected to operate at a “good overall level of service” during all parking hours. No concerns were noted. City of Windsor Transportation Planning has reviewed the TIS and has no comments or concerns.

Provincial Planning Statement 2024 (PPS 2024):

The PPS 2024 provides policy direction on matters of provincial interest related to land use planning in Ontario. All advice, comments, submissions, and municipal decisions shall be consistent with the PPS 2024.

Section 3.1 in the PJR suggests that the listed *"policies of the Provincial Planning Statement are supportive of the Zoning By-law Amendment application"*. The PJR concludes *"that the proposed development is consistent with the ... policies of the Provincial Planning Statement"*. Section 3.5 in the PJR provides a comprehensive planning analysis.

City of Windsor Planning and Development Services concurs with the PPS 2024 analysis in the PJR. The proposed amendment to Zoning By-law 8600 is consistent with the PPS 2024.

Official Plan (OP):

The parcel has a designation of "Mixed Use Corridor" on Schedule D: Land Use in the Official Plan. The PJR notes that per Section 6.5.3.3, the *"Mixed-Use Corridor land use designation does not typically permit building height above four (4)-storeys, however, may be permitted at the discretion of Council and established through a site-specific Zoning By-law Amendment"*. Section 3.5.2.4 in the PJR states that the *"height is appropriate to buffer the single-detached dwelling neighbourhood from the existing VIA Rail Line and is compatible with the massing of other developments within the subdivision including 3565 McHugh Street, 1530 Lauzon Road and 1624 Lauzon Road"*.

Section 3.5 in the PJR provides a comprehensive planning analysis.

City of Windsor Planning and Development Services concurs with the analysis in the PJR. The proposed amendment to Zoning By-law 8600 conforms to the general policy direction of the OP.

Zoning By-Law (ZBL):

The existing Commercial District 2.2 (CD2.2) zoning permits a range of commercial uses and dwelling units in a Combined Use Building. The Applicant is maintaining the CD2.2 zoning district.

The zoning exception – S.20(1)439 – for the lands to the north (1530 and 1624 Lauzon Road) containing the Multiple Dwellings, will be extended to include the subject lands (Recommendations 1 and 2). This is appropriate as the subject lands will contain off-site parking for those Multiple Dwellings.

Clause 1 of proposed S.20(1)439 applies to the lands at 1530 and 1624 Lauzon Road. No provisions have been changed or revised. Only the formatting has changed.

Clause 2 applies to the subject land at the southeast corner of Lauzon Road and Spitfires Way. Sub clause a) permits a Multiple Dwelling as an additional permitted main use, giving the Applicant flexibility in terms of the provision of commercial space.

Sub clause b) increases the maximum building height of the main building from 14.0 m to 28.0 m. The Applicant is proposing two buildings with a total of six storeys. This is consistent with the Multiple Dwellings to the north and at the southeast corner of McHugh Street and Darfield Road.

Sub clause c) states that, for a Combined Use Building, dwelling units may be permitted on the ground floor. This will give the Applicant flexibility in the design of the buildings.

Sub clause d) clarifies that a public parking area may be designated exclusively for the Multiple Dwellings located to the north at 1530 and 1624 Lauzon Road.

These exceptions will have no adverse impact on adjacent uses. The proposed development must comply with all other zoning provisions. The existing CD2.2 zoning and zoning exception 20(1)439 are consistent with the PPS 2024, conform to the general policy direction of the OP, and are appropriate for the proposed development.

Site Plan Control (SPC):

The development as proposed is subject to Site Plan Control. Recommendation 3 requires the submission of various documents, materials and studies, all to the satisfaction of the appropriate municipal department or external agency, that any mitigation measures or specific requirements in those documents be incorporated into an approved site plan and site plan agreement, and that the Site Plan Approval Officer consider matters such as a potential crosswalk at Bowler Drive or any other requests or recommendations from municipal departments or external agencies.

Risk Analysis:

N/A

Climate Change Risks

Climate Change Mitigation:

The proposed development implements Environmental Master Plan Objective C1: Encourage in-fill and higher density in existing built areas. Residential infill intensification minimizes the impact on community greenhouse gas emissions as these developments create complete communities and neighbourhoods while using available infrastructure such as sewers and public transit.

Climate Change Adaptation:

The proposed development offers an opportunity to increase resiliency for the development and surrounding area by supporting a complementary and compact form of housing and intensification that is near existing and future transit, active transportation, and community service options. New construction is required to meet the current provisions of the Building Code, which will be implemented through the building permit process. Incorporation of storm water management best practices is required.

Financial Matters:

N/A

Consultations:

An open house was held on September 8, 2023, from 6 to 8 pm at the WFCU Centre. An Engagement Summary is provided in Appendix A of the Applicant's Planning Justification Report (PJR). See Appendix B of this report for comments from municipal departments and external agencies. Per the Planning Act, statutory notice was advertised in the Windsor Star, a local newspaper. A courtesy notice was delivered to property owners within 200 m of the subject parcel.

Conclusion:

Based on the documents submitted by the Applicant and the analysis in this report, it is my opinion that the recommended amendment to Zoning By-law 8600 is consistent with the PPS 2024 and is in conformity with the Official Plan.

The proposed amendment will permit a multiple dwelling that is compatible with existing and permitted uses in the surrounding area, represents an appropriate increase in density, provides an opportunity for the construction of modern housing stock, and supports a complementary form of housing located near various transportation options. The recommendation to amend Zoning By-law 8600 constitutes good planning.

Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Greg Atkinson, MCIP, RPP
Deputy City Planner - Development

Neil Robertson, MCIP, RPP
City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP

RM

Approvals:

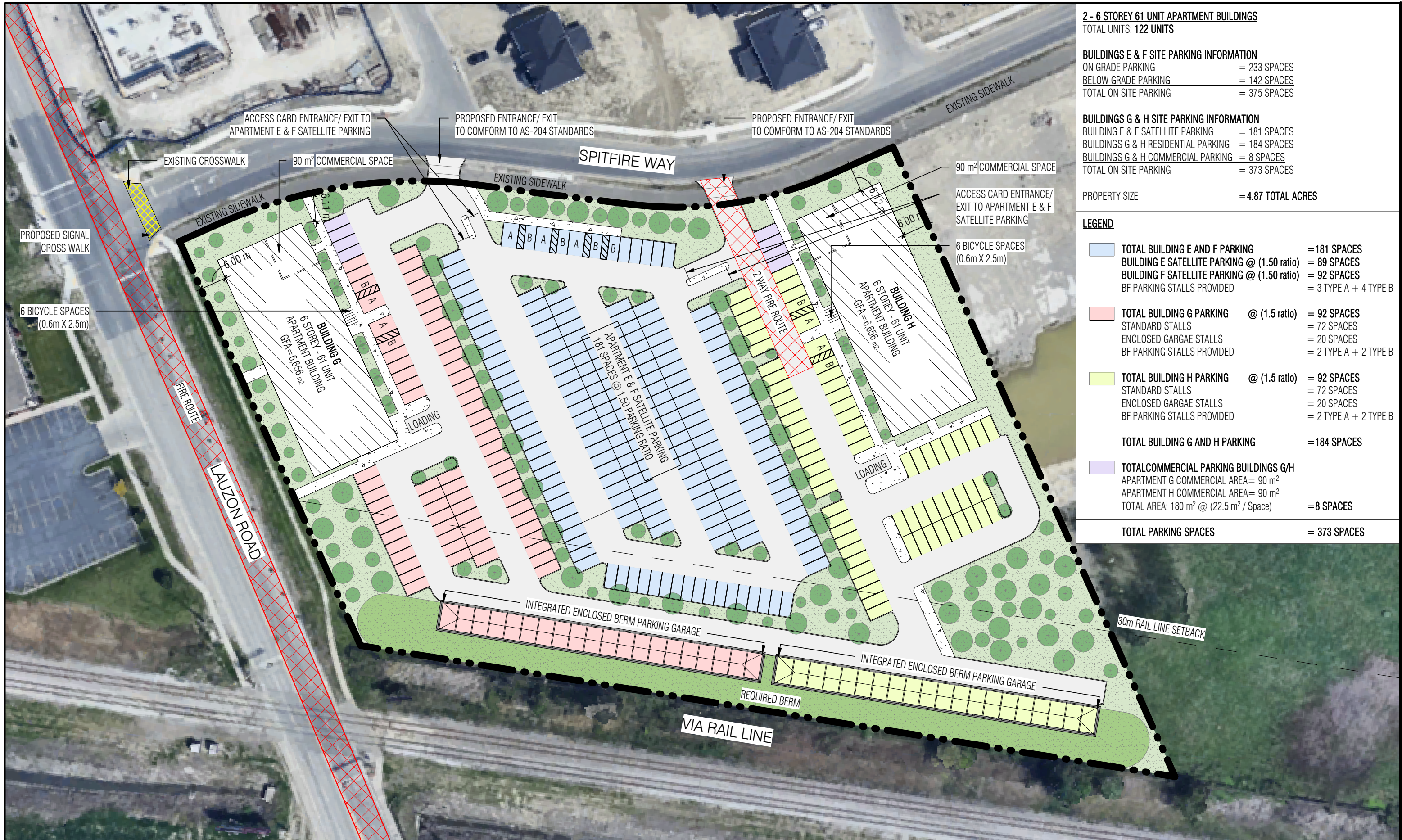
Name	Title
Greg Atkinson	Deputy City Planner - Development
Aaron Farough	Senior Legal Counsel, Legal & Real Estate
Neil Robertson	City Planner
Jelena Payne	Deputy CAO/Commissioner, Economic Development
Ray Mensour	Chief Administrative Officer

Notifications:

Name	Address	Email

Appendices:

- 1 Appendix A - Conceptual Site Plan
- 2 Appendix B - Consultations



2 - 6 STOREY 61 UNIT APARTMENT BUILDINGS			
TOTAL UNITS: 122 UNITS			
BUILDINGS E & F SITE PARKING INFORMATION			
ON GRADE PARKING	=	233 SPACES	
BELOW GRADE PARKING	=	142 SPACES	
TOTAL ON SITE PARKING	=	375 SPACES	
BUILDINGS G & H SITE PARKING INFORMATION			
BUILDING E & F SATELLITE PARKING	=	181 SPACES	
BUILDINGS G & H RESIDENTIAL PARKING	=	184 SPACES	
BUILDINGS G & H COMMERCIAL PARKING	=	8 SPACES	
TOTAL ON SITE PARKING	=	373 SPACES	
PROPERTY SIZE	=	4.87 TOTAL ACRES	
LEGEND			
	TOTAL BUILDING E AND F PARKING	=	181 SPACES
	BUILDING E SATELLITE PARKING @ (1.50 ratio)	=	89 SPACES
	BUILDING F SATELLITE PARKING @ (1.50 ratio)	=	92 SPACES
	BF PARKING STALLS PROVIDED	=	3 TYPE A + 4 TYPE B
	TOTAL BUILDING G PARKING @ (1.5 ratio)	=	92 SPACES
	STANDARD STALLS	=	72 SPACES
	ENCLOSED GARGAE STALLS	=	20 SPACES
	BF PARKING STALLS PROVIDED	=	2 TYPE A + 2 TYPE B
	TOTAL BUILDING H PARKING @ (1.5 ratio)	=	92 SPACES
	STANDARD STALLS	=	72 SPACES
	ENCLOSED GARGAE STALLS	=	20 SPACES
	BF PARKING STALLS PROVIDED	=	2 TYPE A + 2 TYPE B
	TOTAL BUILDING G AND H PARKING	=	184 SPACES
	TOTAL COMMERCIAL PARKING BUILDINGS G/H		
	APARTMENT G COMMERCIAL AREA= 90 m ²		
	APARTMENT H COMMERCIAL AREA= 90 m ²		
	TOTAL AREA: 180 m ² @ (22.5 m ² / Space)	=	8 SPACES
	TOTAL PARKING SPACES	=	373 SPACES

Z-020/25 ZNG/7318 – APPENDIX B –CONSULTATIONS

CANADA POST – BRUNO DESANDO

This development falls within our centralized mail policy.

Canada Post's multi-unit policy, which requires that the owner/developer provide the centralized mail facility (front loading lockbox assembly or rear-loading mailroom [mandatory for 100 units or more]), at their own expense, will be in effect for buildings and complexes with a common lobby, common indoor or sheltered space.

Should the description of the project change, Canada Post requires an update to assess the impact of the change on mail service.

Lock-Box Assembly Requirements - The Canada Post Standards Manual for Builders & Developers can be downloaded at:

https://www.canadapost.ca/cpo/mc/assets/pdf/business/standardsmanual_en.pdf

Compartments Size - Horizontal lock-box models used in mailrooms must have the following minimums:

- Residential compartments must be at least 12.5 x 13.5 cm
- Commercial compartments at least 13.5 x 30.5 cm
- Parcel compartments at least 30.5 x 30.5 cm

Vertical lock-box models must have min comp size of 25 x 12.5 cm. (Most models are 40 x 12.7 cm)

Heights - All lock-box assemblies must be installed in a manner that will not require the delivery employee to reach higher than 170cm or lower than 45cm when delivering to the equipment. With respect to horizontal lock-boxes, the limits above will likely mean that maximum number of compartments that can be included in each column of residential compartments would be eight

Rear-loading Lock-boxes - Projects with more than 100 units are required to be serviced via a rear-loading lock-box assembly. There must be a width of at least 100cm of working space from the back of the boxes to the wall.

A ledge under the bottom row of boxes is also recommended in rear-loading designs. This ledge is to be directly under the bottom row of boxes (no space between ledge and bottom of boxes) and must stick out at least 20cm from the back of the boxes.

Mailroom door is required to provide a minimum 81cm opening. Lighting should be at least 100 lux (measured 75 cm from floor).

Access - All buildings where the lock-boxes are required to be serviced from inside the building are required to install a Canada Post Crown lock in the building intercom. The intercom is pre-fabricated with an internal housing for the lock. The lock can be obtained from the local delivery supervisor.

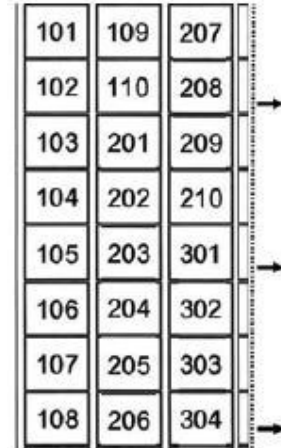
If the building has more than 100 units, a rear-loading lock-box assembly will be installed. The door to the Canada Post delivery area must be fitted with a specific model of deadbolt. This is because Canada Post will supply a key cylinder made specifically for the Canada Post key that will fit inside the deadbolt purchased by the developer.

Z-020/25 ZNG/7318 – APPENDIX B –CONSULTATIONS

Numbering

Compartments should be numbered vertically and left to right on the delivery side of the boxes

101	109	207
102	110	208
103	201	209
104	202	210
105	203	301
106	204	302
107	205	303
108	206	304



Grade-level Components - If the development includes grade level retail or residential units, please take note that door-to-door delivery will not be provided to these units. Canada Post will install a Community Mailbox to provide service to these units. Please coordinate a location with the Canada Post Delivery Planner for the area. If there is no room on the property for the Community Mailbox, service can be provided via another Community Mailbox in the area. Options to service the units from the tower (lobby) lock-boxes or via a front-loading lock box erected on the outside of the building can also be discussed with the Delivery Planner.

CITY OF WINDSOR: ENGINEERING: DEVELOPMENT: ROB PERISSINOTTI

Sewer: The site is serviced by a 300 mm sanitary sewer and a 750 mm storm sewer located within the Spitfire Way right-of-way. If possible existing connections should be utilized. Any redundant connections shall be abandoned in accordance with the City of Windsor Engineering Best Practice B.P 1.3.3.

A sanitary sampling manhole may be required on any new sanitary connection at the property line to the satisfaction of the City Engineer, if one does not already exist.

The applicant will be required to submit, prior to the issuance of permits, a stormwater management plan in accordance with Windsor Essex Region Stormwater Management Standards Manual, restricting stormwater runoff to predevelopment levels. This will include, at a minimum:

- Submission of stormwater management review fee,
- Stormwater management report stamped by a professional engineer
- Site servicing drawings stamped by a professional engineer
- Stormwater management check list (see link below)

For more information of SWM requirements:

<https://essexregionconservation.ca/wp-content/uploads/2018/12/WE-Region-SWM-Standards-Manual.pdf>

<https://www.citywindsor.ca/business/buildersanddevelopers/Documents/Checklist-Rational-Method.pdf>

Z-020/25 ZNG/7318 – APPENDIX B –CONSULTATIONS

Servicing Study: The owner submitted a revised servicing study, completed by Dillon Consulting dated July 23, 2025. The study has been reviewed, and we have the following comments:

- Provide justification for the estimated number of persons per residential unit (2 ppl/unit).

Corner Cut-Off: The owner(s) agrees, prior to the issuance of a construction permit, to gratuitously convey a 6m x 6m (20' x 20') corner cut-off at the intersection of Lauzon Rd and Spitfire Way in accordance with City of Windsor Standard Drawing AS-230.

Contact: Rob Perissinotti, Development Engineer, rperissinotti@citywindsor.ca

CITY OF WINDSOR: ENGINEERING: RIGHT-OF-WAY: MARK SCHAFFHAUSER

Required Drawing Revisions:

1. **Driveway Approaches** – Do not conform to City of Windsor Standards, which must be constructed with straight flares and no raised curbs within the right-of-way.
 - o Modify as per Standard Engineering Drawing AS-204.
2. **Sewer Connections** – All existing and proposed storm, sanitary and water services must be identified on the drawings, as well as the associated mainline sewers/water mains.
 - o Modify drawings to include all sewer connections and water services.
 - o Identify any redundant connections to be abandoned in accordance with Engineering Best Practice BP1.3.3.
3. **Corner Cut-off** – A 6m x 6m (20' x 20') corner cut-off at the intersection of Lauzon Road and Spitfire Way is not shown in the current submission.
 - o Modify drawings to include corner cut-off as per Standard Engineering Drawing AS-230.

Special Provisions to be included in Site Plan Control Agreement:

The following special provisions will be required prior to submitting a building permit application:

Site Plan Control Agreement – The applicant enter into an agreement with the City of Windsor for all requirements under the General Provisions of the Site Plan Control Agreement for the Engineering Department.

Corner Cut-Off – The owner(s) agrees, prior to the issuance of a construction permit, to gratuitously convey a 6m x 6m (20' x 20') corner cut-off at the intersection of Lauzon Rd. and Spitfire Way in accordance with City of Windsor Standard Drawing AS-230.

Contact: Dan Perissinotti, Technologist I, dperissinotti@citywindsor.ca

Z-020/25 ZNG/7318 – APPENDIX B –CONSULTATIONS

CITY OF WINDSOR: PLANNING & DEVELOPMENT SERVICES: HERITAGE PLANNER: TRACY TANG

Heritage: There is no apparent built concern with this property.

Archaeology: The parcel is located within the Archaeological Potential Zone (as per the recently adopted Windsor Archaeological Management Plan, 2024; OPA 181; and updated 2024 archaeological potential model OP Volume I Schedule C-1).

A Stage 1 archaeological assessment and any further recommended assessments are required to be entered into the Ontario Public Register of Archaeological Reports to the satisfaction of the City of Windsor and the Ontario Ministry of Citizenship and Multiculturalism, prior to any additional land disturbances. A final copy of these relevant archaeological reports and GIS study area must be submitted to the City of Windsor.

CITY OF WINDSOR: PLANNING & DEVELOPMENT SERVICES: LANDSCAPE ARCHITECT: RYAN UPTON

There are no objections or documents required from a Landscape Architecture perspective as it pertains to a Zoning By-law Amendment (ZBA). For circulation purposes, these comments will capture Forestry's concerns. At the Site Plan Control stage, the Applicant will be required to submit the following documents:

- 1) **Tree Inventory & Preservation Study (TIPP)** – The purpose of a TIPP is to investigate existing trees, within and adjacent to a development proposal and to determine how protection and enhancement can coincide with proposed development. The TIPP shall capture all live trees over 10cm DBH within the proposed development area, both city and privately owned. This inventory should include trees on adjacent lots in proximity (5 m) to the property lines. See the TIPP Terms of Reference (TOR) for more information.
- 2) **Landscape Plan** – The purpose of a Landscape Plan is to determine landscape features proposed as part of the development. This Plan must be prepared by an OALA Landscape Architect, illustrating the tree preservation fencing location and details on the plans. See Site Plan Control – Application Support Material TOR for more information.
- 3) **Photometric Plan and Light Fixture Data Sheets** – These must be prepared by a qualified Lighting Engineer or Consultant. See Site Plan Control – Application Support Material TOR for more information.

CITY OF WINDSOR: PLANNING & DEVELOPMENT SERVICES: SITE PLAN CONTROL

This development proposal is subject to Site Plan Control under the provisions of the Planning Act and City of Windsor By-law 1-2004. Where prior planning approvals are required for Official Plan or Zoning By-law Amendments, applicants may submit a request for Site Plan Control Pre-Consultation (Stage 1) following the conclusion of the Development & Heritage Standing Committee meeting. Requests can be made through the City's online application portal at <https://ca.cloudpermit.com/login>.

Z-020/25 ZNG/7318 – APPENDIX B –CONSULTATIONS

CITY OF WINDSOR: PLANNING & DEVELOPMENT SERVICES: ZONING COORDINATOR: SAM PERRY

Proposed Zoning: Commercial District 2.2 (CD2.2) with a Zoning Exception

Section 5 - General Provisions

- Exceptions to maximum building height provisions: **To Be Determined (TDB)**

Section 15.2.5 - Commercial District 2.2

- Building height: maximum: **Does not comply**
 - Required: 14.0 m; Provided: 28.0 m
- Amenity Area - Per Dwelling unit minimum: Required: 12.0 m²; Provided: **TBD**
- Combine use Building Dwelling units entrance locations: **Does not comply**
 - Required: Above non-residential units; Provided: On Ground Floor

Section 24 - Parking, Loading and Stacking Provisions

- Required number of Visiting parking :**Does not comply**
 - Required: 12; Provided: 0
- Curb Cut or ramp for accessible parking spaces: **Does not comply**
- For all dwellings or dwelling units in a combined use building, all required parking spaces, visitor spaces and accessible parking shall be located on the same lot as the dwellings or dwelling units they are intended to serve. **Does not comply**

Section 25 - Parking Area Provisions

- Access Area: **Does not comply** - Required: 7.0 m to 9.0 m; Provided: 6.0 m

For a full comprehensive zoning review to be completed the following drawings are required to be provided: Floor plans; Elevations

CITY OF WINDSOR: TRANSPORTATION PLANNING:

- **Land Conveyance:** Not Applicable.
- **Corner Cut-Off:** Not Applicable.
- **Sidewalk:** Not Applicable.
- **Parking:** Please be advised that the regulations governing parking areas, including the required number of regular and accessible parking spaces, loading zones, and bicycle parking, are outlined in Zoning By-law 8600. These requirements are reviewed by Planning & Development Services as part of the development approval process and must be satisfied to obtain site plan approval

In cases where any of these parking requirements are not met, Transportation Planning will require a formal justification. This may include, but is not limited to, a parking study, turning movement analysis, or turning radius template overlays. This documentation is necessary to consider any deviation from the by-law's requirements.

Z-020/25 ZNG/7318 – APPENDIX B –CONSULTATIONS

- **Transportation Impact Study:** We have received and reviewed the Transportation Impact Study, File No. 23-6213, prepared by Dillion Consulting and dated August 2025 (hereinafter referred to as the “Study”). We have no comments or concerns regarding the contents of the Study.
- **Access:** All accesses shall conform to the TAC Geometric Design Guide for Canadian Roads and the City of Windsor Standard Engineering Drawings.
- **Exterior Path:** All new exterior paths of travel must meet the requirements of the Accessibility for Ontarians with Disabilities Act (AODA).
- **Other Comments:** Not Applicable.

Contact: Ellie MehriLou, Supervisor, Corridor Maintenance, EMehrilou@citywindsor.ca.

CITY OF WINDSOR: TRANSIT WINDSOR: JASON SCOTT

Transit Windsor has no objections to this development. The closest existing transit route is the Lauzon 10. The Lauzon 10 has an existing weekday peak frequency of 35 minutes.

Per Transit Windsor’s 2023 Service Plan, the Lauzon 10 is being replaced with two local routes. Both routes will have peak weekday frequencies of 30 minutes and will operate with two way conventional service versus the existing one way loop bringing an improvement in transit service to this area. These routes are proposed to be implemented in conjunction with the completion of Transit Windsor’s new East End Terminal in 2026.

The closest existing bus stop is adjacent to this property on Spitfires Way at Bowler southeast corner providing direct transit access to this development. Transit Windsor has no plans to relocate this bus stop, and this will remain unchanged with the new local route. If the bus stop needs to temporarily be closed for construction, Transit Windsor requires a minimum one week notice.

Z-020/25 ZNG/7318 – APPENDIX B –CONSULTATIONS

ENWIN: HYDRO ENGINEERING: Steve Zambito

No Objection provided adequate clearances are achieved and maintained. ENWIN has existing overhead pole lines along the north limits with 120/240 volt streetlight distribution.

The developer will be responsible to cover all costs associated with any relocation or removal of ENWIN poles, conductors, and down guys. Prior to working in these areas, we would suggest notifying your contractor and referring to the Occupational Health and Safety Act and Regulations for Construction Projects to confirm clearance requirements during construction. We recommend referring to the Ontario Building Code for permanent required clearances for New Building Construction.

ENWIN: WATER ENGINEERING: Bruce Ogg

ENWIN Water has no objections.

ESSEX REGION CONSERVATION AUTHORITY: Alicia Good

We request that the City of Windsor collect the fee of \$400.00 for this development per line item 30 of the ERCA Board Approved 2025 Fee Schedule.

Natural Hazards And Regulatory Responsibilities Under The Conservation Authorities Act. O. Reg 686/21

The following comments reflect ERCA's role in protecting people and property from the threats of natural hazards and regulating development hazards lands under Section 28 of the Conservation Authorities Act.

The above noted lands are subject to our Regulation under the Conservation Authorities Act (Ontario Regulation No. 41/24). The parcel falls within the regulated area of the Detroit River. If any new construction is proposed, the property owner will be required to obtain an approval from the Essex Region Conservation Authority prior to any construction or site alteration or other activities affected by Section 28 of the Conservation Authorities Act.

Please note that the subject property may be located wholly or partially within the Event-Based Area (EBA) identified in the Essex Region Source Protection Plan, which came into effect on October 1, 2015. This Plan was established to safeguard municipal drinking water sources throughout the Essex Region. As such, new development proposals within the EBA may require review and approval by the Essex Region Risk Management Official (RMO) to ensure that any potential threats to drinking water are appropriately mitigated.

If your proposal includes the installation of fuel storage on-site, it is strongly recommended that you contact the RMO to confirm that the handling and storage of fuel will not pose a significant risk to local municipal drinking water sources.

The RMO can be reached at riskmanagement@erca.org. If a Risk Management Plan has previously been negotiated on this property, it will be the responsibility of the new owner to contact the Essex Region Risk Management Official to establish an updated Risk Management Plan. For questions regarding Source Water Protection and the applicable source protection plan policies that may apply, please contact the Essex Region Risk Management Official.

Z-020/25 ZNG/7318 – APPENDIX B –CONSULTATIONS

Final Recommendation: Our office has no objection to Z 020-25 ZNG 7138. As noted above, the property owner will be required to obtain an approval from the Essex Region Conservation Authority prior to any construction or site alteration or other activities affected by Section 28 of the Conservation Authorities Act. Please continue to circulate our office with future Planning Act applications for this site, such as Application for Site Plan Control, as we may have further comments to provide regarding stormwater management or other natural hazard matters.

**Subject: Zoning By-Law Amendment - Z 023/25 (ZNG-7327) 3858-3868
Peter St. - Ward 2**

Reference:

Date to Council: November 3, 2025
Author: Averil Parent
Planner II - Development Review
519-255-6543 x6397
aparent@citywindsor.ca
Planning & Building Services
Report Date: 9/29/2025
Clerk's File #: Z/15051

To: Mayor and Members of City Council

Recommendation:

- I. THAT Zoning By-law 8600 **BE AMENDED** by adding the following zoning exception to Section 91.10:

**10. NORTH SIDE OF PETER STREET BETWEEN CHAPPELL AVENUE AND
HILL AVENUE**

For the lands comprising of Part of Lots 27 & 28, Registered Plan 40 (PIN 01258-0188 LT), the following additional provisions shall apply:

- a) *Building Height: maximum 9.0 m*
- b) *Side Yard Width: minimum 1.20 m*
- c) Section 5.67.1 shall not apply
- d) Section 24.26.5 shall not apply
- e) A screening fence with a minimum height of 1.8 m shall be provided abutting the west lot line (abutting 3872 Peter St.)

[ZDM 4; ZNG/7327]

- II. THAT Zoning By-law 8600 **BE FURTHER AMENDED** by changing the zoning of Part of Lots 27 & 28, Registered Plan 40 (PIN 01258-0188 LT), known municipally as 3858 Peter Street (Roll No. 050-170-14800), situated on the north side of Peter Street between Chappell Avenue and Hill Avenue, from DRD1.1 to MD1.2x(10).

Executive Summary:

N/A

Background:**Application Information:****Municipal Address:** 3858-3868 Peter Street**Roll Number:** 050-170-14800 and 050-170-1480-0030**Ward:** 2 **Planning District:** Sandwich **Zoning District Map:** 4**Applicant/Agent:** Tracey Pillon-Abbs (Pillon-Abbs Inc.)**Owner:** CK Mechanical Heating & Cooling (Jason Curtis)**Proposal:**

The applicant is requesting to rezone the subject property from the existing Development Reserve District 1.1 (DRD1.1) to Manufacturing District 1.2 (MD1.2) to construct a one-storey warehouse on the subject land. The warehouse is proposed to have a GFA of 637 m² and to be used for storage. Nine parking spaces are proposed in front of the building.

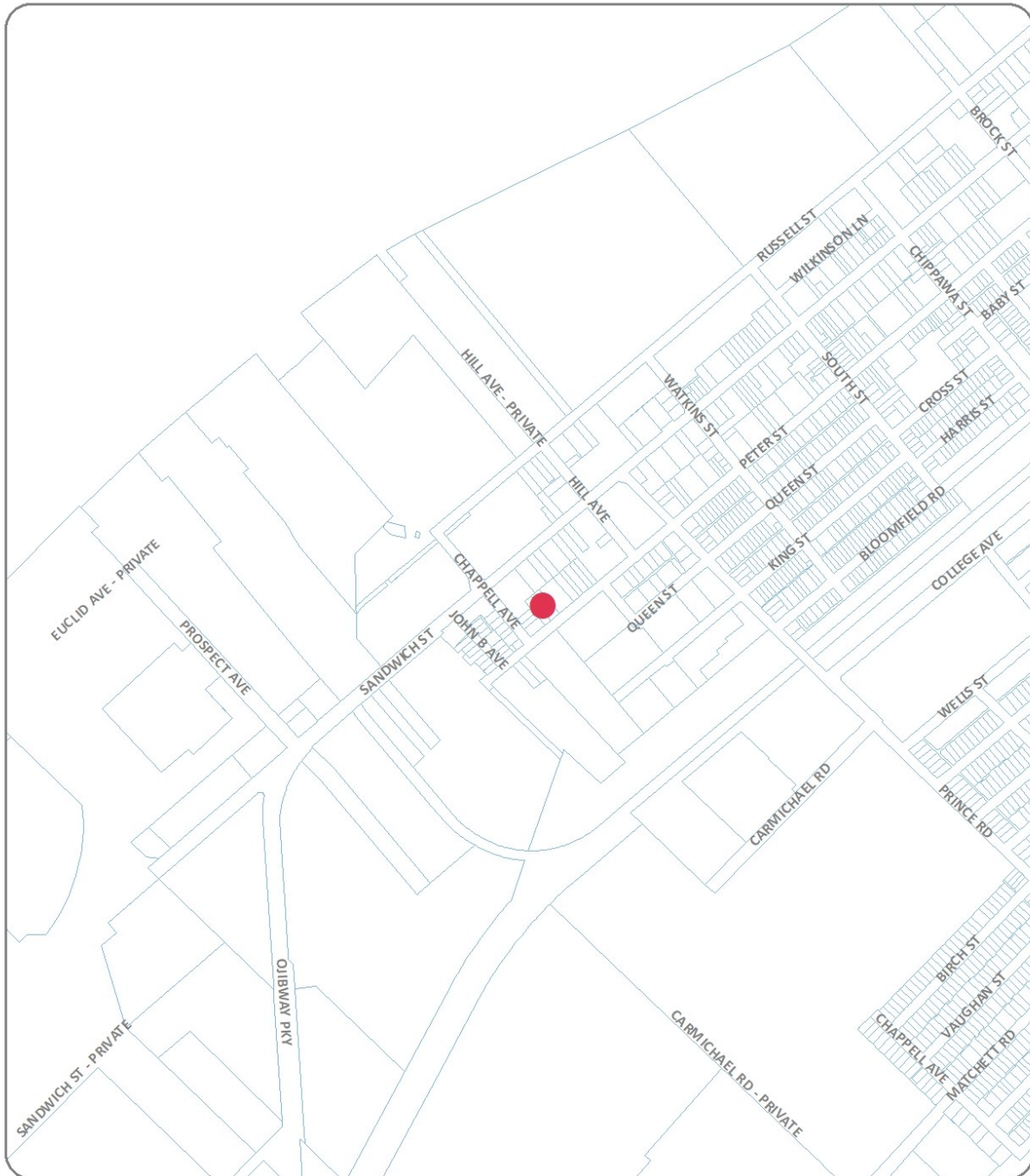
The subject parcel currently includes a single unit dwelling, two accessory buildings (garages) and a driveway leading to a parking area. All existing buildings will be removed to accommodate the development. The parcel is designated Industrial on Schedule D: Land Use in the Official Plan; zoned Development Reserve District 1.1 (DRD1.1) by Zoning By-law 8600; has a lot area of 1,406.79m² and a lot frontage of 22.05m along Peter Street.

The development as proposed is subject to Site Plan Control. All plans, elevations and renderings (Appendix A) are conceptual and subject to change.

Site Information:

OFFICIAL PLAN	ZONING	CURRENT USE	PREVIOUS USE
Industrial	Development Reserve District 1.1 (DRD 1.1)	Single Unit Dwelling with two accessory buildings	N/A
LOT WIDTH	LOT DEPTH	LOT AREA	LOT SHAPE
22.05 m	63.80 m	1,406.79 m ²	Rectangular
<i>All measurements are provided by the agent and are approximate.</i>			

Figure 1: Key Map



KEY MAP - Z-023/25, ZNG/7327



● SUBJECT LANDS

Figure 2: Neighbourhood Map



NEIGHBOURHOOD MAP - Z-023/25, ZNG/7327



SUBJECT LANDS

Neighbourhood:

The subject parcel is located in the Sandwich neighbourhood of Windsor, near the Detroit River (site photos provided in Appendix B). It is in an area consisting of low density residential, institutional, as well as light industrial uses. Peter Street consists of a mixture of residential dwellings and industrial uses, including the applicant, CK Mechanical Heating & Cooling operating at the adjacent property. There are also many vacant lots along Peter St. The character within the 3800 block of Sandwich Street is similar.

To the east are industrial uses, the Sandwich Teen Action Group facility (youth-based non-profit organization), the Essex Terminal Railway (ETR), and Mic Mac Park, a municipal park with a swimming pool, splash pad, sports fields, and picnic areas. To the south, located at 4007 Sandwich Street is the Major FA Tilston Armoury & Police Training Centre, a joint federal-municipal facility used by Canadian Armed Forces and the Windsor Police Service as a training facility and includes an outdoor shooting range. West of Sandwich Street, towards the Detroit River, are several large outdoor industrial uses including salt, aggregate and fuel storage and a salvage operation. North of Prince Road between Sandwich and the Essex Terminal Railway is a residential neighbourhood consisting of primarily of low density low profile residential dwellings with some apartment buildings and institutional uses on Peter Street.

Peter Street is classified as a Local Road, with sidewalks on both sides of the street and alternating side on-street parking. Official Plan *Schedule F: Roads and Bikeways* identifies Sandwich St. as a Class II Arterial Road, and Prince Rd. as a Class I Collector Road with a Bikeway. Existing combined sewer and storm sewers are located in Peter Street.

The closest existing transit route to this property is with the Crosstown 2. The Crosstown 2 has a peak weekday frequency of 10 minutes. As part of Transit Windsor's 2023 City Council approved service plan, the Crosstown 2 will be replaced by a new primary route known as Route 100. Route 100 will have a peak weekday frequency of 10 minutes. Route 100 will be implemented in conjunction with the completion of Transit Windsor's new east end terminal, which is proposed to be completed in 2026. The closest existing bus stop to this development is located on Prince at Peter southeast Corner. This bus stop is approximately 310 metres from this property falling within Transit Windsor's 400 metre walking distance guidelines to a bus stop. This will be maintained with Transit Windsor's City Council approved Transit Master Plan.

Figure 3: Subject Parcel – Rezoning



PART OF ZONING DISTRICT MAP 4

N.T.S.

REQUESTED ZONING AMENDMENT

Applicant: CK Mechanical



SUBJECT LANDS

PLANNING & BUILDING DEPARTMENT



DATE : SEPTEMBER, 2025
FILE NO. : Z-023/25, ZNG/7327

Discussion:

Planning Rationale Report (Pillon Abbs Inc. – September 3rd, 2025)

The Planning Rationale Report (PRR), attached as Appendix C, notes that *“Manufacturing use on the Site represents an efficient development pattern that optimizes the use of land, and will support the City's need for employment growth.”* In addition, *“the proposed development has been designed to be compatible with the existing built-up area, and can make use of existing municipal services.”* The PRR further notes that *“the proposal represents good planning as it addresses the need for the City to provide economic development and employment opportunities.”*

The PRR concludes that *“that the proposed development is consistent with the PPS, conforms with the intent and purpose of the OP and ZBL and represents good planning.”* The Planning Department generally concurs with the PRR.

Provincial Planning Statement (2024)

The Provincial Planning Statement (PPS) provides direction on matters of provincial interest related to land use planning and development and sets the policy foundation for regulating the development and use of land in Ontario.

