

Development & Heritage Standing Committee Meeting Agenda

Date: Monday, October 7, 2024

Time: 4:30 o'clock p.m.

Location: Council Chambers, 1st Floor, Windsor City Hall

All members will have the option of participating in person in Council Chambers or electronically and will be counted towards quorum in accordance with Procedure By-law 98-2011 as amended, which allows for electronic meetings. The minutes will reflect this accordingly. Any delegations have the option to participate in person or electronically.

MEMBERS:

Ward 1 – Councillor Fred Francis

Ward 4 - Councillor Mark McKenzie

Ward 7 - Councillor Angelo Marignani

Ward 9 - Councillor Kieran McKenzie

Ward 10 - Councillor Jim Morrison (Chairperson)

Member Anthony Arbour

Member Joseph Fratangeli

Member Daniel Grenier

Member John Miller

Member Charles Pidgeon

Member Robert Polewski

Member Khassan Saka

Member William Tape

ORDER OF BUSINESS

Item #	Item Description
1.	CALL TO ORDER

READING OF LAND ACKNOWLEDGMENT

We [I] would like to begin by acknowledging that the land on which we gather is the traditional territory of the Three Fires Confederacy of First Nations, which includes the Ojibwa, the Odawa, and the Potawatomi. The City of Windsor honours all First Nations, Inuit and Métis peoples and their valuable past and present contributions to this land.

2.	DISCLOSURES OF PECUNIARY INTEREST AND THE GENERAL NATURE THEREOF
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3.	REQUEST FOR DEFERRALS, REFERRALS OR WITHDRAWALS
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4.	COMMUNICATIONS
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5.	ADOPTION OF THE <i>PLANNING ACT</i> MINUTES
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5.1.	Adoption of the Development & Heritage Standing Committee minutes (<i>Planning Act</i>) of its meeting held September 3, 2024 (SCM 289/2024)
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6.	PRESENTATION DELEGATIONS (<i>PLANNING ACT</i> MATTERS)
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7.	<i>PLANNING ACT</i> MATTERS
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7.1.	Zoning By-law Amendment Application for 4461 Wyandotte Street East, Z-023/24 [ZNG-7221], Ward 5 (S 113/2024)
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7.2.	Zoning By-Law Amendment Z001-24 (ZNG/7164) - Alta Nota Construction – 0 Windsor Ave & 1140 Goyeau St, Ward 3 (S 131/2024)
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8. ADOPTION OF THE MINUTES

9. PRESENTATIONS AND DELEGATIONS (COMMITTEE ADMINISTRATIVE MATTERS)

10. HERITAGE ACT MATTERS

11. ADMINISTRATIVE ITEMS

- 11.1. Brownfield Redevelopment Community Improvement Plan (CIP) application submitted by Farhi Holdings Corporation for 0 Riverside Drive West at Janette Avenue (Ward 3) **(S 123/2024)**
- 11.2. Economic Revitalization Community Improvement Plan (CIP) application submitted by Agri-Box Inc. for 3324 Marentette Avenue (Ward 9) **(S 124/2024)**
- 11.3. Amendment to CR58/2021, as amended by CR285/2023 for Closure of east/west alley located between Matthew Brady Boulevard and east limit of 7730 St. Rose Avenue, Ward 6, SAA-5947 **(S 117/2024)**
- 11.4. Amendment to CR419/2023 for Closure of east/west alley between Dieppe Street and Genevieve Avenue, Ward 6, SAA-6844 **(S 118/2024)**
- 11.5. Amendment to CR437/2023 for Closure of north/south alley located between Taylor Avenue and 1124-1224 Campbell Avenue, Ward 2, SAA-6848 **(S 119/2024)**
- 11.6. Amendment to CR26/2024 for Conveyance of north/south alley located between Closed Manitoba Street right-of-way and 1954 Huron Church Road, Ward 10, Closed by Judge's Order No. 2970/87 **(S 120/2024)**
- 11.7. Closure of east/west alley located between Alexis Road and Chandler Road, Ward 5, SAA-7197 **(S 121/2024)**
- 11.8. Amendment to Sign By-law 250-04 for 9250 Tecumseh Rd E, File No. SGN-002/24 (BILLBOARD) - Ward #7 **(S 111/2024)**
- 11.9. Brownfield Redevelopment Community Improvement Plan (CIP) application submitted by Rykka Care Centres LP for 0 Hanna St (Ward 3) **(S 129/2024)**
- 11.10. Amendment to CR372/2023 for Closure of north/south alley located between Algonquin Street and 1429 Randolph Avenue, Ward 10, SAA-7025 **(S 132/2024)**

12. COMMITTEE MATTERS

13. QUESTION PERIOD

14. ADJOURNMENT



Committee Matters: SCM 289/2024

Subject: Adoption of the Development & Heritage Standing Committee minutes (Planning Act) of its meeting held September 3, 2024

Development & Heritage Standing Committee Meeting
(Planning Act Matters)

Date: Tuesday, September 3, 2024

Time: 4:30 o'clock p.m.

Members Present:

Councillors

Ward 1 - Councillor Fred Francis
Ward 4 - Councillor Mark McKenzie
Ward 7 - Councillor Angelo Marignani
Ward 9 - Councillor Kieran McKenzie
Ward 10 - Councillor Jim Morrison (Chairperson)

Members

Member Anthony Arbour
Member Daniel Grenier

PARTICIPATING VIA VIDEO CONFERENCE ARE THE FOLLOWING FROM ADMINISTRATION:

Tracey Tang, Planner III – Economic Development
Sandra Gebauer, Council Assistant

ALSO PARTICIPATING IN COUNCIL CHAMBERS ARE THE FOLLOWING FROM ADMINISTRATION:

Jelena Payne, Commissioner, Economic Development
Neil Robertson, City Planner
Greg Atkinson, Deputy City Planner
Aaron Farough, Senior Legal Counsel
Patrick Winters, Manager, Development
Emilie Dunnigan, Manager, Development Revenue & Financial Administration
Kevin Alexander, Planner III - Special Projects
Brian Nagata, Planner II – Development Review
Elara Mehrilou, Transportation Planner I
Diana Radulescu, Planner II – Development Review
Natasha McMullin, Clerk Steno Senior
Anna Ciacelli, Deputy City Clerk

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Delegations—participating via video conference

Item 11.5 - Kaela McLellan, applicant
Item 11.3 - Cathy Cooke, area resident

Delegations—participating in person

Item 11.1 - Michel Matte, Owner, Granite Design Works, Inc.
Item 11.1 - Lawrence Anthony Lamont, area resident

1. CALL TO ORDER

The Chairperson calls the meeting of the Development & Heritage Standing Committee to order at 4:30 o'clock p.m.

2. DISCLOSURES OF PECUNIARY INTEREST AND THE GENERAL NATURE THEREOF

None disclosed.

3. REQUEST FOR DEFERRALS, REFERRALS OR WITHDRAWALS

None requested.

4. COMMUNICATIONS

None presented.

5. ADOPTION OF THE *PLANNING ACT* MINUTES

5.1. Adoption of the Development & Heritage Standing Committee minutes (*Planning Act*) of its meeting held August 6, 2024

Moved by: Councillor Angelo Marignani
Seconded by: Councillor Kieran McKenzie

THAT the *Planning Act* minutes of the Development & Heritage Standing Committee meeting held August 6, 2024 **BE ADOPTED** as presented.
Carried.

7. *PLANNING ACT* MATTERS

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7.1. Zoning By-Law Amendment 2019-24 (ZNG/7215) - Architecttura Inc. Architects – 1098 Albert Rd, Ward 5

Diana Radulescu, Planner II – Development Review, is available for questions.

Member Daniel Grenier inquires about the proposed outdoor bicycle parking spaces and whether they are a good amenity given there is active bike theft in the neighbourhood. Ms. Radulescu states that according to the zoning by-law the residential development up to four units does not require bicycle parking spaces but the applicant has included them within the conceptual site plan. This proposal can account for a reduction of one car parking space and connect to existing multi-modal options within the neighbourhood.

Member Grenier inquires with the applicant why three small bedroom units are proposed and not two bedrooms. The applicant or agent were not available to answer questions. Chair Jim Morrison states that the question can be raised at the Council meeting.

Councillor Kieran McKenzie inquires whether administration had considered a parking deficiency to be acceptable due to the proposed bicycle parking. Ms. Radulescu states that during the planning review, the multi-modal opportunities within the neighbourhood are considered when evaluating a proposed parking deficiency. Ms. Radulescu states that this neighbourhood, Ford City, is connected by existing and proposed active transportation, transit connections, sidewalks on both sides of the road and proximity to a main street (Drouillard Rd). Councillor McKenzie notes that it forms part of the decision that leads to the staff recommendation.

Moved by: Councillor Mark McKenzie

Seconded by: Councillor Angelo Marignani

Decision Number: **DHSC 646**

- I. THAT Zoning By-law 8600 **BE AMENDED** by changing the zoning of Lot 106, Plan 865 situated at the northeast corner of Albert Road and Richmond Street, known municipally as 1098 Albert Road, by adding a site-specific exception to Section 20(1) as follows:

512. NORTHEAST CORNER OF ALBERT ROAD AND RICHMOND STREET

For the lands comprising of Lot 106, Plan 865 the following additional regulations shall apply:

- 1) The following are an additional permitted *main use*:
One *Multiple Dwelling* containing a maximum of four *dwelling units*
- 2) The following additional provisions shall apply to an additional permitted *main use*:
 - a) Lot Width – minimum 9.0 m
 - b) Lot Area – minimum 327.0 m²
 - c) Lot Coverage – maximum 55%

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- d) Main Building Height – maximum 7.60 m
- e) Front Yard Depth – minimum 4.5 m
- f) Rear Yard Depth – minimum 7.5 m
- g) Side Yard Width – minimum 0.25 m
- h) Gross Floor Area – *Main Building* – maximum 364 m²
- i) Notwithstanding Section 5.2.20.20, a refuse bin shall be stored in a required *rear yard*.
- j) Notwithstanding section 24.20.5., three parking spaces shall be provided.
- k) Section 24.30.20.3.2 shall not apply.

II. THAT the H symbol **SHALL APPLY** to Lot 106, Plan 865 situated at the northeast corner of Albert Road and Richmond Street, known municipally as 1098 Albert Road and that Zoning By-law 8600 **BE FURTHER AMENDED** by adding the following clause to Section 95.20:

- (10) a) Conveyance of a 4.6 m by 4.6 m corner cut-off at the northeast corner of Albert Road and Richmond Street to the satisfaction of the City Engineer.

[ZNG/7215]

Carried.

Report Number: S 105/2024

Clerk's File: Z/14833

7.2. Impact of Bill 185 Passed by the Provincial Legislature – City Wide

Aaron Farough, Senior Legal Counsel, is available for questions.

Councillor Fred Francis clarifies and inquires what City Council is to expect when development applications are presented to Council, where residents are no longer able to appeal Council decisions. Mr. Farough states that residents are no longer able to appeal unless the resident falls within a specified class under the Planning Act.

Councillor Francis inquires what the communication strategy is now that residents are unable to appeal once an application has been approved. Mr. Farough states that legal requirements have been met where notices have been updated with information regarding the eligible class of appellants, as this is their only chance to raise their concerns. Mr. Farough continues that eligible appellants could have the ability to seek party status prior to the tribunal. City Planner, Neil

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Robertson states that marketing information material is being developed to convey the message to the public of opportunities when they can participate in the process.

Councillor Francis states that he encourages this material to be ready when Ward Meetings begin to inform residents of changes and expectations. Commissioner Jelena Payne agrees and states that with numerous legislative changes and need to inform the public over the last few years, information sheets were prepared and distributed at Ward Meetings. Commissioner Payne also confirmed that information had been sent as tax inserts for residents, but residents are only paying attention when it affects them personally. Commissioner Payne adds that administration is committed to a multi-strategy approach, whereby information can be conveyed and will resonate and educate residents. Councillor Francis states that the information needs to be user friendly and easily accessible to eliminate miscommunication and misinformation with the public.

Chair Morrison inquires if the City of Windsor has agreed to continue holding Public Information Meetings as they are not mandatory. Mr. Robertson states that this is correct.

Chair Morrison inquires if the circulation distance of public notice is 200 meters and whether it is larger than required. Mr. Robertson states that 120 meters are used depending on the complexity of the application to ensure the public has been notified. Chair Morrison states that we do not have the jurisdiction to change this and that it is a provincial policy.