Policy 2.1.6 states that *“complete communities”* shall be supported by *“accommodating an appropriate range and mix of land uses, employment... and other uses to meet long-term needs”*, *“improving accessibility for people of all ages and abilities”*, and *“improving social equity and overall quality of life for people of all ages, abilities, and incomes”*.

The proposed represents an efficient development and land use pattern that will have no adverse impact on the financial well-being of the City of Windsor, land consumption, and servicing costs, and accommodates an appropriate range of employment uses. The proposed amendment is consistent with Policy 2.1.6 of the PPS.

Section 2.3.1 provides general policies for settlement areas. Growth should be focused in the settlement area. Land use patterns should be based on the efficient use of land and resources, optimizing existing and planned infrastructure, support active transportation, and be transit supportive.

The subject parcel is located within the settlement area. The requested zoning amendment promotes land uses that make efficient use of land and existing infrastructure. Active transportation options and transit services are located adjacent to, or near, the parcel. The proposed amendment is consistent with policies in section 2.3.1.

Policy 2.8.1.1 States:

Planning authorities shall promote economic development and competitiveness by:

- a) providing for an appropriate mix and range of employment, institutional, and broader mixed uses to meet long-term needs;*
- b) providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses;*

c) *identifying strategic sites for investment, monitoring the availability and suitability of employment sites, including market-ready sites, and seeking to address potential barriers to investment;*

d) *encouraging intensification of employment uses and compatible, compact, mixed-use development to support the achievement of complete communities; and*

e) *addressing land use compatibility adjacent to employment areas by providing an appropriate transition to sensitive land uses.*

Policy 2.8.2.1 States:

Planning authorities shall plan for, protect and preserve employment areas for current and future uses, and ensure that the necessary infrastructure is provided to support current and projected needs.

The proposed re-development of the site enhances the mix and range of employment uses, incorporates a compatible use for the applicants' adjacent property, and represents an efficient development and land use pattern that will improve the general aesthetic of the site. The development will optimize the use of land, resources and infrastructure servicing the property. No deficiencies in infrastructure have been identified. There are no anticipated environmental or public health and safety concerns with the proposed use of land. The requested amendment is consistent with Policy 2.8.1.1 and Policy 2.8.2.1 of the PPS.

The agent indicates that the objectives of the PPS 2024 have been considered and have informed their professional planning opinion and concept design for the project site. These objectives are discussed in the PRR. Planning & Development Services generally concurs with this and is of the opinion that the proposed amendment to Zoning Bylaw 8600 is consistent with the PPS 2024.

City of Windsor Official Plan

The subject property is located within the Sandwich Planning District on *Schedule A – Planning Districts & Policy Areas*; is within 350m of a “known or suspected former waste disposal site” on *Schedule C – Development Constraints*; is in the “Archaeological Potential Zone” on *Schedule C-1 Archaeological Potential* and designated Industrial land use on *Schedule D – Land Use* in the City of Windsor Official Plan – Volume 1.

Section 6.4.1 lists general objectives of the Employment land policy. Objective 6.4.1.2 seeks to expand Windsor's assessment base by attracting employers and economic development. Objective 6.4.1.3 ensures that employment uses are compatible with other land uses. Objective 6.4.1.4 accommodates a full range of employment activities. Objective 6.4.1.6 requires that employment activities have sufficient and convenient access to all modes of transportation. Objective 6.4.1.8 ensures that adequate infrastructure services are provided. Objective 6.4.1.9 provides direction to maintain and develop viable industrial areas.

The proposed development is compatible with the surrounding area. The business owner is re-investing in the subject property to support the existing employment use. The subject parcel has access to major roads, public transit and active transportation. The requested zoning amendment satisfies the objections in Section 6.4.1 in Volume 1 of the Official Plan.

The parcel directly west of the property is currently a residential legal non conforming use. The property is designated Industrial in *Schedule D – Land Use*, however a single unit dwelling is existing. Provisions in the recommendations aim to mitigate negative impacts on the non-conforming residential use while recognizing that the area is designated for industrial use. It is expected that this area will continue to transition to Industrial uses over time.

Section 6.4.3.1 permits uses in the Industrial land use designation that “...are dependent upon services or otherwise complement to the industrial function of the area”. The proposed warehouse is a permitted use, which will compliment the contractor’s office adjacent to the subject property.

The subject property is within the Olde Sandwich Towne Community Improvement Plan Target Area 3 which includes the area outside of the Commercial Core and the Waterfront. As noted in the CIP Study, the poor condition of housing stock in this area is a major concern. The existing dwelling on this property is in poor condition and will be removed as a result of the proposed development.

Section 3.0 of the Sandwich Town Urban Design Guidelines outlines development guidelines for the Industrial area. The proposed development generally incorporates these guidelines and this can be further implemented during the Site Plan Control process.

The required support study was prepared by Soil & Materials Engineering Inc. (May 23rd 2025) and submitted by the applicant. No concerns were identified. This satisfies policy 5.4.9.2 of the Official Plan.

Stage 1 and Stage 2 Archaeological Assessments were prepared by Cultural Resource Management Group Limited (Sept 3rd 2025) and have been submitted by the applicant. The assessments have also been entered into the Ontario Public Register of Archaeological Reports, all to the satisfaction of the Ministry of Tourism, Culture and Sport. No further archaeological assessment is required. This satisfies the Heritage Conservation policies in Section 9 of the Official Plan.

The proposed amendment to Zoning By-law 8600 will conform to the general policy direction of the Official Plan. Planning and Development Services generally concurs with the Official Plan analysis in Section 5.1.2 of the PRR submitted by the Applicant.

Zoning By-Law 8600

The subject land is zoned Development Reserve District 1.1 (DRD1.1) which only permits an existing dwelling and any accessory uses. The applicant is requesting a rezoning to Manufacturing District (MD1.2) with site specific provisions to facilitate the construction of a one-storey warehouse on the subject land.

The Site is currently subject to S20(1)236, which regulates front yard setback, building height, parking and garages for residential uses in the Olde Sandwich Towne Community Improvement Area (CIP). This zoning designation applies only to Residential land uses and will not apply to the subject parcel following the proposed rezoning.

The applicant has requested site specific exceptions for decreased minimum side yard width abutting residential land use from 6.0 m to 1.27 m; parking spaces and accessible parking spaces to be permitted in the required front yard; and to permit a required yard

to not be maintained exclusively as a landscaped open space yard. The recommendation includes provisions for the requested site specific provisions. In addition, a provision has been recommended that requires the property owner to install a 1.8 m high privacy fence abutting the residential property at 3872 Peter Street. A maximum building height of 9 metres is also recommended.

The Applicant's request for a change in zoning with site-specific provisions is supported in principle. Section 5.1.3 of the PRR outlines that the proposed development complies with all other applicable zoning provisions. Planning and Development Services is of the opinion that there are no adverse impacts as a result of the proposed development.

Site Plan Control

The proposed development is subject to Site Plan Control. The proposed parking area and landscaping will be reviewed in detail through this process.

Risk Analysis:

N/A

Climate Change Risks

Climate Change Mitigation:

The subject land is located within an existing neighbourhood on existing municipal services, therefore reducing the impacts of climate change by locating within the existing built-up area. In general, redevelopment minimizes the impact on community greenhouse gas emissions as these developments create complete communities and neighbourhoods while using available infrastructure such as sewers, sidewalks, and public transit.

Climate Change Adaptation:

The new building may be affected by climate change, in particular with respect to extreme precipitation and an increase in days above 30 degrees. While not the subject of this report, any new construction would be required to meet the current provisions of the Building Code, which would be implemented through the building permit process.

Financial Matters:

N/A

Consultations:

An Open House was held on April 22nd, 2025 where members of the public could attend either in person or online. Notification of this meeting was distributed to residents and property owners within 200m of the subject site. Other than the Applicant, applicant representatives and City Staff, no other people attended the open house. No calls or emails were received by the Planning consultant. Details of the Open House are summarized in Section 3.2 of the PRR.

Comments received from municipal departments and external agencies are attached as Appendix D. Statutory notice was advertised in the Windsor Star. A courtesy notice was

mailed to property owners within 200m of the subject lands. Submitted documents were posted on the City of Windsor [website](#).

Conclusion:

The *Planning Act* requires that a decision of Council in respect of the exercise of any authority that affects a planning matter, “*shall be consistent with*” Provincial Planning Statement 2024. Based on the supporting documents submitted by the Applicant and the analysis in this report, it is my opinion that the requested amendment to Zoning By-law 8600 is consistent with the PPS 2024 and is in conformity with the City of Windsor Official Plan.

The proposed amendment permits a warehouse which is compatible with existing uses in the surrounding neighbourhood. The proposed development improves the aesthetic of the site and supports the City’s need for employment growth in an area with existing municipal services.

The recommendation to amend Zoning By-law 8600 constitutes good planning. Staff recommend approval.

Planning Act Matters:

Averil Parent

Planner II – Development Review

I concur with the above comments and opinion of the Professional Planner.

Greg Atkinson, MCIP, RPP

Neil Robertson, MCIP, RPP

Deputy City Planner - Development

City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP RM

Approvals:

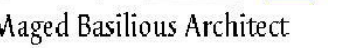
Name	Title
Greg Atkinson	Deputy City Planner - Development
Aaron Farough	Senior Legal Counsel, Legal & Real Estate
Neil Robertson	City Planner
Jelena Payne	Deputy CAO/Commissioner, Economic Development
Ray Mensour	Chief Administrative Officer

Notifications:

Name	Address	Email

Appendices:

- 1 Appendix A - Conceptual Site Plan and Elevations
- 2 Appendix B - Site Photos
- 3 Appendix C - Planning Rational Report
- 4 Appendix D - Consultation

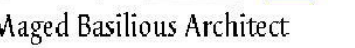


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PARKING CALCULATIONS:				
USE	AREA SF	AREA SM	RATE	PARKING REQD.
FIRST FLOOR				
Warehouse	9,380.00	871.40	200	4.35
Total Required				5.00

ZONING MATRIX (MD1.2)	PROVIDED	REQUIRED	COMPLIANCE
LOT AREA (MIN)	1,409.85 m ²		
BUILDING FLOOR AREA	871.40 m ²		
LOT WIDTH	22.05 m		COMPLIES
BUILDING HEIGHT (MAX.)	9.00 m	14.00 m	COMPLIES
FRONT YARD DEPTH (MIN.)	6.00 m	16.54 m	COMPLIES
REAR YARD DEPTH (MIN)	4.59 m	0.00 m	COMPLIES
INTERIOR SIDE YARD	1.27 m	6.00 m	DOES NOT COMPLY
EXTERIOR SIDE YARD	N/A		N/A
LANDSCAPED OPEN SPACE YARD	18.38%	15%	COMPLIES

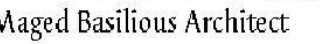




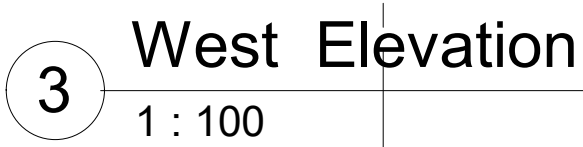
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Scale 1 : 100



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CK Mechanical

3858 Peter St.
WINDSOR, ON

Elevations

A 301

Scale	1 : 100
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APPENDIX B – SITE PHOTOS



View of subject land (3858 – 3868 Peter St.)



View opposite from the subject land



East along Peter St. next to subject site



East along Peter St. across from subject site



West along Peter St. next to subject site



West along Peter St. across from subject site

PLANNING RATIONALE REPORT

ZONING BY-LAW AMENDMENT PROPOSED DEVELOPMENT

3858-3868 Peter Street
Windsor, Ontario

September 3, 2025

Prepared by:



Tracey Pillon-Abbs, RPP
Principal Planner
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1.0 INTRODUCTION

I have been retained by CK Mechanical Heating Cooling Inc. (herein the "Applicant") to provide a land use Planning Rationale Report (PRR) in support of a proposed development located at 3858-3868 Peter Street (herein the "Site") in the City of Windsor, Province of Ontario.

The Site is located in Ward 2 in the Sandwich Planning District.

The Site is made up of 1 existing parcel of land, which currently has an existing single detached dwelling and detached garage, which will be demolished.

It is proposed that the Site will be developed for manufacturing purposes.

It is proposed to construct a new one storey 871.43 m² (9,380 ft²) warehouse on the Site.

The proposed warehouse will be used for storage and will include equipment/stock items such as furnaces, air conditioners, toilets, tubs, plumbing fixtures, fireplaces, etc. The items will be new equipment in boxes stored on shelves.

A total of 9 parking spaces is proposed to be located within the front yard with access from Peter Street.

The Site has access to full municipal services.

The Applicant does own the abutting business (CK Mechanical Hearing and Colling Inc.), located at 3842 Peter Street and will keep the property separate from the Site. There is no intention to merge the parcels.

Pre-consultation (stage 1) was completed by the Applicant (City File #PC-129/24). Comments dated January 10, 2025, were received and have been incorporated into this PRR.

Pre-submission (stage 2) was completed by the Applicant (City File #PC-032/25). Comments dated July 22, 2025, were received and have been incorporated into this PRR.

An application for a Zoning By-law Amendment (ZBA) is required in order to permit the proposed development.

Once the ZBA has been approved, the Applicant will proceed with a Site Plan Control (SPC) application prior to the issuance of a building permit for any construction or site alterations.

The purpose of this report is to review the relevant land use documents, including the Provincial Planning Statement 2024 (PPS), the City of Windsor Official Plan (OP) and the City of Windsor Zoning By-law (ZBL) as it pertains to the ZBA.

This PRR will show that the proposed development is suitable, consistent with the PPS, conforms to the intent and purpose of the OP and ZBL, and represents good planning.

2.0 SITE AND SURROUNDING LAND USES

2.1 Description of Site and Ownership

The Site has been owned by CK Mechanical Heating Cooling Inc. since 2022.

CK Mechanical Heating Cooling Inc. also owns the abutting lands to the north (3842 Peter Street). The Site will not be consolidated. There is no intention to merge the parcels.

The Site is made up of one (1) rectangular-shaped interior parcel of land located on the west side of Peter Street, north of Chappell Avenue and south of Hill Avenue (see the area in **yellow** on Figure 1a – Site Location).

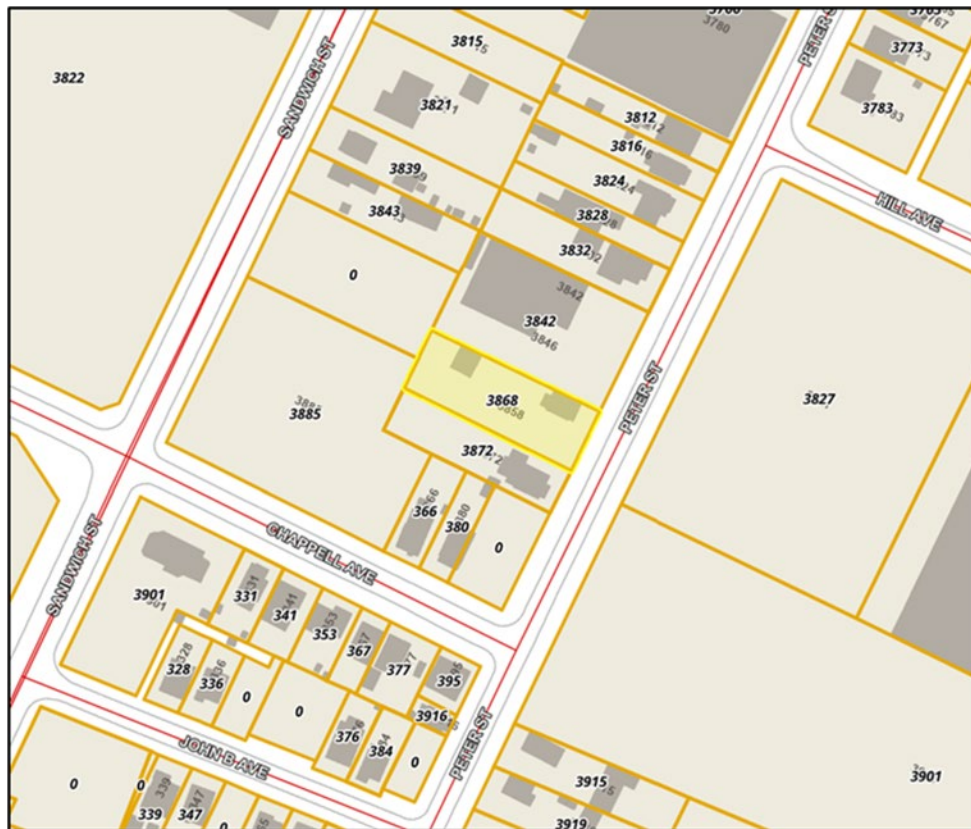


Figure 1a – Site Location (Source: City of Windsor GIS)

The Site is locally known as 3858-3868 Peter Street and is legally described as LT 27 W/S OF PETER ST PL 40 TOWN OF SANDWICH; PT LT 28 W/S OF PETER ST PL 40 TOWN OF SANDWICH AS IN R1354326; WINDSOR, PIN 01258-0188 LT (ARN 050-170-14800 and 050-170-14800-0030).

2.2 Physical Features of the Site

2.2.1 Size and Site Dimension

The Site, subject to the proposed development, consists of a total area of 1,406.79 m², with a lot width of 22.05 m along Peter Street and a lot depth of 63.80 m.

2.2.2 Existing Structures and Previous Use

The Site currently has an existing single detached dwelling and detached garage, which will be demolished (see Figure 1b – Site Street View).



Figure 1b – Site Street View (Source: Pillon Abbs Inc.)

The previous use of the Site is unknown.

2.2.3 Vegetation

The Site has an existing grass area and mature trees.

2.2.4 Topography, Drainage and Soil

The Site is flat and is outside the regulated area of the Essex Region Conservation Authority (ERCA).

The Site is part of the Windsor Area Drainage.

The Site is impacted by Source Water Protection (Significant Groundwater Recharge Area).

The soil is made up of Burford Loam (Bg).

2.2.5 Other Physical Features

There is existing fencing along portions of the Site.

There is an existing gravel driveway with access from Peter Street.

2.2.6 Municipal Services

The property has access to full municipal services.

Peter Street is a 2-way north/south road.

There is on-street parking in the area.

There are streetlights and sidewalks along Peter Street.

There is a fire hydrant located across the Site.

The Site is in close proximity to major transportation corridors, including Sandwich Street, Ojibway Parkway and Hwy 401.

The Site is close to transit, with the nearest bus stop located 300 m from the Site at the corner of Prince Road and Peter Street (Stop ID 1210, Bus 2).

2.2.7 Nearby Amenities

There are several schools, medical facilities, parks and libraries nearby.

There is nearby shopping in the form of plazas and malls, as well as employment, residential, places of worship and local amenities.

2.3 Surrounding Land Uses

Overall, the Site is located in an existing built up area with a mix of uses.

Lot areas and frontages vary in size in the immediate area.

A site visit was undertaken on April 23, 2025. Photos were taken by Pillon Abbs Inc.

West – The lands directly west of the Site are currently vacant and are proposed to be used for residential with frontage along Sandwich Street (see Photo 1 - West).



Photo 1 – West (Source: Pillon Abbs Inc.)

South – The lands directly south of the Site are used for residential use (see Photo 2 - South).



Photo 2 – South (Source: Pillon Abbs Inc.)

East – The lands east of the Site are used for employment use (see Photo 3 - East).



Photo 3 – East (Source: Pillon Abbs Inc.)

North – The lands north of the Site are used for employment and owned by the Applicant (see Photo 4 - North).



Photo 4 – North (Source: Pillon Abbs Inc.)

3.0 PROPOSAL AND CONSULTATION

3.1 Development Proposal

The Site is made up of 1 existing parcel of land, which currently has an existing single detached dwelling and detached garage, which will be demolished.

The Applicant does own the abutting business (CK Mechanical Hearing and Colling Inc.), located at 3842 Peter Street and will keep the property separate from the Site. There is no intention to merge the parcels.

It is proposed that the Site will be developed for manufacturing purposes.

It is proposed to construct a new one storey 871.43 m² (9,380 ft²) warehouse on the Site.

A Concept Plan has been prepared (see Figure 2a – Concept Plan).

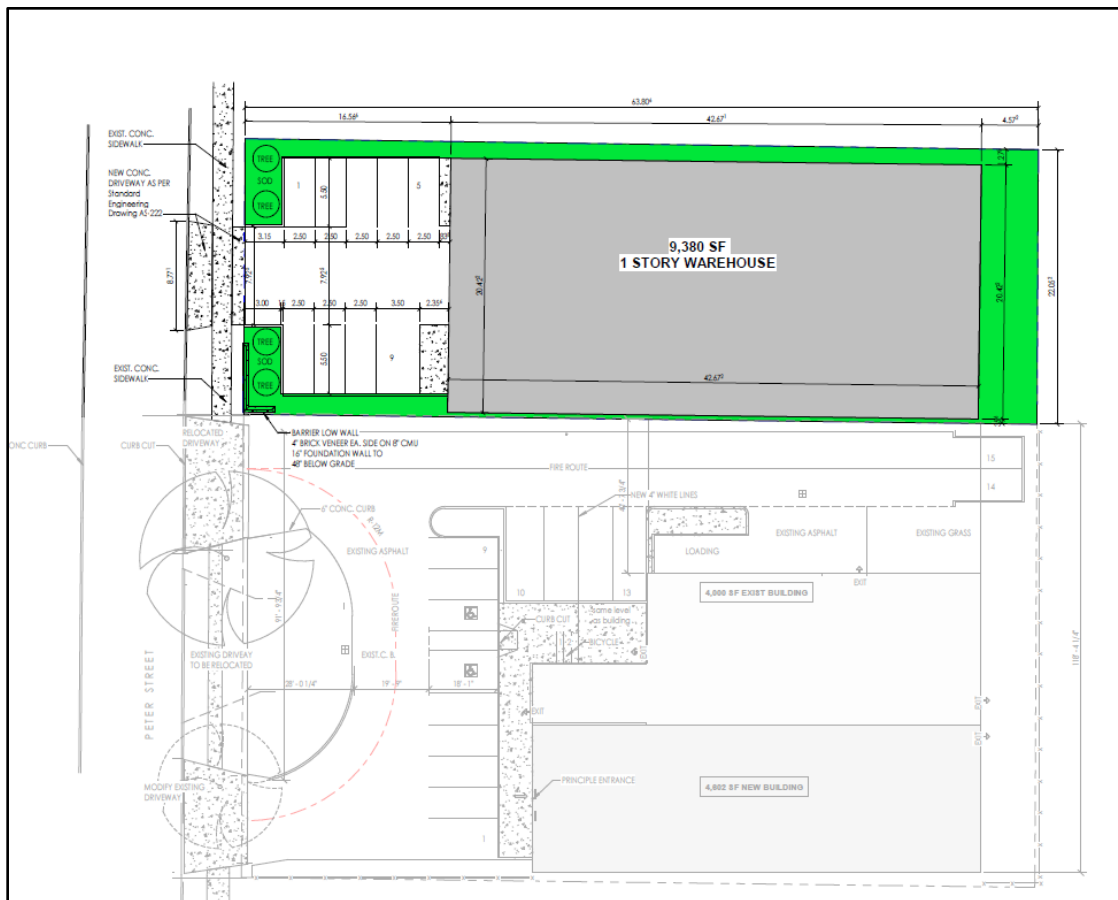


Figure 2a – Concept Plan

The concept plan illustrates a preliminary proposal.

The proposed warehouse will be used for storage. It will include equipment/stock items such as furnaces, air conditioners, toilets, tubs, plumbing fixtures, fireplaces, etc. The items will be new equipment in boxes stored on shelves.

There is no office space proposed.

A sample elevation has been prepared (see Figure 2b – Sample Elevation).

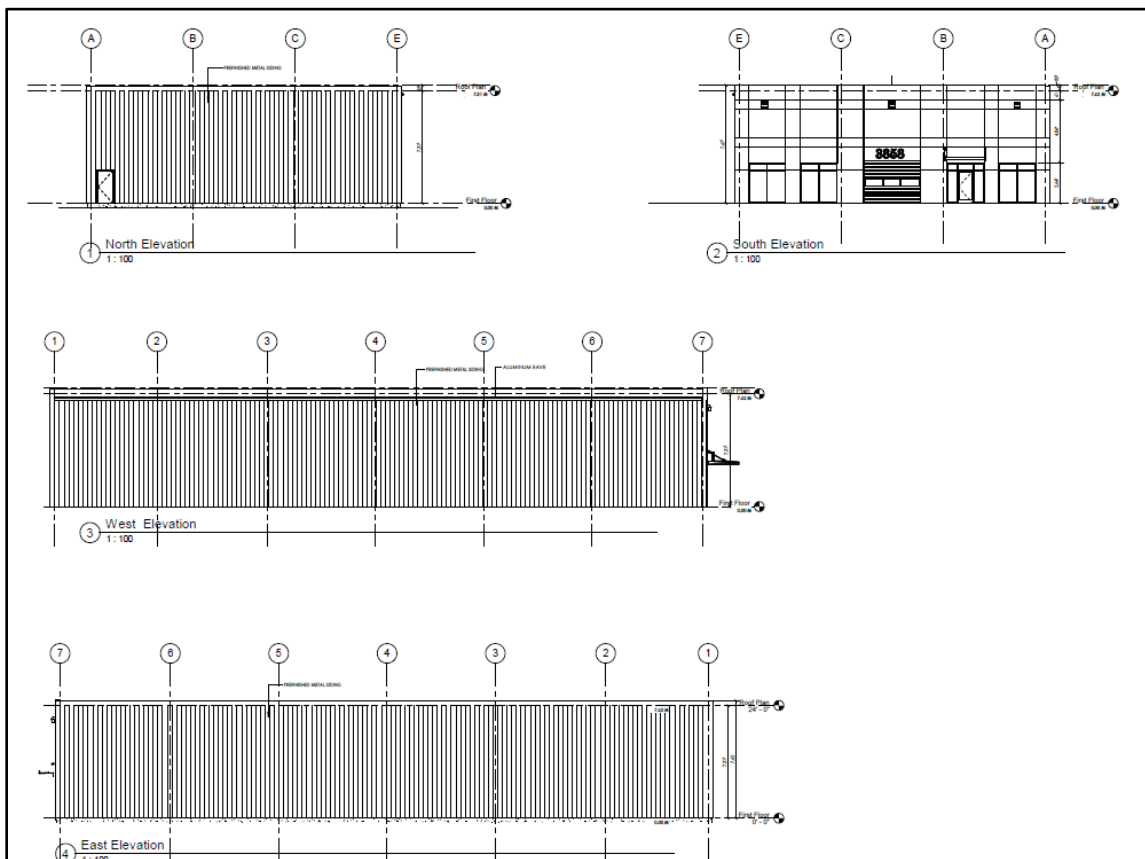


Figure 2b – Sample Elevation

The elevation illustrates a preliminary design of the proposed buildings.

A total of 9 parking spaces are proposed to be located within the front yard with access from Peter Street.

Parking will mainly be used for visitors. There is no staff parking required.

Landscaping will be provided around the proposed building, including tree plantings and a grass area in front of the proposed building.

Fencing can be provided.

The Site has access to full municipal services.

Refuse is proposed to be stored indoors.

There will be no outdoor storage yards.

As part of SPC and building permit requirements, driveway approaches and sewer connections will be provided as per the City standards.

3.2 Public Consultation Strategy

In addition to the statutory public meeting, the *Planning Act* requires that the Applicant submit a proposed strategy for public consultation with respect to an application as part of the complete application requirements.

As part of a public consultation strategy, in addition to the statutory public meeting, an informal electronic public open house was held with area residents and property owners on Tuesday, April 22, 2025, from 6:00 pm to 7:00 pm.

A total of 78 notices were mailed out (200 m radius).

The purpose of the open house was to provide members of the public with opportunities to review and comment on the proposed development.

Other than the City of Windsor staff and the applicant representatives, no other people attended.

No emails or phone calls were received.

4.0 APPLICATIONS AND STUDIES

Pre-consultation (stage 1) was completed by the Applicant (City File #PC-129/24). Comments dated January 10, 2025, were received and have been incorporated into this PRR.

Pre-submission (stage 2) was completed by the Applicant (City File #PC-032/25). Comments dated July 22, 2025, were received and have been incorporated into this PRR.

The purpose of pre-consultation was to identify what applications and support studies were required.

The proposed development requires a Zoning By-law Amendment (ZBA) and required support studies.

The following explains the purpose of the application and other required approvals, as well as a summary of the required support studies.

4.1 Zoning By-Law Amendment

A site specific Zoning By-law Amendment (ZBA) is required to permit the proposed development.

According to Map 4, attached to the ZBL, the Site is currently zoned Development Reserve District 1.1 (DRD1.1) category.

The Site is also subject to S20(1)236, which regulates front yard setback, building height, parking and garages **for residential uses** in the Olde Sandwich Towne Community Improvement Area (CIP).

The zoning for the Site is proposed to be changed to a site specific Manufacturing District 1.2 (MD1.2 - S.20(1)(XXX)) category in order to permit the proposed warehouse.

The proposed development will comply with all zone provisions set out in the MD1.2 with the exception of some zoning relief.

The ZBA is detailed, and the justification is set out in Section 5.1.3 of this PRR.

4.2 Other Applications

Once the ZBA has been approved, the Applicant will proceed with Site Plan Control (SPC) approval prior to the issuance of a building permit for any construction or site alterations.

SPC is a technical review process and will address items such as lighting, landscaping and fencing.

4.3 Supporting Studies

The following studies have been prepared to support the ZBA applications.

4.3.1 Archaeology

A Stage 1 and 2 Archaeological Assessment was prepared by CRM Group, dated April 30, 2025, and revised on September 3, 2025.

A supplementary package was also prepared, dated September 3, 2025.

The purpose of the report was to undertake an archaeological assessment of the proposed development.

Additional work was completed, per the Ministry's request.

No archaeological features or cultural material were identified.

The report concluded that no further assessment was required.

The report has been submitted to the Ministry (MCM File Number 0023438).

4.3.2 Waste Disposal

A Waste Disposal Letter was prepared by CT Soil & Materials Engineering Inc., dated May 23, 2025.

The purpose of the report was to assess potential impacts from 2 active waste disposal sites located within a 500 m radius of the Site.

The report concluded that there is no potential impact on human health or the environment.

4.3.3 Trees

A Tree Inventory and Tree Root Protection Zone Plan was prepared by Bezaire Partners, dated August 20, 2025.

The purpose of the plan was to assess existing trees and their roots.

The report concluded that there are 10 private trees, many of which are in poor condition.

5.0 PLANNING ANALYSIS

5.1 Policy and Regulatory Overview

5.1.1 Provincial Planning Statement

The Provincial Planning Statement, 2024 (PPS) provides policy direction on matters of provincial interest related to land use planning and development, providing for appropriate development while protecting resources of provincial interest, public health and safety, and the quality of the natural and built environments.

The PPS was issued under Section 3 of the Planning Act and came into effect on October 20, 2024.

Decisions affecting planning matters shall be consistent with policy statements issued under the Act.

The following provides a summary of the key policy considerations of the PPS as they relate to the proposed development.

PPS Policy #	Policy	Response
Chapter 1 - Vision	A prosperous and successful Ontario will also support a strong and competitive economy that is investment-ready and recognized for its influence, innovation and diversity. Ontario's economy will continue to mature into a centre of industry and commerce of global significance. Central to this success will be the people who live and work in this province.	The proposed development will contribute positively to promoting efficient land use and development patterns. Manufacturing use on the Site represents an efficient development pattern that optimizes the use of land.
Chapter 2.1.1: Building Homes, Sustaining Strong and Competitive Communities	As informed by provincial guidance, planning authorities shall base population and employment growth forecasts on Ontario Population Projections published by the Ministry of Finance and may modify, as appropriate.	The proposed development will support the City's need for employment growth.

PPS Policy #	Policy	Response
2.1.6	Planning authorities should support the achievement of complete communities by: a) accommodating an appropriate range and mix of land uses, housing options, transportation options with multimodal access, employment , public service facilities and other institutional uses (including schools and associated child care facilities, long-term care facilities, places of worship and cemeteries), recreation, parks and open space, and other uses to meet long-term needs;	The proposed development will support the City's requirement to accommodate for employment to meet long-term needs.
2.3.1.1 – Settlement Areas	Settlement areas shall be the focus of growth and development. Within settlement areas, growth should be focused in, where applicable, strategic growth areas, including major transit station areas.	The Site is located in an existing settlement area of the City of Windsor, which is where growth should occur.
2.3.1.2	Land use patterns within settlement areas should be based on densities and a mix of land uses which: a) efficiently use land and resources; b) optimize existing and planned infrastructure and public service facilities;	The proposed development promotes an efficient development and land use pattern as it uses an existing parcel of land within a settlement area. The Site has access to full municipal infrastructure and is close to nearby public service facilities.
2.8.1.1 - Employment	Planning authorities shall promote economic development and competitiveness by: a) providing for an appropriate mix and range of employment, institutional, and broader mixed uses to meet long-term needs;	The proposed development offers a new employment opportunity. There is a need for the proposed warehouse.

PPS Policy #	Policy	Response
	b) providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses; c) identifying strategic sites for investment, monitoring the availability and suitability of employment sites, including market-ready sites, and seeking to address potential barriers to investment; d) encouraging intensification of employment uses and compatible, compact, mixed-use development to support the achievement of complete communities; and e) addressing land use compatibility adjacent to employment areas by providing an appropriate transition to sensitive land uses.	The proposed development will be suitable for the Site. The proposed development uses the land in an appropriate way and is compact. Infrastructure is provided. The proposed development has been designed to be compatible with the existing built-up area. Buffering can be provided.
2.8.1.2	Industrial, manufacturing and small-scale warehousing uses that could be located adjacent to sensitive land uses without adverse effects are encouraged in strategic growth areas and other mixed-use areas where frequent transit service is available, outside of employment areas.	The proposed use can be located adjacent to residential uses without adverse effects. Buffering and setbacks are provided as a way to mitigate uses.
2.8.1.3	In addition to policy 3.5, on lands within 300 metres of employment areas, development shall avoid, or where avoidance is not possible, minimize and mitigate potential impacts on the long term	The proposed warehouse is appropriate for the Site and does not cause any noise, dust, or vibration.

PPS Policy #	Policy	Response
	economic viability of employment uses within existing or planned employment areas, in accordance with provincial guidelines.	
2.8.2.1 – Employment Areas	Planning authorities shall plan for, protect and preserve employment areas for current and future uses, and ensure that the necessary infrastructure is provided to support current and projected needs.	The Site will continue to be used for employment, which is what it was intended to be used for.
2.8.2.3	Planning authorities shall designate, protect and plan for all employment areas in settlement areas by: a) planning for employment area uses over the long-term that require those locations including manufacturing, research and development in connection with manufacturing, warehousing and goods movement, and associated retail and office uses and ancillary facilities;	The proposed development has been designated by the City in their OP for 'Industrial' use.
Chapter 3.1.1 – Infrastructure and Facilities	Infrastructure and public service facilities shall be provided in an efficient manner while accommodating projected needs.	The Site has access to infrastructure and nearby public service facilities.
3.5.1 – Land Use Compatibility	Major facilities and sensitive land uses shall be planned and developed to avoid, or if avoidance is not possible, minimize and mitigate any potential adverse effects from odour, noise and other contaminants, minimize risk to public health and safety, and to ensure the long-term operational and economic viability of major facilities in accordance with provincial	The proposed warehouse is appropriate for the Site and does not cause any noise, dust, or vibration.

PPS Policy #	Policy	Response
	guidelines, standards and procedures.	
3.6.2 – Sewage, Water and Stormwater	Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. For clarity, municipal sewage services and municipal water services include both centralized servicing systems and decentralized servicing systems.	The development can proceed with municipal services. SWM will be addressed as part of SPC approval.
3.6.8	Planning for stormwater management shall: a) be integrated with planning for sewage and water services and ensure that systems are optimized, retrofitted as appropriate, feasible and financially viable over their full life cycle; b) minimize, or, where possible, prevent or reduce increases in stormwater volumes and contaminant loads;	Stormwater will be reviewed as part of SPC approval.
4.1.1 – Natural Heritage	Natural features and areas shall be protected for the long term.	There are no natural heritage features that impact the Site.
4.2.1 - Water	Planning authorities shall protect, improve or restore the quality and quantity of water by: a) using the watershed as the ecologically meaningful scale for integrated and long-term planning, which can be a foundation for considering cumulative impacts of development; minimizing potential	The Site is outside the regulated area of ERCA.

PPS Policy #	Policy	Response
4.6.1 - Cultural Heritage and Archaeology	Protected heritage property, which may contain built heritage resources or cultural heritage landscapes, shall be conserved.	An assessment has been completed and included engagement.
Chapter 5.1.1 - Protecting Public Health and Safety	Development shall be directed away from areas of natural or human-made hazards where there is an unacceptable risk to public health or safety or of property damage, and not create new or aggravate existing hazards.	There are no natural or human-made hazards.

Therefore, the proposed development is consistent with the PPS.

5.1.2 Official Plan

The City of Windsor Official Plan (OP) was adopted by Council on October 25, 1999, approved in part by the Ministry of Municipal Affairs and Housing (MMAH) on March 28, 2000, and the remainder approved by the Ontario Municipal Board (OMB) on November 1, 2002. The office consolidation version is dated September 7, 2012.

The OP implements the PPS and establishes a policy framework to guide land use planning decisions related to development and the provision of infrastructure and community services throughout the City.

The lands are designated "Industrial" according to Schedule "D" Land Use attached to the OP for the City of Windsor (see Figure 3 – OP).