Councillor Kieran McKenzie inquires if there is a visual PowerPoint presentation prepared for residents to find information. Mr. Farough states that a presentation can be created for the next Council meeting. Mr. Farough adds that the first step had been to update the general notice to inform residents of changes to their appeal rights in relation to applications, and now administration is looking at what can be done beyond notifications.

Councillor McKenzie suggests alternative ways to access the reading material and whether it requires specific direction. Chair Morrison states that he does not believe direction is required and any presentations can be shown during Ward meetings for residents.

Councillor McKenzie inquires about the approval of a draft plan of subdivision and the duration one will last prior to having to reapply. Mr. Farough states that the province has recently created a lapsing provision requirement, of which the City of Windsor had already instilled prior to the provincial requirement, as an incentive for developers to act on application approvals. Mr. Farough adds that with Site Plan Control agreements it has changed timeline requirements.

Councillor McKenzie inquires of the possibility of a lapsing provision in a rezoning circumstance and if it could be considered by the legislature. Mr. Robertson states that it would be a provincial decision as the City does not have regulatory authority to do that currently, as the Planning Act states which types of applications can have provisions. Mr. Robertson adds that in the instance of a rezoning if the application does not proceed, it will revert to the prior zoning designation.

Councillor Angelo Marignani inquires about the changes to the Municipal Act section that prohibits municipalities from directly or indirectly aiding businesses, and how does this conflict with our

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Community Incentive Programs (CIP) and modify the new strategy based on Bill 185. Mr. Robertson states the economic development CIP that is in place is a mechanism under the Planning Act that allows us to provide bonus provisions and incentives in compliance with the Municipal Act and it does not affect our CIPs.

Moved by: Councillor Kieran McKenzie

Seconded by: Councillor Angelo Marignani

Decision Number: **DHSC 647**

- I. THAT the report of the City Solicitor dated July 29, 2024 regarding the legislative changes resulting from provincial Bill 185 and related legislation **BE RECEIVED**; and,
- II. THAT Administration **BE DIRECTED** to provide a brief presentation outlining the legislative changes when this report proceeds to Council.

Carried.

Report Number: S 103/2024

Clerk's File: GP2024

There being no further business the meeting of the Development & Heritage Standing Committee (*Planning Act* Matters) portion is adjourned at 4:55 o'clock p.m.

The Chairperson calls the Administrative Items portion of the Development & Heritage Standing Committee meeting to order at 4:56 o'clock p.m.

8. ADJOURNMENT

There being no further business the meeting of the Development & Heritage Standing Committee (*Planning Act* Matters) portion is adjourned at 4:55 o'clock p.m.

Ward 10 – Councillor Jim Morrison
(Chairperson)

Deputy City Clerk / Supervisor of Council
Services



Council Report: S 113/2024

Subject: Zoning By-law Amendment Application for 4461 Wyandotte Street East, Z-023/24 [ZNG-7221], Ward 5

Reference:

Date to Council: October 7, 2024
Author: Brian Nagata, MCIP, RPP
Planner II - Development Review
(519) 255-6543 ext. 6181

Planning & Building Services
Report Date: September 4, 2024
Clerk's File #: Z/14832

To: Mayor and Members of City Council

Recommendation:

- I. THAT Zoning By-law 8600 **BE AMENDED** for the lands located on the south side of Wyandotte Street East between Jos Janisse Avenue and Ellrose Avenue, described as Lot 27, Plan 1492, PIN No. 01097-1316 LT, by adding a site specific provision to permit *One Semi-Detached Dwelling*, subject to the following additional regulations:

513. SOUTH SIDE OF WYANDOTTE STREET EAST BETWEEN JOS JANISSE AVENUE AND ELLROSE AVENUE

- (1) For the lands comprising of Lot 27, Plan 1492, PIN No. 01097-1316 LT, a *Semi-Detached Dwelling* shall be subject to the following additional provisions:
 1. Notwithstanding Section 11.2.5.2.1, the minimum lot width shall be 13.7 metres.
 2. Notwithstanding Section 11.2.5.2.10, the maximum gross floor area for a main building shall be 851.6 m².

[ZDM 10; ZNG/7221]

- II. THAT the holding (H) symbol **SHALL APPLY** to the lands located on the south side of Wyandotte Street East between Jos Janisse Avenue and Ellrose Avenue, described as Lot 27, Plan 1492, PIN No. 01097-1316 LT and that Zoning By-law 8600 **BE FURTHER AMENDED** by adding the following clause to Section 95.20:
 - (11) a) Gratuitously convey to the Corporation of the City of Windsor, a 1.50-metre-wide strip of land along the Wyandotte Street East

frontage of the subject lands to the satisfaction of the City Engineer.

- b) Submission of a Revised Stormwater Management Plan in accordance with the Windsor Essex Region Stormwater Management Standards Manual to the satisfaction of the City Engineer.

[ZDM 10; ZNG/7221]

Executive Summary:

N/A

Background:

Application Information:

Location: 4461 Wyandotte Street East
(Lot 27, Plan 1492; Roll No. 010-030-08900; PIN No. 01097-1316 LT)

Ward: 5

Planning District: East Windsor

Zoning District Map: 10

Owners: Rock & Cari Doire

Applicant: Same as Owners

Authorized Agent: Pillon Abbs Inc. (Tracey Pillon-Abbs)

Proposal:

The applicant is requesting an amendment to Zoning By-law 8600 to change the zoning for the lands located on the south side of Wyandotte Street East between Jos Janisse Avenue and Ellrose Avenue, known municipally as 4461 Wyandotte Street East (the subject property), by adding a site-specific zoning provision to permit the construction of a Semi-Detached Dwelling with a minimum lot width of 13.7 metres and a maximum gross floor area for a main building of 851.6 m².

Submitted Information: Conceptual Plans (See Appendix A), Deed, Planning Rationale Report (See Appendix G), Stormwater Management Report (See Appendix H), Stormwater Management Submission Requirements (Rational Method), Topographic Survey, WUC Approval (See Appendix I) and Zoning By-law Amendment Application Form

Site Information:

Official Plan	Zoning	Current Use	Previous Use
Mixed-Use Corridor	Residential District 2.2 (RD2.2)	Single Unit Dwelling	Agricultural (prior to 1920)
Lot Width	Lot Depth	Lot Area	Lot Shape
13.7 m	65.7 m	953.0 m ²	Irregular
<i>All measurements are based on the Topographic Survey prepared by Verhaegen Land Surveyors, dated February 2, 2024, Reference No. 23-47-687-00</i>			

The subject property contains a circa 1920 one and three-quarter (1 ¾) storey Single Unit Dwelling. The remainder of the subject property is maintained as landscaped open space.



KEY MAP - Z-023/24, ZNG/7221



● SUBJECT LANDS



PART OF ZONING DISTRICT MAP 10

N.T.S.

REQUESTED ZONING AMENDMENT

Applicant: Rock Doire



SUBJECT LANDS

PLANNING & BUILDING DEPARTMENT



DATE : JULY, 2024
FILE NO. : Z-023/24, ZNG/7221



NEIGHBOURHOOD MAP - Z-023/24, ZNG/7221



SUBJECT LANDS

Neighbourhood Characteristics:

The subject property is located on the eastern side of the East Windsor neighbourhood. The East Windsor neighbourhood constitutes the area north of Tecumseh Road East, east of the former Chesapeake & Ohio Railway rail corridor, south of the Detroit River and west of Raymo Road and Jefferson Boulevard (north and south of the Via Rail rail corridor respectively).

Surrounding Land Uses:

North:

- Low density residential
- Medical offices

East:

- Automobile repair garage
- Low density residential
- Mixed use (retail store and multiple dwelling)

South:

- Low density residential

West:

- Low density residential
- Medium density residential

Municipal Infrastructure:

- Wyandotte Street East is classified as a Class II Arterial Road, which has a four-lane cross section with median, bordered by curbs, gutters, sidewalks, and streetlights on both sides.
- Sanitary sewer, storm sewer and watermain are located within the Wyandotte Street East right-of-way.

Discussion:

The Provincial Policy Statement 2020 (PPS) provides direction on matters of provincial interest related to land use planning and development and sets the policy foundation for regulating the development and use of land in Ontario.

The following policies of the PPS are considered relevant in discussing provincial interests related to this amendment:

1.0 Building Strong Healthy Communities

Policy 1.1.1 states:

- *Healthy, liveable and safe communities are sustained by:*
 - *a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*
 - This amendment will allow for a Semi-Detached Dwelling infill development that optimizes existing municipal services.
 - *b) accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
 - This amendment will allow for the construction of a Semi-Detached Dwelling, further diversifying the range and mix of residential types available in the East Windsor neighbourhood.
 - *c) avoiding development and land use patterns which may cause environmental or public health and safety concerns.*
 - This amendment will not cause any environmental or public health and safety concerns.
 - *e) promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;*
 - This amendment will allow for the redevelopment of the subject property through residential intensification, optimizing existing municipal infrastructure and public service facilities, and avoiding unnecessary land consumption.
 - The redevelopment of the subject property at a higher density, in conjunction with it being on two (2) bus routes and within walking distance of transit stops, also represents a transit-supportive development.
 - 400.0 metres is typically used as an acceptable walking distance to a transit stop.
 - This is reflected within Transit Windsor's 2019 Transit Master Plan and the City of Windsor's Active Transportation Master Plan.

- *f) improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society.*
 - Compliance with the Barrier-Free Design requirements of the *Ontario Building Code* and accessibility requirements under *Ontario Regulation 191/11 Integrated Accessibility Standards to the Accessibility for Ontarians with Disabilities Act, 2005* will be addressed through the Building Permit application process.
- *g) ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs.*
 - The subject property is serviced by municipal electrical, sanitary, storm and water services.
 - There is a community centre, public libraries and schools within proximity to the subject property.

Policy 1.1.3.1 states:

- *Settlement areas shall be the focus of growth and development.*
 - The subject property is located within a Settlement area.

One or more of the aforesaid responses to PPS Policy 1.1.1 also speak to the following relevant PPS Policies:

1.1.3 Settlement Areas	
Policy 1.1.3.2 (Land use patterns criteria)	Sub Policies a) (efficient use of land and resources), b) (efficient use of available services) & f) (transit-supportive)
1.4 Housing	
Policy 1.4.3 (appropriate range and mix of housing options and densities)	Sub Policies b)1 (housing options), b)2 (residential intensification), c) (servicing available) & d) (transit-supportive)
1.6.6 Sewage, Water and Stormwater	
Policy 1.6.6.2 (municipal services preferred form of servicing)	
1.6.7 Transportation Systems	
Policy 1.6.7.4 (transit-supportive development)	
1.7 Long-Term Economic Prosperity	
Policy 1.7.1 (supported by)	Sub Policies b) (housing) & c) (efficient use of land, infrastructure and public service facilities)

1.8 Energy Conservation, Air Quality and Climate Change	
Policy 1.8.1	Sub Policies e) (encourage transit-supportive development)

Official Plan

Relevant excerpts from Volume I of the Official Plan are attached as Appendix C. The following policies from these excerpts are considered relevant in discussing this amendment's conformity with the Official Plan. The basis for these excerpts and relevant policies therein is derived from information obtained from the supporting Schedules to Volume I (refer to Table 1 below).

Table 1 - Volume I Schedules

Schedule	Designation
Schedule A - Planning Districts & Policy Areas	East Windsor Planning District
Schedule A-1 - Special Policy Areas	N/A
Schedule B - Greenway System	Adjacent to a Proposed Recreationway (namely Wyandotte Street East)
Schedule C - Development Constraints Areas	Within 1,000.0 metres of a Rail Yard (namely George Avenue Rail Yard)
Schedule C-1 - Development Constraint Areas Archaeological Potential	Low Potential
Schedule D - Land Use	Mixed-Use Corridor
Schedule E - City Centre Planning District	N/A
Schedule F - Roads and Bikeways	Adjacent to a Class II Arterial Road
Schedule F-1 - Railways	N/A
Schedule G - Civic Image	Adjacent to a Mainstreet
Schedule H - Baseplan Development Phasing	N/A
Schedule J - Urban Structure Plan	Adjacent to a City Corridor (namely Wyandotte Street East) Within proximity to Neighbourhood Node

The land use designation of the subject property was changed from Mixed Use to Mixed-Use Corridor as part of Official Plan Amendment No. 159 (OPA 159). OPA 159 was initiated by the City to implement policies to encourage intensification within areas of the City where present and future residents will be in proximity to goods and services, public transportation, and employment areas while ensuring that the character of existing neighbourhoods is preserved. Council adopted OPA 159 on July 11, 2022.