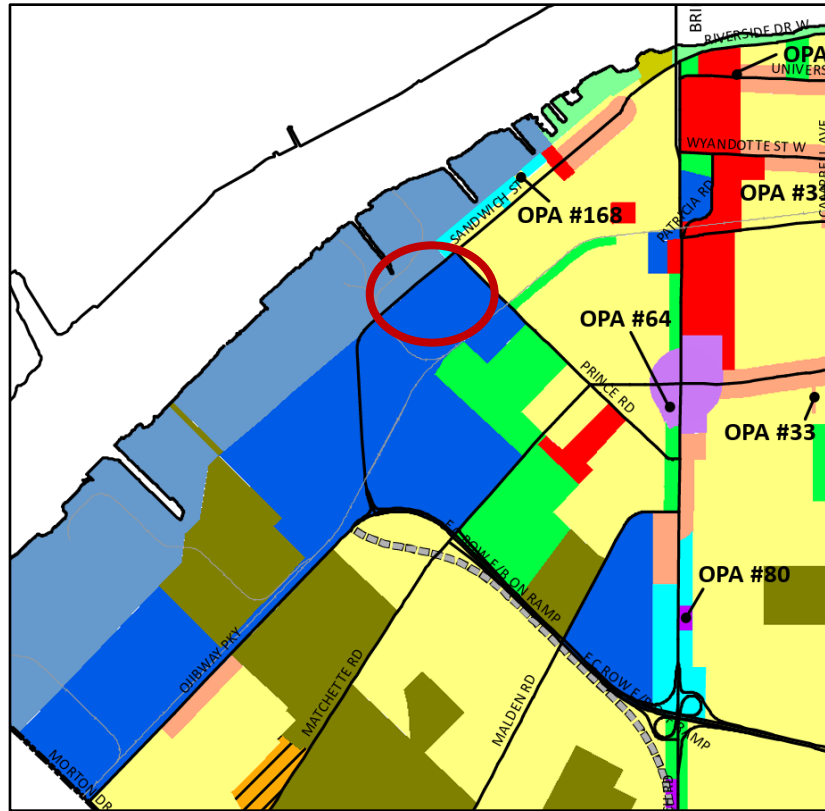


Figure 3 – OP

The Site is also subject to the following:

- Schedule C: Development Constraints - located to the north (+/-350m) of a "known or suspected former waste disposal site"
- Schedule C-1: Archaeological Potential – Located within a "Low Potential" area
- Schedule F: Roads & Bikeways – located on a Local Residential road (Peter Street)
- Schedule F-1: Railways - located to the East (+/-400m) of a railway

It is proposed to maintain the existing 'Industrial' land use designation.

The following provides a summary of the key policy considerations of the OP as it relates to the proposed development.

OP Policy #	Policy	Response
3.1	The planning of Windsor's future is guided by the following vision taken from Dream Dare Do – The	The proposed development will support the City's vision by providing employment in an existing built-up area where

OP Policy #	Policy	Response
	City of Windsor Community Strategic Plan.	citizens can live, work and play. The proposed use will provide for the needs of nearby businesses by providing storage opportunities.
4.0 - Healthy Community	The implementing healthy community policies are interwoven throughout the remainder of the Plan, particularly within the Environment, Land Use, Infrastructure and Urban Design chapters, to ensure their consideration and application as a part of the planning process.	The proposed development will support the City's goal of promoting a healthy and complete community. The proposed development is close to nearby transit, residential, medical facilities, shopping, local/regional amenities and parks.
4.2.6.1 – Employment Opportunities	To provide for a wide range of employment opportunities at appropriate locations throughout Windsor.	The proposed development will provide employment opportunities. Employment (jobs) will be created.
4.2.6.2 – Economic Development	To encourage a range of economic development opportunities to reach full employment.	The proposed development will support economic development in the area. The proposed development will help diversify employment opportunities needed in the area.
5.4.9.2 – Waste Disposal Site	Council shall require proponents of development within 500 metres of a Known or Suspected Waste Disposal Site to prepare a report in accordance with provincial legislation, policy and appropriate guidelines to demonstrate the site is suitable for development.	A portion of the Site is located within 500 m of a 'Known or Suspected Waste Disposal Site'. The required support study has been prepared and summarized in section 4.3.2 of this PRR. No concerns were identified.
6.0 - Preamble	A healthy and livable city is one in which people can enjoy a vibrant economy and a	The proposed development supports the policy set out in the OP as the Site is suited for

OP Policy #	Policy	Response
	sustainable healthy environment in safe, caring and diverse neighbourhoods. In order to ensure that Windsor is such a city, Council will manage development through an approach, which balances environmental, social and economic considerations.	the employment needs of the City.
6.1 - Goals	In keeping with the Strategic Directions, Council's land use goals are to achieve: 6.1.4 The retention and expansion of Windsor's employment base. 6.1.10 Pedestrian oriented clusters of residential, commercial, employment and institutional uses.	The proposed development supports the goals set out in the OP as it provides employment. Employment is encouraged in the area.
6.4.1.1 – Employment (Positive Business Environment)	To ensure Windsor continues to be an attractive place to establish businesses and locate employees.	The proposed development supports this policy.
6.4.1.3 – Compatible Development	To ensure that employment uses are developed in a manner which are compatible with other land uses.	The proposed development supports this policy. Employment is proposed and is compatible with the surrounding area. Buffering (ie fencing and landscaping) can be provided (subject to SPC approval) where there are residential uses.
6.4.1.4 – Range of Uses	To accommodate a full range of employment activities in Windsor.	The proposed development supports this policy.
6.4.2.2 – Attract Business	Council shall encourage businesses and industries to locate and expand in Windsor.	The proposed development supports this policy.

OP Policy #	Policy	Response
6.4.2.4 – Site Plan Control	Council shall require all development within areas designated as Industrial and Business Park to be subject to site plan control, with the exception of Public Open Space uses.	The proposed development will be subject to SPC approval.
6.4.3.1 – Industrial Policies	Uses permitted in the Industrial land use designation identified on Schedule D: Land Use include establishments which may exhibit any or all of the following characteristics: (a) large physical size of site or facilities; (b) outdoor storage of materials or products; (c) large production volumes or large product size; (d) frequent or continuous shipment of products and/or materials; (e) long hours of production and shift operations; (f) likelihood of nuisances, such as noise, odour, dust or vibration; (g) multi-modal transportation facilities; (h) is dependent upon, serves or otherwise complements the industrial function of the area ; and (i) service and repair facilities.	The proposed warehouse is considered a permitted use in the Industrial land use designation and will complement the uses in the area.
6.4.3.3 – Locational Criteria	Industrial development shall be located where: (a) the industrial use can be sufficiently separated and/or buffered from sensitive land uses; (b) there is access to an arterial road; (c) full municipal physical services can be provided; (d) industry related traffic can be directed away from residential areas; (e) peak period public transportation service can be provided; and (f) there is access to designated truck routes.	The Site is located where industrial uses are intended for. Buffering can be addressed as part of SPC approval. There is access to nearby transportation corridors. The Site has access to full municipal services. The proposed use will not impact any nearby residential uses.

OP Policy #	Policy	Response
6.4.3.4 – Evaluation Criteria	At the time of submission, the proponent shall demonstrate to the satisfaction of the Municipality that a proposed industrial development is: (a) feasible having regard to the other provisions of this Plan, provincial legislation, policies and appropriate guidelines and support studies for uses: (i) within or adjacent to any area identified on Schedule C: Development Constraint Areas and described in the Environment chapter of this Plan; (ii) within a site of potential or known contamination; (iii) where traffic generation and distribution is a provincial or municipal concern; and (iv) adjacent to sensitive land uses and/or heritage resources. (b) in keeping with the goals, objectives and policies of any secondary plan or guideline plan affecting the surrounding area; (c) capable of being provided with full municipal physical services and emergency services; (d) provided with adequate off-street parking; and (e) compatible with the surrounding area in terms of siting, orientation, setbacks, parking and landscaped areas.	The PPS has been assessed as part of Section 5.1.1 of this PRR. There are no constraint areas that impact the Site. There is no known contamination. The proposed development is not anticipated to have any impact on traffic. A waste disposal letter has been provided. An archaeology assessment has been provided and summarized in Section 4.3.1 of this PRR. Secondary plans are not impacted. Full municipal services are available. Off-street parking is provided. The proposed development can be constructed to be compatible with the surrounding area.
6.4.3.5 – Design Guidelines	The following guidelines shall be considered when evaluating the proposed design of an Industrial development: (a) the ability to achieve the associated policies as outlined in the Urban Design chapter of this Plan; (b) the provision of appropriate landscaping or other buffers to enhance: (i) (ii) (c) all parking	The final design of the proposed building will be revised as part of SPC approval.

OP Policy #	Policy	Response
	lots, and outdoor loading, storage and service areas; and the separation between the industrial use and adjacent sensitive uses, where appropriate. motorized vehicle access is oriented in such a manner that industry related traffic will be discouraged from using Local Roads where other options are available; (d) pedestrian and cycling access is accommodated in a manner that is distinguishable from the access provided to motorized vehicles and is safe and convenient; (e) (f) loading bays and service areas are located to avoid conflict between pedestrian circulation, service vehicles and movement along the public right-of-way; and the design of the development encourages and/or accommodates public transportation services. (g) The design of the development encourages the retention and integration of existing woodlots, vegetation and drainage corridors where feasible to provide amenity areas for employees and to create a positive visual image of industry in Windsor.	
7.0 - Infrastructure	The provision of proper infrastructure provides a safe, healthy and efficient living environment. In order to accommodate transportation and physical service needs in Windsor, Council is committed to ensuring that infrastructure is provided in a sustainable, orderly and coordinated fashion.	The proposed development is close to nearby transit, off a major transportation corridor and has access to municipal services.

OP Policy #	Policy	Response
9.3.7.1 - Archaeology	Council will integrate heritage conservation into the development and infrastructure approval process by: (a) An archaeological assessment is required as part of a complete application for all development or site alteration application, including municipal projects, if it is determined using the archaeological management plan potential mapping that any part of a potential development area possesses archaeological potential or known archaeological resources as set out in Schedule C-1 Archaeological Potential.	The property is within an area of High Archaeological Potential and may require an Archaeological Assessment as part of any redevelopment. An assessment has been completed and summarized in Section 4.3.1 of this PRR.
11.6.3.1 – Amendment Policies	All amendments to the Zoning By-law(s) shall conform with this Plan. The Municipality will, on each occasion of approval of a change to the zoning by-law(s), specify that conformity with the Official Plan is maintained or that the change will be in conformity upon the coming into effect of an amendment to the Official Plan.	The proposed ZBA does conform with the Windsor OP, as set out in this PRR. The conformity of the current OP is maintained. The Site was always intended to be used for manufacturing.
11.6.3.3 – Evaluation Criteria	When considering applications for Zoning By-law amendments, Council shall consider the policies of this Plan and will, without limiting the generality of the foregoing, consider such matters as the following: (a) The relevant evaluation criteria contained in the Land Use Chapter of this Plan, Volume II: Secondary Plans & Special Policy Areas and other relevant standards and guidelines; (b) Relevant support studies; (c) The comments and recommendations from	The OP policies have been considered in the analysis of the ZBA. This PRR has undertaken the required evaluation of OP policies. There are no Secondary Plans that impact the Site. Support studies have been summarized in Section 4.3 of this PRR. Comments and recommendations from staff

OP Policy #	Policy	Response
	municipal staff and circularized agencies; (d) Relevant provincial legislation, policies and appropriate guidelines; and (e) The ramifications of the decision on the use of adjacent or similar lands.	and agencies have been provided as part of the Stage 1 application review. This PRR has undertaken the required evaluation of provincial legislation in Section 5.1.1. There are no design guidelines that impact this type of development.
3.1.3 - Sandwich Town Community Improvement Plan (CIP) – Industrial Area, Target Area 3	Target Area 3 comprises a large part of the Project Area and contains a range of uses. This includes residential, institutional, commercial and industrial uses.	The Site is designated Industrial in the OP.
	As noted in the Community Planning Study, the poor condition of the housing stock in much of the Project Area (and particularly much of Target Area 3) is a major concern.	The existing dwelling is in poor condition and will be removed.
3.2 Olde Sandwich Towne – Community Improvement Plan (CIP) – Supplementary Development and Urban Design Guidelines	Many of the properties related to industrial uses are large and have outdoor storage. It is the City's intention to concentrate on the front of the property to create an attractive streetscape and positive image of the area. The following guidelines apply to all development within the industrial area: 1. Design new buildings and landscaping so that they dominate the streetscape in the industrial area. Ensure that landscape buffer strips are at least 3.0m wide.	The CIP urban design guidelines will be respected. The CIP is only a guideline. The final design of the proposed building will be provided as part of the SPC application. At that time, the character and materials used will be assessed. Landscaping will be provided around the parking area and the proposed warehouse. An appropriate amount of buffering is provided.

OP Policy #	Policy	Response
		Windows and door windows will be clear vision glass (not tinted), and there will be no lifestyle posters in the windows. Materials can also be made to be consistent with the properties in the area, including façade design. The current ZBA is a change in land use only.
	2. Limit parking in a building's front yard to visitor parking, with staff parking located at the side and/or rear of the building. Screen parking from the street with landscaping, fencing, berms or a combination of these features.	Parking will be provided in the front of the proposed building for visitors. Parking in the front yard is limited, not prohibited. There is no need for staff parking. Parking will be located where it is safe and visible from the street for visitors. Landscaping is provided.
	3. Locate the office component of a building closer to the street than the plant or warehouse component so that it is visible from the street and breaks up building mass.	There is no office proposed. The warehouse is set back from the road to use the Site more efficiently. The proposed setbacks also provide more privacy for the abutting properties. The landscape area distance adjacent to Peter Street will be consistent with the residential properties along Peter Street, with a combination of decorative fencing, low

OP Policy #	Policy	Response
		masonry walls, and vegetation to be used to screen the parking area from the road.
	4. Site and design buildings to recognize and incorporate the natural features of a site such as existing trees , contours, water course and ponds. Use Carolinian plant materials to promote the natural heritage of the area.	There are no natural features on the Site or near the Site. A TIPP has been prepared and summarized in Section 4.3.3 of this PRR. New landscaping and tree plantings will be reviewed at the time of SPC.
	5. Set development back from existing natural features to create appropriate buffer areas.	There are no natural features that impact the Site. Screening, buffering and fencing will be provided. Adjacent property can be protected with appropriate buffering.
	6. Site buildings to ensure that adjacent properties are protected from potential nuisances from the new building and the site, such as illumination, noise and odour where applicable.	The proposed development is a low profile form of development which incorporates sufficient setbacks to allow for appropriate landscaping and buffering. Lighting and landscaping will be addressed as part of SPC approval. There are no anticipated nuisances as warehousing is generally a quiet use of land. There are no loading docks, emissions, truck traffic or noise generated by the warehouse.

OP Policy #	Policy	Response
	7. Incorporate outdoor amenity areas, such as a patio for lunch, into the overall site design and define such areas by building façades, fencing or landscaping.	Lighting and landscaping will be addressed as part of SPC approval. There are landscaped areas in the front yard. The zoning does not regulate amenity space in the ZBL for industrial uses. This is just a guideline. The overall design is subject to SPC approval. Façades, fencing and landscaping will be addressed at that time.
	8. Provide sufficient area on the site for truck turning and circulation.	Vehicles will be able to access the building from the parking area. No large trucks will access the Site.
	9. Locate waste storage areas inside buildings wherever possible. Where outside, ensure that waste disposal areas and containers are not visible from the street through such screening measures as building design and siting, landscaping and planting, or fencing.	There is no outdoor storage of waste or recycling. Refuse will be collected inside the proposed building.
	10. Locate loading and outdoor storage areas in the rear and/or interior side yard for new buildings.	No loading or outdoor storage areas are proposed.
	11. Locate loading and servicing areas away from the street frontages and visually screen such areas from public views and adjacent residential properties by landscape features and/or fencing that is compatible with the building's architecture.	Loading can be provided from the parking area. The ZBA requests relief from the ZBL regarding loading space, as none is proposed. Landscaping and fencing will be provided.

OP Policy #	Policy	Response
		The proposed building will create privacy from abutting properties. The final design is subject to SPC approval.
	12. Use landscaping as a major visual element in unifying the streetscape and softening long expanses of blank walls and fencing.	The landscape area distance adjacent to Peter Street will be consistent with the residential properties along Peter Street, with a combination of decorative fencing, low masonry walls, and vegetation to be used to screen the parking area from the road.
	13. Develop stormwater management ponds as natural landscaped features that contribute to the landscaped environment on the site and the surrounding area.	There are no SWM ponds on or near the Site.

Therefore, based on the above-noted analysis, the proposed development conforms to the purpose and intent of the OP.

5.1.3 Zoning By-law

The City of Windsor Zoning By-Law (ZBL) #8600 was passed by Council on July 8, 2002, and then a further Ontario Municipal Board (OMB) decision was issued on January 14, 2003.

A ZBL implements the PPS and the City OP by regulating the specific use of property and providing for its day-to-day administration.

According to Map 4 attached to the ZBL the Site is currently zoned Development Reserve District 1.1 (DRD1.1) category (see Figure 4 – ZBL).

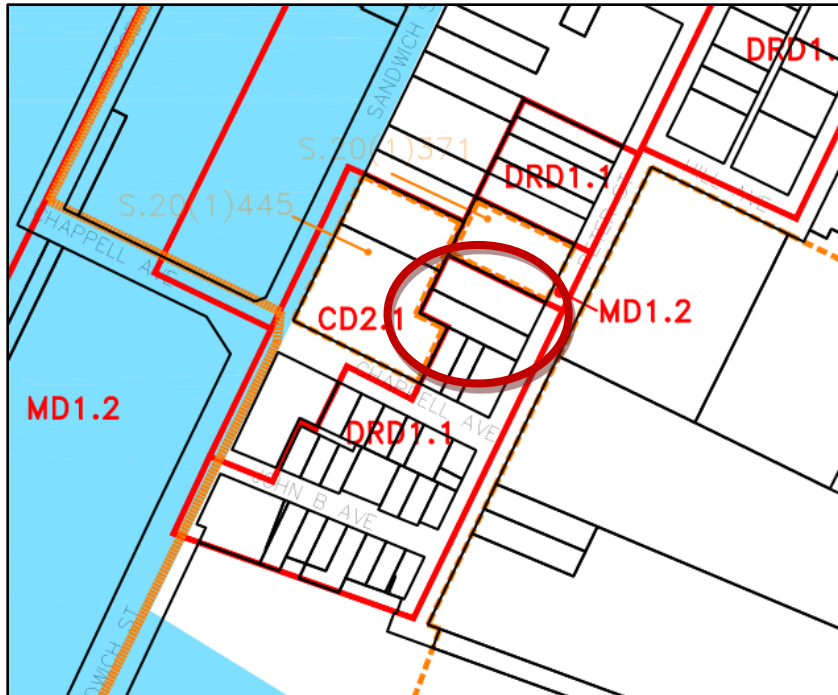


Figure 4 –ZBL

The Site is also subject to S20(1)236, which regulates front yard setback, building height, parking and garages for residential uses in the Olde Sandwich Towne Community Improvement Area (CIP).

The zoning for the Site is proposed to be changed to a site specific Manufacturing District 1.2 (MD1.2 - S.20(1)(XXX)) category in order to permit the proposed warehouse.

WAREHOUSE means a building used for the storage of parts, materials, equipment or other goods and products and may include their loading, unloading, packaging, repacking, or unpacking. A self-storage facility or transport terminal is not a warehouse.

A review of the MD1.2 zone provisions, as set out in Section 18.2 of the ZBL is as follows:

Zone Regulations	Required MD 1.2 Zone	Proposed MD1.2 - S.20(1)(XXX)	Compliance and/or Relief Requested with Justification
Permitted Uses	Ambulance Service Building Materials Recycling Store Bulk Storage Facility Business Office Contractor's Office	Warehouse	Subject to the ZBA The Site is big enough to accommodate the warehouse.

Zone Regulations	Required MD 1.2 Zone	Proposed MD1.2 - S.20(1)(XXX)	Compliance and/or Relief Requested with Justification
	Equipment Rental Shop Food Catering Service Food Processing Facility Laundry Plant Manufacturing Facility Medical Appliance Facility Micro-Brewery Public Parking Area Repair Shop – Heavy Repair Shop – Light Self-Storage Facility Towing Service Warehouse Water Transportation Facility Welding Shop Any of the following Ancillary Uses: Automobile Sales Lot Car Wash Automatic Car Wash Coin Operated Club Food Outlet – Drive-through Food Outlet – Take-out Gas Bar Health Studio Restaurant Restaurant with Drive-through Retail Store – Equipment & Supplies Veterinary Office Wholesale Store Any of the following Existing Uses: Transport Terminal Any use accessory to any of the above uses, including a		The proposed use is compatible with the surrounding area. Buffering can be provided (appropriate setbacks, fencing and landscaping).

Zone Regulations	Required MD 1.2 Zone	Proposed MD1.2 - S.20(1)(XXX)	Compliance and/or Relief Requested with Justification
	Caretaker's Residence or a Retail Store		
Prohibited Uses	Outdoor storage of aggregate	No outdoor storage of aggregate proposed	Complies
Max. Building Height	14.0 m	9.0 m (1 storey)	Complies
Min. Front Yard Depth	6.0 m	16.54 m	Complies
Min. Side Yard Width	a) From a side lot line that abuts a lot on which a dwelling or dwelling unit is located – 6.0 m	South - 1.27 m	Relief requested. The general intent of a side yard width requirement is to ensure privacy, natural light, ventilation, aesthetics, utilities, fire safety, maintenance, and to prevent encroachments. Access to the side yards and rear yard for maintenance and emergency services can be provided. There are no encroachments proposed. Additional landscaping is proposed as a buffer. Fencing can also be provided, subject to SPC approval. There will be no windows along the side of the proposed warehouse.

Zone Regulations	Required MD 1.2 Zone	Proposed MD1.2 - S.20(1)(XXX)	Compliance and/or Relief Requested with Justification
			The building will provide privacy for the abutting properties.
Min. Landscaped Open Space Yard	15.0% of lot area – 211.02 m2 (based on 1,406.79 m2 lot area)	18.38 % of the lot area – 259.19 m2	Complies
Max. Gross Floor Area – main building	Retail Store - 25.0% of the GFA of the main use	N/A – no retail proposed	Complies
Min. Parking Spaces Required (Table 24.20.20.5.1)	Warehouse - 1 for each 200 m ² GFA Based on 871.43 m2 = 4.36 (4 rounding down)	9 total parking spaces proposed	Complies
Minimum Required Accessible Parking Spaces (24.24.1.1)	1 to 25 = 1 Type A 0 Type B	1	Complies
Location of Parking, Visitor Parking or Accessible Parking Spaces (24.26.5)	A parking space, visitor parking space or accessible parking space is prohibited in a required front yard or a required landscaped open space yard, except on a lot occupied by a single-unit dwelling, semi-detached dwelling, duplex dwelling or a townhome dwelling	Parking spaces and accessible parking spaces be permitted in the required front yard.	Relief requested. The proposed parking space locations are preferred to be located near Peter Street as opposed to abutting the residential uses. Parking in the front will provide better safety for visitors as opposed to being located at the rear of the building. Parking will have clear visibility from the street.

Zone Regulations	Required MD 1.2 Zone	Proposed MD1.2 - S.20(1)(XXX)	Compliance and/or Relief Requested Justification with
	unit, a parking space, visitor parking space or accessible parking space is permitted in a required front yard.		Landscaping, screening and buffering can be provided and will be subject to SPC approval.
Minimum Required Loading Spaces (24.40.1.9)	For any other non-residential building, the number of loading spaces - Over 275 m ² to 2,500 m ² Total = 1 loading space required	0	Relief required Loading can be provided from the parking area for vehicles. It is not anticipated that large trucks will access the Site..
Minimum Parking Area Separation (25.5.20.1)	Any other street – 3.00 m	3.00 m	Complies
	An interior lot line or alley - 0.90 m	1.27 m	Complies
	A building wall in which is located a main pedestrian entrance facing the parking area – 2.00 m	2.35 m	Complies
Required Yard in Manufacturing Districts 5.67.1	In any MD1. or MD2. zoning district, a required yard shall be maintained exclusively as a landscaped open space yard, save and except that an access area may cross a required yard.	A required yard shall not be maintained exclusively as a landscaped open space yard.	Relief required. There is sufficient landscaped open space provided on the Site. The parking area will be properly screened.

Therefore, based on the above-noted analysis, the proposed development conforms to the purpose and intent of the ZBL.

In addition to the proposed permitted use of a warehouse, the proposed development will comply with all zone provisions set out in the MD1.2, with the following requested relief:

1. *Decrease the minimum side yard width abutting residential from 6.0 m to 1.27 m,*
2. *Permit parking spaces and accessible parking spaces to be permitted in the required front yard,*
3. *Decrease the minimum loading space from 1 to 0, and*
4. *To permit a required yard to not be maintained exclusively as a landscaped open space yard.*

6.0 SUMMARY AND CONCLUSION

6.1 Context and Site Suitability Summary

6.1.1 Site Suitability

The Site is ideally suited for development for the following reasons:

- The land area is sufficient to accommodate the proposed development,
- The Site is generally level, which is conducive to easy vehicular movements,
- The Site will be able to accommodate municipal water, storm and sewer systems,
- There are no anticipated noise, privacy, traffic or parking concerns,
- There are no natural heritage concerns,
- There are no cultural heritage concerns,
- There are no hazards, and
- The location of the proposed development is appropriate.

6.1.2 Compatibility of Design

The proposed development has been designed to be compatible with the existing built-up area.

The proposed development is a low profile form of development which incorporates sufficient setbacks to allow for appropriate landscaping and buffering.

The Site is capable of accommodating the proposed development in terms of scale, massing, height and siting.

The proposed warehouse is an appropriate use of the Site and does not cause any noise, dust or vibration.

The proposed development will create a buffer between the existing residential and manufacturing uses.

The proposed development will help diversify employment opportunities needed in the area and create new jobs.

6.1.3 Good Planning

The proposal represents good planning as it addresses the need for the City to provide economic development and employment opportunities.

The Site was always intended to be used for manufacturing.

The proposed use will provide for the needs of nearby businesses by providing storage opportunities.

The proposed use of the Site represents an efficient development pattern that optimizes the use of land.

The proposed development will not change lotting or street patterns in the area.

6.1.4 Natural Environment Impacts

The proposal does not have any negative natural environmental impacts.

6.1.5 Municipal Services Impacts

Municipal services are available, which is the preferred form for development.

There are no parking or traffic concerns.

6.1.6 Social, Heritage and/or Economic Conditions

The proposed development does not negatively affect the social environment as the Site is in close proximity to major transportation corridors, transit, residential, open space and community amenities.

Development in an existing built-up area of the City contributes toward the goal of 'live, work and play' where citizens share a strong sense of belonging and a collective pride of place.

The proposal does not cause any public health and safety concerns. The proposal represents a cost effective development pattern that minimizes land consumption and servicing costs.

There will be no urban sprawl as the proposed development is within the existing settlement area and is an ideal infilling opportunity.

There are no cultural heritage resources that impact the Site.

6.2 Conclusion

In summary, it would be appropriate for Council for the City of Windsor to approve the ZBA application to permit the proposed development on the Site.

This PRR has shown that the proposed development is consistent with the PPS, conforms with the intent and purpose of the OP and ZBL and represents good planning.

Planner's Certificate:

I hereby certify that this report was prepared by Tracey Pilon-Abbs, a Registered Professional Planner, within the meaning of the Ontario Professional Planners Institute Act, 1994.


Tracey Pilon-Abbs, RPP
Principal Planner



APPENDIX D – CONSULTATION

TRANSIT WINDSOR – JASON SCOTT

Transit Windsor has no objections to this development. The closest existing transit route to this property is with the Crosstown 2. The Crosstown 2 has a peak weekday frequency of 10 minutes. As part of Transit Windsor's 2023 City Council approved service plan, the Crosstown 2 will be replaced by a new primary route known as Route 100. Route 100 will have a peak weekday frequency of 10 minutes. Route 100 will be implemented in conjunction with the completion of Transit Windsor's new east end terminal, which is proposed to be completed sometime in 2026. The closest existing bus stop to this development is located on Prince at Peter southeast Corner. This bus stop is approximately 310 metres from this property falling within Transit Windsor's 400 metre walking distance guidelines to a bus stop. This will be maintained with Transit Windsor's City Council approved Transit Master Plan.

CANADA POST – BRUNO DESANDO

Canada Post has no comments for the attached application.

URBAN DESIGN – SOPHIA DI BLASI

No Urban Design comments for this application.

HERITAGE PLANNING – TRACY TANG

Archaeological Assessment Report Entitled "STAGE 1 & 2: Archaeological Assessment 3858-3868 Peter Street Part Of Lots 27 & 28, West Side Of Peter Street On Plan 40 Geographic Township Of Sandwich West, Part Of R1354326 City Of Windsor, County Of Essex, Ontario ", Dated Sep 3, 2025, Filed with MCM on Sep 8, 2025, MCM Project Information Form Number P1116-0044-2025, MCM File Number 0023438", has been entered into the Ontario Public Register of Archaeological Reports. Although the report recommends that no further archaeological assessment of the property is recommended, the applicant is still to note the following archaeological precautions.

1. Should archaeological resources be found during grading, construction or soil removal activities, all work in the area must stop immediately and the City's Planning & Building Department, the City's Manager of Culture and Events, and the Ontario Ministry of Citizenship and Multiculturalism must be notified and confirm satisfaction of any archaeological requirements before work can recommence.
2. In the event that human remains are encountered during grading, construction or soil removal activities, all work in that area must be stopped immediately and the site secured. The local police or coroner must be contacted to determine whether or not the skeletal remains are human, and whether the remains constitute a part of a crime scene. The Local police or coroner will then notify the Ontario Ministry of Citizenship and Multiculturalism and the Registrar at the Ministry of Government and Consumer Services if needed, and notification and satisfactory confirmation be given by the Ministry of Citizenship and Multiculturalism.

Contacts:

Windsor Planning & Development Services Department:

519-255-6543 x6179, ttang@citywindsor.ca, planningdept@citywindsor.ca

Windsor Manager of Culture and Events (A):

Michelle Staadegaard, (O) 519-253-2300x2726, (C) 519-816-0711,

mstaadegaard@citywindsor.ca
Ontario Ministry of Citizenship and Multiculturalism
Archaeology Programs Unit, 1-416-212-8886, Archaeology@ontario.ca
Windsor Police: 911
Ontario Ministry of Public and Business Service Delivery and Procurement
Registrar of Burial Sites, War Graves, Abandoned Cemeteries and Cemetery Closures:
Ian Hember, 1-437-244-9840, Ian.hember@ontario.ca

ZONING COORDINATOR – STEFAN PAVLICA

- **Current Zoning Designation:** Development Reserve District 1.1 (DRD1.1)
- **Proposed Zoning Designation:** Manufacturing District 1.2 (MD1.2)

- **Existing Use [as per historical Building Permit(s) / Planning Act Applications(s)]:**
 - o *Single Unit Dwelling*

- **Proposed Use:**
 - o *Warehouse*

- **Section 5 – General Provisions:**
 - o Required Yard in Manufacturing Districts [5.67.1]:
 - In any MD1. or MD2. *zoning district*, a *required yard* shall be maintained exclusively as a *landscaped open space yard*, save and except that an *access area* may cross a *required yard*:
 - 6.0m – Front Yard Depth (Required)
 - **3.0m (Proposed)**
 - o All other provisions in Section 5 – General Provisions:
 - Not applicable

- **Section 18.2.1 – Manufacturing District 1.2 (MD1.2):**
 - o Maximum Building Height [18.2.5.4]:
 - 14.0m (Required)
 - 7.6m (Provided)
 - o Minimum Front Yard Depth [18.2.5.5]:
 - 6.0m (Required)
 - 16.4m (Provided)
 - o Minimum Side Yard Width [18.2.5.7]:
 - 6.0m (Required)
 - a) From a *side lot line* that abuts a *lot* on which a *dwelling* or *dwelling unit* is located: 6.0m
 - **1.27m (Provided)**
 - o Minimum Landscaped Open Space Yard [18.2.5.8]:
 - 15.0% of *lot area* (Required)
 - 18.1% (Provided)
 - o Maximum Gross Floor Area – *Retail Store* [18.2.5.10]:
 - 25.0% of the *GFA* of the *main building* (Required)
 - N/A (Provided)

- **Section 20 – Site Specific Zoning Exemptions:**
 - o **S.20(1)236: NOT APPLICABLE**

- For all residential uses in Target Area 3 of the Olde Sandwich Towne Community Improvement Area, shown delineated by a heavy black line on Schedule 'A' of By-law 30-2009, the following shall apply:
 - a) Regulations:
 - 1. Where a vacant interior lot abuts lots with existing dwellings, the minimum required front yard shall be equal to the average of the setbacks from the front lot line of the dwellings on the abutting lots.
 - b) Prohibitions:
 - 1. Building height in excess of 2 storeys shall be prohibited
 - 2. Notwithstanding any other provisions in this by-law, parking shall be prohibited in a required front yard
 - 3. No new structures, additions, or encroachments shall be constructed in an existing front yard
 - 4. Garages are prohibited unless located a minimum of 6 m to the rear of the main wall.
- **Section 24 – Parking, Loading, and Stacking Provisions:**
 - Required Number of Parking Spaces [24.20.5.1]:
 - 4 (Required)
 - 9 (Provided)
 - Required Number of Type A Accessible Parking Spaces [24.24.1]:
 - 1 (Required)
 - 1 (Provided)
 - Required Number of Type B Accessible Parking Spaces [24.24.1]:
 - 0 (Required)
 - 0 (Provided)
 - [24.24.15.1]: An access area shall be **provided for all accessible parking spaces.**
 - [24.24.20.1]: Where a parking area is bounded by perimeter curbing which separates the principal pedestrian entrance of a building from the parking area, **there shall be provided and maintained at least one curb cut or ramp** that has a minimum width of 1.2 m and a maximum slope of 1:8 where elevation is less than 7.5 cm or 1:10 where elevation is 7.5 cm to 20 cm
 - Required Number of Bicycle Parking Spaces [24.30.1]:
 - 0 (Required)
 - 0 (Provided)
 - Required Number of Loading Spaces [24.40.1.9]:
 - 1 (Required)
 - 1 (Provided)
- **Section 25 – Parking Area Regulations:**
 - Construction and Maintenance of Parking Area:
 - [25.5.10.3] **A curb shall bound the perimeter of a parking area** and shall separate a landscaped open space yard, landscaped open space island or parking area separation from the parking area.

RIGHT-OF-WAY – MARK SCHAFFHAUSER

Required Drawing Revisions:

1. **Sewer Connections** – All existing and proposed storm, sanitary and water services must be identified on the drawings, as well as the associated mainline sewers/water mains.
 - Modify drawings to include all sewer connections and water services.

- Identify any redundant connections to be abandoned in accordance with Engineering Best Practice BP1.3.3.

Special Provisions (to be included in Site Plan Control Agreement)

The following special provisions will be required prior to submitting a building permit application:

Site Plan Control Agreement – The applicant enter into an agreement with the City of Windsor for all requirements under the General Provisions of the Site Plan Control Agreement for the Engineering Department.

If you have any further questions or concerns, please contact Karen Kong, of this department at kkong@citywindsor.ca

SITE PLAN CONTROL

The development proposal is subject to Site Plan Control pursuant to the Planning Act and City of Windsor By-law 1-2004. Where preceding development applications are required, inclusive of Official Plan and Zoning By-law Amendments, request for Site Plan Control Pre-Consultation Stage 1 may be made following completion of the requisite Development and Heritage Standing Committee meeting at <https://ca.cloudpermit.com/login>

This proposal was previously submitted to Site Plan (S1-2023-45), but was rejected because ZBA was required. If this current ZBA application is approved, a Stage 2 Site Plan application will be considered.

WINDSOR FIRE – JOHN SMITH

No objections from Windsor Fire.

TRANSPORTATION PLANNING – ELARA MEHRILLOU

- **Land Conveyance**

Not Applicable.

- **Corner Cut-Off**

Not Applicable.

- **Sidewalk**

Not Applicable.

- **Parking**

Please be advised that the requirements for parking area regulations, including the number of regular, accessible, loading, and bicycle parking spaces, are detailed in the City of Windsor's Zoning By-law 8600. These requirements are reviewed by the Planning Department and must be met for approval.

In cases where any of these parking requirements are not met, Transportation will require a formal justification. This may include, but is not limited to, a parking study, turning movement

analysis, or turning radius template overlays. This documentation is necessary to consider any deviation from the by-law's standards.

- **Transportation Impact Study**

Not Applicable.

- **Access**

All accesses shall conform to the TAC Geometric Design Guide for Canadian Roads and the City of Windsor Standard Engineering Drawings.

- **Exterior Path**

All new exterior paths of travel must meet the requirements of the Accessibility for Ontarians with Disabilities Act (AODA).

- **Other Comments**

Not Applicable.

Should you have any further questions or concerns, please contact Ellie MehriLou, of this department at EMehrilou@citywindsor.ca.

LANDSCAPE ARCHITECT – RYAN UPTON

As requested, the materials for the file titled above have been reviewed. There are no objections or documents required from a Landscape Architecture perspective as it pertains to a Zoning By-law Amendment (ZBA).

The applicant shall attempt to preserve existing privately owned trees on the subject site. If trees within the subject site are to be removed for development, replacement trees at a like-for-like size ratio (cumulative dbh) are strongly encouraged for their various environmental benefits.

The Applicant is required to produce/provide the following documents (at the Site Plan Control stage):

- 1) Tree Inventory & Preservation Study
- 2) Landscape Plan – The purpose of a Landscape Plan is to determine landscape features proposed as part of the development. This Plan must be prepared by an OALA Landscape Architect, illustrating the tree preservation fencing location and details on the plans. Please see Site Plan Control – Application Support Material TOR for more information.
- 3) Photometric Plan and Light Fixture Data Sheets – These must be prepared by a qualified Lighting Engineer or Consultant. Please see Site Plan Control – Application Support Material TOR for more information.

If you have any questions or concerns, please feel free to contact me.

ENWIN

HYDRO ENGINEERING: Jerry Raniwsky

No Objection to Re-zoning based on the information provided.

Please note the following:

- Existing ENWIN 120/240V overhead hydro service triplex to residential dwelling at 3858 Peter Street

We recommend referring to the Occupational Health and Safety Act for minimum safe limits of approach during construction and the Ontario Building Code for adequate clearance requirements.

WATER ENGINEERING: Bruce Ogg

ENWIN Water has no objections to the rezoning.

DEVELOPMENT ENGINEERING – DANIEL LOPEZ

We have reviewed the subject Rezoning application and have the following comments:

Sewers

The site may be serviced by a 300mm sanitary sewer within the Peter St right-of-way. If possible, existing connections should be utilized. Any redundant connections shall be abandoned in accordance with the City of Windsor Engineering Best Practice B.P 1.3.3.

The applicant will be required to submit, prior to the issuance of permits, a stormwater management plan in accordance with Windsor Essex Region Stormwater Management Standards Manual, restricting stormwater runoff to predevelopment levels.

This will include, at a minimum:

- Submission of stormwater management review fee,
- Stormwater management report stamped by a professional engineer
- Site servicing drawings stamped by a professional engineer
- Stormwater management check list (see link below)

For more information of SWM requirements, visit: link

<https://essexregionconservation.ca/wp-content/uploads/2018/12/WE-Region-SWM-Standards-Manual.pdf>.

<https://www.citywindsor.ca/business/buildersanddevelopers/Documents/Checklist-Rational-Method.pdf>

Right of-Way

Peter St is classified as Local Residential Road according to the Official Plan requiring a right-of-way width of 20.10m; the current right-of-way is 20.10m therefore, no conveyance is required currently.

If you have any further questions or concerns, please contact Daniel Lopez, of this department at dlopez@citywindsor.ca

PARKS DEVELOPMENT – HODA KAMELI

The Parks Development has no comments to this liaison.

ECONOMIC DEVELOPMENT PLANNER - KEVIN ALEXANDER

All previous comments (from the Stage 2 Liaison) below apply regarding the siting of the building and compatibility with the area materials to be used through the Site Plan Control process.

As suggested in previous comments landscape setbacks and some building treatment can be included as part of the ZBA. The landscape setback from Peter Street is not appropriate based on the adjacent CK Mechanical Office setback area and the residential properties along Peter Street. Please see the example below:

That the following site-specific provisions shall apply to the subject lands:

2.1 Permitted Use:

- A one-storey warehouse, including accessory office and loading facilities, shall be permitted.

2.2 Site Design Requirements:

- The front façade of the warehouse shall be constructed using a combination of **brick and stone** materials.
- All windows on the building shall be **clear vision glass** (non-tinted, non-reflective).
- The site shall comply with all other applicable provisions of the MD1.2 zoning district under By-law 8600, unless otherwise specified herein.

2.3 Landscaping and Screening:

- A landscaped buffer shall be provided along any lot line abutting a residential or green space zone, in accordance with Section 5.2 of By-law 8600.
- An 8.0 metre landscape buffer shall be provided along the Peter Street property line to be consistent with the set back of the adjacent CK Mechanical Offices and the residential properties in the area.

Previous Comments

Previous Stage 1 (PC 129-24) apply, however if the Zoning By-law Amendment is approved and property rezoned from DRD1.1 zoning district to MD1.2 then the property is exempt from Demolition Control By-law 20-2007. As of January 15, 2024, existing dwelling units located on property zoned Manufacturing District (MD) in Zoning By-law 8600 are exempt from Demolition Control By-law 20-2007.

As identified in the Stage 1 Planning Consultation, the proposed warehouse does not reflect the character and materials used of the existing residential properties in the neighbourhood and the approved mixed-use 11 storey development to south and west of the subject property. If the warehouse is permitted, then the site should be consolidated with the adjacent CK Mechanical operation to the North and any additional parking required can be accommodated at the rear of the 3858 Peter Street to be consistent with the goals of the Sandwich CIP regarding front yard parking or parking areas adjacent the street.

Material proposed on the warehouse should be consistent with the residential properties in the area and with the existing CK mechanical building to the north. Façade materials proposed for the warehouse building should be at least partially stone/brick similar to the façade of the

existing CK Mechanical Building to the north. This requirement can be included as part of the zoning By-law Amendment.

Windows and door windows should be clear vision glass (not tinted) and there should be no lifestyle posters in the windows such as on the existing CK Mechanical Building to the north.

The landscape area distance adjacent Peter Street should be consistent with the residential properties along Peter Street and a combination of decorative fencing, low masonry walls, and vegetation should be used to screen parking areas from the street

The proposal should address the Sandwich Town Urban Design Guidelines for industrial properties (See Page 6-7 at the link below) if the zoning amendment is approved.

Sandwich UD Guidelines JN27 2011-FOR WEB

Although, details such as use of materials, window and door treatment, and landscaping can be dealt with through the Site Plan Control process it may be worth while to consider some of these requirements as part of the zoning by-law amendment to ensure some level of consistency between the residential, commercial, mixed-use, and industrial uses that exist in the immediate area.

Subject: Zoning By-Law Amendment - Z 021/25 (ZNG-7323) - 520 Grand Marais Rd. W - Ward 10

Reference:

Date to Council: November 3, 2025
Author: Averil Parent
Planner II - Development Review
519-255-6543 x6397
aparent@citywindsor.ca
Planning & Building Services
Report Date: 10/1/2025
Clerk's File #: Z/15052

To: Mayor and Members of City Council

Recommendation:

- I. THAT Zoning By-law 8600 **BE AMENDED** by adding the following zoning exception to Section 91.10:

11. **NORTH SIDE OF GRAND MARAIS ROAD WEST, BETWEEN BRUCE AVENUE AND AVONDALE AVENUE**

For the lands comprising of Part Lot 78, Concession 3, the following additional provisions shall apply:

- a) *Lot Width: minimum 11.8 m*

[ZDM 8; ZNG/7323]

- II. THAT Zoning By-law 8600 **BE FURTHER AMENDED** by changing the zoning of Part Lot 78, Concession 3, known municipally as 520 Grand Marais Road West (Roll No. 080-380-05700) situated on the north side of Grand Marais Road West between Bruce Ave and Avondale Ave, from RD1.4 to RD1.2x(11).

Executive Summary:

N/A

Background:

Application Information:

Municipal Address: 520 Grand Marais Rd. W. **Roll Number:** 080-380-05700

Ward: 10 **Planning District:** South Windsor **Zoning District Map:** 8

Applicant/Agent: Tracey Pillon-Abbs (Pillon-Abbs Inc.)

Owner: Yunus Orak

Proposal:

The applicant has requested to rezone the subject property from Residential District 1.4 (RD 1.4) to Residential District 1.2 (RD1.2) with reduced minimum lot width from 12 m to 11.8 m. This will facilitate the division of the existing residential parcel into two lots. The lot has 25.07 m frontage on Grand Marais Rd. W and has an area of 743 m².

The subject parcel is currently occupied by a single detached dwelling and accessory structures. The single detached dwelling is to remain, and the accessory structures are to be demolished. Parking for the retained lot will continue to be provided from Bruce Avenue. The severed lot is proposed to contain a single unit dwelling without ADUs. A total of 1 new dwelling unit is proposed. One parking space located within the front yard with access from Grand Marais Rd. W is proposed.

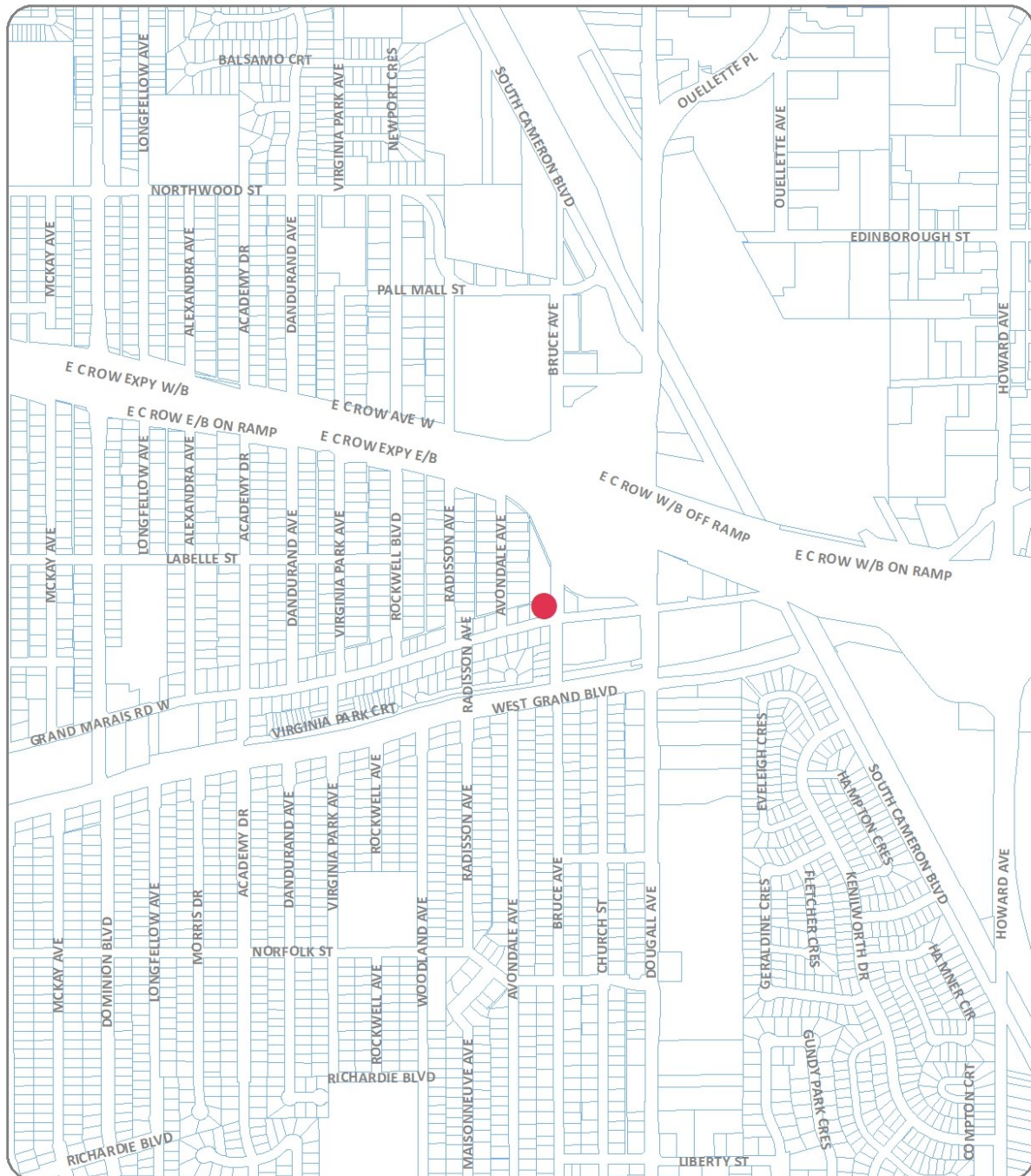
The applicant has submitted a conceptual site plan, elevations drawings and floor plans (Appendix A).

The development as proposed is not subject to Site Plan Control. All plans, elevations and renderings are conceptual and subject to change.

Site Information:

OFFICIAL PLAN	ZONING	CURRENT USE	PREVIOUS USE
Residential	Residential District 1.4 (RD1.4)	Single Unit Dwelling	N/A
LOT WIDTH	LOT DEPTH	LOT AREA	LOT SHAPE
25.07 m	30.48 m	743 m ²	Rectangular
<i>All measurements are provided by the agent and are approximate.</i>			

Figure 1: Key Map



KEY MAP - Z-021/25, ZNG/7323



● SUBJECT LANDS

Figure 2: Neighbourhood Map



NEIGHBOURHOOD MAP - Z-021/25, ZNG/7323



SUBJECT LANDS

Neighbourhood:

The subject parcel is located in a residential area consisting of low to medium density dwellings as well as a mix of commercial uses located along Dougall Avenue. See Appendix B for site images.

To the west is low density residential, namely single unit dwellings. South of the property are additional single unit dwellings, a condominium development, as well as the Grand Marais Drain. To the north there is open space as well as the exchange ramps of the EC Row Expressway and Dougall Avenue. Directly east of the subject property is a municipal park and further east are the commercial uses along Dougall Avenue. Schools in the area include EJ Lajeunesse catholic high school, Christ the King French Immersion Catholic School, and Glenwood Public School. The nearest library is the Windsor Public Library's Budimir Branch 1.2 km to the west.

Grand Marais Road West is classified as a Local Road in this area, with sidewalks and bike lanes on both sides of the street. *Schedule F: Roads and Bikeways* classifies Dougall Ave as a Class II Arterial Road, and West Grand Blvd as a Class II Collector Road. There is a Bikeway running along sections of the Grand Marais Drain. There are separated sanitary and storm sewers servicing the property however the storm sewer must be extended in order to service the proposed severed parcel.

The closest existing transit route to this property is Route 205. Route 205 has a peak weekday frequency of 15 minutes. The closest existing bus stop to this property is located on Dougall at West Grand southwest corner. This bus stop is approximately 390 metres from this property falling within Transit Windsor's walking distance guidelines of 400 metres to a bus stop. This will be maintained with Transit Windsor's City Council approved Transit Master Plan.

Figure 3: Subject Parcel – Rezoning



PART OF ZONING DISTRICT MAP 8

N.T.S.

REQUESTED ZONING AMENDMENT

Applicant: Yumus Orak



SUBJECT LANDS

PLANNING & BUILDING DEPARTMENT



DATE : SEPTEMBER, 2025
FILE NO. : Z-021/25, ZNG/7323

Discussion:

Planning Rationale Report (Pillon Abbs Inc. – September 2, 2025)

The Planning Rationale Report (PRR), attached as Appendix C, notes that *“the proposed density is appropriate for the Site and will be similar to the residential neighbourhood in the area.”* and that *“the design and style of the proposed building will blend well with the scale and massing of the existing surrounding area.”* The PRR further notes that *“the site has access to existing infrastructure and nearby public service facilities.”*

The PRR concludes that *“that the proposed development is consistent with the PPS, conforms with the intent and purpose of the OP and ZBL and represents good planning.”* The Planning Department generally concurs with the PRR.

Provincial Planning Statement (2024)

The Provincial Planning Statement (PPS) provides direction on matters of provincial interest related to land use planning and development and sets the policy foundation for regulating the development and use of land in Ontario.

Policy 2.1.6 states that *“complete communities”* shall be supported by *“accommodating an appropriate range and mix of land uses, housing options, ... and other uses to meet long-term needs”, “improving accessibility for people of all ages and abilities”, and “improving social equity and overall quality of life for people of all ages, abilities, and incomes”.*

The proposed severance and additional single unit dwelling will provide a form of housing that is appropriate in terms of the range and mix of the surrounding neighbourhood context. It represents a similar style housing on a smaller lot that will meet long-term needs, and improves accessibility, social equity & overall quality of life for people of all ages, abilities, & incomes. The proposed amendment is consistent with Policy 2.1.6 of the PPS.

Policy 2.2 requires that an *“appropriate range and mix of housing options and densities”* be provided by *“permitting and facilitating...all housing options required to meet the social, health, economic and well-being requirements of current and future residents...and all types of residential intensification ... development and introduction of new housing options within previously developed areas”.* The modest increase in density supports efficient use of land and infrastructure as well as the use of active transportation.

Policy 2.3.1 provides general policies for settlement areas. Growth should be focused in the settlement area. Land use patterns should be based on the efficient use of land and resources, optimizing existing and planned infrastructure, support active transportation, and be transit supportive.

The proposed rezoning to facilitate a future severance and new single unit dwelling represents a housing option that will meet the various requirements of current and future residents, is a type of residential infill development, and promotes the efficient use of land, resources, and infrastructure, and supports active transportation. Transit and active transportation are available in the neighbourhood. The proposed amendment is consistent with Policy 2.2 and Policy 2.3.1.

Policy 2.9 requires that the impacts of a changing climate and reduction in greenhouse gas emissions shall be supported with compact, transit-supportive, and complete communities, consider stormwater management systems, and promote active transportation.

The proposed rezoning to facilitate a future severance and single unit dwelling is a residential infill and intensification project, thereby not requiring settlement area expansion. It is a compact development with access to existing transit and active transportation and is located within an area that is serviced by municipal wastewater, stormwater and water services. The proposed amendment is consistent with Policy 2.9.

The proposed development represents an efficient development and land use pattern that will have no adverse impact on the financial well-being of the City of Windsor, land consumption and servicing costs, accommodates an appropriate range of residential uses, and optimizes existing investments in transit and infrastructure. There are no anticipated environmental or public health and safety concerns with the proposed use of land.

The agent indicates that the objectives of the PPS 2024 have been considered and have informed their professional planning opinion and concept design for the project site. These objectives are discussed in the PRR. Planning & Development Services generally concurs with this and is of the opinion that the proposed amendment to Zoning Bylaw 8600 is consistent with the PPS 2024.

City of Windsor Official Plan

The subject property is located within the South Windsor Planning District in *Schedule A – Planning Districts & Policy Areas* of Volume I.

The subject parcel has a Residential land use designation in *Schedule D – Land Use Plan*. Section 6.3.2.1 stipulates that low profile and medium profile dwelling units are permitted within the Residential land use designation. The proposed development of a new single unit dwelling on a smaller parcel of land is classified as a Low Profile development under Section 6.2.1.2 (a) and is compatible and complementary with the surrounding land uses (Section 6.3.2.5 (c)). The development is of a scale that is compatible with the profile and uses of the surrounding neighbourhood. No deficiencies in municipal physical and emergency services have been identified (Section 6.3.2.5 (e)). The proposed rezoning conforms to the policies in Sections 6.3.2 of the Official Plan.

The Official Plan has policies that encourage redevelopment and intensification. Objective 6.3.1.1 supports a complementary range of housing forms and tenures in all neighbourhoods. Objective 6.3.1.2 seeks to promote compact neighbourhoods and balanced transportation systems. Objective 6.3.1.3 seeks to promote selective residential redevelopment, infill and intensification initiatives. The proposed development represents a complementary and compact form of housing, redevelopment, and intensification that is located near active and public transportation. The proposed rezoning satisfies the objectives set out in Section 6.3.1 of the Official Plan.

The proposed amendment to Zoning By-law 8600 will conform to the general policy direction of the Official Plan. Planning and Development Services generally concurs with the Official Plan analysis in Section 5.1.2 of the PRR submitted by the Applicant.

Zoning By-Law 8600

The subject land is zoned Residential District 1.4 (RD1.4) which permits single unit dwellings and existing semi-detached and duplex dwellings. The applicant is requesting a rezoning to Residential District 1.2 (RD1.2) to facilitate a future severance and permit one new single unit dwelling on a smaller lot. This will allow for infilling and more housing in the existing built-up area.

The zoning review conducted by Planning Staff identified the need for a site specific exception for reduced minimum lot width from 12.0 m to 11.8 m for the retained lot. Planning Staff are of the opinion that the reduction in lot width will have no adverse impact on the proposed development or on surrounding uses. The recommendation includes this provision. The development is compliant with all other provisions in the RD1.2 category of the zoning By-law including the parking provisions.

The Applicant's request for a change in zoning with one site-specific provision is supported in principle. Section 5.1.3 of the PRR outlines that the proposed development complies with all other applicable zoning provisions. Planning and Development Services is of the opinion that there are no adverse impacts as a result of the proposed development.

Site Plan Control

The proposed development is not subject to Site Plan Control.

Risk Analysis:

N/A

Climate Change Risks

Climate Change Mitigation:

The subject land is located within an existing neighbourhood on existing municipal services, therefore reducing the impacts of climate change by locating within the existing built-up area. In general, residential intensification minimizes the impact on community greenhouse gas emissions as these developments create complete communities and neighbourhoods while using available infrastructure such as sewers, sidewalks, and public transit.

Climate Change Adaptation:

The new dwelling may be affected by climate change, in particular with respect to extreme precipitation and an increase in days above 30 degrees. While not the subject of this report, any new construction would be required to meet the current provisions of the Building Code, which would be implemented through the building permit process. The proposed development provides an opportunity to increase resiliency in the surrounding area through supporting a complementary and compact form of housing, redevelopment, and intensification that is near existing and future transit and active transportation options.

Financial Matters:

N/A

Consultations:

An Open House was held on August 29th, 2024 where members of the public could attend either in person or online. Notification of this meeting was distributed to residents and property owners within 200m of the subject site. In addition to the Applicant, applicant representatives and City Staff, 1 resident attended the open house. Details of the Open House are summarized in Section 3.2 of the PRR.

Comments received from municipal departments and external agencies are attached as Appendix D. Statutory notice was advertised in the Windsor Star. A courtesy notice was mailed to property owners within 200m of the subject lands. Submitted documents were posted on the City of Windsor [website](#).

Conclusion:

The *Planning Act* requires that a decision of Council in respect of the exercise of any authority that affects a planning matter, “*shall be consistent with*” Provincial Planning Statement 2024. Based on the supporting documents submitted by the Applicant and the analysis in this report, it is my opinion that the requested amendment to Zoning By-law 8600 is consistent with the PPS 2024 and is in conformity with the City of Windsor Official Plan.

The proposed amendment facilitates the severance of a parcel of land for the purpose of building one new single unit dwelling which is compatible with existing uses in the surrounding neighbourhood. The proposed development represents a modest increase in density and provides an opportunity for residential intensification, while also supporting a complementary form of housing located near multimodal transportation options.

The recommendation to amend Zoning By-law 8600 constitutes good planning. Staff recommend approval.

Planning Act Matters:

Averil Parent

Planner II – Development Review

I concur with the above comments and opinion of the Professional Planner.

Greg Atkinson, MCIP, RPP

Neil Robertson, MCIP, RPP

Deputy City Planner - Development

City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP RM

Approvals:

Name	Title
Greg Atkinson	Deputy City Planner – Development
Aaron Farough	Senior Legal Counsel, Legal and Real Estate
Neil Robertson	City Planner
Jelena Payne	Deputy CAO/Commissioner, Economic Development
Ray Mensour	Chief Administrative Officer

Notifications:

Name	Address	Email

Appendices:

- 1 Appendix A - Conceptual Site Plan, Elevations, Floor Plans
- 2 Appendix B - Site Photos
- 3 Appendix C - Planning Rational Report
- 4 Appendix D - Consultation

General:
Site plan generated is based upon municipal zoning information as obtained from the local building department where the project is to be constructed. Concept Studio Desings will not be responsible for determining other restrictions that are applied to the property (ie. easements, restrictive covenants etc.).

Upon receipt of the building permit, Concept Studio Desings is to be notified immediately, by the permit holder, of any issues or concerns the building department has noted on the permit set. If not notified, Concept Studio Desings is not responsible for any associated costs or damages.

Engineered shop drawings, for any "manufactured component" forming part of the building (ie. engineered roof trusses, engineered floor joist, timbers etc.), must be submitted to Concept Studio Desings for review and written approval. If engineered shop drawings are not submitted and approved, Concept Studio Desings is not responsible for any associated costs or damages.

Materials or construction procedures with are prohibited by law or shall cause a harmful effect to the natural environment or to the health of any person on the site during construction and/or during occupancy shall not be used in this project.

All trades shall conform with all the applicable federal, provincial & local codes, rules and regulations. In case of conflict, the most stringent requirement shall apply.

This building has been designed in accordance with the OBC 2012. A copy of the code book should be retained by the builder/general contractor for reference by the site construction personal. All construction shall conform to all requirements of the currant code.

Mechanical system design is by the mechanical contractor. Design and installation shall comply with parts 6, 9 and all other applicable parts of the OBC 2012

Plumbing systems design is by others. Design and installation shall comply with parts 7, 9 and all other applicable parts of the OBC 2012

These notes are for general reference only; where conflicts exist between these notes and current codes the more stringent requirements shall prevail.

Do not scale drawings; use printed dimensions only. If any discrepancy occurs, notify the designer and/or owner immediately for direction.

Soils:
When soil test are not provided, the soil bearing capacity is assumed to be 3000psf. Bearing material shall be dense sand or dence clay. If any other materials or lower bearing capacity are encountered notify the architect /engineer for re-evaluation of footing size.

Sump Pumps:
Provide sump pumps in basements if recommended by soil engineer or as required by local code official.

Foundation Notes:
Foundations and footings have been designed based on a minimum soil bearing capacity of 3,000 p.s.f.

Compressive strength of concrete after 28 days shall be at least 32 MPa for a garage and carport floors and all exterior flatwork. All concrete used for garage and carport floors and exterior steps shall have air entrainment of 5% to 8%. Concrete work and placement shall conform to the latest specification of C.R.S.I. and A.C.I.

Compressive strength of concrete after 28 days shall be at least 20 MPa for foundation walls.

Minimum footing depth shall be 4'-0" below finished grade.

Remove all fill and organic materials from areas to receive floor slabs. Prepare areas per soils engineer's recommendation.

All reinforcing bars, dowels, and ties shall conform to A.S.T.M A615 Grade 60. Reinforcing steel shall be continuous and shall have minimum 36 bar diameter lap, unless shown or noted. All reinforcing bars shall be deformed.

Provide temporary bracing as required to insure the stability of the structure until the permanent framing is in place.

All block shall be type N-1; mortar is to be type "N"; horizontal wire reinforcing shall be at 16" o.c. in all masonry walls.

Provide sill plate anchor bolts at 6'-0" o.c. (max.) and 12" from end of sill plates. Anchor bolts shall be ½" diameter (min.) and shall extend 15" (min.) into grouted concrete block or 8" (min.) into poured in place concrete footing or 8" into grouted concrete block plus 7" into poured in place concrete footing.

Provide 24" rigid insulation at all perimeter slab on grade conditions. See drawings for thickness.

Waterproof all brick, block and poured concrete walls at any below grade conditions unless directed otherwise by the soils engineer.

Provide 6 mil vapor barrier under all concrete slab on grade conditions and at all attached garage area concrete slabs.

All poured concrete walls to be backfilled with sandy type soil and be well braced until concrete is thoroughly cured and additional weight of the building is in place. Do not use frozen material for backfill.

Loading Conditions:			
	Live load	Dead load	Total
Floor habitable	40	15	55 p.s.f.
Floor with marble, stone or other hard finish material on grout bed.	40	35	75 p.s.f.
Wind load	30	25	25 p.s.f.
Roof pitched or flat	30	15	45 p.s.f.
Flat with ballust	30	25	55 p.s.f.

(Note): All floors were designed to a total load of 50 p.s.f. If a hard finish material in a grout bed is to be installed or other special loading conditions are anticipated consult designer for a structural analysis of the condition.

Trusses:
Floor truss manufacturer shall design and provide trusses to have a maximum deflection of 3/8" for spans greater than 16'-0" and 1/480 for spans under 16'-0".

Truss manufacturer shall be responsible for all truss designs including girders, hangers, bearing seats and anchors for trusses.

Truss framing shown on plans is for general reference and to indicate bearing locations. Manufacturer shall notify designer if additional bearing points and/or walls are needed prior to fabrication and erection.

All roof trussing shall be braced per manufacturer's recommendations or as required on drawings.

Framing & Materials:
Studs (bearing walls): Spruce-pine-fir, kiln dried, No. 2 or better.

Studs (non-bearing walls): Spruce-pine-fir, kiln dried, stud grade or better.

Joists, rafters, and headers: Fiber bending stress 1250 PSI elasticity Modulus 1,400,000 PSI or better.

Wall plates, non-structural blocking: Spruce-pine-fir, kiln dried, utility grade or better

Perimeter sill plates: Spruce-pine-fir, kiln dried, No. 2 or better.
Set perimeter sill plates on sill sealer.
Furring: Spruce-pine-fir, kiln dried, No.3 or better.

Use metal joist hangers only where joists hang from beams, walls or other supports. No joist angles allowed.
All structural hangers to be "Simpson" or approved equal

Floor Truss framing and TJI floor joist on drawings is designed for carpet, wood or ceramic tile floor finishes. If the floor material changes, notify the designer immediately for a structural redesign of the floor system to accommodate the dead load of the new floor material.

All micro lam beams are by Trus Joist MacMillan and are to be joined together per manufacturer printed specifications.

Provide 2 x 6 blocking at 16" o.c. between rim joist and header joist under all partitions parallel to floor framing direction. Provide solid bearing under all point load conditions to top of foundation wall on steel beam.to

Studs in all walls to be spaced 16" o.c. unless noted otherwise. All studs to be continuous from floor to upper floor or roof construction.

Bearing Walls:
Provide 2 x 4 solid blocking at 16" o.c. on 2 x 4 ledger boards between header joists (see drawings for size of member) under all in-line bearing partitions from floor above.

Provide solid blocking at all point load conditions continuous to solid bearing at headers or foundation.

Provide solid blocking at all bearing walls perpendicular to framing direction.

Wall framing:
Exterior wood framed walls over 9'-0" in height shall be of minimum 2 x 6 construction. All studs shall be continuous from floor to underside of floor or roof framing above.

All structural mullions to have minimum double stud construction continuous from floor to underside of floor or roof framing above. Window transom headers shall span between continuous studs with flush hanger brackets as required.

Provide continuous studs to underside of roof framing at all sloped ceiling conditions. (Balloon construction.)

Lower level (basement) exterior frame walls shall be minimum 2 x 6 framing at 16" o.c. with pressure treated base plate. Interior lower level bearing walls shall be 2 x 6 framing at 16" o.c.

Wall Sheathing
Provide structural grade OSB wall sheathing for lateral loading. When non-structural sheathing is used provide let-in diagonal wind bracing or other type of bracing at all exterior corners of structure.

Roofing:
Asphalt shingles shall not be installed on roof slopes below two units vertical in 12 units horizontal (2:12). Double-layer underlayment shall be required on roof slopes below four units vertical in 12 units horizontal (4:12). Single-layer underlayment is required on all other roof slopes. Asphalt shingles shall be secured to the roof with not less than four fasteners per strip shingle, or not less than two fasteners per individual shingle. Shingle headlap shall not be less than 2 inches (51mm). Installation at valleys, use "cut valley" method.

Roof Penetrations:
All plumbing, mechanical vent stacks and furnace flues shall be offset to rear roof lines. Flashing at all penetrations as required.

Attic Access:
A readily-accessible opening not less than 22" x 28" shall be provided to any attic area having a clear height of over 30".

Stairs:
All stairs shall conform to OBC section 9.8 for allowable riser height and tread depth. (Minimum 9 1/4" treads and maximum 7 7/8" risers in single family dwellings.)

Handrails shall be provided on at least one side of stairways of two (2) risers or more having a width of less than 44". Provide additional handrails as required by code on wider stairways.

Handrail to have a diameter size of 1 ½" min, 2" max.

All handrails shall be located at a height of 34" min. and 38" max. above nose of tread. The size and shape of handrails shall conform to current code requirements.

Guard rail:
Balusters shall be spaced so that a sphere with a diameter of 4 inches cannot pass through the opening.

Top of railings shall be a minimum of 42" high above finished floor or nose of stair tread. The space below a guard rail shall be constructed such that a sphere with a diameter of 6 inches shall not be able to pass through any opening.

Doors:
All doors shall be 6' - 8" high unless noted otherwise.
Doors between house and garage to be solid core fire rated steel door with automatic closer.

Windows and Glazing:
A minimum of one (1) window in each sleeping area shall meet emergency egress requirements. Window contractor shall provide egress hardware necessary to allow windows to meet applicable egress requirements.

Provide flashing at all window head, jamb, and sill conditions.

Fixed glass sizes shown are for reference only. Glazing contractor shall field measure all rough openings for fixed glass prior to fabrication.

Operating sash are shown for basic sizing only. Final size for rough opening and glazing shall be per selected window manufacturer's standards.

- Provide the appropriate safety glass (in accordance with all applicable building codes) for all hazardous locations listed below:
- Glazing in ingress and egress doors except wired glass in required fire doors and jalousies.
 - Glazing in fixed sliding panels of sliding type doors (patio and mail type).
 - Glazing in storm doors.
 - Glazing in all unframed swinging doors.
 - Glazing in shower and bathtub doors and enclosures.
 - Glazing, operable or inoperable, adjacent to a door in all buildings and within the same plane as the door whose nearest vertical edge is within twelve (12) inches of the door in a closed position and whose bottom edge is less than sixty (60) inches above the floor or walking surface.
 - Glazing in fixed panels having a glazed area in excess of nine (9) square feet with lowest edge less than eighteen (18" inches above the finished floor or walking surface within thirty-six (36) inches of such glazing. In lieu of safety glazing such glazed panels may be protected with a horizontal member not less than one and one half (1 ½) inches in width when located between twenty-four (24) and thirty-six (36) inches above the walking surfaces.

Insulation:
Provide insulation as required to meet OBC 2012 Code

Provide 24" wide rigid insulation at all perimeter slab on grade conditions. See drawings for thickness.

Provide minimum 6" batt insulation at all bond conditions (R=22)

Provide minimum 3 ½" batt insulation around all skylight shafts (R=11)

Thermal batt and blanket insulation shall have a kraft faced vapor barrier.

Insulation shall be installed in such a manner as to allow free air flow from the soffit to the roof space. Ventilation of concealed roof spaces shall be maintained.

Gypsum Board:
The garage shall be completely separated from the residence and its attic area by means of ½" gypsum board applied to the garage side.

Smoke Detectors:
Each sleeping area shall be provided with a minimum of one (1) smoke detector (local fire department approved and Underwriter's Laboratories listed and labeled) installed adjacent to the sleeping area. The smoke detector shall be installed in accordance with all applicable codes. Where more than one (1) detector is required to be installed within an individual dwelling unit, the detectors shall be wired in such a manner that the actuation of one (1) alarm will actuate all the alarms in the individual unit. At least one alarm shall be provided at each floor.