The coinciding Zoning By-law Amendment [Z-019/21 (ZNG/6756)] did not include updates to the zoning districts to conform with the new land use designations.

The applicant is not required to bring the subject property's zoning district into conformity with the Mixed-Use Corridor land use designation through this amendment. However, the changes requested through this amendment are required to comply with the Mixed-Use Corridor policies.

Volume I

Chapter 3 - Development Strategy

This amendment complies with the following applicable key policy direction for managing growth consistent with the Vision of the City of Windsor Community Strategic Plan.

3.2 - Growth Concept

3.2.1 - Safe, Caring and Diverse Communities

Encouraging a range of housing types will ensure that people have an opportunity to live in their neighbourhoods as they pass through the various stages of their lives. Residents will have a voice in how this new housing fits within their neighbourhood. As the city grows, more housing opportunities will mean less sprawl onto agricultural and natural lands. (Policy 3.2.1.2)

3.3.2 - Corridors

City Corridors

..... There are higher density employment and residential opportunities, with a significant amount of retail to support both every day needs, but also needs beyond the day such as furniture and appliance stores, home improvement stores, and stores that carry specialty items. (Policy 3.3.2.1)

Chapter 6 - Land Use:

6.1 Goals

This amendment complies with the following applicable land use goals:

- *Safe, caring and diverse neighbourhoods. (Goal 6.1.1)*
- *Housing suited to the needs of Windsor's residents. (Goal 6.1.3)*
- *To direct residential intensification to those areas of the City where transportation, municipal services, community facilities and goods and services are readily available. (Goal 6.1.14)*

6.5 Commercial

6.5.1 Objectives

The following objectives and policies establish the framework for development decisions in all Commercial areas.

- *To promote residential intensification with Medium and High-Profile buildings to meet the housing needs of the City in appropriate areas in proximity to municipal services, transit and employment areas. plan (Objective 6.5.1.8)*

6.5.3 Mixed Use Corridor

Permitted Uses

Uses permitted in the Mixed-Use Corridor land use designation are primarily retail, wholesale store and service-oriented uses and, to a lesser extent, office uses.

Medium and High-Profile residential uses either as stand-alone buildings or part of a commercial-residential mixed-use buildings shall be throughout the Corridors. (Policy 6.5.3.1)

- The RD2.2 zoning permits Low-Profile residential uses, which includes a Semi-Detached Dwelling.

Street Presence

Council will encourage Mixed-Use Corridor development to provide a continuous street frontage and presence. Accordingly, development along a Mixed-Use Corridor shall be: (Policy 6.5.3.3)

- *(a) no more than four storeys in height, except on lands at an intersection of any combination of the following roads: Class I Arterial Road, Class II Arterial Road, Class I Collector Road, or Class II Collector Road. The height of buildings shall generally not exceed the width of the road right-of-way abutting the development site;*
 - The maximum main building height provision of 9.0 metres for all uses permitted under the RD2.2 zoning can only accommodate up to three (3) storeys.
 - Relief from the maximum main building height provision is not being sought.
- *(c) Encouraged to locate the buildings at the street frontage lot line with parking accommodated at the rear of the site.*
 - The minimum front yard depth provision for all uses permitted under the RD2.2 zoning is 6.0 metres.
 - Section 3.1.19. Above Ground Electrical Conductors of the *Ontario Building Code* requires a minimum building setback of 3.0 to 5.0 metres from the hydro lines running in front of the subject property.

- The properties within the 4300 to 4400 block of Wyandotte Street East all have a front yard depth over 6.0 metres.

Locational Criteria

Mixed Use Corridor development shall be located where: (Policy 6.5.3.6)

- *(a) there is access to Class I or Class II Arterial Roads or Class I Collector Roads;*
 - The subject property has access to Wyandotte Street East, a designated Class II Arterial Road.
- *(b) full municipal physical services can be provided; and*
 - Refer to the response provided to PPS Policy 1.1.1 g) herein.

Evaluation Criteria

At the time of submission, the proponent shall demonstrate to the satisfaction of the Municipality that a proposed mixed use corridor development is: (Policy 6.5.3.7)

- *(a) feasible having regard to the other provisions of this Plan, provincial legislation, policies and appropriate guidelines and support studies for uses:*
 - *(iii) where traffic generation and distribution is a provincial or municipal concern; and*
 - The Transportation Planning Department did not identify any concerns with traffic generation and distribution.
- *(c) capable of being provided with full municipal physical services and emergency services;*
 - Refer to the response provided to PPS Policy 1.1.1 g) herein for details on the municipal physical services available to the subject property.
 - The subject property is served by Essex-Windsor EMS, Windsor Fire & Rescue Services (Fire Hall No. 2) and Windsor Police Service.
- *(d) provided with adequate off-street parking;*
 - The proposed development will accommodate the required number of parking spaces onsite.
- *(e) compatible with the surrounding area in terms of scale, massing, height, siting, orientation, setbacks, parking and landscaped areas; and*
 - The requested relief from the minimum lot width and maximum gross floor area provisions of the RD2.2 zoning does not impact the proposed development's compatibility with the surrounding area.

- The proposed development will comply with the remaining provisions of the RD2.2 zoning.
- The subject property is a lot on a Registered Plan of Subdivision (Lot 27, Plan 1492).

Design Guidelines

This amendment will not impact the scale, massing, height, siting, orientation, setbacks, parking and landscaped areas achieved through the RD2.2 zoning.

Neighbourhood Involvement

Council will encourage the improvement of areas designated as Mixed-Use Corridor Commercial Corridor to be undertaken in consultation with the surrounding neighbourhood. (Policy 6.5.3.11)

- Pre-consultation for this amendment occurred prior to the Planning Department requiring applicants to host an open house for neighbouring property owners.

Chapter 7 - Infrastructure:

7.2 Transportation System

7.2.6 Road Network Policies

Gratuitous Conveyances

As a condition of development approval, council shall require gratuitous land conveyances to the Municipality where it has been determined that the existing right-of-way width is insufficient based on the requirements set out in Schedule 'X', or other provisions of this Official Plan. The size and dimension of each such conveyance shall be determined by what is identified in Schedule 'X', or other provisions of this Official Plan. Generally, equal widths of land will be taken from both sides of the road. (Policy 7.2.6.21)

- The Engineering - Development, Engineering - R.O.W. and Transportation Planning Departments have identified that the existing right-of-way width for Wyandotte Street East is insufficient.
- Schedule X: Right-of-Way Widths to Volume III of the Official Plan identifies a required right-of-way width of 28.0 metres for the subject portion of Wyandotte Street East.
- Therefore, the aforesaid Departments are requesting that the owner gratuitously convey to the Corporation, a 1.50-metre-wide strip of land along the Wyandotte Street East frontage of the subject property, thus being the owner's contribution towards achieving the required right-of-way width.
- The Planning Department is recommending that a site-specific holding provision be added to require the owner, prior to the issuance of a building permit, to gratuitously convey the requested lands for the Wyandotte Street East right-of-way.

Chapter 11 - Tools:

Land use compatibility throughout Windsor is an implementation goal to be achieved when administering a planning tool under this Chapter. Compatibility between land uses is also an objective of the Zoning By-law Amendment planning tool (Policy 11.6.1.2).

- Land use compatibility was considered as part of the evaluation of the applicable Official Plan and PPS policies referenced herein.

Policy 11.6.3.3 states:

- *When considering applications for Zoning By-law amendments, Council shall consider the policies of this Plan and will, without limiting the generality of the foregoing, consider such matters as the following:*
 - *(a) The relevant evaluation criteria contained in the Land Use Chapter of this Plan, Volume II: Secondary Plans & Special Policy Areas and other relevant standards and guidelines;*
 - *(b) Relevant support studies;*
 - *(c) The comments and recommendations from municipal staff and circularized agencies;*
 - *(d) Relevant provincial legislation, policies and appropriate guidelines; and*
 - *(e) The ramifications of the decision on the use of adjacent or similar lands.*
 - This amendment is not anticipated to have any ramifications on the use of adjacent or similar lands.

The aforesaid matters were considered as part of the evaluation of the applicable Official Plan and PPS policies referenced herein.

Zoning By-Law

Relevant excerpts from Zoning By-law 8600 are *attached* as Appendix D.

The subject property is within a RD2.2 zone of Zoning By-law 8600, which permits a Semi-Detached Dwelling use.

The applicant is requesting an amendment to Zoning By-law 8600 to add a site-specific provision to allow a Semi-Detached Dwelling with a minimum lot width of 13.7 metres and a maximum gross floor area for a main building of 851.6 m².

The applicant's request has been considered and is supported in this report in conjunction with the site-specific holding provision being recommended by the Planning Department herein. The holding provision will remain until such time that the conditions referenced herein have been fulfilled to the satisfaction of the designated approval authority.

The RD2.2 zoning requires a minimum lot width of 15.0 metres for a Semi-Detached Dwelling. The southeast corner of the subject property is not square because of the

sight triangle provided for the intersecting alleys (refer to Figure 1 below). This factor reduces the lot width from 14.4 metres to 13.7 metres, as it is based on average lot width when the side lot lines are not parallel.

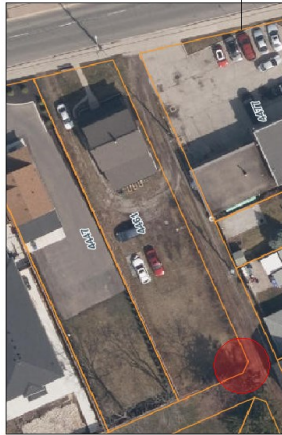


Figure 1 - Sight Triangle for Alleys

The RD2.2 zoning allows a maximum gross floor area of 400.0 m² for a Semi-Detached Dwelling. This provision was added by By-law 101-2022 on July 11, 2022, for the purpose of implementing the recommendations coming out of the [Multi-Residential Interim Control By-law Study Background Report](#) (the study).

The study was prepared as part of Interim Control By-law 103-2020 (ICBL). ICBL was passed to respond to ongoing intensification and redevelopment pressures around the City's post-secondary institutions, hospitals and City Centre. ICBL temporarily prohibited the use of land for dwellings with five or more dwelling units to allow the municipality to study and review the Official Plan and Zoning By-law 8600 concerning related land use policies and provisions respectively.

The main purpose of the study was to:

- Identify areas that can accommodate additional residential density;
- Establish policy framework to guide growth to those areas;
- Develop density targets for those areas; and,
- Ensure compatibility within the existing neighbourhood context.

This specific provision was added to address concerns that the size of recently constructed buildings and the density are not in character with the neighbourhoods surrounding the University of Windsor.

The subject property is not within one of the aforesaid neighbourhoods and has a Mixed-Use Corridor land use designation, the later of which permits Medium and High Profile Residential uses and higher density developments.

No other zoning deficiencies have been identified or supported.

A draft amending by-law is attached as Appendix G. Subsection 24 (1) of the *Planning Act*, R.S.O. 1990, c. P.13., prohibits a by-law from being passed that does not conform

with the Official Plan. As discussed through the Official Plan section herein, the proposed amendment conforms to the applicable policies of the Official Plan.

Risk Analysis:

N/A

Climate Change Risks

Climate Change Mitigation:

Residential intensification minimizes the impact on the community greenhouse gas emissions. Development within existing communities and neighbourhoods while using currently available infrastructure such as sewers, sidewalks, and public transit helps to mitigate development impact.

Climate Change Adaptation:

N/A

Financial Matters:

N/A

Consultations:

Comments received from City Departments, external agencies and members of the public on this application were taken into consideration when preparing this report. A record of the comments is included as Appendix E herein.

There are no objections to the proposed amendment.

Public Notice: Statutory notice was advertised in the Windsor Star, a local daily newspaper. A courtesy notice was mailed to property owners and residents within 200 metres of the subject property.

Conclusion:

The *Planning Act* requires that a decision of Council in respect of the exercise of any authority that affects a planning matter, “shall be consistent with” Provincial Policy Statement 2020. The recommended zoning amendment has been evaluated for consistency with the Provincial Policy Statement 2020 and conformity with the policies of the City of Windsor Official Plan.

The recommended Zoning By-law amendment is consistent with the PPS, conforms with the policy direction of the City of Windsor Official Plan, is compatible with existing and permitted uses in the surrounding neighbourhood and constitutes good planning.

Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Greg Atkinson, MCIP, RPP
Manager of Development

Neil Robertson, MCIP, RPP
City Planner

I am not a Registered Professional Planner and have reviewed as a Corporate Team Leader

JP

JM

Approvals:

Name	Title
Greg Atkinson	Manager of Development/Deputy City Planner
Neil Robertson	City Planner / Executive Director, Planning & Development Services
Aaron Farough	Senior Legal Counsel, Legal Services & Real Estate
Jelena Payne	Commissioner, Economic Development
Joe Mancina	Chief Administration Officer

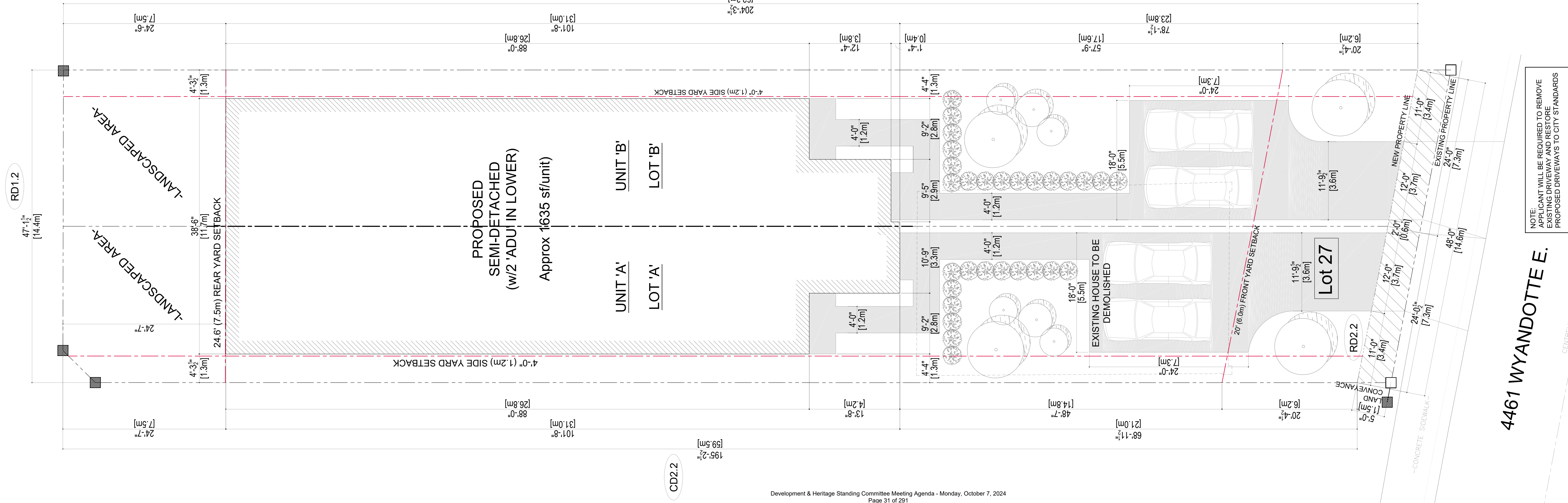
Notifications:

Name	Address	Email
Pillon Abbs Inc. (Tracey Pillon-Abbs)	23669 Prince Albert Road Chatham, Ontario N7M 5J7	tracey@pillonabbs.ca
Councillor Ed Sleiman (Ward 5)	350 City Hall Square West, Suite 530 Windsor, Ontario N9A 6S1	edsleiman@gmail.com
Abutting property owners, tenants/occupants within 200-meter (656 feet) radius of the subject property		

Appendices:

- 1 Appendix A - Conceptual Plan
- 2 Appendix B - Site Images
- 3 Appendix C - Excerpts from Official Plan Volume I
- 4 Appendix D - Excerpts from Zoning By-law 8600
- 5 Appendix E - Consultations
- 6 Appendix F - Draft Amending By-law
- 7 Appendix G - Planning Rationale Report (Scoped)

- 8 Appendix H - Stormwater Management Report
- 9 Appendix H - Stormwater Management Report Appendix E
- 10Appendix I - WUC Approval



UNIT 'A' / LOT 'A'	
SITE INFO	
LOT AREA	4643 SF
ALLOWABLE COVERAGE	2089 SF (45%)
PROPOSED COVERAGE	1754 SF (37.8%)
LOT COVERAGE	
PROPOSED BUILDING	1635 SF
BALCONY	50 SF
FRONT PORCH	69 SF
TOTAL LOT COVERAGE	1754 SF
BUILDING AREAS	
LOWER (ADU)	1635 SF
GRADE ENTRANCE (Uncovered)	78 SF
MAIN FLOOR	1634 SF
BALCONY	50 SF
UPPER FLOOR	1635 SF
BALCONY	50 SF
FRONT PORCH	69 SF
FRONT YARD	
TOTAL FRONT YARD AREA	1865 SF
HARD SURFACE (Driveway and sidewalks)	923 SF
LANDSCAPED AREA	942 SF (50.5%)
PARKING SPACES	2

UNIT 'B' / LOT 'B'	
SITE INFO	
LOT AREA	4760 SF
ALLOWABLE COVERAGE	2142 SF (45%)
PROPOSED COVERAGE	1761 SF (37.0%)
LOT COVERAGE	
PROPOSED BUILDING	1635 SF
BALCONY	69 SF
FRONT PORCH	57 SF
TOTAL LOT COVERAGE	1761 SF
BUILDING AREAS	
LOWER (ADU)	1635 SF
GRADE ENTRANCE (Uncovered)	60 SF
MAIN FLOOR	1635 SF
BALCONY	69 SF
UPPER FLOOR	1635 SF
BALCONY	69 SF
FRONT PORCH	57 SF
FRONT YARD	
TOTAL FRONT YARD AREA	1984 SF
HARD SURFACE (Driveway and sidewalks)	911 SF
LANDSCAPED AREA	1070 SF (53.9%)
PARKING SPACES	2

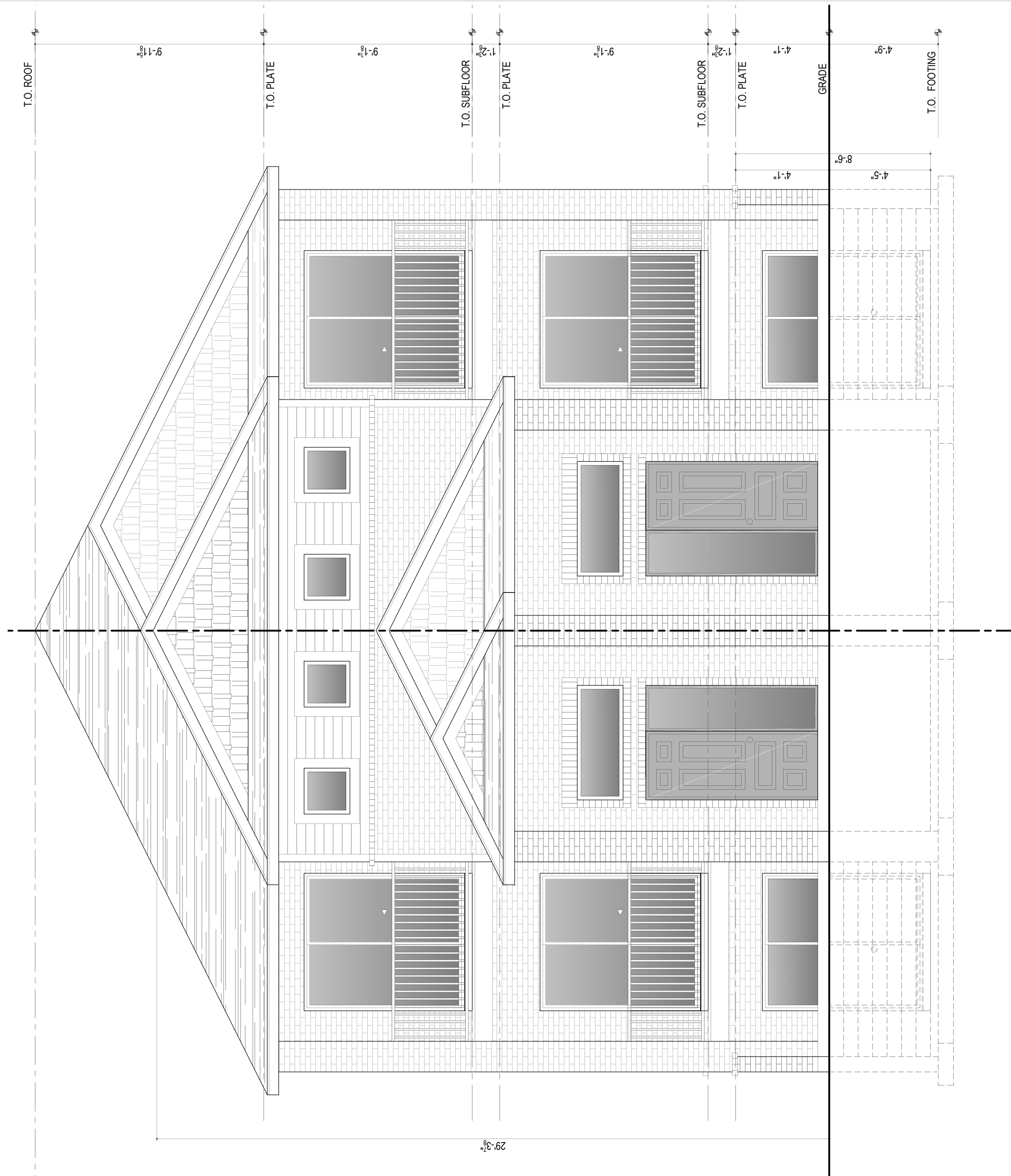
SITE INFO	
ADDRESS	LOT NUMBER
4461 WYANDOTTE E WINDSOR, ON	PART 27
	ZONING
	RD2.2
	ALLOWABLE COVERAGE
	4231 SF (45%)
	PROPOSED COVERAGE
	3513 SF (37.3%)
	292.92 m² (37.3%)
LOT COVERAGE	
PROPOSED BUILDING	3269 SF
BALCONIES	119 SF
FRONT PORCH	125 SF
TOTAL LOT COVERAGE	3513 SF
	326.37 m²
GFA	
SECOND FLOOR	2849 SF
MAIN FLOOR (includes hallways and stairs)	3269 SF
CELLAR	3048 SF
TOTAL GFA	9166 SF
	851.6 m²

1. THIS DESIGNER IS NOT RESPONSIBLE FOR ERCA EASEMENTS, SITE PLAN CONTROL, APPROVALS AND DEVELOPMENTAL SITE CONDITIONS AND LOT RESTRICTIONS.

2. THIS PLAN IS NOT A LEGAL SURVEY. ALL DIMENSIONS SHOULD BE VERIFIED BY AN O.L.S. THIS SITE PLAN IS PROVIDED AS A GUIDE TO LOCATE THE BUILDING.