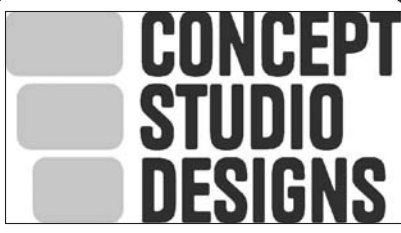
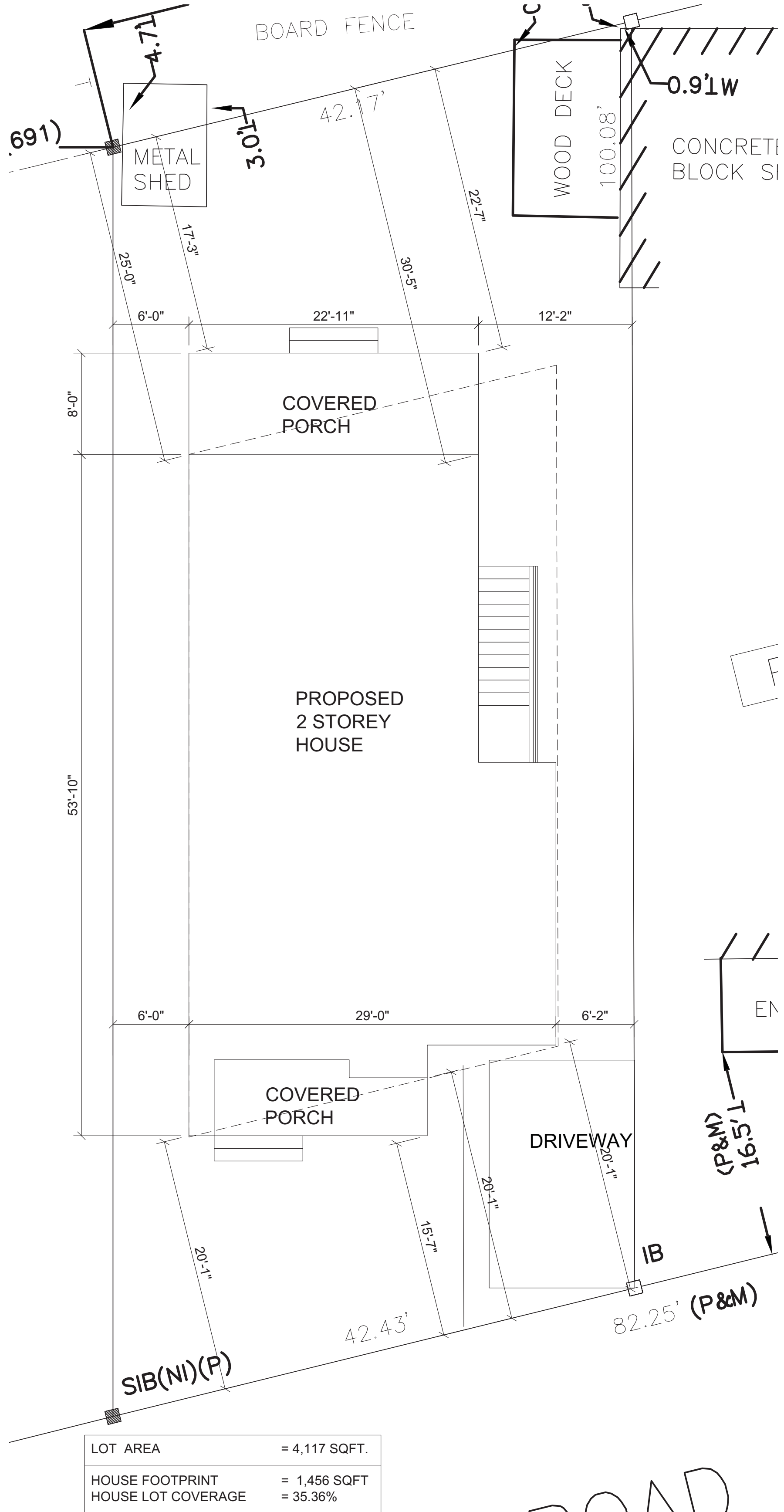
Electrical:
Electrical system design is by others. Design and installation shall comply to Ontario electrical safety code - current addition

Bedroom outlets - All Branch circuits that supplay 125 Volts, single phase, 15 and 20 amp outlets installed in dwelling units bedrooms shall be protected by an arc-fault circuit interrupter listed to provide protection of the entire branch circuit

Corridor and Stairway Lighting
Electrical contractor shall provide fixtures with adequate illumination to meet the required foor candle levels at first floor and stair treads per code

Contractor may provide additional fixtures not shown on plan to meet these requirements

The controls for activation of the stairway lighting shall be operable from he top and bottom of the stairway without traversing any step of the stair. All switches that control interior stairway lighting shall be illuminated switches (Controls)



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Date	ISSUED FOR

Project Name and Address
ORAK CONSTRUCTION
NEW 2 STOREY HOUSE
GRAND MARIS RD W, WINDSOR

Project Number	25-010
CAD DWG FILE	-
DRAWN BY	C.S.
CHK'D BY	C.S.
Scale	AS NOTED

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ORAK CONSTRUCTION
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Project Number	25-010
CAD DWG FILE	--
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MAIN FLOOR PLAN

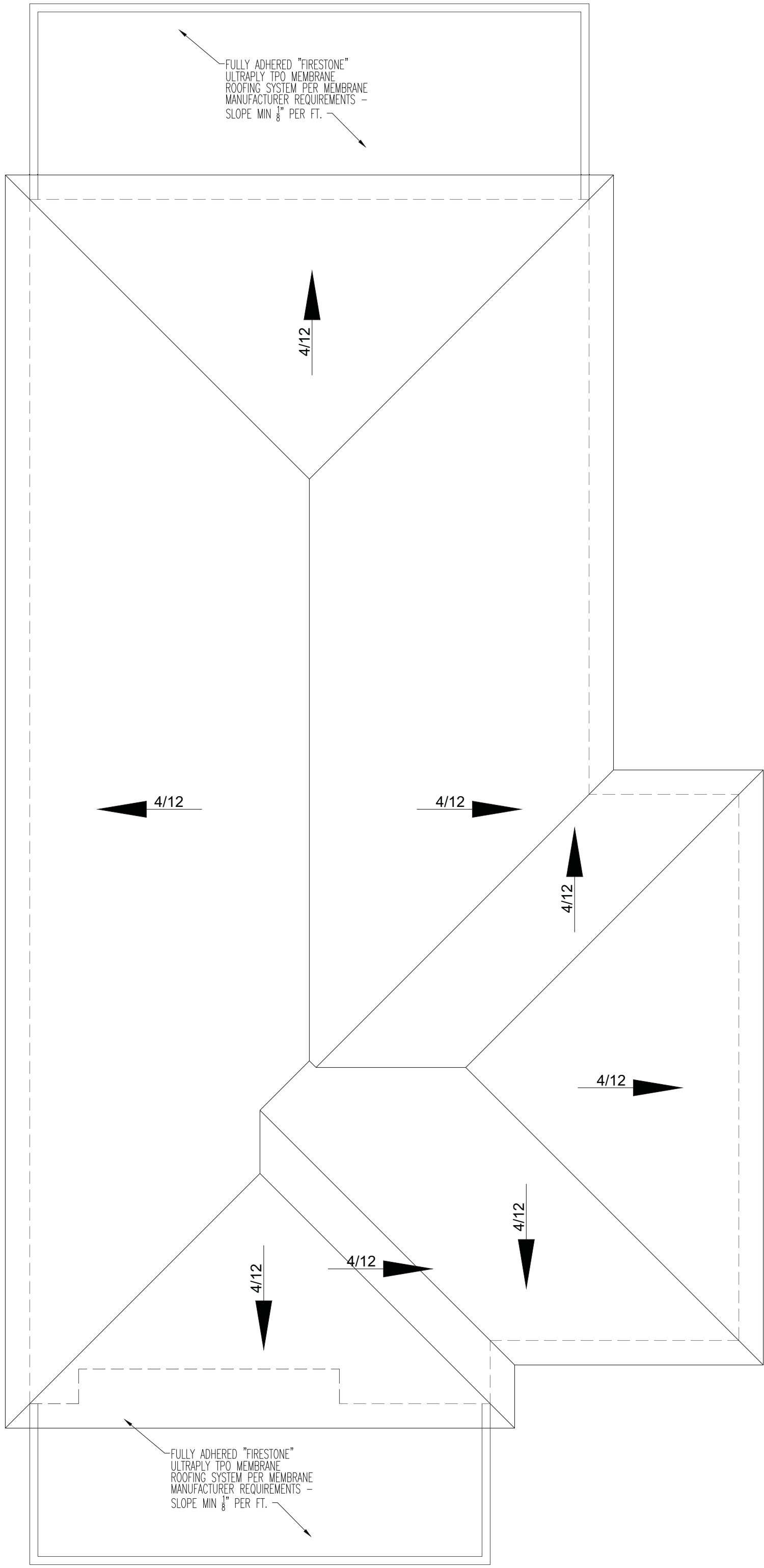
LINTEL SIZE	ROOF + CEILING	ROOF + CEILING 1 STORY	ROOF + CEILING 2 STORY
2-2"x4"	3'-0"	2'-5"	2'-1"
2-2"x6"	4'-5"	3'-3"	2'-10"
2-2"x8"	5'-4"	3'-11"	3'-5"
2-2"x10"	6'-7"	4'-9"	4'-2"
2-2"x12"	7'-7"	5'-5"	4'-9"

LINTEL SIZE	ROOF + CEILING	ROOF + CEILING 1 STORY	ROOF + CEILING 2 STORY
2-2"x4"	4'-1"	3'-5"	3'-0"
2-2"x6"	6'-4"	4'-10"	4'-6"
2-2"x8"	7'-8"	5'-11"	5'-4"
2-2"x10"	9'-5"	7'-3"	6'-6"
2-2"x12"	10'-11"	8'-5"	7'-6"

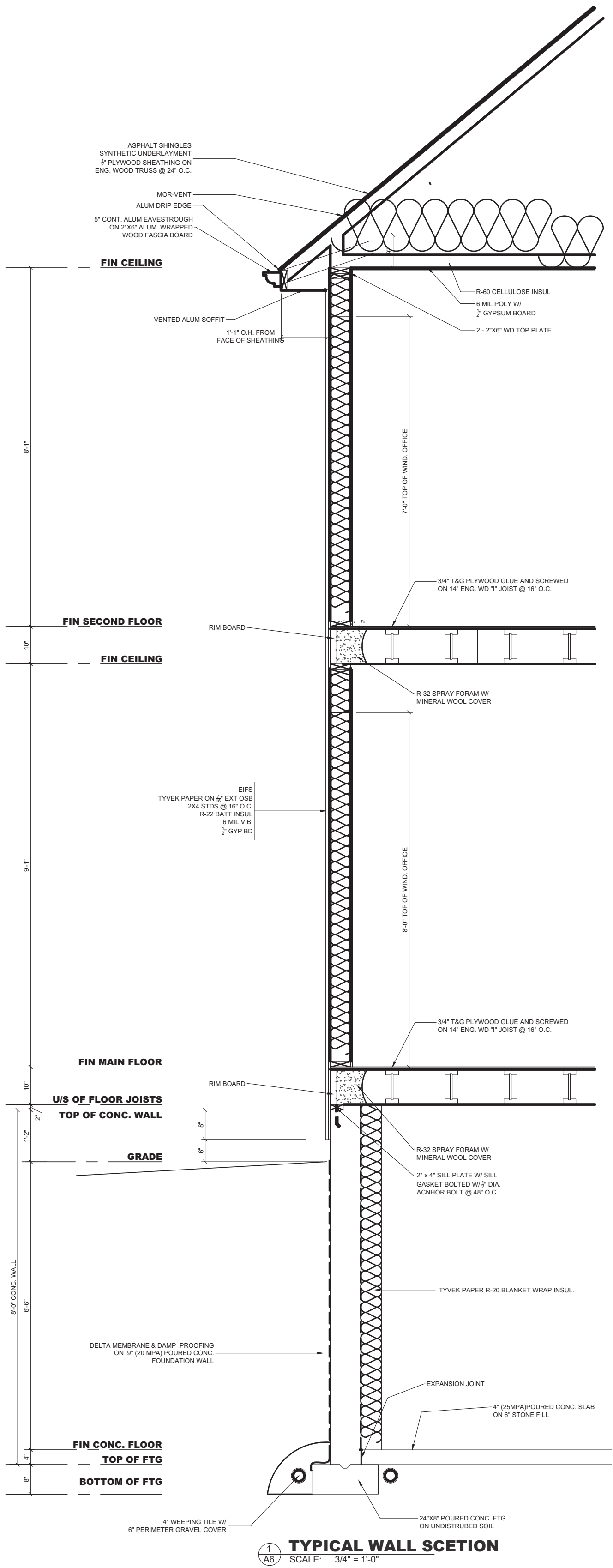
STEEL LINTEL SCHEDULE	
MAX CLEAR SPAN	LINTEL SIZE
5'-0" OR LESS	3 1/2" X 3 1/2" X 1/8"
7'-0" OR LESS	4 X 3 1/2" X 1/8"
8'-0" OR LESS	5" X 3 1/2" X 1/8"
9'-0" OR LESS	5" X 3 1/2" X 1/8"
10'-0" OR LESS	6" X 3 1/2" X 1/8"



SCALE: $\frac{1}{4}" = 1'-0"$



ROOF PLAN
SCALE: 3/4" = 1'-0"



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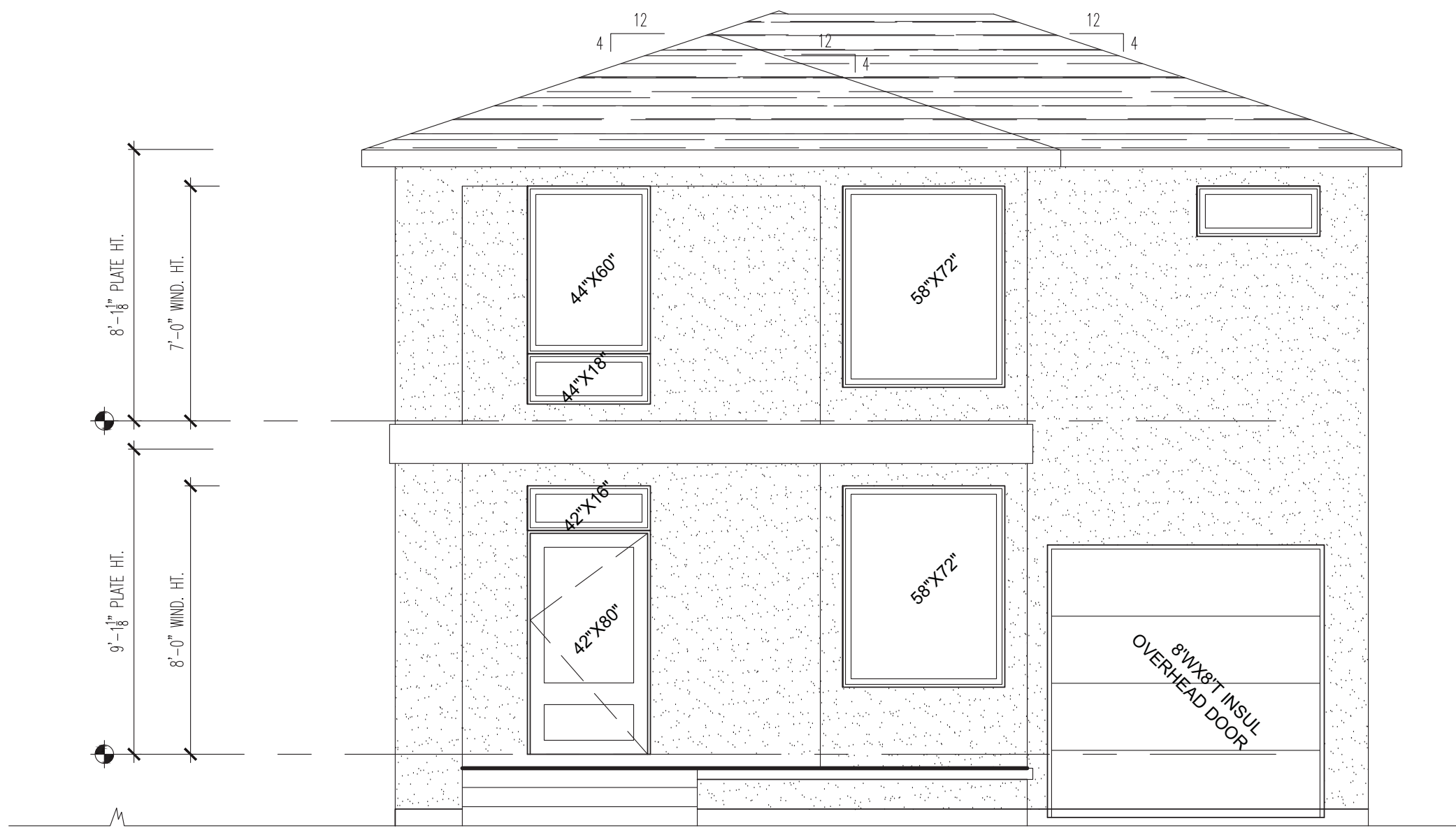
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Date	ISSUED FOR

Project Name and Address
ORAK CONSTRUCTION
NEW 2 STOREY HOUSE
GRAND MARIS RD W, WINDSOR

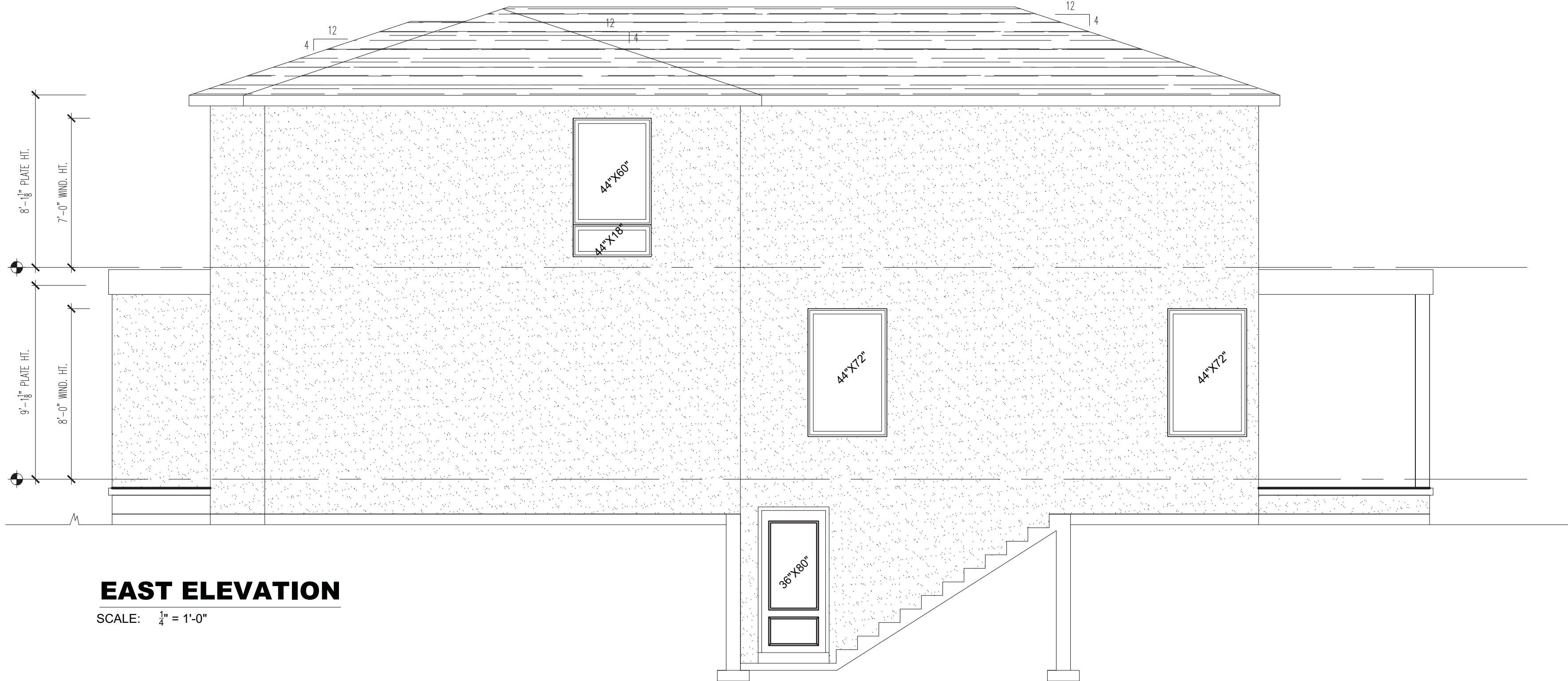
Project Number
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SOUTH ELEVATION

SCALE: $\frac{1}{4}" = 1'-0"$



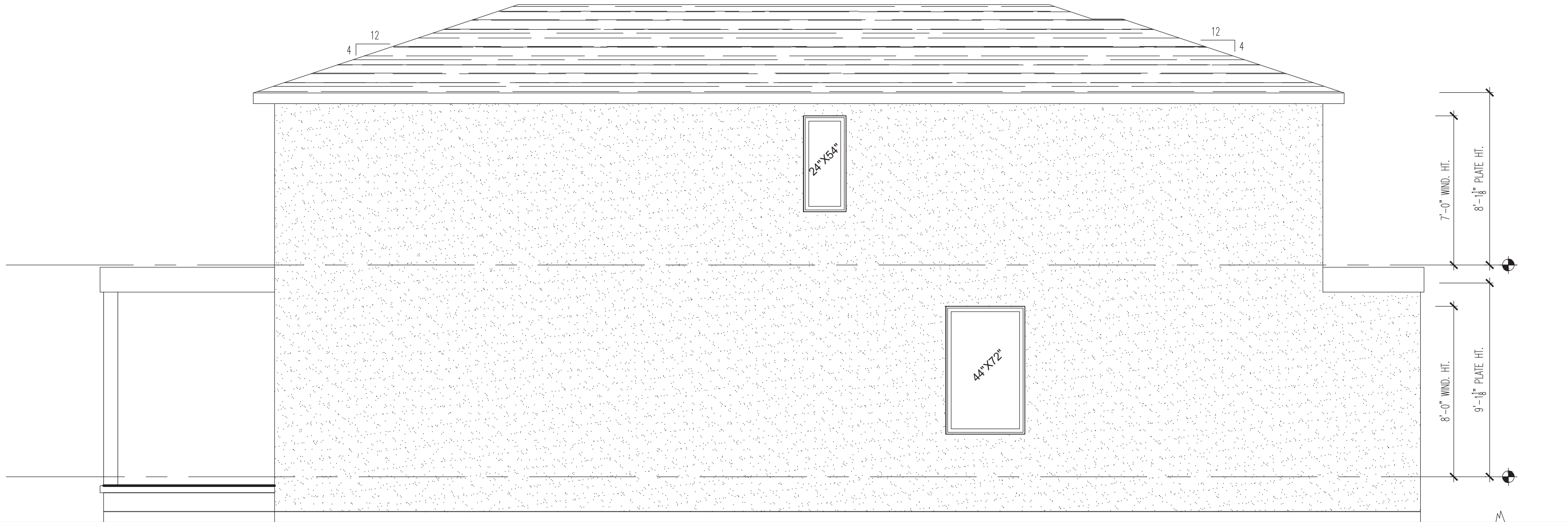
EAST ELEVATION

SCALE: $\frac{1}{4}" = 1'-0"$



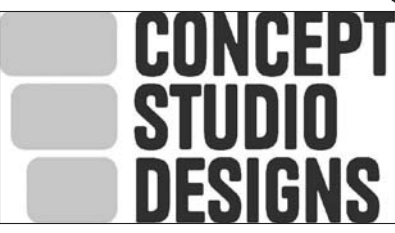
NORTH ELEVATION

SCALE: $\frac{1}{4}" = 1'-0"$



WEST ELEVATION

SCALE: $\frac{1}{4}" = 1'-0"$



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Date	ISSUED FOR

Project Name and Address

RAMI
NEW 2 STOREY HOUSE
LOT 2 SUMMIT ST, LAKESHORE ON

Project Number

24-004

CAD DWG FILE

--

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APPENDIX B – SITE PHOTOS



View of subject land (520 Grand Marais Rd. W.)



View opposite from the subject land



East along Grand Marais Rd. W. next to subject site



East along Grand Marais Rd.W. across from subject site



West along Grand Marais Rd. W. next to subject site



West along Grand Marais Rd.W. across from subject site

PLANNING RATIONALE REPORT

ZONING BY-LAW AMENDMENT PROPOSED RESIDENTIAL DEVELOPMENT

**520 Grand Marais Road West
Windsor, Ontario**

September 02, 2025

Prepared by:



Tracey Pillon-Abbs, RPP
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1.0 INTRODUCTION

I have been retained by Deniz Orak on behalf of Yunus Orak (herein the “Applicant”) to provide a land use Planning Rationale Report (PRR) in support of a proposed residential development to be located at 520 Grand Marais Road West (herein the “Site”) in the City of Windsor, Province of Ontario.

The Site, in Ward 10 (South Windsor Planning District), is made up of one corner parcel of land, which currently has a single detached dwelling and accessory structures.

The single detached dwelling is to remain, and the accessory structures are to be demolished.

The Site is located on the north side of Grand Marais Road West, east of Avondale Ave and west of Bruce Avenue.

It is proposed that the Site be redeveloped for residential purposes

The Applicant proposes the division of the existing residential parcel into two lots, in the following manner:

- Lot 1 is proposed to contain a new 2 storey single unit dwelling, and
- Lot 2 would contain the existing residential dwelling.

The Site has access to full municipality services.

Pre-consultation (stage 1) was completed by the Applicant (City File #PC-011/24). Comments dated May 22, 2024, have been received and included in this PRR.

Pre-submission (stage 2) was completed by the Applicant (City File #PC-009/25). Comments dated February 11, 2025, have been received and included in this PRR.

A site-specific Zoning By-law Amendment (ZBA) is required in support of the proposed development in addition to the required support studies.

The purpose of the ZBA is to change the zoning to permit a new single unit dwelling, on a smaller lot.

Once the ZBA has been approved, the Applicant will proceed with a Consent (CON) application to sever the Site.

The purpose of this report is to review the relevant land use documents, including the Provincial Planning Statement 2024 (PPS), the City of Windsor Official Plan (OP), and the City of Windsor Zoning By-law (ZBL), as they pertain to the ZBA application.

This PRR will show that the proposed development is a suitable development, is consistent with the PPS, conforms to the intent and purpose of the OP and ZBL, and represents good planning.

2.0 SITE AND SURROUNDING LAND USES

2.1 Legal Description and Ownership

The Site, in Ward 10 (South Windsor Planning District), is made up of one corner parcel of land and has been owned by Yunus Orak since 2023.

The Site is located on the north side of Grand Marais Road West, east of Avondale Ave and west of Bruce Avenue (see the area outlined in **yellow** on Figure 1 – Site Location).

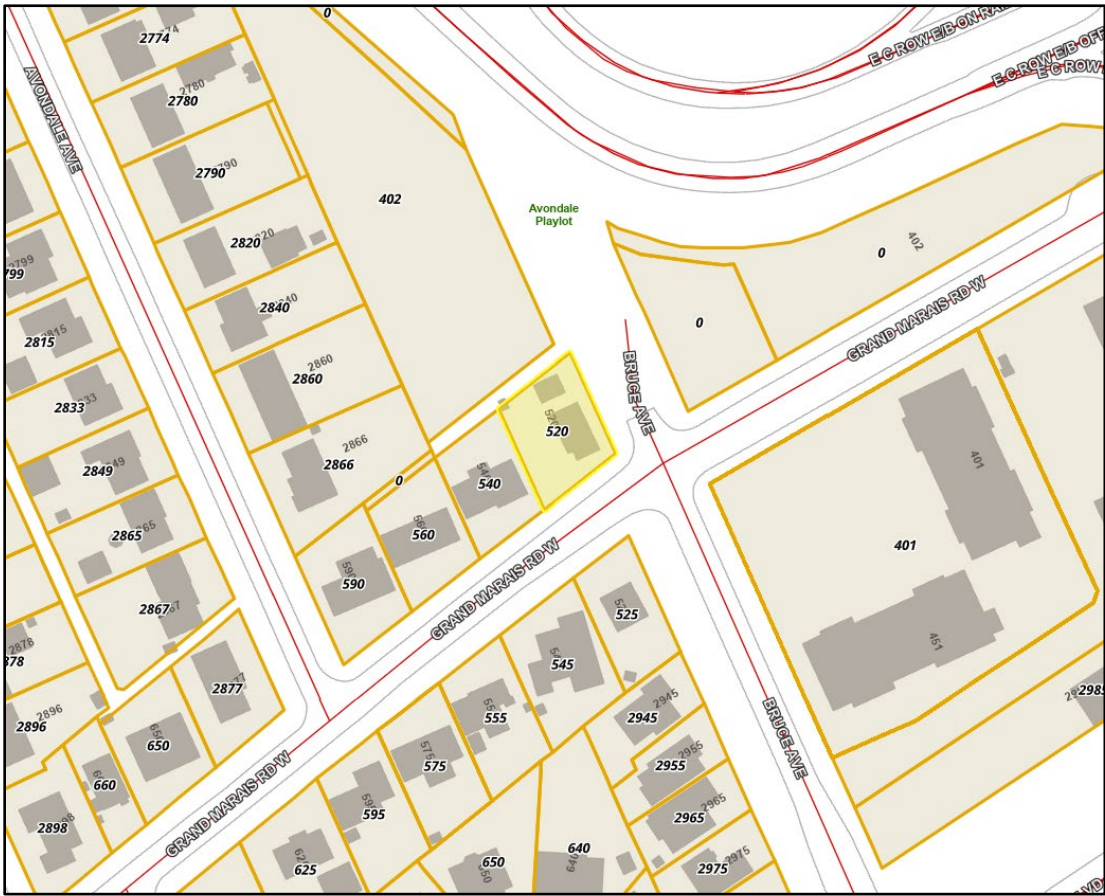


Figure 1 – Site Location (Source: Windsor GIS)

The Site is owned, locally known as and legally described as follows:

Address	Legal Description	PIN	ARN
520 Grand Marais Road West, City of Windsor, Province of Ontario	PT LT 78 CON 3 SANDWICH WEST AS IN R1261841; WINDSOR	01311-1112 (LT)	080-380-05700

2.2 Physical Features of the Site

2.2.1 Size and Site Dimension

The entire Site, subject to the proposed development, consists of a total area of 743 m², with a frontage of 25.07 m along Grand Marais Road West and a depth of 30.48 m along Bruce Avenue.

2.2.2 Existing Structures and Previous Use

The Site currently has a single detached dwelling and accessory structures.

The single detached dwelling is to remain, and the accessory structures are to be demolished.

The previous use was agriculture.

2.2.3 Vegetation

The property currently has a mown lawn, landscaping and trees.

2.2.4 Topography

The Site is generally level, and it is outside of the Essex Region Conservation Authority (ERCA) regulated area.

2.2.5 Other Physical Features

Fencing is located along a portion of the Site.

There is an alley located to the north of the Site.

2.2.6 Municipal Services

The Site has access to municipal water, storm, and sanitary services.

Transit is also available. The Site is close to major roadways.

2.2.7 Nearby Amenities

There are many schools, parks, places of worship and libraries in close proximity to the Site.

There is nearby shopping in the form of plazas and malls, medical facilities, as well as employment, and local amenities.

2.3 Surrounding Land Uses

Overall, the Site is located in an established residential neighbourhood.

The proposed dwelling will be infilled between two existing single detached dwellings in a neighbourhood that is comprised of single, detached residences and other residential densities such as the medium density, three storey apartment building to the south-east.

There are residential uses on the south and west of the Site. North of the site is E.C. Row Expressway, and east of the Site is Acondale Park.

There are several different lot frontages and lot areas of neighbouring properties.

3.0 PROPOSAL AND CONSULTATION

3.1 Development Proposal

The Site, in Ward 10 (South Windsor Planning District), is made up of one corner parcel of land, which currently has a single detached dwelling and accessory structures.

The single detached dwelling is to remain, and the accessory structures are to be demolished.

It is proposed that the Site be redeveloped for residential purposes

The Applicant proposes the division of the existing residential parcel into two lots.

The purpose of the ZBA is to change the zoning to permit a single unit dwelling, on a smaller lot.

A draft reference plan has been prepared to illustrate the division of the lots (see Figure 2a – Proposed Severance).

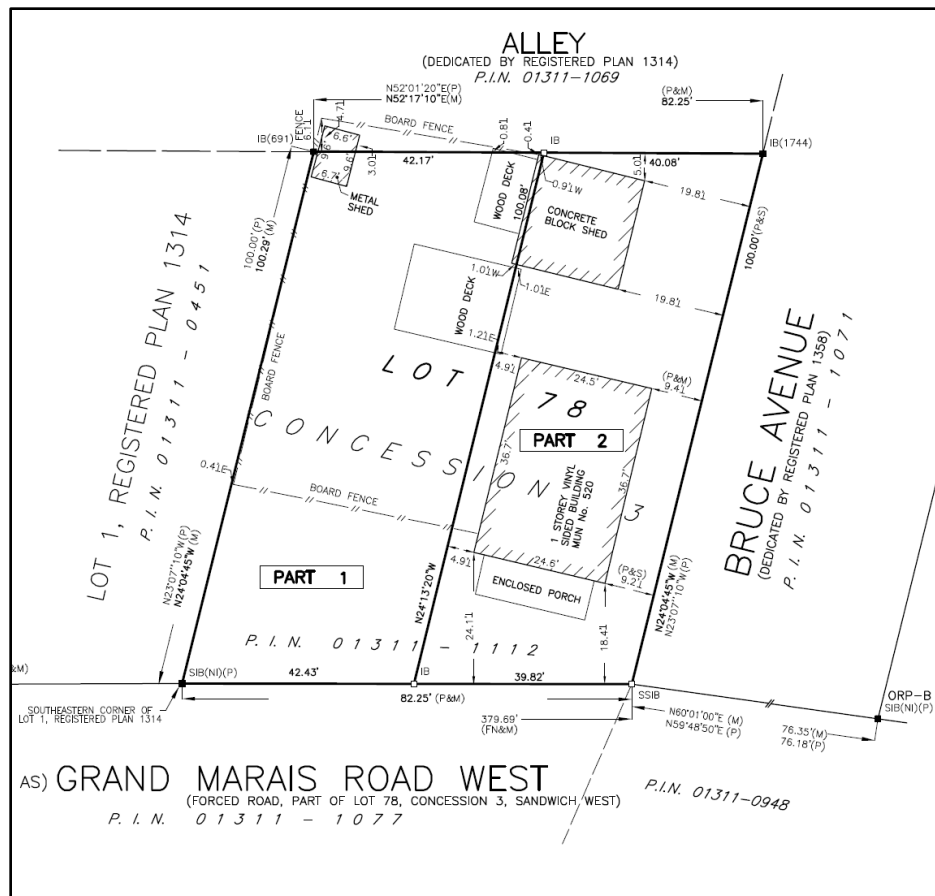


Figure 2a –Proposed Severance

Lot 1 is proposed to contain a new 2 storey single unit dwelling (shown as Part 1 on the draft reference plan). The proposed lot will have 12.93 m of frontage along Grand Marais Road west and an irregular depth.

Lot 2 would contain the existing residential dwelling (shown as Part 2 on the draft reference plan). The proposed lot will have 12.14 m of frontage along Grand Marais Road west and an irregular depth of 30.48 m along Bruce Avenue.

A concept plan has been prepared to illustrate how the severed parcel will be developed (see Figure 2b –Concept Plan).

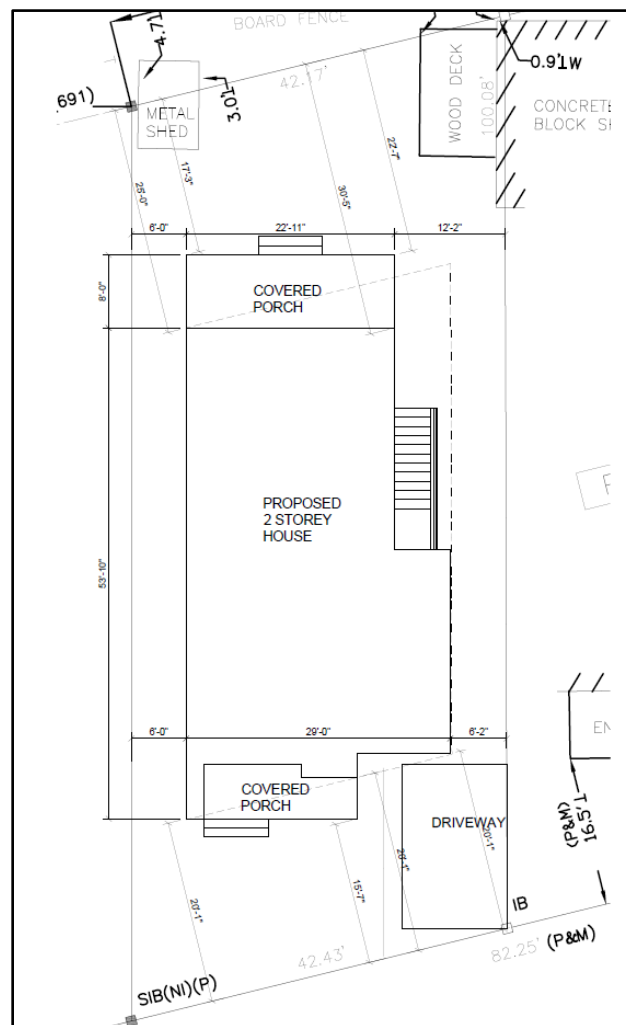


Figure 2b –Concept Plan

The proposed new dwelling will have a building footprint of 135.27 m² and includes the dwelling, covered porch and attached garage.

No Additional Dwelling Units (ADU) are proposed.

Elevations of the proposed building have been prepared (see Figure 2c – Elevations).

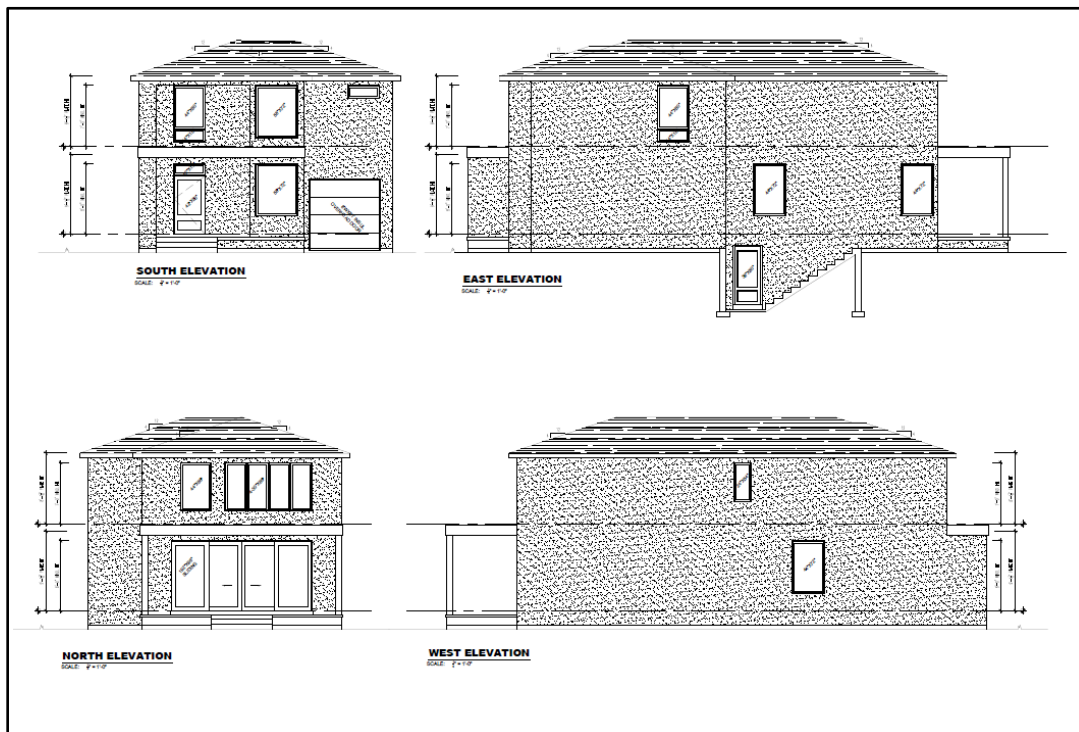


Figure 2c – Elevations

Parking will be located in private garages as well as on a paved driveway.

The existing metal shed, garage and wood deck are to be removed.

Amenity areas, paved sidewalks and landscaping will be provided.

The Site has access to full municipality services, including water, sanitary sewers and storm sewers.

As part of the building permit application, the Applicant will provide detailed drawings which will include final driveway approaches and sewer connections.

The Applicant does not intend to apply to close the existing alley adjacent to the Site.

3.2 Public Consultation Strategy

In addition to the statutory public meeting, the *Planning Act* requires that the Applicant submit a proposed strategy for public consultation with respect to an application as part of the complete application requirements.

As part of a public consultation strategy, in addition to the statutory public meeting, an informal electronic public open house was held with area residents and property owners on August 29, 2024, from 5:00 pm to 6:00 pm.

A total of **130 notices** were sent.

The open house provided members of the public **200 m** radius of the Site with opportunities to review and comment on the proposed development.

There was one person who attended the virtual open house. They live on Radisson Court West and spoke about all new low, medium and high density residential development being a problem. Their interest was in stopping all new development and putting a freeze on any new residential growth.

As a response, it was explained how the proposed development is a suitable development, is consistent with the PPS, conforms to the intent and purpose of the OP and ZBL, and represents good planning.

4.0 APPLICATION AND STUDIES

Pre-consultation (stage 1) was completed by the Applicant (City File #PC-011/24). Comments dated May 22, 2024, have been received and included in this PRR.

Pre-submission (stage 2) was completed by the Applicant (City File #PC-009/25). Comments dated February 11, 2025, have been received and included in this PRR.

The following explains the purpose of the application as well as a summary of the required support studies.

4.1 Zoning By-law Amendment

A site-specific Zoning By-law Amendment (ZBA) is required in support of the proposed development.

The current zoning of the Site, subject to development, is Residential District 1.4 (RD1.4) category, as shown on Map 8 of the City of Windsor Zoning By-law #8600.

It is proposed to amend the existing zoning to a site specific Residential District 1.2 (RD1.2 - S.20(1)(XXX)) category to permit the proposed development of a new single unit dwelling.

No relief from the RD1.2 zoning provisions is requested.

The ZBA is detailed, and the justification is set out in Section 5.1.3 of this PRR.

4.2 Other Applications

Once the ZBA application has been approved, the Applicant will proceed with a building permit.

Once the ZBA has been approved, the Applicant will proceed with a Consent (CON) application to sever the Site.

The Site is not subject to Site Plan Control (SPC).

4.3 Supporting Studies

The following supporting studies have been identified as required as part of the application.

4.3.1 Archaeology

A Stage 1 and Stage 2 Archaeology Assessment was prepared by AMICK Consultants Limited, dated September 2, 2025.