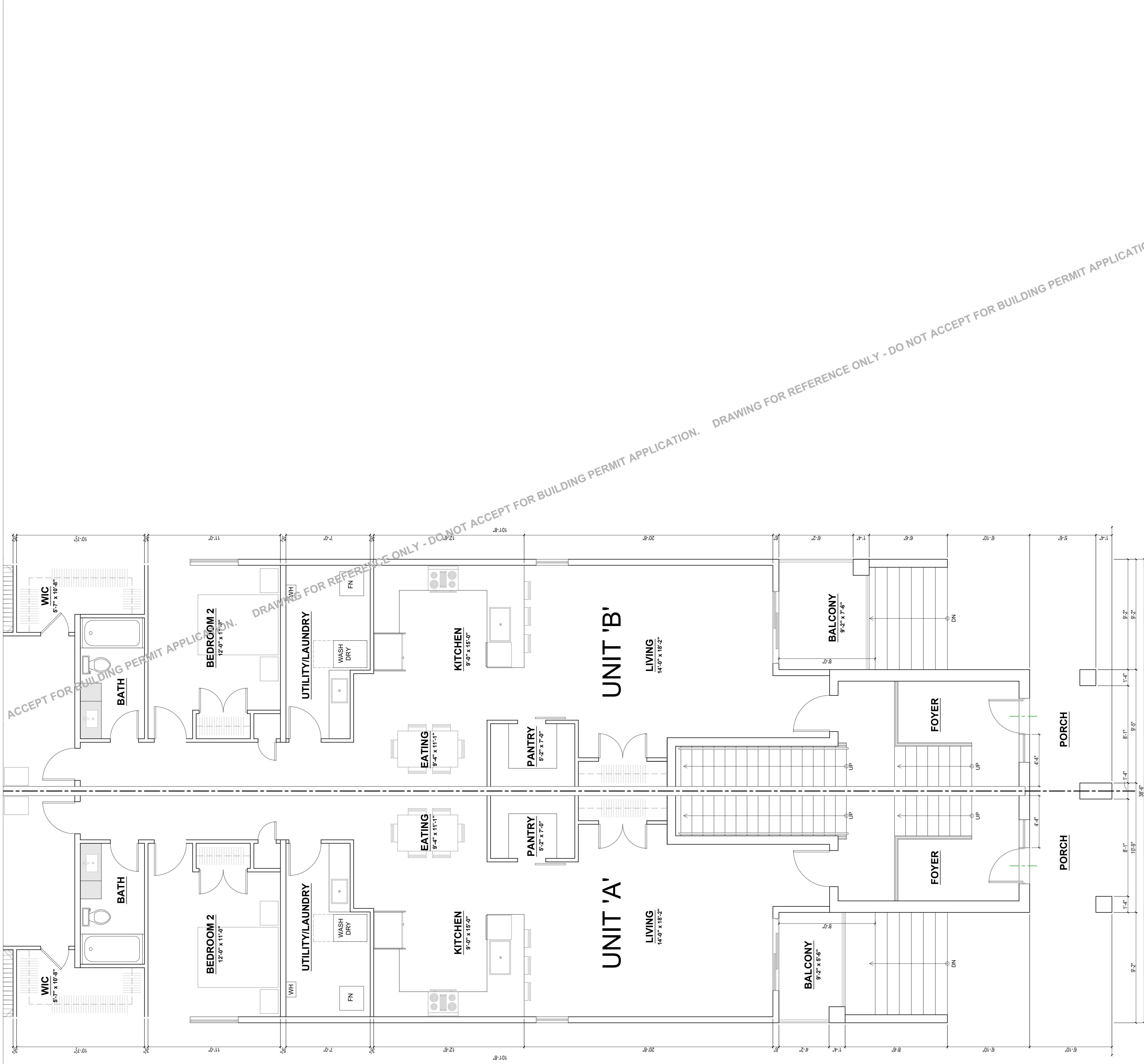
SITE PLAN LEGEND	
SETBACKS	PROPOSED BUILD
PROPERTY LINES	HARD SURFACES

SITE PLAN NOTES	
SITE PLAN TAKEN FROM SURVEY PROVIDED BY OWNER	



1. PROPERTY PLAN
A00.1 / 1/8" = 1'

2. FRONT ELEVATION
A00.1 / 1/4" = 1'



ACCEPT FOR BUILDING PERMIT APPLICATION. DRAWING FOR REFERENCE ONLY - DO NOT ACCEPT FOR BUILDING PERMIT APPLICATION

1 MAIN FLOOR PLAN
1/8" = 1'

APPENDIX “B”
Site Photos (Google Street View - November 2023)



Figure 1 - Looking south towards 4461 Wyandotte Street East



Figure 2 - Looking southeast towards 4461 Wyandotte Street East



Figure 3 - Looking southwest towards 4461 Wyandotte Street East

APPENDIX “C”

Excerpts from Official Plan Volume I

3. Development Strategy

3.2 Growth Concept

3.2.1 Safe, Caring and Diverse Community

*NEIGHBOURHOOD
HOUSING VARIETY*

- 3.2.1.2 Encouraging a range of housing types will ensure that people have an opportunity to live in their neighbourhoods as they pass through the various stages of their lives. Residents will have a voice in how this new housing fits within their neighbourhood. As the city grows, more housing opportunities will mean less sprawl onto agricultural and natural lands.

3.3.2 Corridors

*CITY
CORRIDORS*

- 3.3.2.1 City Corridors serve to connect the City Centre Growth Centre and Regional Commercial Centres. City corridors radiate from these Centres following numerous high frequency transit corridors. City corridors connect to Regional Commercial Centres along selected arterial roads but do not extend as far outward or as numerous as corridors connected to the City Centre. These corridors are intended to provide services for those living in close proximity to the area but also those who may arrive by transit, bicycle and by car.

There are higher density employment and residential opportunities, with a significant amount of retail to support both every day needs, but also needs beyond the day such as furniture and appliance stores, home improvement stores, and stores that carry specialty items.....

Residential development may include high profile (26 to 58 metres in height), medium profile (14 to 26 metres in height) and residential over retail at street, as well as row housing and lofts.

6. Land Use

6.1 Goals

In keeping with the Strategic Directions, Council's land use goals are to achieve:

<i>NEIGHBOURHOODS</i>	6.1.1	Safe, caring and diverse neighbourhoods.
<i>RESIDENTIAL</i>	6.1.3	Housing suited to the needs of Windsor's residents.
<i>RESIDENTIAL INTENSIFICATION</i>	6.1.14	To direct residential intensification to those areas of the City where transportation, municipal services, community and goods and services are readily available. (added by OPA #159 –AP PROVED July 11, 2022, B/L# 100-2022)

6.5 Commercial

6.5.1 Objectives

RESIDENTIAL INTENSIFICATION

6.5.1.8

To promote residential intensification with Medium and High Profile buildings to meet the housing needs of the City in appropriate areas in proximity to municipal services, transit and employment areas. (Added by OPA #159 – APPROVED July 11, 2022, B/L# 100-2022)

6.5.3 Mixed Use Corridor

PERMITTED USES

6.5.3.1

Uses permitted in the Mixed Use Corridor land use designation are primarily retail, wholesale store (added by OPA 58, 24 07 2006) and service oriented uses and, to a lesser extent, office uses.

Medium and High Profile residential uses either as stand-alone buildings or part of a commercial-residential mixed use buildings shall be throughout the Corridors. (Added by OPA #159 – APPROVED July 11, 2022 , B/L# 100-2022)

STREET PRESENCE

6.5.3.3

Council will encourage Mixed-Use Corridor development to provide a continuous street frontage and presence. Accordingly, development along a Mixed-Use Corridor shall be:

- (a) no more than four storeys in height, except on lands at an intersection of any combination of the following roads: Class I Arterial Road, Class II Arterial Road, Class I Collector Road, or Class II Collector Road. The height of buildings shall generally not exceed the width of the roadright-of-way abutting the development site; and
- (c) Encouraged to locate the buildings at the street frontage lot line with parking accommodated at the rear of the site.
(Added by OPA #159 – APPROVED July 11, 2022 , B/L# 100-2022)

*LOCATIONAL
CRITERIA*

6.5.3.6

Mixed Use Corridor development shall be located where:

- (a) there is access to Class I or Class II Arterial Roads or Class I Collector Roads;
- (b) full municipal physical services can be provided; and

*EVALUATION
CRITERIA*

6.5.3.7

At the time of submission, the proponent shall demonstrate to the satisfaction of the Municipality that a proposed mixed use corridor development is:

- (a) feasible having regard to the other provisions of this Plan, provincial legislation, policies and appropriate guidelines and support studies for uses:

(iii) where traffic generation and distribution is a provincial or municipal concern; and

(c) capable of being provided with full municipal physical services and emergency services;

(d) provided with adequate off-street parking;

(e) compatible with the surrounding area in terms of scale, massing, height, siting, orientation, setbacks, parking and landscaped areas; and

*DESIGN
GUIDELINES*

6.5.3.8

The following guidelines shall be considered when evaluating the proposed design of a Mixed Use Corridor development:

(a) the ability to achieve the associated policies as outlined in the Urban Design chapter of this Plan;

(b) the provision of appropriate landscaping or other buffers to enhance:

(i) all parking lots, and outdoor loading and service areas; and

(ii) the separation between the use and adjacent sensitive uses, where appropriate;

- (c) as a general rule, the height of buildings are consistent with the height of buildings which characterize the Mixed Use Corridor. Where Council deems it desirable that higher profile development be permitted in an existing Mixed Use Corridor Commercial Corridor, the development should be built at a human scale by utilizing one or both of the following measures:
 - (i) treatment of the lower floors of building(s) to provide continuity; and/or
 - (ii) setting back the upper floors of building(s) from the street to avoid overpowering effects at-grade;
- (d) where possible, parking is located in the rear of the property to encourage continuous building facades adjacent to the street; and
- (e) measures are taken in site design which provide for ease of access for pedestrians between the public sidewalk and building main entrances in a manner which is distinguishable from access provided for vehicles.
- (f) Council will adopt Design Guidelines that will assist in the design and review of development applications in a manner that will ensure implementation of these policies. (Added by OPA #159 – APPROVED July 11, 2022, B/L# 100-2022)

NEIGHBOURHOOD
INVOLVEMENT

6.5.3.11

Council will encourage the improvement of areas designated as Mixed Use Corridor to be undertaken in consultation with the surrounding neighbourhood.

7. Infrastructure

7.2 Transportation System

7.2.6 Road Network Policies

GRATUITOUS
CONVEYANCES

7.2.6.21

As a condition of development approval, council shall require gratuitous land conveyances to the Municipality where it has been determined that the existing right-of-way width is insufficient based on the requirements set out in Schedule 'X', or other provisions of this Official Plan. The size and dimension of each such conveyance shall be determined by what is identified in Schedule 'X', or other provisions of this Official Plan. Generally, equal widths of land will be taken from both sides of the road.

11. Tools

11.6 Zoning

11.6.1 Objectives

COMPATIBLE
USES

11.6.1.2

To ensure compatibility between land uses.

11.6.3 Zoning By-law Amendment Policies**EVALUATION
CRITERIA****11.6.3.3**

When considering applications for Zoning By-law amendments, Council shall consider the policies of this Plan and will, without limiting the generality of the foregoing, consider such matters as the following:

- (a) The relevant evaluation criteria contained in the Land Use Chapter of this Plan, Volume II:
- (b) Relevant support studies;
- (c) The comments and recommendations from municipal staff and circularized agencies;
- (d) Relevant provincial legislation, policies and appropriate guidelines; and
- (e) The ramifications of the decision on the use of adjacent or similar lands.

APPENDIX “D”

Excerpts from Zoning By-law 8600

SECTION 11 - RESIDENTIAL DISTRICTS 2. (RD2.)

(B/L 10358 Jul 16/1990; B/L 11093 Jul 20/1992; B/L 12651 Approved by OMB Order R960323, Feb 25/1997
 B/L 169-2001 Jun 1/2001; B/L 33-2001 Oct 23/2001, OMB Decision/Order No. 1716 Case No. PL010233
 B/L 370-2001 Nov 15/2001; B/L 363-2002 Dec 31/2002; B/L 142-2006 Aug 24/2006; B/L 114-2016 Sep 19/2016)
 B/L 164-2017, Dec. 7/2017 [ZNG/5270]

11.2 RESIDENTIAL DISTRICT 2.2 (RD2.2)

11.2.1 PERMITTED USES

One Double Duplex Dwelling

One Duplex Dwelling

One Multiple Dwelling containing a maximum of four dwelling units

One Semi-Detached Dwelling

One Single Unit Dwelling

Townhome Dwelling

Any use accessory to any of the preceding uses

11.2.5 PROVISIONS

.1 Duplex Dwelling

.1	Lot Width – minimum	12.0 m
.2	Lot Area – minimum	360.0 m ²
.3	Lot Coverage – maximum	45.0%
.4	Main Building Height – maximum	9.0 m
.5	Front Yard Depth – minimum	6.0 m
.6	Rear Yard Depth – minimum	7.50 m
.7	Side Yard Width – minimum	1.20 m
.10	Gross Floor Area – Main Building– maximum	400 m ²

.2 Semi-Detached Dwelling

.1	Lot Width – minimum	15.0 m
.2	Lot Area – minimum	450.0 m ²
.3	Lot Coverage – maximum	45.0%
.4	Main Building Height – maximum	9.0 m
.5	Front Yard Depth – minimum	6.0 m
.6	Rear Yard Depth – minimum	7.50 m
.7	Side Yard Width – minimum	1.20 m
.10	Gross Floor Area – Main Building– maximum	400 m ²

.3 Single Unit Dwelling

.1	Lot Width – minimum	9.0 m
.2	Lot Area – minimum	270.0 m ²
.3	Lot Coverage – maximum	45.0%

- | | | |
|-----|-------------------------------------------|--------------------|
| .4 | Main Building Height – maximum | 9.0 m |
| .5 | Front Yard Depth – minimum | 6.0 m |
| .6 | Rear Yard Depth – minimum | 7.50 m |
| .7 | Side Yard Width – minimum | 1.20 m |
| .10 | Gross Floor Area – Main Building– maximum | 400 m ² |
- .4 Double Duplex Dwelling or Multiple Dwelling
- | | | |
|----|--------------------------------|----------------------|
| .1 | Lot Width – minimum | 18.0 m |
| .2 | Lot Area – minimum | 540.0 m ² |
| .3 | Lot Coverage – maximum | 45.0% |
| .4 | Main Building Height – maximum | 9.0 m |
| .5 | Front Yard Depth – minimum | 6.0 m |
| .6 | Rear Yard Depth – minimum | 7.50 m |
| .7 | Side Yard Width – minimum | 1.80 m |
- .5 Townhome Dwelling
- | | | |
|----|-----------------------------------------------|----------------------|
| .1 | Lot Width – minimum | 20.0 m |
| .2 | Lot Area – per <i>dwelling unit</i> – minimum | 200.0 m ² |
| .3 | Lot Coverage – maximum | 45.0% |
| .4 | Main Building Height – maximum | 9.0 m |
| .5 | Front Yard Depth – minimum | 6.0 m |
| .6 | Rear Yard Depth – minimum | 7.50 m |
| .7 | Side Yard Width – minimum | 1.50 m |

(AMENDED by B/L 101-2022, July 11, 2022)

APPENDIX “E” Consultations

CALDWELL FIRST NATION COMMUNITY

No comments provided

ENGINEERING - DEVELOPMENT

July 8, 2024 (Comments to Revised Stormwater Management Report)

Please note that the report submitted has enough information for us to assess the feasibility and functionality of the proposed SWM system for the site, and it is deemed acceptable for the Rezoning application purpose. A revised report is required for the Building Permit stage.

If the property is severed, new connections to the storm and sanitary sewers will be necessary. Please reach out to Engineering ROW at engineeringdept@citywindsor.ca for more info. Stormwater Management requirements in accordance with the latest regional guide, including Quality requirements

https://www.essexregionconservation.ca/files/ugd/24e1b4_578198d4bf7441248100c019c612df5c.pdf

May 31, 2024 (Comments to Stage 2: Planning Consultation Application)

Site Servicing - The site may be serviced by a 250 mm sanitary sewer and a 450 mm storm sewer located within the Wyandotte Street right-of-way. If possible existing connections should be utilized. Any redundant connections shall be abandoned in accordance with the City of Windsor Engineering Best Practice B.P 1.3.3.

Stormwater Management - A Stormwater Management Report has been received; however, the report submission is incomplete.

The submission for a Storm Detention Scheme will include, at a minimum:

- a. Submission of stormwater management review fee, **(OUTSTANDING)**
- b. Stormwater management report stamped by a professional engineer
- c. Stormwater management check list (see link below)
- d. Site servicing drawings stamped by a professional engineer

The Stormwater Management Report dated March 28, 2024, by NAJM Engineering Inc., has been received and reviewed. While the report is acceptable as a functional submission to support the subject rezoning application, a revised report addressing the comments in Appendix A will be required before a Building Permit can be issued.

The applicant will be required to submit, **prior to the issuance of Building Permits**, a revised and complete stormwater management plan in accordance with Windsor Essex Region Stormwater Management Standards Manual.

Right-of-Way - Wyandotte Street East is designated as an Arterial Class 2 roadway according to the Official Plan, requiring a right-of-way width of 28 metres. The current right-of-way width is deficient, and a 1.5 metre land conveyance is required along the frontage of this property.

In summary we have no objection to the proposed development, subject to the following requirements:

Land Conveyance - Prior to the issuance of a construction permit, the owner(s) shall agree to gratuitously convey to the Corporation, 1.5 metres across the frontage of the subject property for the Wyandotte Street public right-of-way.

[Juan Paramo - Development Engineer]

ENGINEERING - R.O.W.

June 7, 2024 (Comments to Stage 2: Planning Consultation Application)

Revisions required to drawings:

1. **Driveway Approaches** - Do not conform to City of Windsor Standards, which must be constructed with straight flares and no raised curbs within the right-of-way.
 - a. Modify as per Standard Engineering Drawing AS-204.
 - b. Close all redundant curb cuts
2. **Sewer Connections** - The site is serviced by a 250 mm PVC sanitary sewer and a 450 mm RCP sewer located within the Wyandotte St E Street right-of-way. All existing and proposed storm, sanitary and water services must be identified on the drawings, as well as the associated mainline sewers/water mains.
 - a. Identify any redundant connections to be abandoned in accordance with Engineering Best Practice BP1.3.3.
3. **Land Conveyance** - Schedule X classifies Wyandotte St E as a Class 2 Collector Road, requiring a right-of-way width of 28 metres. The current submission does not include this conveyance.
 - a. Modify drawings to include land conveyance of 1.5 metres as specified above.

[Adam Pillon - Manager of Right-of-Way]

ENWIN UTILITIES LTD. - HYDRO ENGINEERING

October 11, 2023 (Comments to Stage 1: Planning Consultation Application)

No Objection.

Please be advised of the overhead 16kV power lines on the northern limit of the property along Wyandotte St E.

Please be advised of the overhead 16kV power lines and single-phase transformer on the eastern limit of the property along the alley.

Please be advised of the overhead 120/240V secondary conductor along the northern and eastern limit of the property.

Prior to working in these areas, we suggest notifying your contractor and referring to the *Occupational Health and Safety Act* and Regulations for Construction Projects to confirm clearance requirements during construction and demolition. Also, we suggest referring to the *Ontario Building Code* for required clearances for New Building Construction.

[Nillavon Balachandran - Hydro Engineering Technologist]

ENWIN UTILITIES LTD. - WATER ENGINEERING

October 11, 2023 (Comments to Stage 1: Planning Consultation Application)

Water Engineering has no objections. There is an existing 25 mm water service for this property that would need to be upgraded for the new proposed dwelling.

[Bruce Ogg - Water Project Review Officer]

PARKS DESIGN & DEVELOPMENT

October 10, 2023 (Comments to Stage 1: Planning Consultation Application)

No comments or concerns from Parks Design & Development, Natural Areas and Forestry.

[Sherif Barsom - Landscape Architect]

PLANNING DEPARTMENT - LANDSCAPE ARCHITECT

September 27, 2023 (Comments to Stage 1: Planning Consultation Application)

The proposed residential development falls below the residential threshold for site plan control. There are no additional studies required from a landscape architectural or urban design perspective related to the subject site. The applicant is to be aware that there are two large trees in the rear of the property which will provide shade in the summer and reduce energy (A/C) costs. As the plan indicates that this area is to provide some of the landscape open space required for such a development, it is strongly recommended that the owner preserve these trees to help offset the amount of hard surface paving that is being proposed, and to mitigate the heat islands effect that will be created from the development.

[Stefan Fediuk - Landscape Architect / Acting Senior Urban Designer]

PLANNING DEPARTMENT - SITE PLAN CONTROL**October 6, 2023 (Comments to Stage 1: Planning Consultation Application)**

Site Plan is not applicable for this proposed development pursuant to the Planning Act and City of Windsor By-law 1-2004.

[Jacqueline Cabral- Clerk Steno]

TRANSIT WINDSOR**September 29, 2023 (Comments to Stage 1: Planning Consultation Application)**

Transit Windsor has no objections to this development. The closest existing transit route to this property is with the Crosstown 2. The closest existing bus stop to this property is almost directly adjacent to this property on Wyandotte at Ellrose Southwest Corner providing direct transit access to this development. This will be maintained with our City Council approved Transit Master Plan.

[Jason Scott - Manager of Transit Planning]

TRANSPORTATION PLANNING**May 22, 2024 (Comments to Stage 2: Planning Consultation Application)**

The Official Plan classifies Wyandotte St E as a Class 2 Arterial Road with a required right-of-way width of 28 metres per Schedule X of the Official Plan. The current right-of-way is insufficient; therefore, a conveyance of 1.5 metres is required.

All parking must comply with ZBL 8600.

All accesses shall conform to the TAC Geometric Design Guide for Canadian Roads and the City of Windsor Standard Engineering Drawings.

Only one access is allowed unless severed.

All exterior paths of travel must meet the requirements of the Accessibility for Ontarians with Disabilities Act (AODA).

[Elara MehriLou - Transportation Planner I]

WALPOLE ISLAND FIRST NATION

No comments provided

WINDSOR POLICE SERVICE

No comments provided

APPENDIX “F” Draft Amending By-law

B Y - L A W N U M B E R -2024

A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600
CITED AS THE "CITY OF WINDSOR ZONING BY-LAW"

Passed the day of , 2024.

WHEREAS it is deemed expedient to further amend By-law Number 8600 of the Council of The Corporation of the City of Windsor, cited as the "City of Windsor Zoning By-law" passed the 31st day of March, 1986, as heretofore amended:

THEREFORE the Council of The Corporation of the City of Windsor enacts as follows:

1. That subsection 1 of Section 20, of said by-law, is amended by adding the following clause:

**513. SOUTH SIDE OF WYANDOTTE STREET EAST BETWEEN JOS JANISSE AVENUE
AND ELLROSE AVENUE**

For the lands comprising of Lot 27, Plan 1492, PIN No. 01097-1316 LT, a *Semi-Detached Dwelling* shall be subject to the following additional provisions:

1. Notwithstanding Section 11.2.5.2.1, the minimum lot width shall be 13.7 metres.
2. Notwithstanding Section 11.2.5.2.10, the maximum gross floor area for a main building shall be 851.6 m².

[ZDM 10; ZNG/7221]

2. The said by-law is further amended by changing the Zoning District Maps or parts thereof referred to in Section 1, of said by-law and made part thereof, so that the lands described in Column 3 are delineated by a broken line and further identified by the zoning symbol shown in Column 5:

1. Item Number	2. Zoning District Map Part	3. Lands Affected	4. Zoning Symbol	5. New Zoning Symbol
1	10	Lot 27, Plan 1492 PIN No. 01097-1316 LT (located on the south side of Wyandotte Street East between Jos Janisse Avenue and Ellrose Avenue)	RD2.2	RD2.2 S.20(1)H513

3. THAT the holding (H) symbol **SHALL APPLY** to the lands located on the south side of Wyandotte Street East between Jos Janisse Avenue and Ellrose Avenue, described as Lot 27, Plan 1492, PIN No. 01097-1316 LT and that Zoning By-law 8600 **BE FURTHER AMENDED** by adding the following clause to Section 95.20:

- (11) a) Gratuitously convey to the Corporation of the City of Windsor, a 1.50-metre-wide strip of land along the Wyandotte Street East frontage of the subject lands to the satisfaction of the City Engineer.
- b) Submission of a Revised Stormwater Management Plan in accordance with the Windsor Essex Region Stormwater Management Standards Manual to the satisfaction of the City Engineer.

[ZDM 10; ZNG/7221]

DREW DILKENS, MAYOR

CLERK

First Reading - , 2024

Second Reading - , 2024

Third Reading - , 2024

SCHEDULE 2

1. By-law _____ has the following purpose and effect:

To amend the zoning of the lands located on the south side of Wyandotte Street East between Jos Janisse Avenue and Ellrose Avenue, legally described as Lot 27, Plan 1492 PIN No. 01097-1316 LT, to permit the development of a semi-detached dwelling on the subject land.

The amending by-law maintains the RD2.2 zoning on the subject land and adds a special zoning provision permitting a semi-detached dwelling on a lot with a reduced lot width and increased gross floor area for a main building.

2. Key map showing the location of the lands to which By-law _____ applies.



PART OF ZONING DISTRICT MAP 10

N.T.S.