The purpose of the study was to assess whether there were any heritage resources that impacted the Site.

The entire Site was subject to the assessment.

It was concluded that no archaeological resources were encountered, and that no further archaeological assessment of the Site was warranted.

The report has been filed with the Ministry (MCM File Number 0024057).

5.0 PLANNING ANALYSIS

5.1 Policy and Regulatory Overview

5.1.1 Provincial Planning Statement

The Provincial Planning Statement, 2024 (PPS) provides policy direction on matters of provincial interest related to land use planning and development.

The PPS was issued under Section 3 of the Planning Act and came into effect on October 20, 2024.

Decisions under the Planning Act must conform to the PPS.

The following provides a summary of the key policy considerations of the PPS as it relates to the proposed development.

PPS Policy #	Policy	Response
Chapter 1 - Vision	Ontario will increase the supply and mix of housing options, addressing the full range of housing affordability needs. Every community will build homes that respond to changing market needs and local demand. Providing a sufficient supply with the necessary mix of housing options will support a diverse and growing population and workforce, now and for many years to come.	The proposed development provides more housing.
Chapter 2.1.4 – Buildings Homes, Sustaining Strong and Competitive Communities	To provide for an appropriate range and mix of housing options and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall: a) maintain at all times the ability to accommodate	The proposed development will help provide more housing to meet the needs of the City. Full municipal services are available.

PPS Policy #	Policy	Response
	residential growth for a minimum of 15 years through lands which are designated and available for residential development; and b) maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned, including units in draft approved or registered plans.	
2.1.6	Planning authorities should support the achievement of complete communities by: a) accommodating an appropriate range and mix of land uses, housing options, transportation options with multimodal access, employment, public service facilities and other institutional uses (including schools and associated child care facilities, long term care facilities, places of worship and cemeteries), recreation, parks and open space, and other uses to meet long-term needs; b) improving accessibility for people of all ages and abilities by addressing land use barriers which restrict their full participation in society;	The proposed development is consistent with the policy to achieve complete communities, as the Site is located in an existing built up area. The proposed development will provide for more housing. The Site has access to transportation options, public service facilities, other institutional uses, and parks. Accessibility will be addressed at the time of the building permit.
2.2.1 - Housing	Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected needs of current and future residents of the regional market area by:	The proposed development is new housing for the area. The proposed development supports the City's housing targets.

PPS Policy #	Policy	Response
	a) establishing and implementing minimum targets for the provision of housing that is affordable to low and moderate income households, and coordinating land use planning and planning for housing with Service Managers to address the full range of housing options including affordable housing needs; b) permitting and facilitating: 1. all housing options required to meet the social, health, economic and well being requirements of current and future residents, including additional needs housing and needs arising from demographic changes and employment opportunities; and 2. all types of residential intensification, including the development and redevelopment of underutilized commercial and institutional sites (e.g., shopping malls and plazas) for residential use, development and introduction of new housing options within previously developed areas, and redevelopment, which results in a net increase in residential units in accordance with policy 2.3.1.3; c) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation; and	The needs of the residents can be accommodated as the Site is located near local amenities. The Site offers an opportunity for gentle infilling. The proposed density is appropriate for the Site and will be similar to the residential neighbourhood in the area. Residents will have access to nearby transit.

PPS Policy #	Policy	Response
	d) requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations.	
2.3.1.1 – Settlement Area	Settlement areas shall be the focus of growth and development. Within settlement areas, growth should be focused in, where applicable, strategic growth areas, including major transit station areas.	The Site is located in an existing settlement area.
2.3.1.2	Land use patterns within settlement areas should be based on densities and a mix of land uses which: a) efficiently use land and resources; b) optimize existing and planned infrastructure and public service facilities; c) support active transportation; d) are transit-supportive, as appropriate.	The Site offers an opportunity for infilling as the Site is large and underutilized. The total density of the proposed development is considered appropriate. The design and style of the proposed building will blend well with the scale and massing of the existing surrounding area. The proposed development is an efficient use of the Site. Residents will have immediate access to local amenities. Transit is available in the area. The Site is located close to major roadways.
2.3.1.3	Planning authorities shall support general intensification and redevelopment to support	The proposed development provides an infill opportunity for an existing parcel of land.

PPS Policy #	Policy	Response
	the achievement of complete communities, including by planning for a range and mix of housing options and prioritizing planning and investment in the necessary infrastructure and public service facilities.	The Site was always intended for residential use. The design of the proposed development will provide a compact form.
2.3.1.4	Planning authorities shall establish and implement minimum targets for intensification and redevelopment within built-up areas, based on local conditions.	The City has established targets. The proposed development will assist in meeting those targets as the Site is located in an existing built-up area and will add new residential housing.
2.3.1.6	Planning authorities should establish and implement phasing policies, where appropriate, to ensure that development within designated growth areas is orderly and aligns with the timely provision of the infrastructure and public service facilities.	The Site has access to existing infrastructure and nearby public service facilities.
Chapter 3.1.1 – Infrastructure and Facilities	Infrastructure and public service facilities shall be provided in an efficient manner while accommodating projected needs.	The proposed development has access to full municipal services. There are nearby public service facilities.
3.3.3 - Transportation	Planning authorities shall not permit development in planned corridors that could preclude or negatively affect the use of the corridor for the purpose(s) for which it was identified.	The proposed development will not have a negative impact on nearby transportation and infrastructure corridors.
3.6.2	Municipal sewage services and municipal water services	The proposed development will be serviced by municipal

PPS Policy #	Policy	Response
	are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. For clarity, municipal sewage services and municipal water services include both centralized servicing systems and decentralized servicing systems.	sewer, water and storm, which is the preferred form of servicing for settlement areas.
4.2 - Water	Planning authorities shall protect, improve or restore the quality and quantity of water by: b) minimizing potential negative impacts, including cross-jurisdictional and cross-watershed impacts;	The Site is outside of the ERCA regulated area.
4.6.2 - Cultural Heritage and Archaeology	Planning authorities shall not permit development and site alteration on lands containing archaeological resources or areas of archaeological potential unless the significant archaeological resources have been conserved.	A Stage 1 and 2 assessment has been completed and summarized in Section 4.3.1 of this PRR. Engagement was also included during the assessment.
5.1.1 – Protecting Public Health and Safety	Development shall be directed away from areas of natural or human-made hazards where there is an unacceptable risk to public health or safety or of property damage, and not create new or aggravate existing hazards.	There are no natural or human-made hazards that apply to this Site. There is no risk to the public.

Therefore, the proposed development is consistent with the PPS and the Province's vision for long-term prosperity and social well-being.

5.1.2 Official Plan

The City of Windsor Official Plan (OP) was adopted by Council on October 25, 1999, approved in part by the Ministry of Municipal Affairs and Housing (MMAH) on March 28, 2000, and the remainder approved by the Ontario Municipal Board (OMB) on November 1, 2002. The office consolidation version is dated September 7, 2012.

The OP implements the PPS and establishes a policy framework to guide land use planning decisions related to development and the provision of infrastructure and community services throughout the City.

The current land use designation of the Site, subject to development, is 'Residential', as shown on Schedule D: Land Use of the City of Windsor Official Plan.

The Site is also subject to the following:

- Schedule A: Planning Districts & Policy Areas – South Windsor Planning District
- Schedule B: Greenway System – Located on a Recreationway (Grand Marais Rd W)
- Schedule C: Development Constraint Areas: Located north of a floodplain area
- Schedule C-1: Development Constraint Areas: Archaeological Potential – Designated within “Low Archaeological Potential”
- Schedule F: Roads & Bikeways – Located on a Class II Collector Road and Bikeway (Grand Marais Rd W)

It is proposed that the Site be maintained under the same land use designation.

The following provides a summary of the key policy considerations of the OP as it relates to the proposed development.

OP Policy #	Policy	Response
6.2.1.2 - General Policies, Type of Development Profile	For the purpose of this Plan, Development Profile refers to the height of a building or structure. Accordingly, the following Development Profiles apply to all land use designations on Schedule D: Land Use unless specifically provided elsewhere in this Plan: (a) Low Profile developments are buildings or structures generally no greater than three (3) storeys in height; (b) Medium Profile	The proposed development is considered low profile and is consistent in design with existing residences in the neighbourhood.

OP Policy #	Policy	Response
	developments are buildings or structures generally no greater than six (6) storeys in height; and (c) High Profile developments are buildings or structures generally no greater than fourteen (14) storeys in height.	
6.3.1.1 - Residential	To support a complementary range of housing forms and tenures in all neighbourhoods.	The proposed single unit dwelling will complement the existing neighbourhood. The height will be similar to the nearby single detached dwellings.
6.3.1.2	To promote compact neighbourhoods which encourage a balanced transportation system.	The Site is close to major roadways.
6.3.1.3	To promote residential redevelopment, infill and intensification initiatives in locations in accordance with this plan.	The proposed development will provide for infilling.
6.3.2.1 – Permitted Uses	Uses permitted in the Residential land use designation identified on Schedule D: Land Use include Low Profile , and Medium Profile dwelling units. High Profile Residential Buildings shall be directed to locate in the City Centre, Mixed Use Centres and Mixed Use Corridors.	The proposed development is considered low profile. The proposed 2 storey single unit dwelling will maintain the same maximum height as the nearby dwellings, which makes it very compatible with the neighbourhood.
6.3.2.3 – Types of Low Profile Housing	For the purposes of this Plan, Low Profile housing development is further classified as follows: (a) small scale forms: single detached , semi-detached, duplex and row and multiplexes with up to 8 units; and (b) large scale forms: buildings with more than 8 units.	Single unit dwellings are considered small-scale and are permitted.

OP Policy #	Policy	Response
6.3.2.4 – Locational Criteria	Residential intensification shall be directed to the Mixed Use Nodes and areas in proximity to those Nodes. Within these areas Medium Profile buildings, up to four (4) storeys in height shall be permitted. These taller buildings shall be designed to provide a transition in height and massing from low-profile areas. New residential development and intensification shall be located where: (a) There is access to a collector or arterial road; (b) Full municipal physical services can be provided; (c) Adequate community services and open spaces are available or are planned; and (d) Public transportation service can be provided.	The Site is close to major roadways, has access to full municipal services, is close to nearby amenities and is close to transit.
6.3.2.5 – Evaluation Criteria (existing neighbourhood)	At the time of submission, the proponent shall demonstrate to the satisfaction of the Municipality that a proposed residential development within an area having a Neighbourhood development pattern is: (a) feasible having regard to the other provisions of this Plan, provincial legislation, policies and appropriate guidelines and support studies for uses: (i) within or adjacent to any area identified on Schedule C: Development Constraint Areas and described in the Environment chapter of this Plan; (ii) adjacent to sources of nuisance, such as noise, odour, vibration and dust; (iii) within a site of potential or known contamination; (iv) where traffic generation and	This PRR has evaluated the PPS in Section 5.1.1. There are no constraint areas or nuisances that impact the Site. There is no known contamination. Traffic generation is not expected to have any significant impact. There are no heritage resource concerns on or near the Site. The required support study has been completed and is summarized in Section 4.3.1 of this PRR. There are no secondary plans that impact the Site.

OP Policy #	Policy	Response
	distribution is a provincial or municipal concern; and (v) adjacent to heritage resources. (b) in keeping with the goals, objectives and policies of any secondary plan or guideline plan affecting the surrounding area; (c) In existing neighbourhoods, compatible with the surrounding area in terms of scale, massing, height, siting, orientation, setbacks, parking and amenity areas. In Mature Neighbourhoods as shown on Schedule A-1, compatible with the surrounding area, as noted above, and consistent with the streetscape, architectural style and materials, landscape character and setback between the buildings and streets; (d) provided with adequate off street parking; (e) capable of being provided with full municipal physical services and emergency services; and (f) Facilitation a gradual transition from Low Profile residential development to Medium and/or High Profile development and vice versa, where appropriate, in accordance with Design Guidelines approved by Council.	The proposed development is compatible with the existing neighbourhood. Existing single unit dwellings are a permitted use. A new single unit dwelling is proposed. No relief from the RD1.2 is being requested. The Site is a large lot to accommodate the proposed development. The proposed building height will be similar to single detached dwelling in the area as it pertains to height, massing and scale. The massing of the development has been carefully designed to emulate the scale of the existing structures and the surrounding context. The building will be oriented to face the roadway, similar to the existing built environment. Lot area and lot frontages in the area vary in size in the existing built up area. The Site is capable of accommodating the proposed development in terms of scale, massing, height, and siting. The Site is not in a mature neighbourhood, as shown on Schedule A-1.

OP Policy #	Policy	Response
		Off-street parking is provided. On-street parking is also available, if required. Amenity areas and landscaping will be provided (front and rear of the Site). Full municipal and emergency services are available. No transition between uses is required as part of the building design.
7.0 - Infrastructure	The provision of proper infrastructure provides a safe, healthy and efficient living environment. In order to accommodate transportation and physical service needs in Windsor, Council is committed to ensuring that infrastructure is provided in a sustainable, orderly and coordinated fashion.	The proposed development is close to nearby transit, off a major roadway, and has access to full municipal services.
9.3.4.1 – Heritage Resources	Council will protect heritage resources by: (a) Requiring that development or infrastructure undertakings on lands containing potential archaeological resources avoid the destruction or alteration of these resources in Schedule C-1 Archaeological Potential; or where this is not possible, requiring the proponent to mitigate the impact to archaeological resources through documentation and removal in advance of land disturbances, in accordance with the Ontario Heritage Act and the policies contained within the Windsor	The subject property is located within an area of archaeological potential. The required support study has been prepared and summarized in Section 4.3.1 of this PRR.

OP Policy #	Policy	Response
	Archaeological Management Plan.	

Therefore, the proposed development will conform to the purpose and intent of the City of Windsor OP.

5.1.3 Zoning By-law

The City of Windsor Zoning By-law (ZBL) #8600 was passed by Council on July 8, 2002, and then a further Ontario Municipal Board (OMB) decision was issued on January 14, 2003.

A ZBL implements the PPS and the City OP by regulating the specific use of property and providing for its day-to-day administration.

The current zoning of the Site, subject to development, is Residential District 1.4 (RD1.4) category, as shown on Map 8 of the City of Windsor Zoning By-law #8600.

It is proposed to amend the existing zoning to a site specific Residential District 1.2 (RD1.2 - S.20(1)(XXX)) category to permit the proposed development of a new single unit dwelling.

The purpose of the ZBA is to change the zoning to permit a single unit dwelling, on a smaller lot.

A review of the RD1.2 zone provisions, as set out in Section 10.2 of the ZBL is as follows:

Zone Regulations	Required RD1.4 Zone	Required RD1.2 Zone	Proposed RD1.2 - S.20(1)(XXX) Zone Lot 1, Severed (Part 1 on draft reference plan)	Proposed RD1.2 - S.20(1)(XXX) Zone Lot 2, Retained (Part 2 on draft reference plan)	Compliance and/or Relief Requested with Justification
Permitted Uses	Existing Duplex Dwelling Existing Semi-Detached Dwelling One Single Unit Dwelling	Existing Duplex Dwelling Existing Semi-Detached Dwelling One Single Unit	New single unit dwelling (no ADU) Existing accessory structure (to be demolished)	Existing single unit dwelling (no ADU) Existing accessory structure (to	Complies, subject to the ZBA approval. The purpose of the ZBA is to change the zoning to permit a new single

Zone Regulations	Required RD1.4 Zone	Required RD1.2 Zone	Proposed RD1.2 - S.20(1)(XXX) Zone Lot 1, Severed (Part 1 on draft reference plan)	Proposed RD1.2 - S.20(1)(XXX) Zone Lot 2, Retained (Part 2 on draft reference plan)	Compliance and/or Relief Requested with Justification
	Any use accessory to the preceding uses	Dwelling Any use accessory to the preceding uses		be demolished)	unit dwelling, on a smaller lot.
Lot Width – minimum	18.0 m	12.0 m	12.9 m (average)	12.18 m (average)	Complies
Lot Area – minimum	540.0 m ²	360.0 m ²	382.85 m ²	361.11 m ²	Complies
Lot Coverage – maximum	45.0 %	45.0%	35.36%	Lot – 361.11 m ² Existing dwelling – 83.53 m ² Lot coverage 23.13 %	Complies
Main Building Height - maximum	9.0 m	9.0 m	7.5 m	Existing	Complies
Front Yard Depth – minimum	6.0 m	6.0 m	6.0 m Covered porch would be allowed to encroach further toward the front yard.	4 m - Existing	Complies

Zone Regulations	Required RD1.4 Zone	Required RD1.2 Zone	Proposed RD1.2 - S.20(1)(XXX) Zone Lot 1, Severed (Part 1 on draft reference plan)	Proposed RD1.2 - S.20(1)(XXX) Zone Lot 2, Retained (Part 2 on draft reference plan)	Compliance and/or Relief Requested with Justification
Rear Yard Depth – minimum	7.50 m	7.50 m	7.50 m	>7.50 m - Existing	Complies
Side Yard Width – minimum	1.20 m	1.20 m	East – 1.88 m West - 1.83 m	East – 2.8 m - Existing West – New 1.49 m	Complies
Gross Floor Area – Main Building – maximum	400 m2	400 m2	120 m2	Existing	Complies

Therefore, the proposed development will comply with the purpose and intent of the ZBL.

Further, the proposed development will comply with all zone provisions set out in the RD1.2 Zone, and no relief is requested.

6.0 SUMMARY AND CONCLUSION

6.1 Summary

6.1.1 Site Suitability

The Site is ideally suited for residential development for the following reasons:

- The land area is sufficient to accommodate the proposed development,
- The Site is generally level,
- The Site will be able to accommodate municipal water, storm and sewer,
- There are no anticipated traffic or parking concerns, and
- The location of the proposed development is appropriate.

6.1.2 Compatibility of Design

The proposed development will be limited to a 2 storey, low profile building, which is a compatible density for the Site and with the surrounding area.

The proposed residential use will provide new housing in an existing built-up area.

The Site is capable of accommodating the proposed development in terms of scale, massing, height, and siting.

6.1.3 Good Planning

The proposal represents good planning as it addresses the need for the development of a parcel of land, which contributes to affordability and infill requirements.

Residential use on the Site represents an efficient development pattern that optimizes the use of land.

6.1.4 Environment Impacts

The proposal does not have any negative impact on the natural environment.

6.1.5 Municipal Services Impacts

There will be no negative impacts on the municipal system as the proposed use will not add to the capacity in a significant way.

6.1.6 Social, Cultural, and Economic Conditions

The proposed development does not negatively affect the social environment as the Site is in close proximity to major transportation corridors and nearby amenities.

Infilling in an existing built-up area of the City contributes toward the goal of 'live, work and play' where citizens share a strong sense of belonging and a collective pride of place.

The proposal does not cause any public health and safety concerns.

The proposal represents a cost-effective development pattern that minimizes land consumption and servicing costs.

There will be no urban sprawl as the proposed development is within the existing settlement area and is an ideal infilling opportunity.

There are no cultural heritage resources that impact the Site.

6.2 Conclusion

In summary, it would be appropriate for the City of Windsor to approve the ZBA application to permit the proposed development on the Site.

This PRR has shown that the proposed development is consistent with the PPS, conforms with the intent and purpose of the OP and ZBL and represents good planning.

Planner's Certificate:

I hereby certify that this report was prepared by Tracey Pillon-Abbs, a Registered Professional Planner, within the meaning of the Ontario Professional Planners Institute Act, 1994.


Tracey Pillon-Abbs, RPP
Principal Planner



APPENDIX D – CONSULTATION

CANADA POST – BRUNO DESANDO

Canada Post has no comments for the attached application.

TRANSIT WINDSOR – JASON SCOTT

Transit Windsor has no objections to this development. The closest existing transit route to this property is Route 205. Route 205 has a peak weekday frequency of 15 minutes. The closest existing bus stop to this property is located on Dougall at West Grand southwest corner. This bus stop is approximately 390 metres from this property falling within Transit Windsor's walking distance guidelines of 400 metres to a bus stop. This will be maintained with Transit Windsor's City Council approved Transit Master Plan.

ZONING COORDINATOR – SAM PERRY

Below is the zoning review for the above mentioned property:

- **Current Zoning:** Residential District 1.4 (RD1.4)
- **Proposed Zoning:** Residential District 1.2 (RD1.2)
- **Proposed Use:** two storey single unit dwelling
- **Section 5 - General Provisions**
 - **Retained Lot:**
 - Encroachment into a Yard: **does not comply** [5.30]
 - Existing Garage is over severance line
 - Accessory Building: **does not comply** [5.10.11]
 - Accessory Building covers more than 10% of lot
 -
 - **Severed Lot:**
 - Complies
- **Section 10.2.5 - Residential District 1.2**

Retained lot:

- **Minimum Lot Width:** **does not comply**
 - 12.0m (Required)
 - **11.8m (Provided)**
- **Minimum Lot Area:**
 - 360.0m² (Required)
 - 362.1m² (Provided)
- **Maximum Lot Coverage:**
 - 45.0% (Required)
 - 33.8% (Provided)
- **Maximum Main Building Height:**
 - 9.0m (Required)
 - Existing
- **Minimum Front Yard Depth:**

- 6.0m (Required)
- Existing
- **Minimum Rear Yard Depth:**
 - 7.50m (Required)
 - Existing
- **Minimum Side Yard Width:**
 - 1.20m (Required)
 - 1.52m (Provided)
- **Maximum Gross Floor Area – Main Building:**
 - 400.0m² (Required)
 - Existing

Severed lot:

- **Minimum Lot Width:**
 - 12.0m (Required)
 - 12.6m (Provided)
- **Minimum Lot Area:**
 - 360.0m² (Required)
 - 383.5m² (Provided)
- **Maximum Lot Coverage:**
 - 45.0% (Required)
 - 30.9% (Provided)
- **Maximum Main Building Height:**
 - 9.0m (Required)
 - 6.8m (Provided)
- **Minimum Front Yard Depth:**
 - 6.0m (Required)
 - 6.27m (Provided)
- **Minimum Rear Yard Depth:**
 - 7.50m (Required)
 - 7.61m (Provided)
- **Minimum Side Yard Width:**
 - 1.20m (Required)
 - 1.83m (Provided)
- **Maximum Gross Floor Area – Main Building:**
 - 400.0m²(Required)
 - 329.0m²(Provided)

Section 24 - Parking, Loading and Stacking Provisions

- **Retained Lot:**
 - Complies
 - **Severed Lot:**
 - complies
- Showing that the existing fence and wood deck are built onto city property

WINDSOR FIRE – JOHN SMITH

No objections from Windsor Fire.

LANDSCAPE ARCHITECT – RYAN UPTON

As requested, the materials for the file titled above have been reviewed.

There are no objections or documents required from a Landscape Architecture perspective as it pertains to a Zoning By-law Amendment (ZBA).

There are no City owned trees, however, there are private trees on the property that may be impacted by the development. The applicant is strongly encouraged to attempt to preserve existing privately owned trees on the subject site. If trees within the subject site are to be removed for development, it is strongly encouraged to plant replacement native trees for their various environmental benefits at a like-for-like trunk caliper size ratio (cumulative d.b.h.).

Please note for circulation purposes that these comments should capture Forestry's concerns, unless otherwise noted by Forestry.

If you have any questions or concerns, please feel free to contact me.

TRANSPORTATION PLANNING – ELARA MEHRILOU

- **Land Conveyance**

Not Applicable.

- **Corner Cut-Off**

A 4.6m X 4.6m corner-cut off is required at Bruce Avenue and Grand Marais Rd West intersection.

- **Sidewalk**

A Sidewalk contribution or contribution per Engineering Right-of-Way requirements is required along the frontage of Bruce Ave.

- **Parking**

Please be advised that the requirements for parking area regulations, including the number of regular, accessible, loading, and bicycle parking spaces, are detailed in the City of Windsor's Zoning By-law 8600. These requirements are reviewed by the Planning Department and must be met for approval.

In cases where any of these parking requirements are not met, Transportation will require a formal justification. This may include, but is not limited to, a parking study, turning movement analysis, or turning radius template overlays. This documentation is necessary to consider any deviation from the by-law's standards.

- **Transportation Impact Study**

Not Applicable.

- **Access**

All accesses shall conform to the TAC Geometric Design Guide for Canadian Roads and the City of Windsor Standard Engineering Drawings.

- **Exterior Path**

All new exterior paths of travel must meet the requirements of the Accessibility for Ontarians with Disabilities Act (AODA).

- **Other Comments**

Not Applicable.

Should you have any further questions or concerns, please contact Ellie MehriLou, of this department at EMehrilou@citywindsor.ca.

HERITAGE – TRACY TANG

Heritage

There is no apparent built heritage concern with this property.

Archaeology

Archaeological Assessment Report Entitled "STAGE 1-2 ARCHAEOLOGICAL ASSESSMENT Property Located at 520 Grand Marais Road West, Part of Lot 78, Concession 3, (Geographical Township of Sandwich), City of Windsor, County of Essex (AMICK Corporate Project #2025-849/MCM File #P038-1606-2025)", dated September 02, 2025, PIF #P038-1606-2025, has been entered into the Ontario Public Register of Archaeological Reports. Although the report recommends that no further archaeological assessment of the property is recommended, the applicant is still to note the following archaeological precautions.

1. Should archaeological resources be found during grading, construction or soil removal activities, all work in the area must stop immediately and the City's Planning & Development Services Department, the City's Manager of Culture and Events, and the Ontario Ministry of Citizenship and Multiculturalism must be notified and confirm satisfaction of any archaeological requirements before work can recommence.
2. In the event that human remains are encountered during grading, construction or soil removal activities, all work in that area must be stopped immediately and the site secured. The local police or coroner must be contacted to determine whether or not the skeletal remains are human, and whether the remains constitute a part of a crime scene. The Local police or coroner will then notify the Ontario Ministry of Citizenship and Multiculturalism and the Registrar at the Ministry of Government and Consumer Services if needed, and notification and satisfactory confirmation be given by the Ministry of Citizenship and Multiculturalism.

Contacts:

Windsor Planning & Development Services Department:

519-255-6543 x6179, ttang@citywindsor.ca, planningdept@citywindsor.ca

Windsor Manager of Culture and Events (A):

Michelle Staaedegaard, (O) 519-253-2300x2726, (C) 519-816-0711, mstaaedegaard@citywindsor.ca

Ontario Ministry of Citizenship and Multiculturalism

Archaeology Programs Unit, 1-416-212-8886, Archaeology@ontario.ca

Windsor Police: 911

Ontario Ministry of Public and Business Service Delivery and Procurement

Registrar of Burial Sites, War Graves, Abandoned Cemeteries and Cemetery Closures:

Ian Hember, 1-437-244-9840, ian.hember@ontario.ca

RIGHT-OF-WAY – MARK SCHAFFHAUSER

Required Drawing Revisions:

1. **Driveway Approaches** – Do not conform to City of Windsor Standards, which must be constructed with straight flares and no raised curbs within the right-of-way.
 - Modify as per Standard Engineering Drawing AS-221 or AS-222.
2. **Sewer Connections** – All existing and proposed storm, sanitary and water services must be identified on the drawings, as well as the associated mainline sewers/water mains.
 - Modify drawings to include all sewer connections and water services.
 - Identify any redundant connections to be abandoned in accordance with Engineering Best Practice BP1.3.3.
3. **Alley Closing**
 - Modify drawings to include alley closure

Right-of-Way Permit Requirements

Alley Closing – Prior to the issuance of a Building Permit, the owner shall apply to the Street and Alley Closing Committee to close the existing alley adjacent to the subject property.

CCTV Sewer Inspection – The Owner further agrees to provide at its entire expense CCTV inspection satisfying City of Windsor Standard Specification S-32, for all existing and newly constructed sanitary and storm sewer infrastructure. The inspection shall include both mainline sewers and private drain connections, and shall be required to be submitted prior to the issuance of construction permits;

Driveway Approaches – The Owner further agrees that driveway approaches shall be constructed in such width and location as shall be approved by the City Engineer, with straight flare driveway approaches and no raised curbs within the right-of-way. The Owner shall have the option of constructing said driveway approaches as follows:

1. Residential Property
 - a. Asphalt in accordance with City of Windsor Standard Drawing AS-221; or
 - b. Concrete in accordance with City of Windsor Standard Drawing AS-222

Sewer Connections – The site is serviced by a 250mm sanitary sewer located within the Grand Marais Road West right-of-way. All existing and proposed storm, sanitary and water services must be identified on the drawings, as well as the associated mainline sewers/water mains.

- Modify drawings to include all sewer connections and water services.
- Identify any redundant connections to be abandoned in accordance with Engineering Best Practice BP1.3.3.
- Storm Sewer must be extended west from manhole 8R59 to front newly severed property

If you have any further questions or concerns, please contact Dan Perissinotti, of this department at dperissinotti@citywindsor.ca.

ENWIN

HYDRO ENGINEERING: Nillavon Balachandran

No objection.

Please be advised of the overhead 27.6kV, double circuit, primary distribution hydro pole line along the southern limit of the property.

Prior to working in these areas, we suggest notifying your contractor and referring to the Occupational Health and Safety Act and Regulations for Construction Projects to confirm clearance requirements during construction and demolition. Also, we suggest referring to the Ontario Building Code for required clearances for New Building Construction.

WATER ENGINEERING: Bruce Ogg

ENWIN Water has no objections.

DEVELOPMENT ENGINEERING – JUAN PARAMO

We have reviewed the subject Rezoning application and have the following comments:

Sewers - The site may be serviced by a 250mm sanitary sewer located within Grand Marais Road West right-of-way. If possible existing connections should be utilized. Any redundant connections shall be abandoned in accordance with the City of Windsor Engineering Best Practice B.P 1.3.3.

There is currently no storm sewer service along the frontage of the new lot. The proponent will be required to provide appropriate storm sewer services to the new lot, prior to a Building Permit application.

Overall, the level of service for the sanitary sewer system in this neighborhood is not expected to experience any significant degradation as a result of this development.

In summary we have no objection to the proposed development.

If you have any further questions or concerns, please contact Jaegar King, of this department at jking@citywindsor.ca



Committee Matters: SCM 287/2025

Subject: Adoption of the Development & Heritage Standing Committee minutes of its meeting held September 2, 2025

Development & Heritage Standing Committee Meeting

Date: Tuesday, September 2, 2025

Time: 4:30 o'clock p.m.

Members Present:

Councillors

Ward 1 - Councillor Fred Francis

Ward 4 - Councillor Mark McKenzie

Ward 7 - Councillor Angelo Marignani

Ward 9 - Councillor Kieran McKenzie

Ward 10 - Councillor Jim Morrison (Chairperson)

Members

Member Anthony Arbour

Member Joseph Fratangeli

Member Robert Polewski

Member Khassan Saka

Member William Tape

Members Regrets

Member Daniel Grenier

Member John Miller

Member Charles Pidgeon

PARTICIPATING VIA VIDEO CONFERENCE ARE THE FOLLOWING FROM ADMINISTRATION:

Sandra Gebauer, Council Assistant

ALSO PARTICIPATING IN COUNCIL CHAMBERS ARE THE FOLLOWING FROM ADMINISTRATION:

Jelena Payne, Deputy Chief Administrative Officer / Commissioner, Economic Development

Neil Robertson, City Planner

Greg Atkinson, Deputy City Planner – Development

Jason Campigotto, Deputy City Planner – Growth

Aaron Farough, Senior Legal Counsel

Minutes

Development & Heritage Standing Committee Tuesday, September 2, 2025

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Sahar Jamshidi, Manager, Road Safety
Patrick Winters, Manager, Development
Laura Diotte, Manager, Planning
Emilie Dunnigan, Manager Development Revenue & Financial Administration
Elara Mehrilou, Supervisor, Corridor Maintenance
Brian Nagata, Planner III – Development
Justina Nwaesei, Planner III – Development
Tracy Tang, Planner III – Heritage
Simona Simion, Planner III – Economic Development
Natasha McMullin, Clerk Steno Senior
Anna Ciacelli, Deputy City Clerk

Delegations—participating via video conference

Item 7.2 & 7.4 - Tracey Pillon-Abbs, Principal Planner, Pillon Abbs Inc.
Item 11.5 - Dawne Martens, Property Manager, 2424718 Ontario Limited
Item 11.6 - Patrick Clark, CLP Law, Agent on behalf of 1912944 Ontario Ltd.

Delegations—participating in person

Item 7.1 - Jerry Kavanaugh, Agent for the Applicant & Senior Directing Partner, Architectural Design Associates Inc.
Item 7.3 - Hal Kersey, President, HRK Realty Services Ltd. Agent for the Applicant, Claudio Martini, Executive Vice President, Sterling Ridge Group, and Alawi Altahhan, Director Project Management, Quality Assurance & Document Control, Sterling Ridge Group
Item 11.6 - Jeffrey Nanson, Mousseau, Deluca, McPherson, Prince LLP, on behalf of the Applicant Vito Maggio Holdings Inc.
Item 11.6 - Elizabeth Fisk, Area Resident
Item 11.6 - Kevin Kavanaugh, Kinart, Inc.

1. CALL TO ORDER

The Chairperson calls the meeting of the Development & Heritage Standing Committee to order at 4:30 o'clock p.m.

2. DISCLOSURES OF PECUNIARY INTEREST AND THE GENERAL NATURE THEREOF

None disclosed.

3. REQUEST FOR DEFERRALS, REFERRALS OR WITHDRAWALS

None requested.

Minutes

Development & Heritage Standing Committee Tuesday, September 2, 2025

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4. COMMUNICATIONS

None presented.

8. ADOPTION OF THE MINUTES

8.1. Adoption of the Development & Heritage Standing Committee minutes of its meeting held July 7, 2025

Moved by: Member William Tape

Seconded by: Member Joseph Fratangeli

THAT the minutes of the Development & Heritage Standing Committee meeting held July 7, 2025 **BE ADOPTED** as presented.

Carried.

Report Number: SCM 215/2025

8.2. Adoption of the Development & Heritage Standing Committee minutes of its meeting held August 5, 2025

Moved by: Councillor Fred Francis

Seconded by: Councillor Angelo Marignani

THAT the minutes of the Development & Heritage Standing Committee meeting held August 5, 2025 **BE ADOPTED** as presented.

Carried.

Report Number: SCM 252/2025

9. PRESENTATIONS AND DELEGATIONS (COMMITTEE ADMINISTRATIVE MATTERS)

10. HERITAGE ACT MATTERS

10.1. Request to Extend MRLA's Consulting Services to Stage 2 of the Walkerville HCD - Ward 4

Councillor Mark McKenzie inquires as to why consulting services would be hired in this instance, and if the consultations could be done in-house instead. Tracy Tang, Planner III – Heritage (Acting) appears before the Development and Heritage Standing Committee regarding the administrative report dated August 8, 2025, entitled “Request to Extend MRLA's Consulting Services to Stage 2 of the Walkerville HCD - Ward 4” and replies that Stage Two of the project will require public open

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houses, and the creation of the policy document and urban design guidelines, all of which would benefit from the consultants' experience.

Member William Tape inquires whether consulting elements could be priced out differently to ensure optimum pricing. Ms. Tang replies that Stage One of the project could not fully predict what would be necessary for Stage Two, which is why each stage was packaged and priced individually.

Moved by: Councillor Fred Francis

Seconded by: Councillor Angelo Marignani

Decision Number: **DHSC 763**

- I. THAT Council **APPROVE** the request to extend the sole source award to M. R. Letourneau and Associates Inc. for Stage 2 of the Walkerville Heritage Conservation District (HCD) Plan & Guidelines for the sum of \$120,000 (excluding HST); and,
- II. THAT the Purchasing Manager **BE AUTHORIZED** to amend Contract P.O. CO7042 to M. R. Letourneau and Associates Inc. for an additional \$120,000 (excluding HST) to a revised upset limit of \$208,400 for the provision of undertaking the Walkerville Heritage Conservation District (HCD) Plan & Guidelines; satisfactory in financial content to the City Treasurer, and in technical content to the Deputy CAO/Commissioner of Economic Development.

Carried.

Report Number: S 104/2025

Clerk's File: MBA/1700

There being no further business the meeting of the Development & Heritage Standing Committee (*Heritage Act* Matters) portion is adjourned at 4:38 o'clock p.m.

The Chairperson calls the *Planning Act* Matters portion of the Development & Heritage Standing Committee meeting to order at 4:40 o'clock p.m.

5. ADOPTION OF THE *PLANNING ACT* MINUTES

5.1. Adoption of the Development & Heritage Standing Committee minutes (*Planning Act*) of its meeting held August 5, 2025

Moved by: Councillor Kieran McKenzie

Seconded by: Councillor Mark McKenzie

THAT the *Planning Act* minutes of the Development & Heritage Standing Committee meeting held August 5, 2025 **BE ADOPTED** as presented.

Carried.

Report Number: SCM 258/2025

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6. PRESENTATION DELEGATIONS (*PLANNING ACT MATTERS*)

See items 7.1 through 7.4

7. *PLANNING ACT MATTERS*

7.1. Zoning By-law Amendment Application for 7900 Anchor Drive & 8100 Twin Oaks Drive, Z-016/25 [ZNG-7310], Ward 9

Moved by: Councillor Kieran McKenzie

Seconded by: Councillor Fred Francis

Decision Number: **DHSC 759**

- I. THAT Zoning By-law 8600 **BE AMENDED** by changing the zoning of Parts 1 to 4, Plan 12R-22845 (PIN 01408-2002 & 01408-2003) known municipally as 7900 Anchor Drive and 8100 Twin Oaks Drive (Roll No. 070-650-01014 & 070-650-01012), situated at the northeast corner of Anchor Drive and Twin Oaks Drive, by deleting and replacing Section 20(1)209 as follows:

209. NORTHEAST CORNER OF ANCHOR DRIVE AND TWIN OAKS DRIVE

For the lands comprising Parts 1 to 4, Plan 12R-22845 (PIN 01408-2002 & 01408-2003), the following additional provisions shall apply:

- a) Notwithstanding Section 24.20.5, for a *Medical Office* having a *maximum gross floor area* of 3,300.0 m², a *minimum* of 102 *parking spaces* shall be provided.