SCHEDULE 2

Applicant: Rock Doire



SUBJECT LANDS

PLANNING & BUILDING DEPARTMENT



DATE : JULY, 2024
FILE NO. : Z-023/24, ZNG/7221

PLANNING RATIONALE REPORT (SCOPED)

ZONING BY-LAW AMENDMENT PROPOSED RESIDENTIAL DEVELOPMENT

**4461 Wyandotte Street East
City of Windsor, Ontario**

July 9, 2024

Prepared by:



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1.0 INTRODUCTION

I have been retained by Keen Studio on behalf of Rock Doire (herein the "Applicant"), to provide a land use Scoped Planning Rationale Report (PRR) in support of a proposed development to be located at 4461 Wyandotte Street East (herein the "Site") in the City of Windsor, Province of Ontario.

The Site, in Ward 5 (East Windsor Planning District), is made up of one (1) interior parcel of land, which is currently being used for residential purposes with a single unit dwelling.

It is proposed to demolish the existing residential dwelling and construct a new two (2) storey semi-detached dwelling with two (2) Additional Dwelling Units (ADU) within each semi-detached dwelling.

A total of six (6) residential units are proposed.

On site parking for a total of four (4) spaces is proposed (2 on each proposed lot). Access will be from Wyandotte Street East onto private driveways.

The Site has access to full municipality services.

The tenure of each unit will be rentals.

A site-specific Zoning By-law Amendment (ZBA) is required in support of the proposed development.

Once the ZBA application has been approved, the Applicant will proceed with a building permit.

It is proposed to then sever the semi-detached dwelling along the common wall.

Pre-consultation (stage 1) was completed by the Applicant (City File #PC-022/23) as well as pre-submission (stage 2) (City File #PC-040/24).

The purpose of this report is to review the relevant land use documents, including the Provincial Policy Statement 2020 (PPS), the City of Windsor Official Plan (OP), and the City of Windsor Zoning By-law (ZBL).

This PRR will show that the proposed development is suitable development, is consistent with the PPS, conforms to the intent and purpose of the OP and ZBL, and represents good planning.

2.0 SITE AND SURROUNDING LAND USES

2.1 Legal Description and Ownership

The Site is made up of one (1) interior parcel of land located on the south side of Wyandotte Street East between Jos Janisse Avenue and Ellrose Avenue (see the area in red on Figure 1 – Site Location).

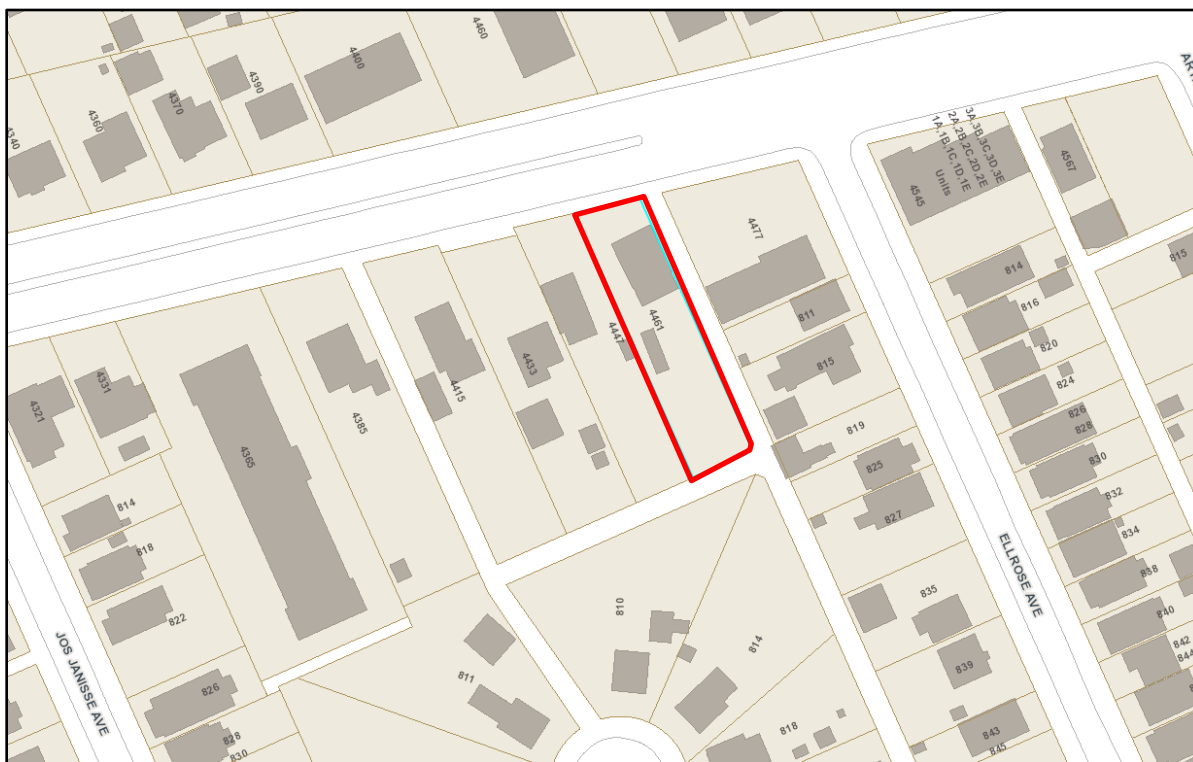


Figure 1 – Site Location (Source: Windsor GIS)

The Site is part of the East Windsor Planning District and is located in the City of Windsor Ward 5.

The Site is owned, locally known as and legally described as follows:

Address	Legal Description	PIN	ARN	Owner	Purchased Date
4461 Wyandotte Street East	Lot 27, Plan 1492; CITY OF WINDSOR	01097-1316 (LT)	3739-010-030-08900-0000	Rock Doire Cari Doire	2020/03/27

2.2 Physical Features of the Site

2.2.1 Size and Site Dimension

The Site, subject to the proposed development, consists of a total area of 873.57 m² (0.087 ha), with 13.7 m of width along Wyandotte Street East and an irregular depth of 59.5 m along the east side and 62.3 m along the west side.

A 1.5 m road widening land conveyance is required.

2.2.2 Existing Structures and Previous Use

The Site is currently being used for residential purposes with a single unit dwelling.

There are no known previous uses.

2.2.3 Vegetation

The property currently has a mown lawn and scattered trees.

2.2.4 Topography

The Site is generally level.

2.2.5 Other Physical Features

There are 2 existing driveway accesses from Wyandotte Street East.

Fencing is located along a portion of the Site owned by others.

There is an alley along the south and east sides of the Site.

2.2.6 Municipal Services

The property has access to municipal water, storm, and sanitary services.

There are streetlights and sidewalks located in the area.

The Site has access to transit.

The Site has access to major transportation corridors.

2.2.7 Nearby Amenities

There are many schools, parks and libraries in close proximity to the Site.

There is nearby shopping in the form of plazas and malls, as well as employment, places of worship, and local amenities.

2.3 Surrounding Land Uses

Overall, the Site is in an existing built up area with a mix of land uses, which includes commercial and residential uses.

A site visit was conducted, and photos were taken on December 12, 2023, by Pillon Abbs Inc.

The following is a summary of the abutting land uses:

Direction	Abutting Land Use
North	Commercial and residential
South	Residential
East	Commercial and residential
West	Residential

3.0 PROPOSAL AND CONSULTATION

3.1 Development Proposal

It is proposed that the Site be developed for residential purposes.

The existing residential dwelling is proposed to be demolished.

A new two (2) storey semi-detached dwelling with two (2) Additional Dwelling Units (ADU) within each semi-detached dwelling is proposed to be constructed.

A concept plan was prepared by Keen Design Studio, dated August 28, 2023, and further revised, dated July 2, 2024 (see Figure 2a –Concept Plan).

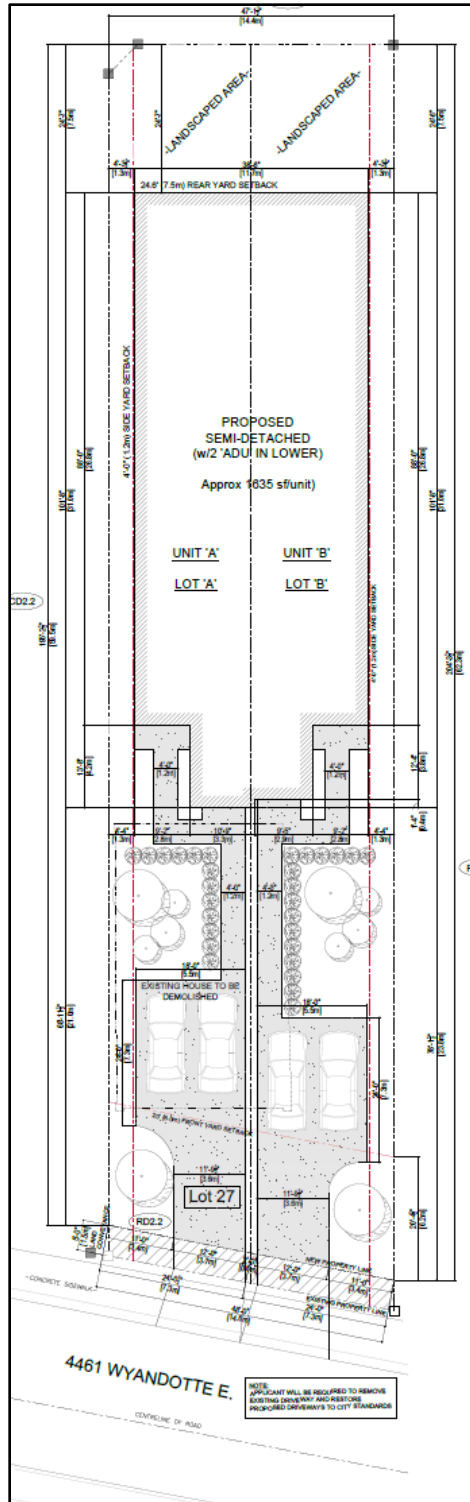


Figure 2a –Concept Plan

4461 Wyandotte St, Windsor, Ontario

The Concept Plan is preliminary in order to illustrate how the Site can be developed.

The building will occupy a total of 326.37 m² of the Site and will be a total of 8.94 m (2 storeys) in height.

The dwelling will be severed along the common wall, as shown as Unit 'A' and Unit 'B' on the concept plan.

A total of six (6) residential units are proposed (3 on each lot).

The building will include 1 and 2 bedroom units.

The tenure of each unit will be rental.

The proposed dwelling will face Wyandotte Street East.

Elevations of the proposed building have been prepared (see Figure 2b – Elevations).