[ZDM 15; ZNG/7310]; and,

- II. THAT, at the discretion of the City Planner, Deputy City Planner, or Site Plan Approval Officer, the following **BE SUBMITTED** with an application for Site Plan Approval:
 - a. Parking Study, prepared by R.C. Spencer Associates Inc. Consulting Engineers, dated January 2025; and,
- III. THAT the Site Plan Approval Officer **BE DIRECTED** to incorporate the following, subject to any updated information, into an approved site plan and executed and registered site plan agreement:
 - a. Prior to the issuance of a construction permit, the Owner agrees to submit application for and execute an agreement with the Corporation for the proposed encroachments into the right-of-way (encroaching sign on Anchor Drive) to the satisfaction of the City Engineer.

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- b. Prior to the issuance of a construction permit, the Owner shall include the City of Windsor within the sewer easement agreement of Part 3 and Part 4 in Plan 12R-22845 to convey to the Corporation this easement for the purposes of construction and/or maintenance of the existing sewer; and,

IV. THAT the Site Plan Approval Officer **CONSIDER** the following matter in an approved site plan and/or executed and registered site plan agreement:

- a. Prior to the issuance of a construction permit, the Owner agrees to follow all drainage and flood proofing recommendations that the Essex Region Conservation Authority (ERCA) may have with respect to the subject land, based on final approval by the City Engineer. If applicable, the Owner will obtain all necessary permits from ERCA with respect to the drainage works on the subject lands.

Carried.

Report Number: S 98/2025

Clerk's File: ZB/15010

7.2. Zoning By-law Amendment Application for 0 & 1841 Longfellow Avenue, Z-017/25 [ZNG-7312], Ward 10

Moved by: Councillor Mark McKenzie

Seconded by: Councillor Angelo Marignani

Decision Number: **DHSC 760**

- I. THAT Zoning By-law 8600 **BE AMENDED** by adding the following zoning exception to Section 91.10:

5. WEST SIDE OF LONGFELLOW AVENUE BETWEEN ARCADIA STREET AND CALUMET STREET

For the lands comprising of Part of Lot 138, Lots 139 & 140, and Part of Closed Alley, Plan 557 (PIN 01313-1596 LT & 01313-1597 LT), a *Semi-Detached Dwelling* shall be an additional permitted *main use* and shall be subject to the *Semi-Detached Dwelling* provisions of Section 10.1.5, save and except that the *maximum gross floor area* shall be 410 m².

[ZDM 4, 7 & 8; ZNG/7312]; and,

- II. THAT Zoning By-law 8600 **BE FURTHER AMENDED** by changing the zoning of Part of Lot 138, Lots 139 & 140, and Part of Closed Alley, Plan 557 (PIN 01313-1596 LT & PIN 01313-1597 LT), situated at the west side of Longfellow Avenue between Arcadia Street and Calumet Street from RD1.1 to RD1.1x(5).

Carried.

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Report Number: S 99/2025

Clerk's File: ZB/15011

7.3. ZBA Application for 0 Salter Avenue, Z-009/25 [ZNG-7298], Ward 3

Moved by: Councillor Kieran McKenzie

Seconded by: Councillor Mark McKenzie

Decision Number: **DHSC 761**

- I. THAT Zoning By-law 8600 **BE AMENDED** by adding the following zoning exception to Section 91.10:

6. WEST SIDE OF SALTER AVENUE, NORTH OF UNIVERSITY AVENUE WEST

For the lands comprising Lots 7 & 8, Plan 380 (PIN 01196-0469), a *Multiple Dwelling* shall be subject to the following additional provisions:

- a) *Front Yard Depth: minimum 5.50 m*
- b) *Side Yard Width: minimum 1.80 m*
- c) *Dwelling Units: maximum 8*
- d) Notwithstanding clause .6 of Table 25.5.20.1, the *minimum* separation of a *parking area* from a *building* wall containing a *habitable room window* or containing both a main pedestrian entrance and a *habitable room window* facing the *parking area* where the *building* is located on the same *lot* as the *parking area* shall be 3.75 m.

[ZDM 3; ZNG/7298]; and,

- II. THAT Zoning By-law 8600 **BE FURTHER AMENDED** by changing the zoning of Lots 7 & 8, Plan 380 (PIN 01196-0469), situated on the west side of Salter Avenue, north of University Avenue West, from RD2.2 to RD3.1x(6).

Carried.

Report Number: S 100/2025

Clerk's File: ZB/15012

7.4. Approval of a Plan of Condominium with Exemption under Section 9(3) of the *Condominium Act*, 4470 North Service Road East; File No.: CDM 001-25 [CDM-7316]; Ward 5

Moved by: Councillor Angelo Marignani

Seconded by: Member Anthony Arbour

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Decision Number: **DHSC 762**

THAT the application of Atwan Development Inc. for an exemption under Section 9(3) of The *Condominium Act* for approval of a plan of condominium (Standard Condominium), comprised of a total of 41 industrial units within three new structures under construction as shown on the attached Map Nos. CDM-001/25-1, CDM-001/25-2, CDM-001/25-3, and CDM-001/25-4, on a parcel legally described as Part of Lot 108, Concession 2, (PIN 01562-1055 LT), located on the north side of North Service Road East ROW, west of Pillette Road, **BE APPROVED** for a period of three (3) years.

Carried.

Report Number: S 97/2025

Clerk's File: ZP/15007

There being no further business the meeting of the Development & Heritage Standing Committee (*Heritage Act* Matters) portion is adjourned at 4:58 o'clock p.m.

The Chairperson calls the Administrative Items portion of the Development & Heritage Standing Committee meeting to order at 4:58 o'clock p.m.

11. ADMINISTRATIVE ITEMS

11.5. Downtown CIP - 423-437 Ouellette Ave - Ward 3

Dawne Martens, Property Manager, 2424718 Ontario Limited

Dawne Martens, Property Manager, 2424718 Ontario Limited, appears before the Development and Heritage Standing Committee regarding the administrative report dated August 1, 2025, entitled "Downtown CIP – 423-437 Ouellette Ave – Ward 3", and is available for questions.

Moved by: Councillor Mark McKenzie

Seconded by: Councillor Fred Francis

Decision Number: **DHSC 768**

- I. THAT the request made by 2424718 Ontario Limited (Owner) for the proposed development at 423-437 Ouellette Avenue to participate in:
 - a. The Upper Storey Residential Conversion Grant Program **BE APPROVED** for \$5,000 for every new residential unit created on the upper storey of an existing mixed-use building (8 new dwelling units to be created), up to a maximum of \$40,000 towards eligible costs pursuant to the Downtown Windsor Enhancement Strategy and Community Improvement Plan; and,
 - b. The Commercial/Mixed Use Building Facade Improvement Grant Program **BE APPROVED** for a 50% matching grant to a maximum of \$20,000 towards eligible costs for the façade improvements as shown and described in Appendix B to Report

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S 102/2025 pursuant to the Downtown Windsor Enhancement Strategy and Community Improvement Plan; and,

- c. The Building/Property Improvement Tax Increment Grant Program **BE APPROVED** for the lesser of 100% of the municipal portion of the tax increment resulting from the proposed development for five (5) years in accordance with the Downtown Windsor Enhancement Strategy and Community Improvement Plan, or eligible costs; and,

- II. THAT Administration **BE DIRECTED** to prepare the Grant Agreement(s) between the City of Windsor and 2424718 Ontario Limited (Owner) to implement all grant programs in accordance with all applicable policies, requirements, and provisions contained within the Downtown Windsor Enhancement Strategy and Community Improvement Plan to the satisfaction of the City Planner as to content, the City Solicitor as to legal form, and the City Treasurer as to financial implications; and,
- III. THAT the CAO and City Clerk **BE AUTHORIZED** to sign the Grant Agreement(s) in content satisfactory to the City Planner, in financial content to the satisfaction of the City Treasurer and in form satisfactory to the City Solicitor; and,
- IV. THAT Grant funds up to \$40,000 under the Upper Storey Residential Conversion Grant Program **BE TRANSFERRED** from the CIP Reserve Fund 226 to the Downtown Windsor Enhancement Strategy and Community Improvement Plan Project 7011022 when work is completed to the satisfaction of the City Planner; and,
- V. THAT Grant funds up to \$20,000 under the Commercial/Mixed Use Building Facade Improvement Grant Program **BE TRANSFERRED** from the CIP Reserve Fund 226 to the Downtown Windsor Enhancement Strategy and Community Improvement Plan Project 7011022 when work is completed to the satisfaction of the City Planner; and,
- VI. THAT the City Treasurer **BE AUTHORIZED** to issue payment up to \$60,000 for grants from the Downtown Windsor Enhancement Strategy and Community Improvement Plan Project 7011022 for 257 Wyandotte St E to 2424718 Ontario Limited (Owner) upon completion subject to the satisfaction of the City Planner and Chief Building Official; and,
- VII. THAT the approval to participate in the Building/Property Improvement Tax Increment Grant Program **EXPIRE** if the grant agreement is not signed by applicant within one (1) year following Council approval. The City Planner may extend the deadline for up to one (1) year upon request from the applicant; and,
- VIII. THAT the Upper Storey Residential Conversion Grant and the Commercial/Mixed Use Building Facade Improvement Grant approved **SHALL LAPSE** and **BE UNCOMMITTED** and returned to CIP Reserve Fund 226 if the applicant has not completed the work and fulfilled the conditions within 2 years of the approval date.

Carried.

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Report Number: S 102/2025
Clerk's File: Z/15021

11.6. Part Closure of north/south alley located between Brant Street and Wyandotte Street East, Ward 4, SAA-7198

Jeffrey Nanson, Mousseau, Deluca, McPherson, Prince LLP, on behalf of Applicant Vito Maggio Holdings Inc.

Jeffrey Nanson, Mousseau, Deluca, McPherson, Prince LLP, on behalf of Applicant Vito Maggio Holdings Inc., appears before the Development and Heritage Standing Committee regarding the administrative report dated May 8, 2025, entitled "Part Closure of north/south alley located between Brant Street and Wyandotte Street East, Ward 4" to explain his client's position and expresses his concern regarding this matter being brought to committee again; and concludes by indicating that his client has already begun the process of amending the property in question.

Patrick Clark, CLP Law, Agent on behalf of 1912944 Ontario Ltd.

Patrick Clark, CLP Law, Agent on behalf of 1912944 Ontario Ltd., appears before the Development and Heritage Standing Committee regarding the administrative report dated May 8, 2025, entitled "Part Closure of north/south alley located between Brant Street and Wyandotte Street East, Ward 4" speaking on behalf of his client, the owner of the neighbouring property abutting the alley, and indicates that his client and other neighbouring property owners did not receive proper notice of the alley closure and were not given the opportunity to make their objections known, nor was his client given the option to purchase part of the alley abutting her property.

Elizabeth Fisk, Area Resident

Elizabeth Fisk, Area Resident, appears before the Development and Heritage Standing Committee regarding the administrative report dated May 8, 2025, entitled "Part Closure of north/south alley located between Brant Street and Wyandotte Street East, Ward 4", representing herself and other area residents to express their opposition to the alley closure. She states that area residents were not notified of the proposed closure anytime in the many months that it had been under consideration. Neighbours' concerns include access to their properties, reduced access for deliveries and emergency vehicles, noise, and rodents from garbage; and conclude by requesting that the decision to close the alley be reconsidered.

Kevin Kavanaugh, Kinart, Inc.

Kevin Kavanaugh, Kinart, Inc., appears before the Development and Heritage Standing Committee regarding the administrative report dated May 8, 2025, entitled "Part Closure of north/south alley located between Brant Street and Wyandotte Street East, Ward 4" to express his concerns as a neighbouring property owner; and concludes by indicating that the proposed closure will create a sharp angle at the end of the alley which will make it difficult for residents' vehicles or emergency vehicles to access properties.

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Councillor Mark McKenzie asks Kevin Kavanaugh if the Walkerville BIA is aware of residents' concerns regarding the property owner requesting the alley closure. Mr. Kavanaugh replies that neighbouring property owners have complained to by-law enforcement about the property's garbage bins and fencing obstructing the right-of-way.

Councillor Mark McKenzie asks Elizabeth Fisk to elaborate on her concerns. Ms. Fisk feels that the closure of the throughway to Wyandotte Street will create more foot traffic in the alley directly abutting her property, with increased noise, garbage, and potential for property damage. She also feels changing the alley structure is not consistent with the Walkerville Heritage District designation.

Councillor Mark McKenzie asks Jeffrey Nanson if his client has considered how garbage pickup and deliveries will be handled with part of the alley closed. Mr. Nanson states that his client's intention is to make the area cleaner and safer by closing off the garbage and delivery area so the fencing will no longer obstruct the right-of-way. His client feels that the patio will decrease foot traffic in the alley as there will no longer be a throughway to Wyandotte Street.

Councillor Kieran McKenzie asks Patrick Clark what his client intends to do with the property should she be permitted to purchase her half of the alley in question. Mr. Clark replies that his client is in the planning process of rebuilding on the property, as the previous building was destroyed by fire. Councillor Kieran McKenzie points out that there is a hydro pole on her property line that could inhibit Mr. Clark's client's plans unless it is relocated.

Councillor Angelo Marignani asks Jeffrey Nanson for clarification on his client's plan for garbage disposal, especially considering residents' concerns about rodents in the alley and on their properties. Mr. Nanson replies that his client is more than willing to work with the Committee and Council to ensure that garbage pickup and garbage storage will be handled to their satisfaction.

Councillor Mark McKenzie inquires whether garbage pickup for the property could be moved to the front of the building rather than off the alley/parking lot. Administration indicates that garbage pickup will eventually be moved to the front of the building in BIA's.

Councillor Kieran McKenzie asks Administration to explain why the neighbouring properties were not notified about the prior meetings where this subject was discussed. Neil Robertson, City Planner, appears before the Development and Heritage Standing Committee regarding the administrative report dated May 8, 2025, entitled "Part Closure of north/south alley located between Brant Street and Wyandotte Street East, Ward 4" to explain that it was a clerical error and the public notifications were not carried over into the report.

Councillor Kieran McKenzie then asks a question about procedure, since this matter had already been decided by Council. He asks what would happen should the Committee recommend changing the decision. Neil Robertson replies that after consulting with the City Clerk, the neighbours' concerns were considered new information and would need to be brought back to Committee.

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Councillor Kieran McKenzie asks if this exposes the city to legal risk. Aaron Farough, Senior Legal Council, appears before the Development and Heritage Standing Committee regarding the administrative report dated May 8, 2025, entitled “Part Closure of north/south alley located between Brant Street and Wyandotte Street East, Ward 4” to explain that because there has not yet been a by-law passed to close the alley in question, there is still leeway to revisit the decision.

Councillor Kieran McKenzie asks Administration about the hydro pole located on the west side of the alley, abutting the neighbouring property owner’s lot. He believes that the location of the pole limits the property owner’s ability to build up to, or extend onto the alley in question, should she be allowed to purchase half of it. Brian Nagata, Planner III – Development, appears before the Development and Heritage Standing Committee regarding the administrative report dated May 8, 2025, entitled “Part Closure of north/south alley located between Brant Street and Wyandotte Street East, Ward 4” and indicates that the Ontario Building Code requires a minimum of 3 to 5 meters of set back between a hydro pole and a building, depending on the voltage of the lines.

Councillor Kieran McKenzie asks how wide the alley is, and Mr. Nagata replies it is approximately 4.27 meters wide. Councillor Kieran McKenzie asks if there is enough space for the proponent to build the patio considering the proximity to the hydro pole. Mr. Nagata replies that only buildings and structures are subject to the 3 to 5 meter set back.

Councillor Angelo Marignani asks about alley access for emergency vehicles, and if the proposed closure of one end of the alley would be a concern. Mr. Nagata replies that he would have to get confirmation from Fire Services.

Councillor Angelo Marignani asks about a similar scenario down the street from the proponent’s location, where an alley was closed to allow for a patio. Mr. Nagata replies that in that situation, the alley closure was granted on condition of an easement, and with the understanding that the patio may have to be moved to allow for work on the sewer lines beneath it.

Councillor Angelo Marignani asks if any restrictions were put on the patio structure to allow for access to hydro lines above. Mr. Nagata replies that a blanket easement was likely a condition to allow for utility access, but he would have to check the report to verify.

Councillor Angelo Marignani inquires about the property abutting the alley in question that was destroyed in a fire. He asks to confirm if a new build on that property would no longer be grandfathered to allow construction up to the hydro pole. Brian Nagata confirms this is correct, and any new build would now be subject to the 3 to 5 meter set back.

Councillor Fred Francis asks Administration if the new information received today would change the recommendation of the report. Mr. Robertson replies that the recommendation brought forward today takes this new information into account.

Councillor Fred Francis inquires as to why the recommendation would be the same after these concerns were presented. Mr. Robertson explains that the viability of the adjacent property was examined. The recommendation does not impede the redevelopment of that site in any way, and

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Building Code requirements would in fact push any new build further away from the alley in question.

Councillor Fred Francis then asks, should the recommendation move forward, and the patio be built, what recourse do the neighbouring residents have should their concerns prove true or grow worse in addition to how the city could address these concerns at that point, with respect to by-law, licensing, or enforcement. Mr. Robertson replies that any or all of the mentioned avenues could be pursued should neighbours choose.

Councillor Angelo Marignani asks Administration to address the fact that residents in the area were not notified of the original meeting. Mr. Nagata replies that the Notice of Application was distributed, but the notification of the DHSC meeting was not. Mr. Robertson adds that steps are being taken to prevent a similar occurrence from happening again.

Councillor Marignani requests that the Clerk's Office speak to the matter. Anna Ciacelli, Deputy City Clerk, appears before the Development and Heritage Standing Committee regarding the administrative report dated May 8, 2025, entitled "Part Closure of north/south alley located between Brant Street and Wyandotte Street East, Ward 4" and explains the process of notification and the clerk's office intention to review current notification procedures including what information can be published on reports in line with Privacy Concerns, moving forward, a new system will be put in place to allow for checks and balances when reports are received to ensure such oversights do not happen in the future.

Moved by: Councillor Mark McKenzie

Seconded by: Councillor Fred Francis

Decision Number: **DHSC 769** CR286/2025 DHSC 744

- I. That the 33.83 metre portion of the 4.57-metre-wide north/south alley located between Brant Street and Wyandotte Street East, and shown on Drawing No. CC-1855 (attached hereto as Appendix "A"), and hereinafter referred to as the "Subject Alley", **BE ASSUMED** for subsequent closure; and,
- II. That the Subject Alley **BE CLOSED AND CONVEYED** in as is condition to the owner (the "Owner") of the abutting property known municipally as 1850 & 1862 Wyandotte Street East, legally described as Part of Lots 4 & 5 and Block E, Plan 211, and as necessary, in a manner deemed appropriate by the City Planner, subject to the following:
 - a. Easements, subject to being accepted in the City's standard form and in accordance with the City's standard practice, being granted to:
 - i. Bell Canada to accommodate existing infrastructure;
 - ii. Enbridge Gas Inc. to accommodate existing underground infrastructure;
 - iii. ENWIN Utilities Ltd. to accommodate existing pole, anchors and overhead plant;

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- iv. Managed Network System Inc. (MNSi.) to accommodate existing aerial infrastructure; and
 - v. The Corporation of the City of Windsor to accommodate existing 1,125.0 millimetre brick combined sewer with catch basin.
 - b. Payment of the survey cost associated with the closure of the Subject Alley by the Owner; and,
- III. That Conveyance Cost **BE SET** as follows:
- a. For alley conveyed to abutting lands zoned CD2.1 at \$10.00 per square foot with easements plus HST (if applicable); and,
- IV. That The City Planner **BE REQUESTED** to supply the appropriate legal description, in accordance with Drawing No. CC-1855; and,
- V. That The City Solicitor **BE REQUESTED** to prepare the necessary by-law(s); and,
- VI. That The Chief Administrative Officer and City Clerk **BE AUTHORIZED** to sign all necessary documents approved as to form and content satisfactory to the City Solicitor; and,
- VII. That the matter **BE COMPLETED** electronically pursuant to By-law Number 366-2003.
Carried.
Councillor Angelo Marignani voting nay.

Report Number: SCM 190/2025, S 70/2025, AI 16/2025 & AI 21/2025
Clerk's File: SAA2025

11.1. Response to CQ 28-2024 – Downtown Residential Parking Requirements

Moved by: Councillor Fred Francis
Seconded by: Councillor Mark McKenzie

Decision Number: **DHSC 764**

THAT the report of the Senior Planner – Development dated May 28, 2025 entitled “Response to CQ 28-2024 – Downtown Residential Parking Requirements” **BE RECEIVED** for information.
Carried.

Report Number: C 85/2025
Clerk's File: ST2025

11.2. Brownfield CIP Application - 844 Bridge Ave - Ward 2

Moved by: Councillor Angelo Marignani
Seconded by: Councillor Mark McKenzie

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Decision Number: **DHSC 765**

- I. THAT the request made by 15847796 Canada Inc. to participate in the Environmental Site Assessment Grant Program **BE APPROVED** for the completion of a proposed Phase II Environmental Site Assessment Study and Remediation Work Plan for the property located at 844 Bridge Ave pursuant to the City of Windsor Brownfield Redevelopment Community Improvement Plan; and,
- II. THAT the City Treasurer **BE AUTHORIZED** to issue payment up to a maximum of \$14,377.50 based upon the completion and submission of a Phase II Environmental Site Assessment Study completed in a form acceptable to the City Planner and City Solicitor; and,
- III. THAT the grant funds in the amount of \$14,377.50 under the Environmental Site Assessment Grant Program **BE TRANSFERRED** from the CIP Reserve Fund 226 to Brownfield Strategy Remediation (Project #7069003) when the eligible work is completed to the satisfaction of the City Planner; and,
- IV. THAT should the proposed Phase II Environmental Site Assessment Study and Remedial Work Plan not be completed within two (2) years of Council approval, the approval **BE RESCINDED** and the funds be uncommitted and made available for other applications.

Carried.

Report Number: S 94/2025

Clerk's File: Z/15019

11.3. Demolition Application subject to Demolition Control - 1968 George Ave - Ward 5

Moved by: Councillor Fred Francis

Seconded by: Councillor Mark McKenzie

Decision Number: **DHSC 766**

- I. THAT the Chief Building Official **BE AUTHORIZED** to issue a demolition permit for the residential dwelling located at 1968 George Ave to facilitate redevelopment of the property; and,
- II. THAT the Chief Building Official **BE DIRECTED** to require, as a condition of the demolition permit, that:
 - a. Redevelopment be substantially complete within two years of demolition permit issuance; and,
 - b. If redevelopment, including construction of a new building, is not substantially complete within two years of the commencement of demolition the maximum fee (\$20,000) shall be entered on the collectors roll of the property; and,

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- III. THAT the City Solicitor **BE DIRECTED** to register a notice of Condition II of this report in the land registry office against the property.

Carried.

Report Number: S 95/2025

Clerk's File: SW2025

11.4. Downtown CIP - 257 Wyandotte St E - Ward 3

Moved by: Councillor Mark McKenzie

Seconded by: Councillor Angelo Marignani

Decision Number: **DHSC 767**

- I. THAT the request made by Ali Seblini (applicant and owner) for Yasmeen Properties Inc. for the proposed development at 257 Wyandotte St E. to participate in:
- a) The Commercial/Mixed Use Building Facade Improvement Program **BE APPROVED** for 50% of the eligible costs of the facade improvements, to a maximum of \$30,000; and,
 - b) The Retail Investment Grant Program **BE APPROVED** for 50% of the eligible costs per retail unit to a maximum of \$15,000; and,
 - c) The Building/Property Improvement Tax Increment Grant Program **BE APPROVED** for the lesser of 100% of the municipal portion of the tax increment resulting from the proposed development for five (5) years, plus an additional five (5) years as a Catalyst Project, or eligible costs, in accordance with the Downtown Windsor Enhancement Strategy and Community Improvement Plan to the satisfaction of the City Planner; and,
- II. THAT Administration **BE DIRECTED** to prepare the Grant Agreement(s) between the City of Windsor and Yasmeen Properties Inc. to implement all grant programs in accordance with all applicable policies, requirements, and provisions contained within the Downtown Windsor Enhancement Strategy and Community Improvement Plan to the satisfaction of the City Planner as to content, the City Solicitor as to legal form, and the City Treasurer as to financial implications; and,
- III. THAT the CAO and City Clerk **BE AUTHORIZED** to sign the Grant Agreement(s) in content satisfactory to the City Planner, in financial content to the satisfaction of the City Treasurer and in form satisfactory to the City Solicitor; and,
- IV. THAT the approval to participate in the Building/Property Improvement Tax Increment Grant Program **EXPIRE** if the grant agreement is not signed by applicant within one (1) year following Council approval. The City Planner may extend the deadline for up to one (1) year upon request from the applicant; and,

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- V. THAT Grant funds up to \$45,000 under the Downtown Windsor Enhancement Strategy and Community Improvement Plan **BE TRANSFERRED** from the CIP Reserve Fund 226 to the Downtown Windsor Enhancement Strategy and Community Improvement Plan Project 7011022 when work is completed to the satisfaction of the City Planner; and,
- VI. THAT the City Treasurer **BE AUTHORIZED** to issue payment up to \$45,000 for grants from the Downtown Windsor Enhancement Strategy and Community Improvement Plan Project 7011022 for 257 Wyandotte St E to Yasmeen Properties Inc. upon completion of facade and retail improvements subject to the satisfaction of the City Planner and Chief Building Official; and,
- VII. THAT for the Commercial/Mixed Use Building Facade Improvement and Retail Investment grants that have lapsed, that all funds **BE UNCOMMITTED** and returned to CIP Reserve Fund 226 if the applicant has not completed the work and fulfilled the conditions within 2 years of the approval date; and,
- VIII. THAT any unused portion of the CIP grants **BE RETURNED** to the CIP Reserve Fund 226. Carried.

Report Number: S 101/2025
Clerk's File: Z/15020

12. COMMITTEE MATTERS

None presented.

13. QUESTION PERIOD

None registered.

14. ADJOURNMENT

There being no further business the meeting of the Development & Heritage Standing Committee (Administrative Items) is adjourned at 5:59 o'clock p.m. The next meeting of the Development & Heritage Standing Committee will be held on October 6, 2025.
Carried.

Ward 10 – Councillor Jim Morrison
(Chairperson)

Deputy City Clerk / Supervisor of
Council Services



Committee Matters: SCM 325/2025

Subject: Adoption of the Development & Heritage Standing Committee minutes of its meeting held October 6, 2025

Development & Heritage Standing Committee Meeting

Date: Monday, October 6, 2025

Time: 4:30 o'clock p.m.

Members Present:

Councillors

Ward 1 - Councillor Fred Francis

Ward 7 - Councillor Angelo Marignani

Ward 9 - Councillor Kieran McKenzie

Ward 10 - Councillor Jim Morrison (Chairperson)

Councillor Regrets

Ward 4 - Councillor Mark McKenzie

Members

Member Anthony Arbour

Member Regrets

Member Daniel Grenier

Member Robert Polewski

PARTICIPATING VIA VIDEO CONFERENCE ARE THE FOLLOWING FROM ADMINISTRATION:

Matthew Johnson, Executive Director, Economic Development

Sandra Gebauer, Council Assistant

ALSO PARTICIPATING IN COUNCIL CHAMBERS ARE THE FOLLOWING FROM ADMINISTRATION:

Jelena Payne, Deputy Chief Administrative Officer / Commissioner, Economic Development

Neil Robertson, City Planner

Greg Atkinson, Deputy City Planner – Development

Jason Campigotto, Deputy City Planner – Growth

Aaron Farough, Senior Legal Counsel

Sahar Jamshidi, Manager, Road Safety

Patrick Winters, Manager, Development

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Laura Diotte, Manager, Planning
Elara Mehrilou, Supervisor, Coordinator Maintenance
Shannon Mills, Technologist III
Frank Garado, Planner III – Policy & Special Studies
Adam Szymczak, Planner III - Development
Brian Nagata, Planner III - Development
Anna Ciacelli, Deputy City Clerk

Delegations—participating via video conference

None.

Delegations—participating in person

Item 7.1, 7.2, & 7.3 - Tracey Pillon-Abbs, Principal Planner, Pillon Abbs Inc.
Item 7.1 - Brent Klundert, President, BK Cornerstone
Item 7.1 - Andi Shallvari, Representing Property Owners
Item 7.2 - Dan Coccimiglio, Area Resident
Item 11.1 - Kip Brouwer, Area Resident
Item 11.1 - Grazyna Stachyra, Area Resident

1. CALL TO ORDER

The Chairperson calls the meeting of the Development & Heritage Standing Committee to order at 4:30 o'clock p.m.

2. DISCLOSURES OF PECUNIARY INTEREST AND THE GENERAL NATURE THEREOF

None disclosed.

3. REQUEST FOR DEFERRALS, REFERRALS OR WITHDRAWALS

See item 7.1.

4. COMMUNICATIONS

None presented.

5. ADOPTION OF THE *PLANNING ACT* MINUTES

5.1. Adoption of the Development & Heritage Standing Committee minutes (*Planning Act*) of its meeting held September 2, 2025

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Moved by: Councillor Angelo Marignani
Seconded by: Member Anthony Arbour

THAT the *Planning Act* minutes of the Development & Heritage Standing Committee meeting held September 2, 2025 **BE ADOPTED** as presented.
Carried.

Report Number: SCM 288/2025

6. PRESENTATION DELEGATIONS (*PLANNING ACT* MATTERS)

See items 7.1. through 7.3.

7. *PLANNING ACT* MATTERS

7.1. ZBA Application - 475 Cabana Road W - Z012-2025 [ZNG/7306] -Ward 1

Moved by: Councillor Fred Francis
Seconded by: Councillor Angelo Marignani

- 1) THAT the report of the Planner III – Policy & Special Studies dated August 6, 2025 entitled, “ZBA Application - 475 Cabana Road West – Z 012-2025 [ZNG/7306] - Ward 1” **BE REFERRED** back to administration to allow for their review and comment regarding the new proposal; and that the information **BE BROUGHT FORWARD** to a future Development & Heritage Standing Committee meeting.

Carried.

Report Number: S 103/2025
Clerk's File: ZB/15009

7.2. ZBA Application - 619 Cabana Rd W – Z 013-2025 (ZNG/7307) - Ward 1

Moved by: Councillor Kieran McKenzie
Seconded by: Member Anthony Arbour

Decision Number: **DHSC 770**

- I. THAT Zoning By-law 8600 **BE AMENDED** by adding the following zoning exception to Section 91.10:
 - x. **SOUTH SIDE OF CABANA ROAD WEST, BETWEEN DOUGALL AVENUE AND MCGRAW AVENUE**

For the lands comprising of the west Part Lot 5, Registered Plan 1478, a *Multiple Dwelling* containing up to 6 dwelling *units* shall be an additional permitted *main use*,

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and the following additional provisions shall apply to a *Multiple Dwelling* containing up to 6 dwelling units:

- a) *Lot Width: minimum* 15.0 m
- b) *Lot Area: minimum* 540 m²
- c) *Lot Coverage: maximum* 35.0%
- d) *Building height: maximum* 9.0 m
- e) *Front Yard Depth: minimum / maximum* 6.0 m / 7.0 m
- f) *Rear Yard Depth for main building: minimum* 20.0 m
- g) *Side Yard Width: minimum* 1.20 m
- h) In any *required front yard*, a *parking space* is prohibited.
- i) Notwithstanding Section 25.5.20.1.5, the *minimum* parking area separation from a *building* wall in which is located a main pedestrian entrance facing the *parking area* shall be 1.5 m.
- j) A *screening fence* with a *minimum* height of 1.8 m shall be provided along the south *lot line*.
- k) A landscaped area with a minimum width of 2.0 m shall be provided abutting the south *lot line*.
- l) A minimum of 50% of the north exterior wall elevation shall be covered in masonry, brick or any combination thereof. A minimum of 35% of east and west exterior wall elevations shall be covered in masonry, brick or any combination thereof.

[ZDM 9; ZNG/7307]

- II. THAT Zoning By-law 8600 **BE FURTHER AMENDED** by changing the zoning of the west Part Lot 5, Registered Plan 1478, situated on the south side of Cabana Road West between Dougall Avenue and McGraw Avenue, from RD1.4 to RD1.4x(x).
- III. THAT when Site Plan Control is applicable:
 - A. Prior to the submission of an application for site plan approval, at the discretion of the City Planner, Deputy City Planner, or Site Plan Approval Officer:

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Development & Heritage Standing Committee

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1. Those documents submitted in support of the application for amendment to the Zoning By-law 8600 **BE UPDATED** to reflect the site plan for which approval is being sought, and any comments from municipal departments and external agencies included in Appendix D.

Carried.

Councillor Fred Francis voting nay.

Report Number: C 108/2025

Clerk's File: ZB/15008

7.3. Rezoning Application – 4325-4445 Cabana Rd E - Z-018/25 ZNG/7315 - Ward 9

Moved by: Councillor Fred Francis

Seconded by: Councillor Angelo Marignani

Decision Number: **DHSC 771**

1. THAT Zoning By-law 85-18 **BE AMENDED** by renumbering paragraph 3.101b to 3.101c and paragraph 3.101a to 3.101b and by adding the following new definition to Section 3:

3.101a **POWER GENERATION FACILITY** is an industrial activity and means premises used to generate electricity and may include a power distribution station or a transformer station.
[ZNG/7315]

2. THAT Zoning By-law 85-18 **BE FURTHER AMENDED** by adding the following defined area to Section 14:

14.3.17 Defined Area M1-17 as shown on Zoning Map T12 of this By-law.

a) Permitted Uses

- 1) All uses permitted in the Industrial (M1) zone save and except a dwelling or dwelling unit.
- 2) *Power Generation Facility.*

b) Permitted Buildings and Other Structures

No building or structures shall be used or erected in Defined Area M1-17 except for the following purposes:

- 1) buildings and structures for the permitted uses.
- 2) accessory buildings and structures for the permitted uses.

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c) Other Requirements

- 1) The regulations of subsection 14.1.3 to 14.2.4 and all other applicable regulations shall apply to any use permitted in subsection 14.3.17 a).

(ZNG/7315)

3. THAT Zoning By-law 85-18 **BE FURTHER AMENDED** by changing the zoning of CON 7; PT GORE LOT 17; RP 12R8108; PARTS 4; 5; 7 & 8; PT PARTS 1 TO 3; 6 & 9; RP 12R28421; PARTS 4 TO 6; PIN 75235-0176, situated at the southwest corner of Cabana Road East and 8th Concession Road (4325-4445 Cabana Road East; Roll No. 090-010-04950) from M1 to M1-17.
4. THAT, when Site Plan Control is applicable:
 - A. The following additional materials **BE SUBMITTED** with an application for site plan approval, and **BE SUBJECT** to the satisfaction of the City Planner, Deputy City Planner, or Site Plan Approval Officer:
 - 1) A noise study.
 - 2) A copy of the confirmation email from Transport Canada that an Aeronautical Assessment Form (AAF) has been submitted to Transport Canada.
 - 3) Confirmation of submission of a Land Use Form to the NAV CANADA Land Use Web Submission Portal.
 - B. The Site Plan Approval Officer **BE DIRECTED** to incorporate the following into an approved site plan and an executed and registered site plan agreement:
 - 1) Any noise mitigation measures identified in a Noise Study, subject to the approval of the Site Plan Approval Officer.
 - 2) Provision of Transport Canada's determination regarding the proposed Power Generation Facility to the satisfaction of the City Planner, Deputy City Planner, or Site Plan Approval Officer.
 - 3) Provision of NAV CANADA's assessment of the proposed Power Generation Facility to the satisfaction of the City Planner, Deputy City Planner, or Site Plan Approval Officer.

Carried.

Report Number: S 109/2025
Clerk's File: Z/15030

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There being no further business the meeting of the Development & Heritage Standing Committee (*Planning Act* Matters) portion is adjourned at 5:59 o'clock p.m.

The Chairperson calls the Administrative Items portion of the Development & Heritage Standing Committee meeting to order at 6:00 o'clock p.m.

8. ADOPTION OF THE MINUTES

None presented.

9. PRESENTATIONS AND DELEGATIONS (COMMITTEE ADMINISTRATIVE MATTERS)

See item 11.1.

10. HERITAGE ACT MATTERS

None presented.

11. ADMINISTRATIVE ITEMS

11.1. Joy Road Sanitary Sewer - Oversizing - Ward 10

Kip Brouwer, Area Resident

Kip Brouwer, Area Resident, appears before the Development and Heritage Standing Committee regarding the administrative report dated September 17, 2025, entitled "Joy Road Sanitary Sewer - Oversizing - Ward 10" and is available for questions.

Grazyna Strachyra, Area Resident

Grazyna Strachyra, Area Resident, appears before the Development and Heritage Standing Committee regarding the administrative report dated September 17, 2025, entitled "Joy Road Sanitary Sewer - Oversizing - Ward 10" to express concerns about whether connecting to the sanitary sewer will be mandatory for residents. Ms. Strachyra also asks if the city will perform the sewer hookup, or if residents will have to find their own contractors and inquires about the approximate cost to residents for this work.

Councillor Jim Morrison seeks to clarify whether Ms. Strachyra's property pertains to this project. Patrick Winters, Manager, Development, appears before the Development and Heritage Standing Committee regarding the administrative report dated September 17, 2025, entitled "Joy Road Sanitary Sewer - Oversizing - Ward 10" to explain that the matter before Committee is only concerning the oversizing of the sewer for a potential future extension easterly. The sewer extension itself is a separate project that will be subject to the Local Improvement policies of the

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city, and the delegate's property would be part of that Local Improvement in the future. Mr. Winters states that the city does intend to install sanitary sewers on Joy Road, but that project is currently subject to appeal with the Ontario Land Tribunal.

Councillor Angelo Marignani asks if there is a mandatory requirement for properties to connect to sanitary sewers once they are installed. Aaron Farough, Senior Legal Council, appears before the Development and Heritage Standing Committee regarding the administrative report dated September 17, 2025, entitled "Joy Road Sanitary Sewer - Oversizing - Ward 10" and replies it is his understanding that a resident has appealed the decision to make the sanitary sewer installation subject to a Local Improvement assessment charge and the city must apply to the Ontario Land Tribunal in order to continue.

Councillor Angelo Marignani asks where the funding for this project is coming from. Patrick Winters replies that the city would be paying for the cost of oversizing the sewer, as is typical when developers are required to install larger-sized sewers.

Councillor Kieran McKenzie asks if the Joy Road residents would be given consideration for cost mitigation as this project is not a community-driven Local Improvement. Shannon Mills, Technologist III, appears before the Development and Heritage Standing Committee regarding the administrative report dated September 17, 2025, entitled "Joy Road Sanitary Sewer - Oversizing - Ward 10" to explain that the city is seeking an extension of the one-year compliance period to three years, so residents are not required to connect to the sewer right away. If the Local Improvement proceeds in future, they will be charged the local improvement rates to connect. If the Local Improvement does not go through, residents will be required to find a contractor to connect them to the sewer at a potentially higher cost.

Councillor Kieran McKenzie asks if residents who have recently replaced their septic systems would be given any extra consideration with regards to connecting to the new sanitary sewer. Patrick Winters replies yes, that is the intention, and the city is working to minimize the impact to residents as much as possible.

Councillor Kieran McKenzie asks if residents will have the option to connect to the sewer sooner should they wish. Patrick Winters replies that waiting for the Local Improvement to be approved will benefit residents with a lower connection cost.

Moved by: Councillor Kieran McKenzie

Seconded by: Councillor Fred Francis

Decision Number: **DHSC 772**

- I. THAT Council **APPROVE** \$35,773.54 (inclusive of HST) to be paid to Brouwer Enterprises Inc. as the City of Windsor's portion of the sanitary sewer oversizing costs for the Joy Road Sanitary Sewer Development to be funded from the Project ID #7035119 – New Infrastructure Development; and further,

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- II. THAT Council **APPROVE** an extension of the standard one-year compliance period to a total of three years for the property owners of lands municipally known as 4610, 4635, and 4650 Joy Road, to meet the requirements of the Property Standards By-law 9-2019, Schedule A, section 1.21. The extended compliance period shall begin on the date the City issues a formal notice to the property owners requiring connection to the sanitary sewer.

Carried.

Report Number: S 117/2025

Clerk's File: SL2025

11.2. Part Closure of east/west alley located between Seventh Street and Eighth Street, SAA-7320 - Ward 1

Moved by: Councillor Fred Francis

Seconded by: Councillor Angelo Marignani

Decision Number: **DHSC 773**

- I. THAT the 125.0 metre portion of the 4.57-metre-wide east/west alley, located between Seventh Street and Eighth Street, and shown on Drawing No. CC-1873 (attached hereto as Appendix "A"), and hereinafter referred to as the "Subject Alley", **BE ASSUMED** for subsequent closure;
- II. THAT the portion of the Subject Alley shown as Part 1 on Drawing No. CC-1873 **BE CLOSED AND CONVEYED** in as is condition to the abutting property owners and as necessary, in a manner deemed appropriate by the City Planner;
- III. THAT the portion of the Subject Alley shown as Part 2 on Drawing No. CC-1873 **BE CLOSED AND RETAINED** by The Corporation of the City of Windsor and as necessary, in a manner deemed appropriate by the City Planner;
- IV. THAT Conveyance Cost **BE SET** as follows in accordance with CR106/2003, as amended by CR427/2003 & CR182/2005:
- a. For alley conveyed to abutting lands zoned RD1.1, \$1.00 plus HST, deed preparation fee and a proportionate share of the survey costs as invoiced to The Corporation of the City of Windsor by an Ontario Land Surveyor;
- V. THAT The City Planner **BE REQUESTED** to supply the appropriate legal description, in accordance with Drawing No. CC-1873;
- VI. THAT The City Solicitor **BE REQUESTED** to prepare the necessary by-law(s);
- VII. THAT The Chief Administrative Officer and City Clerk **BE AUTHORIZED** to sign all necessary documents approved as to form and content satisfactory to the City Solicitor; and,

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Monday, October 6, 2025

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VIII. THAT the matter **BE COMPLETED** electronically pursuant to By-law Number 366-2003.
Carried.

Report Number: S 115/2025

Clerk's File: SAA2025

12. COMMITTEE MATTERS

12.1. Minutes of the International Relations Committee of its meeting held September 5, 2025

Moved by: Councillor Fred Francis

Seconded by: Councillor Angelo Marignani

Decision Number: **DHSC 774**

THAT the minutes of the International Relations Committee meeting held September 5, 2025 **BE RECEIVED** as presented.

Carried.

Report Number: SCM 293/2025

Clerk's File: MB2025

12.2. Report No. 59 of the International Relations Committee

Moved by: Councillor Fred Francis

Seconded by: Councillor Angelo Marignani

Decision Number: **DHSC 775**

THAT Report No. 59 of the International Relations Committee indicating:

THAT the results of the e-mail poll conducted on July 2, 2025 approving the following recommendation **BE CONFIRMED AND RATIFIED**:

THAT the International Relations Committee **MOVE FORWARD** with scheduling reciprocal site visits with Arlington, Texas, as a starting point for exploring a potential partnership; and,

THAT the delegations **INCLUDE** representation from the Committee as well as representation from Tourism and Economic Development; and,

THAT the Committee **APPROVE** a budget in the upset amount of \$15,000 to support costs associated with both the incoming and outgoing delegations.

BE APPROVED.

Carried.

Report Number: SCM 297/2025

Clerk's File: MB2025

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Development & Heritage Standing Committee Monday, October 6, 2025

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13. QUESTION PERIOD

None registered.

14. ADJOURNMENT

There being no further business the meeting of the Development & Heritage Standing Committee (Administrative Items Matters) is adjourned at 6:12 o'clock a.m. The next meeting of the Development & Heritage Standing Committee will take place on November 3, 2025
Carried.

Ward 10 – Councillor Jim Morrison
(Chairperson)

Deputy City Clerk / Supervisor of
Council Services

Subject: Notice of Intent to Partially Demolish a Heritage Listed Property – 734 Kildare Road, Warren Seagrave House - Ward 4

Reference:

Date to Council: November 3, 2025
Author: Tracy Tang, MCIP, RPP
Planner III - Heritage (A)
Email: ttang@citywindsor.ca
Phone: 519-255-6543 X 6179

Erin Jeong
Community Development Planning Assistant
Email: sjeong@citywindsor.ca
Phone: 519-255-6543 X 6438
Planning & Building Services
Report Date: 10/10/2025
Clerk's File #: MBA/14987

To: Mayor and Members of City Council

Recommendation:

- I. **THAT** the Notice of Intent to partially demolish the heritage listed property at 734 Kildare Road, Warren Seagrave House, to remove the existing one-storey detached garage structure at the rear of the property **BE RECEIVED**.

Executive Summary:

N/A

Background:

The property at 734 Kildare Road, known as the Warren Seagrave House, was listed on the Windsor Municipal Heritage Register on August 27, 2007. It is identified on the Heritage Register as a local vernacular style house built circa 1900. The property is also located within the Walkerville Heritage Area.



Front (west-facing) facade of 734 Kildare Road, with the detached garage visible at the rear (photo taken by City Staff on October 8, 2025).

On September 9, 2025, the Property Owners reached out to Heritage Planning staff to inquire about the process to facilitate the demolition of the one-storey detached garage structure at the rear of their property. As the proposal is for the demolition/removal of a building or structure on a heritage listed property, Heritage Planning staff advised the Property Owners of the heritage planning requirements. On September 15, 2025, the Property Owners formally submitted a complete Notice of Intent to Partially Demolish package (Appendix 'A').

Legal Provisions:

The subject property is listed (not designated) on the Windsor Municipal Heritage Register. Section 27 of Part IV of the *Ontario Heritage Act (OHA)* states that “the register may include property ... that the council of the municipality believes to be of cultural heritage value or interest”, without being designated. Also, “[T]he owner of the property shall not demolish or remove a building or structure on the property or permit the demolition or removal of the building or structure unless the owner gives the council of the municipality at least 60 days notice in writing of the owner’s intention to demolish or remove the building or structure or to permit the demolition or removal of the building or structure.” The 60 days only begins after notice is received accompanying plans and information as Council may require. City of Windsor Council approved “Requirements and Procedures, Application for Demolition of Heritage-Listed Properties” (Council Decision # M163-2015) which outlines the required information for demolition, and notes that Administration has 30 days to evaluate if the information submitted is sufficient. Only after determination has been made that the required information has been submitted, does the 60-day count begin.

For heritage-listed (non-designated) properties, the *OHA* does not provide explicit provisions for Council to comment on or approve/consent to alterations. The *OHA* only requires notification to Council for the demolition/partial demolition of a heritage-listed property. There is no explicit provision for Council to approve or consent to the demolition and no provision to apply conditions. Should Committee or Council wish to comment on alterations to a heritage-listed (non-designated) property, the only course of action available is to initiate designation.

Through the *Ontario Heritage Act*, City Council (with Committee consultation) may either initiate designation during the 60 days after notice of intent to demolish, or decide

to take no action. If a property is proposed for designation, a notice of intent to designate must include a statement explaining the cultural heritage value or interest of the property and a description of the heritage attributes of the property, which are those features that are considered important to retain if any alterations to the property are proposed after designation. "Cultural heritage value or interest" is to be considered according to Ontario Regulation 9/06.

Discussion:

Proposal:

The complete Notice of Intent to Demolish package for 734 Kildare Road was submitted with a heritage permit application form to detail the proposal and photographs of the current condition of the garage structure. The Property Owners are requesting to demolish the one-storey detached garage structure at the rear of the property and restore the grading of the site. The complete package is included as Appendix 'A' of this report.



The rear detached garage structure that is proposed to be demolished (photos provided by Property Owners).

The detached one-storey garage structure is located at the rear south-east corner of the property. The garage is clad in beige painted stucco siding and features a gable roof. The garage was built in 1900 along with the main residential building on the property. The Property Owners explained that the garage currently extends beyond the rear property line.



Damaged cladding, rotting wood, and cracks on the rear detached garage structure (photos taken by City Staff on October 8, 2025).

The Property Owners have noted that the existing garage structure is in poor physical condition. The roof is sagging, there is visible rot in the walls, the exterior finish is deteriorating, and the structure was previously infested with rats, now exterminated. Additionally, the structure extends beyond the rear property line, which contributes to site constraints. Based on these factors, it is the Property Owners' position that repair or restoration is not feasible, and demolition is necessary. The demolition of the rear detached garage is intended to resolve the structural issues and deterioration and allow for the site to be graded.

Heritage Considerations:

The Warren Seagrave House is not designated under the *Ontario Heritage Act*. The property is listed on the Windsor Municipal Heritage Register for its local vernacular architectural style. The proposed demolition of the detached garage, located at the rear of the property, is due to advanced deterioration, structural failure, and encroachment beyond the rear property line. The garage does not appear to have any physical or design value and has been altered using modern materials. It does not contribute any heritage value to the property.

In review of the proposal, Heritage Planning staff are of the opinion that the removal of the garage does not adversely impact the heritage character of the listed property. Proposal appears to be compatible with both the heritage listed property and the Walkerville Heritage Area. The *Standards & Guidelines for the Conservation of Historic Places in Canada* have been considered, however they largely apply to character-defining elements. The garage is not a character-defining element of the heritage listed property. The garage structure is currently damaged and failing, and visible from the public realm (sidewalk on Kildare Road). Its removal could improve the appearance of the property and neighbourhood.

A Building Permit is required for the removal of the garage structure, and is subject to the Notice of Intent to Partially Demolish. The Property Owners may proceed with the Building Permit should Council receive the notice and not initiate heritage designation of the property.

Official Plan Policy:

Chapter 9 of the City of Windsor Official Plan, Heritage Conservation, includes the following objectives related to the recognition, conservation, and enhancement of heritage resources:

CONSERVATION 9.2.1 MANAGEMENT

To conserve Windsor's heritage resources for the benefit of the community and posterity in a manner which respects their architectural, historical, and contextual significance and ensures their future viability as functional components of Windsor's urban environment.

HERITAGE 9.3.5.1 AREAS AND HERITAGE CONSERVATION DISTRICTS

Council will enhance heritage resources by:

- (a) Ensuring that within any Heritage Area or Heritage Conservation District that:
 - (ii) Development be of compatible height, massing, scale, setback and architectural style;

Risk Analysis:

The proposed partial demolition would allow the Owners to proceed with removing a deteriorated structure from their property and regrading the site. The notification of the proposed partial demolition authorized by the *Ontario Heritage Act* serves as an opportunity to provide a process to designate when required to prevent inappropriate and concerning changes applied to the heritage property. Initiating designation of the property would put a hold on any building permits and therefore pause any ability to undertake the works proposed. This is very seldom the recommended action, and it would only be recommended in circumstances where the heritage integrity of the listed property is at risk.

The initiation of Part IV individual heritage designation of the property as a reaction to this Notice of Intent to Partially Demolish is not recommended by Administration because of three key reasons. Firstly, the designation process would place delays on the ability of the Property Owners to address the deteriorated structure on their property. Secondly, the proposed demolition has no adverse impact on the heritage value of the property or integrity of the Walkerville Heritage Area. Thirdly and finally, the designation of this heritage-listed property is being considered as part of the Walkerville Heritage Conservation District project, under Part V of the *Ontario Heritage Act*.

Climate Change Risks

Climate Change Mitigation:

N/A

Climate Change Adaptation:

N/A

Financial Matters:

There is no cost to the City; the Property Owners will be paying the full cost of the proposed demolition and site regrading.

Consultations:

Heritage Planning staff discussed the details of the proposal and application requirements with the Property Owners, William A McCay and Linda A McCay. In the preparation of this report, Heritage Planning staff consulted with the Building Department regarding permit requirements and Aaron Farough, Senior Legal Counsel, Legal & Real Estate Department, regarding the legal provisions within the *Ontario Heritage Act*.

Conclusion:

Administration recommends that City Council receive the proposed partial demolition of the Heritage Listed Property – 734 Kildare Road, Warren Seagrave House. The demolition/removal of the existing one-storey detached garage structure at the rear of the property is proposed to address the structural issues and deterioration of the garage. The proposal will have no negative impacts to the heritage character of the listed property nor to the Walkerville Heritage Area.

Planning Act Matters:

N/A

Approvals:

Name	Title
Jason Campigotto	Deputy City Planner – Growth (A)
Neil Robertson	City Planner
Aaron Farough	Senior Legal Counsel, Legal & Real Estate
Jelena Payne	Deputy CAO/Commissioner of Economic Development
Ray Mensour	Chief Administrative Officer

Notifications:

Name	Address	Email

Appendices:

- 1 Appendix 'A' – Notice of Intent to Partially Demolish Package
- 2 Appendix 'B' – Additional Photos of 734 Kildare Road



Planning Department
Suite 320-350 City Hall Sq W
Windsor ON N9A 6S1

HERITAGE PERMIT APPLICATION

Revised 11/2023

APPLICATION TYPE

☐ **Minor Heritage Permit**
(Delegated Authority Approval)

☐ **Major Heritage Permit**
(City Council Approval)

☒ **Demolition**

1. APPLICANT, REGISTERED OWNER, AND AGENT INFORMATION

Provide in full the name of the applicant, registered owner and agent, the name of the contact person, and address, postal code, phone number, and email address. If the applicant or registered owner is a numbered company, provide the name of the principals of the company. If there is more than one applicant or registered owner, copy this page, complete in full and submit with this application.

APPLICANT

Contact Name(s) William A McCay
Company or Organization _____
Mailing Address 734 Kildare Rd
City, Province Windsor, Ontario Postal Code N8Y3H2
Email [REDACTED] Phone(s) [REDACTED]

REGISTERED OWNER IF NOT APPLICANT

Contact Name(s) Linda A McCay
Company or Organization _____
Mailing Address 734 Kildare Rd
City, Province Windsor, Ontario Postal Code N8Y3H2
Email [REDACTED] Phone(s) [REDACTED]

AGENT AUTHORIZED BY REGISTERED OWNER TO FILE THE APPLICATION

Contact Name(s) _____
Company or Organization _____
Mailing Address _____
City, Province _____ Postal Code _____
Email _____ Phone(s) _____

Who is the primary contact?

☒ Applicant

☐ Registered Owner

☐ Agent

2. SUBJECT PROPERTY

Municipal Address: 734 Kildare Rd

Legal Description (if known): PLAN 211; PT LOTS 3 & 5; PT BLOCK P

Building/Structure Type:

☒ Residential ☐ Commercial ☐ Industrial ☐ Institutional

Heritage Designation:

☐ Part IV (Individual) ☐ Part V (Heritage Conservation District)

By-law #: _____ District: _____

Is the property subject to a Heritage Easement or Agreement?

☐ Yes ☒ No

3. TYPE OF APPLICATION

Check all that apply:

☐ Demolition/Removal of heritage attributes ☐ Addition ☐ Erection ☐ Alteration*
☒ Demolition/Removal of building or structure ☐ Signage ☐ Lighting

*The Ontario Heritage Act's definition of "alter" means to change in any manner and includes to restore, renovate, repair or disturb.

4. HERITAGE DESCRIPTION OF BUILDING

Describe the existing design or appearance of buildings, structures, and heritage attributes where work is requested. Include site layout, history, architectural description, number of storeys, style, features, etc..

The Warren Seagrave House was constructed in a local vernacular style. The house is
clad in horizontal siding and features a full width covered front porch supported by
Doric-style columns, a bay window, a chimney, and a hipped roof with large gable end
dormers. The detached garage is located in the south-east corner of the property and
features a gable roof.

5. PROPOSED WORK

Provide a detailed written description of work to be done, including any conservation methods you plan to use. Provide details, drawings, and written specifications such as building materials, measurements, window sizes and configurations, decorative details, etc.. Attach site plans, elevations, product spec sheets, etc. to illustrate, if necessary.

Demolition of existing garage and restoration of site grading

6. HERITAGE PERMIT RATIONALE

Explain the reasons for undertaking the proposed work and why it is necessary.

This very old garage has rot in the walls, the roof is sagging, and the exterior finish is deteriorated. Rats were living in the walls but have been exterminated. The structure actually extends past the rear property line. It is not feasible to do repairs.

Describe the potential impacts to the heritage attributes of the property.

I do not feel the removal of this structure will have any negative impact on the heritage attributes of the property.

7. CHECKLIST OF MATERIALS SUBMITTED

Check all that apply:

Required:

- ☒ Photographs (showing the current condition and context of existing buildings, structures, and heritage attributes that are affected by the application)
- ☒ Site plan/ Sketch (showing buildings on the property and location of proposed work(s))
- ☐ Architectural drawings of proposed work(s) (e.g. existing and proposed elevations, floor plans, roof plans, etc., as determined by Heritage Planning staff)
- ☐ Specifications of proposed work(s) (e.g. construction specification details)

HERITAGE PERMIT APPLICATION

Revised 11/2023

Potentially required (to be determined by Heritage Planning staff):

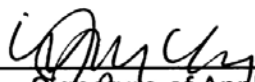
- ☐ Registered survey
- ☐ Material samples, brochures, product data sheets etc.
- ☐ Cultural Heritage Evaluation Report
- ☐ Heritage Impact Assessment (HIA)
- ☐ Heritage Conservation Plan
- ☐ Building Condition Assessment

8. NOTES FOR DECLARATION

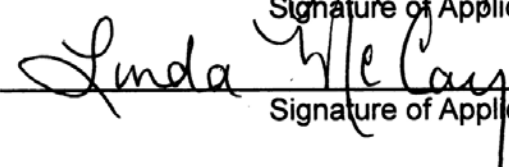
The applicant hereby declares that the statements made herein and information provided are, to the best of their belief and knowledge, a true and complete representation of the purpose and intent of this application.

The applicant agrees that the proposed work shall be done in accordance with this application, including attachments, and understands that the issuance of the Heritage Alteration Permit under the Ontario Heritage Act shall not be a waiver of any of the provisions of any By-Law of the Corporation of the City of Windsor, or the requirements of the Building Code Act, RSO 1980, c51.

The applicant acknowledges that in the event a permit is issued, any departure from the conditions imposed by the Council of the Corporation of the City of Windsor, or plans and specifications approved is prohibited and could result in the permit being revoked. The applicant further agrees that if the Heritage Alteration Permit is revoked for any cause of irregularity, in the relation to non-conformance with the said agreements, By-Laws, acts or regulations that, in consideration of the issuance of the permit, all claims against the City for any resultant loss or damage are hereby expressly waived.



Signature of Applicant(s)



Signature of Applicant(s)

Sept 14/25

Date

Sept 14/25

Date

SCHEDULE A

A. Authorization of Registered Owner for Agent to Make the Application

If the applicant is not the registered owner of the land that is the subject of this application, the written authorization of the registered owner that the applicant is authorized to make the application must be included with this application form or the authorization below must be completed.

I, Linda A McCay, am the registered owner of the land that is
name of registered owner

subject of this application for a Heritage Alteration Permit and I authorize

William McCay to make this application on my behalf.
name of agent


Signature of Registered Owner

Sept 14, 2025
Date

If Corporation – I have authority to bind the corporation.

B. Consent to Enter Upon the Subject Lands and Premises

I, Linda A McCay, hereby authorize the members of the Windsor Heritage Committee and City Council and staff of the Corporation of the City of Windsor to enter upon the subject lands and premises described in Section 3 of the application form for the purpose of evaluating the merits of this application and subsequently to conduct any inspections on the subject lands that may be required as condition of approval. This is their authority for doing so.


Signature of Registered Owner

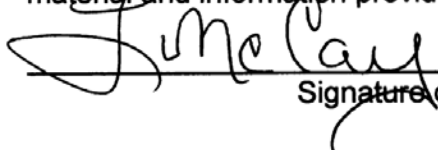
Sept 14, 2025
Date

If Corporation – I have authority to bind the corporation.

C. Acknowledgement of Applicant

I understand that receipt of this application by the City of Windsor Planning Department does not guarantee it to be a complete application. Further review of the application will occur and I may be contacted to provide additional information and/or resolve any discrepancies or issues with the application as submitted.

I further understand that pursuant to the provisions of the *Ontario Heritage Act* and the *Municipal Freedom of Information and Protection of Privacy Act*, this application and all material and information provided with this application are made available to the public.


Signature of Applicant

Sept 14, 2025
Date

HERITAGE PERMIT APPLICATION

Revised 11/2023

SCHEDULE A

A. Authorization of Registered Owner for Agent to Make the Application

If the applicant is not the registered owner of the land that is the subject of this application, the written authorization of the registered owner that the applicant is authorized to make the application must be included with this application form or the authorization below must be completed.

I, William A McCay, am the registered owner of the land that is
name of registered owner

subject of this application for a Heritage Alteration Permit and I authorize

William McCay to make this application on my behalf.
name of agent

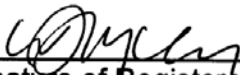

Signature of Registered Owner

Sept 14, 2025
Date

If Corporation – I have authority to bind the corporation.

B. Consent to Enter Upon the Subject Lands and Premises

I, William A McCay, hereby authorize the members of the Windsor Heritage Committee and City Council and staff of the Corporation of the City of Windsor to enter upon the subject lands and premises described in Section 3 of the application form for the purpose of evaluating the merits of this application and subsequently to conduct any inspections on the subject lands that may be required as condition of approval. This is their authority for doing so.


Signature of Registered Owner

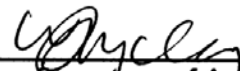
Sept 14, 2025
Date

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Signature of Applicant

Sept 14, 2025
Date

THIS PLAN IS NOT VALID
UNLESS IT IS AN EMBOSSED
ORIGINAL COPY
ISSUED BY THE SURVEYOR
In accordance with
Regulation 1026, Section 29 (3)PLAN OF SURVEY
OF
PART OF LOTS 3 AND 5
IN BLOCK 'P'
REGISTERED PLAN 211
IN THE
CITY OF WINDSOR
COUNTY OF ESSEX, ONTARIO

© VERHAEGEN LAND SURVEYORS

SCALE : 1"=20'



LEGEND

- DENOTES SURVEY MONUMENT FOUND
□ DENOTES SURVEY MONUMENT SET
SIB DENOTES STANDARD IRON BAR
SSIB DENOTES SHORT STANDARD IRON BAR
IB DENOTES IRON BAR
PB DENOTES PLASTIC BAR
WIT DENOTES WITNESS
MEAS DENOTES MEASURED
JDB DENOTES J.D. BARNES LIMITED
- ALL SET SSIB AND PB MONUMENTS WERE USED DUE TO LACK OF OVERBURDEN
AND/OR PROXIMITY OF UNDERGROUND UTILITIES IN ACCORDANCE WITH
SECTION 11 (4) OF O.REG. 525/91.
- (S/P) DENOTES SET PROPORTIONALLY (OU) DENOTES ORIGIN UNKNOWN
(P) DENOTES REGISTERED PLAN 211
(P1) DENOTES PLAN OF SURVEY BY (KVL), DATED: DEC 23, 1976, PLAN FILE: W76-L885
(P2) DENOTES PLAN OF SURVEY BY (1194), DATED: APRIL 14, 1986, PLAN FILE: 4V-8561
(1744) DENOTES VERHAEGEN STUBBERFIELD HARTLEY BREWER BEZAIRE INC., O.I.S.

THIS PLAN MUST BE READ IN CONJUNCTION WITH PART 2 AS SHOWN BELOW
THIS REPORT WAS PREPARED FOR WILLIAM McCAY

SURVEYOR'S REAL PROPERTY REPORT PART 2 OF 2 PARTS

Please note the following:

1. Iron Survey Monuments mark all corners of the parcel.
2. The parcel has an Area of 5941 Sq. Ft. (0.14 Acres).
3. The location of the stucco garage that extends 0.5' into the City Alley.

SURVEYOR'S CERTIFICATE

I CERTIFY THAT:

1. THIS SURVEY AND PLAN ARE CORRECT AND IN ACCORDANCE WITH THE SURVEYS ACT, THE SURVEYORS ACT AND THE REGULATIONS MADE UNDER THEM.
2. THIS SURVEY WAS COMPLETED ON THE 20th DAY OF APRIL, 2021.

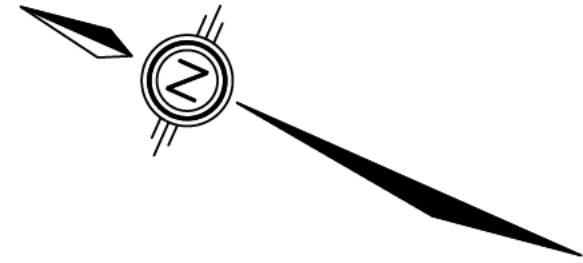
DATE MAY 19, 2021

ROY A. SIMONE
ONTARIO LAND SURVEYOR
for VERHAEGEN STUBBERFIELD HARTLEY
BREWER BEZAIRE INC.

VERHAEGEN
LAND SURVEYORS
A DIVISION OF J.D. BARNES LTD.
944 OTTAWA STREET, WINDSOR, ON, N8X 2E1
T: (519) 258-1772 F: (519) 258-1791 www.jdbarnes.com

SURVEYING
MAPPING
GIS

DRAWN BY: A.J.M.	CHECKED BY: DM/RAS	REFERENCE NO.: 21-47-136-00
FILE: 21-47-136-00.dwg	E-211-0	CAD Date: May 19, 2021 3:59 PM CAD File: 21-47-136-00.dwg



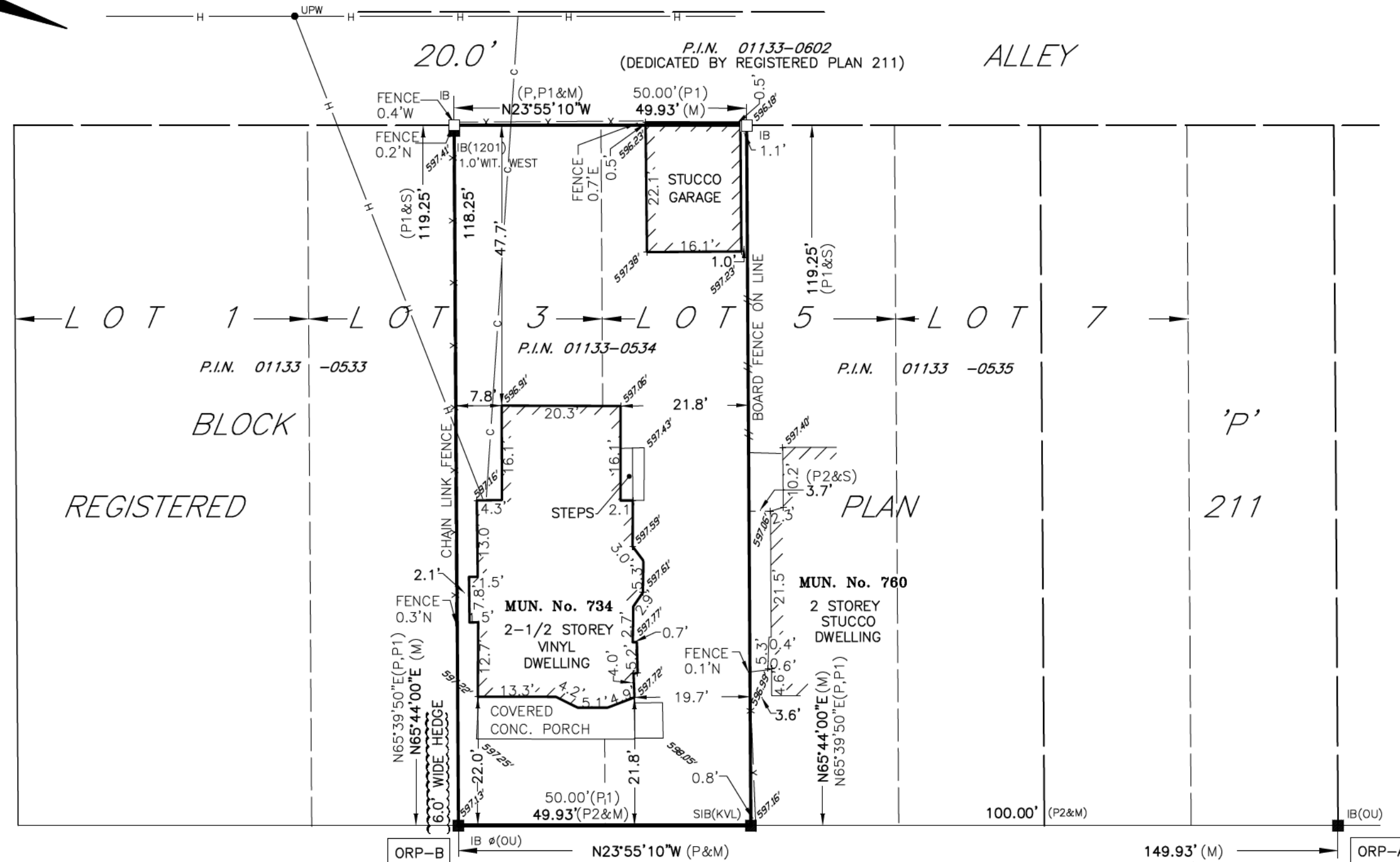
"IMPERIAL"

Distances and coordinates
shown on this plan are in feet
and can be converted to
metres by multiplying by 0.3048

NOTES

BEARINGS ARE UTM GRID DERIVED FROM OBSERVED REFERENCE POINTS "A" AND "B" BY
REAL TIME NETWORK OBSERVATIONS AND ARE REFERRED TO UTM ZONE 17 (81° WEST
LONGITUDE) NAD83 (CSRS) (2010.0).DISTANCES ON THIS PLAN ARE GROUND AND CAN BE CONVERTED TO GRID BY MULTIPLYING
BY THE COMBINED SCALE FACTOR OF 0.99991512

TUSCARORA ROAD



INTEGRATION DATA

COORDINATES ARE DERIVED FROM GRID OBSERVATIONS USING THE CAN-NET NETWORK SERVICE AND ARE REFERRED TO UTM ZONE 17 (81° WEST LONGITUDE) NAD83 (CSRS) (2010.0). COORDINATE VALUES ARE TO AN URBAN ACCURACY IN ACCORDANCE WITH SECTION 14(2) O.REG 216/10		
POINT ID	NORTHING	EASTING
ORP-A	N15378558.06	E1096494.09
ORP-B	N15378695.03	E1096433.34
COORDINATES CANNOT, IN THEMSELVES, BE USED TO RE-ESTABLISH CORNERS OR BOUNDARIES SHOWN ON THIS PLAN.		

FOR BEARING COMPARISON, A ROTATION OF 1°44'50" (CLOCKWISE) WAS APPLIED TO
P, P1 TO CONVERT TO GRID BEARINGS.

KILDARE ROAD

(DEDICATED BY REGISTERED PLAN 211)
P.I.N. 01133-0597

ELEVATIONS

ELEVATIONS SHOWN ON THIS PLAN ARE IN FEET TO CANADIAN GEODETIC DATUM 1928

BENCH MARK

BENCH MARK 824 ELEVATION 598.57'
M.B. 801 LINCOLN ROAD; THE PLATE IS LOCATED ON THE NORTH WALL 0.5'
FEET FROM THE ENTRANCE AND 2.0' FEET ABOVE GRADE.

AREA

0.14 ACRES





Appendix B – Additional Photos of 734 Kildare Rd

Taken by City Staff on October 8, 2025



Front (west-facing) and side (south-facing) facades of 734 Kildare Road, with the garage visible at the rear.



Front (west-facing) and side (north-facing) facades of the detached garage structure that is proposed to be demolished.



The rear alley (east-facing) and side (north-facing) facades of the detached garage structure that is proposed to be demolished. Large portions of the cladding material have fallen off or been removed.



Details on the detached garage structure. The windows have been covered for protection. Large cracks and patches are visible on the cladding material. Exposed wood material has been rotting and damaged by water and weathering.

Subject: Amendment to CR15/2020 for Closure of Perth Avenue located north of Chappus Street, Ward 1, SAS-5686

Reference:

Date to Council: November 3, 2025
Author: Brian Nagata, MCIP, RPP
Planner III - Development (A)
(519) 255-6543 ext. 6181

Planning & Building Services
Report Date: October 3, 2025
Clerk's File #: SAA2025

To: Mayor and Members of City Council

Recommendation:

I. THAT CR15/2020, adopted on January 6, 2020, **BE AMENDED** as follows:

By **DELETING** Section III in its entirety and replacing it with the following:

III. THAT Conveyance Cost **BE SET** as follows:

i. For street conveyed to abutting lands zoned DRD1.1, \$2.00 per square foot without easements plus HST and deed preparation fee.

II. THAT CR15/2020, adopted on January 6, 2020, **BE FURTHER AMENDED** to replace Drawing No. CC-1743 with the drawing attached hereto as Appendix "A".

Executive Summary:

N/A



STREET & ALLEY CLOSING (SAA/5686)

APPLICANT : JON LEEPER

 REQUEST FOR CLOSURE

PLANNING DEPARTMENT - PLANNING POLICY

DATE: OCTOBER, 2025

1:750



Background:

CR15/2020 was adopted by Council on January 6, 2020, directing administration to undertake the necessary steps to assume, close and convey the following street:

- Perth Avenue, north of Chappus Street.

The closure application was made by the owner of 1692 Chappus Street to satisfy an Order to Comply issued against them for the unauthorized use of Perth Avenue for the storage of personal items.

Since CR15/2020, it's been determined that Drawing CC-1743, that was included in CR15/2020, is incorrect. Drawing CC-1743 incorrectly identifies the northerly 7.78 feet-wide by 8.01-feet wide portion of Perth Avenue, south of the property known municipally as 1210-1484 Chappus Street, as an alley.

Perth Avenue consists of all of property ownership parcel P.I.N. No. 01263-1152 LT, and as such does not require a Reference Plan to be transferred to 1692 Chappus Street. The total size of the property is 6,734.98 square feet.

Discussion:

CR15/2020 must be amended to remove the survey costs as invoiced to The Corporation of the City of Windsor by an Ontario Land Surveyor from the Conveyance Cost set forth under Section III.

CR15/2020 must be further amended to replace Drawing No. CC-1743 with a new drawing (attached hereto as Appendix "A") that identifies all of Perth Avenue between Chappus Street and 1210-1484 Chappus Street.

Risk Analysis:

The recommended amendment to CR15/2020 poses no known risk to the City.

Climate Change Risks**Climate Change Mitigation:**

N/A

Climate Change Adaptation:

N/A

Financial Matters:

The recommended amendment to CR15/2020 does not impact the conveyance cost and there will be no additional costs to the operating department.

Reference Plan 12R-28371 was obtained and paid for by the City in October 2020. Therefore, no additional survey costs will be required, due to the ability to now transfer the Perth Avenue right-of-way to 1692 Chappus Street using the property ownership parcel (P.I.N. No. 01263-1152 LT).

The conveyance cost to be invoiced to the applicant is estimated as \$13,470 plus HST and will be recorded as a revenue to the Legal Department. The applicant has already submitted payment of \$529 for deed preparation, which has been allocated to the appropriate departments as per By-law Number 366-2003.

Consultations:

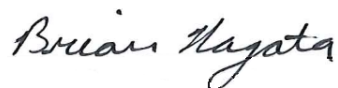
John Colella, Administrative Inspector, Operations
Josie Gualtieri, Financial Planning Administrator
Stephanie Santos, Coordinator Real Estate Services

Conclusion:

The Planning Department recommends that CR15/2020 be amended to remove the survey costs set forth under Section III as in Recommendation I of this report.

The Planning Department further recommends that CR15/2020 be amended to replace Drawing No. CC-1743 as in Recommendation II of this report.

Municipal Act Matters:



Brian Nagata, MCIP, RPP
Planner III - Development (A)

I concur with the above comments and opinion of the Registered Professional Planner.

Laura Diotte, MCIP, RPP
Manager - Planning (Development Applications)

Neil Robertson, MCIP, RPP
City Planner

I am not a Registered Professional Planner and have reviewed as a Corporate Team Leader

JP RM

Approvals:

Name	Title
Brian Nagata	Planner II - Development Review
Laura Diotte	Manager - Planning (Development Applications)
Emilie Dunnigan	Manager of Development Revenue & Financial Administration
Jamelah Hersh	Senior Legal Counsel
Neil Robertson	City Planner

Jelena Payne	Deputy CAO/Commissioner, Economic Development
Janice Guthrie	Commissioner, Finance & City Treasurer
Ray Mensour	Chief Administrative Officer

Appendices:

- 1 Appendix A - Drawing No. CC-1743