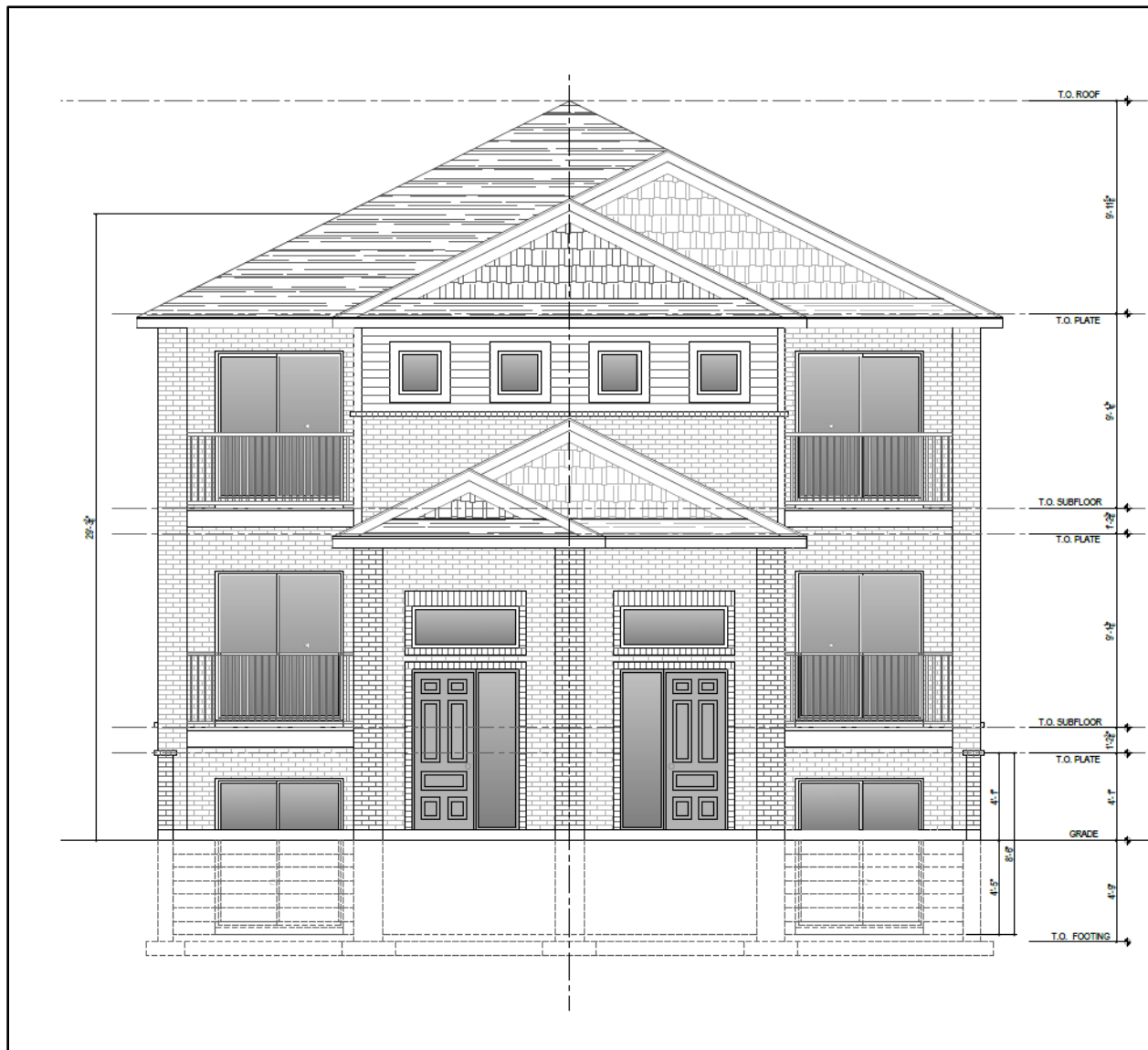


Figure 2b – Elevation

The Elevation is preliminary in order to illustrate how the Site can be developed.

The main access to the building will be from the north side of the Site.

Outdoor seating area, landscaping and amenity space will be provided.

Vehicle access to the Site will be from 2 new driveways along Wyandotte Street East.

The parking area is proposed to be paved and located on the north side of the proposed building.

On site parking for a total of four (4) spaces is proposed (2 on each proposed lot).

Vehicle access will be from Wyandotte Street East onto private driveways.

The refuse (garbage and recycling) will be located in the individual units.

The site proposed to use existing municipal infrastructure.

A 1.5 m land conveyance is required along the frontage of this property and has been included in the concept plan.

3.2 Public Consultation Strategy

In addition to the statutory public meeting, the *Planning Act* requires that the Applicant submit a proposed strategy for public consultation with respect to an application as part of the complete application requirements.

At this time, as the application is considered minor, the required statutory public meeting is proposed.

No informal open house was held.

4.0 APPLICATION AND STUDIES

Pre-consultation (stage 1) was completed by the Applicant (City File #PC-022/23) as well as pre-submission (stage 2) (City File #PC-040/24).

Comments were received and have been incorporated into this PRR.

The proposed development requires an application for a Zoning By-law Amendment (ZBA).

The following explains the purpose of the application as well as a summary of the required support studies.

4.1 Zoning By-law Amendment

A site-specific Zoning By-law Amendment (ZBA) is required in support of the proposed development.

The current zoning of the Site, subject to development, is Residential District 2.2 (RD2.2) category as shown on Map 10 of the City of Windsor Zoning By-law #8600.

It is proposed to further amend the zoning to Residential District 2.2 (RD2.2 - S.20(1)(XXX)) category to permit a semi-detached dwelling with relief from certain regulations.

The ZBA is detailed, and the justification is set out in Section 5.1.3 of this PRR.

4.2 Other Application

Once the ZBA application has been approved, the Applicant will proceed with a building permit.

4.3 Supporting Studies

The following supporting studies have been prepared as part of the application.

4.3.1 Servicing

A Stormwater Management Report (SWM) was prepared by N.A.J.M Engineering Ltd, dated March 28, 2024.

The report provided a review of existing and proposed storm sewer servicing and conditions.

It was concluded that the stormwater management system will address the requirements of the City of Windsor.

The City has noted that a revised SWM report will be required as part of the building permit stage.

5.0 PLANNING ANALYSIS

5.1 Policy and Regulatory Overview

5.1.1 Provincial Policy Statement

The Provincial Policy Statement, 2020 (PPS) provides policy direction on matters of provincial interest related to land use planning and development providing for appropriate development while protecting resources of provincial interest, public health and safety, and the quality of the natural and built environments.

The PPS is issued under Section 3 of the Planning Act and came into effect on May 1, 2020. It applies to all land use planning matters considered after this date.

The PPS supports improved land use planning and management, which contributes to a more effective and efficient land use planning system.

The Site is within a 'Settlement Area', as defined by the PPS.

The following provides a summary of the key policy considerations of the PPS as it relates to the proposed development.

PPS Policy #	Policy	Response
1.0Ontario's long-term prosperity, environmental health and social well-being depend on wisely managing change and promoting efficient land use and development patterns.....	The City has directed growth where the Site is located, which will contribute positively to promoting efficient land use and development patterns.
1.1.1	Healthy, liveable and safe communities are sustained by: a)promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term; b)accommodating an appropriate affordable and market-based range and mix	The proposed development is consistent with the policy to build strong, healthy, and livable communities as it provides for a new housing choice in an existing built up area. There are no environmental or public health and safety concerns as the area is established.

PPS Policy #	Policy	Response
	<p>of residential types, employment, institutional, recreation, park and open space, and other uses to meet long-term needs;</p> <p>c)avoiding development and land use patterns which may cause environmental or public health and safety concerns;</p> <p>d)avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;</p> <p>e) promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;</p> <p>f)improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;</p> <p>g)ensuring that necessary infrastructure and public service facilities are or will be</p>	<p>The development pattern does not require expansion of the settlement area.</p> <p>The Site has access to full municipal services.</p> <p>Accessibility of units will be addressed at the time of the building permit.</p> <p>Public service facilities are available, such as local schools.</p> <p>The development pattern is proposed to be an efficient development of land.</p>

PPS Policy #	Policy	Response
	<p>available to meet current and projected needs;</p> <p>h) promoting development and land use patterns that conserve biodiversity; and</p> <p>i) preparing for the regional and local impacts of a changing climate.</p>	
1.1.3.1	Settlement areas shall be the focus of growth and development.	The Site is within an existing settlement area.
1.1.3.2	<p>Land use patterns within settlement areas shall be based on densities and a mix of land uses which:</p> <p>a) efficiently use land and resources;</p> <p>b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;</p> <p>c) minimize negative impacts to air quality and climate change, and promote energy efficiency;</p> <p>d) prepare for the impacts of a changing climate;</p> <p>e) support active transportation;</p> <p>f) are transit-supportive, where transit is planned,</p>	<p>The Site offers an opportunity for intensification.</p> <p>The design and style of the proposed building will blend well with the scale and massing of the existing surrounding area.</p> <p>Residents will have immediate access to local amenities.</p> <p>Transit is available for the area.</p> <p>The Site is located close to major roadways.</p>

PPS Policy #	Policy	Response
	exists or may be developed; and g) are freight-supportive.	
1.1.3.3	Planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.	The proposed development is located on a Site that is physically suitable. The Site is generally level, which is conducive to easy vehicular movements. The intensification can be accommodated for the proposed development as it is an appropriate development of the Site. Parking will be provided on-site.
1.1.3.4	Appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety.	The proposed building will be built with a high standard of construction. There will be no risks to the public.
1.1.3.6	New development taking place in designated growth areas should occur adjacent to the existing built-up area and should have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities.	The proposed development does have a compact built form. The proposed building size will allow for the efficient use of land, pedestrian and vehicle access, infrastructure, and public services.

PPS Policy #	Policy	Response
1.4.1- Housing	<p>To provide for an appropriate range and mix of housing options and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall:</p> <p>a) maintain at all times the ability to accommodate residential growth for a minimum of 15 years through residential intensification and redevelopment and, if necessary, lands which are designated and available for residential development; and</p> <p>b) maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned to facilitate residential intensification and redevelopment, and land in draft approved and registered plans.</p>	<p>The proposed development is consistent with the PPS in that it provides for a new housing choice.</p> <p>The proposed development will make efficient use of land, resources, existing infrastructure, and public service facilities by increasing the number of residential dwelling units within the settlement area.</p> <p>The proposed development encourages the utilization of existing transit stops located close to the Site to meet the health and well-being of future residents.</p> <p>The proposed development will provide for an infill and intensification opportunity in the existing built-up area.</p> <p>Municipal services are available.</p>
1.4.3	<p>Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs of current and future residents of the regional market area.</p>	<p>The proposed development provides for a new housing choice and utilizes the Site in an efficient manner.</p> <p>The development will allow a new building to be used for residential purposes. This will address the need for housing in the area.</p>

PPS Policy #	Policy	Response
		<p>The proposed density will have a positive impact on the area.</p> <p>The Site is close to nearby amenities.</p> <p>There is suitable infrastructure, including transit.</p>
1.6.1 - Infrastructure	Infrastructure and public service facilities shall be provided in an efficient manner that prepares for the impacts of a changing climate while accommodating projected needs.	The development can proceed on full municipal services.
1.6.6.2	Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. Within settlement areas with existing municipal sewage services and municipal water services, intensification and redevelopment shall be promoted wherever feasible to optimize the use of the services.	The proposed development will be serviced by municipal sewer, water and storm, which is the preferred form of servicing for settlement areas.
1.6.6.7	<p>Planning for stormwater management shall:</p> <p>a) be integrated with planning for sewage and water services and ensure that systems are optimized, feasible and</p>	There will be no negative impacts on the municipal system, and it will not add to the capacity in a significant way.

PPS Policy #	Policy	Response
	<p>financially viable over the long term;</p> <p>b) minimize, or, where possible, prevent increases in contaminant loads;</p> <p>c) minimize erosion and changes in water balance, and prepare for the impacts of a changing climate through the effective management of stormwater, including the use of green infrastructure;</p> <p>d) mitigate risks to human health, safety, property and the environment;</p> <p>e) maximize the extent and function of vegetative and pervious surfaces; and</p> <p>f) promote stormwater management best practices, including stormwater attenuation and re-use, water conservation and efficiency, and low impact development.</p>	<p>There will be no risk to health and safety.</p> <p>There are no natural heritage features located on the Site.</p>
1.6.7.1	Transportation systems should be provided which are safe, energy efficient, facilitate the movement of people and goods, and are appropriate to address projected needs.	The Site is in close proximity to major roadways.
1.6.7.2	Efficient use should be made of existing and planned infrastructure, including through the use of transportation demand	The proposed development contributes to the City's requirements for development within a built-up area.

PPS Policy #	Policy	Response
	management strategies, where feasible.	The area is serviced by transit.
1.6.7.4	A land use pattern, density and mix of uses should be promoted that minimize the length and number of vehicle trips and support current and future use of transit and active transportation.	The proposed development is near many local amenities, and residents would not have to travel far to access necessities.
2.2.1 - Water	Planning authorities shall protect, improve or restore the quality and quantity of water.	No water issues are anticipated.
3.0 – Healthy and Safety	Development shall be directed away from areas of natural or human-made hazards where there is an unacceptable risk to public health or safety or of property damage, and not create new or aggravate existing hazards.	There are no natural or human-made hazards.

Therefore, the proposed development is consistent with the PPS and the Province's vision for long-term prosperity and social well-being.

5.1.2 Official Plan

The City of Windsor Official Plan (OP) was adopted by Council on October 25, 1999, approved in part by the Ministry of Municipal Affairs and Housing (MMAH) on March 28, 2000, and the remainder approved by the Ontario Municipal Board (OMB) on November 1, 2002. The office consolidation version is dated September 7, 2012.

The OP implements the PPS and establishes a policy framework to guide land use planning decisions related to development and the provision of infrastructure and community services throughout the City.

The Site is part of the East Windsor Planning Area, as shown on Schedule A: Planning Districts & Policy Areas.

The current land use designation of the Site, subject to development, is 'Mixed Use Corridor', as shown on Schedule D: Land Use Plan of the City of Windsor Official Plan (as amended by OPA 159) (see Figure 3 –OP).

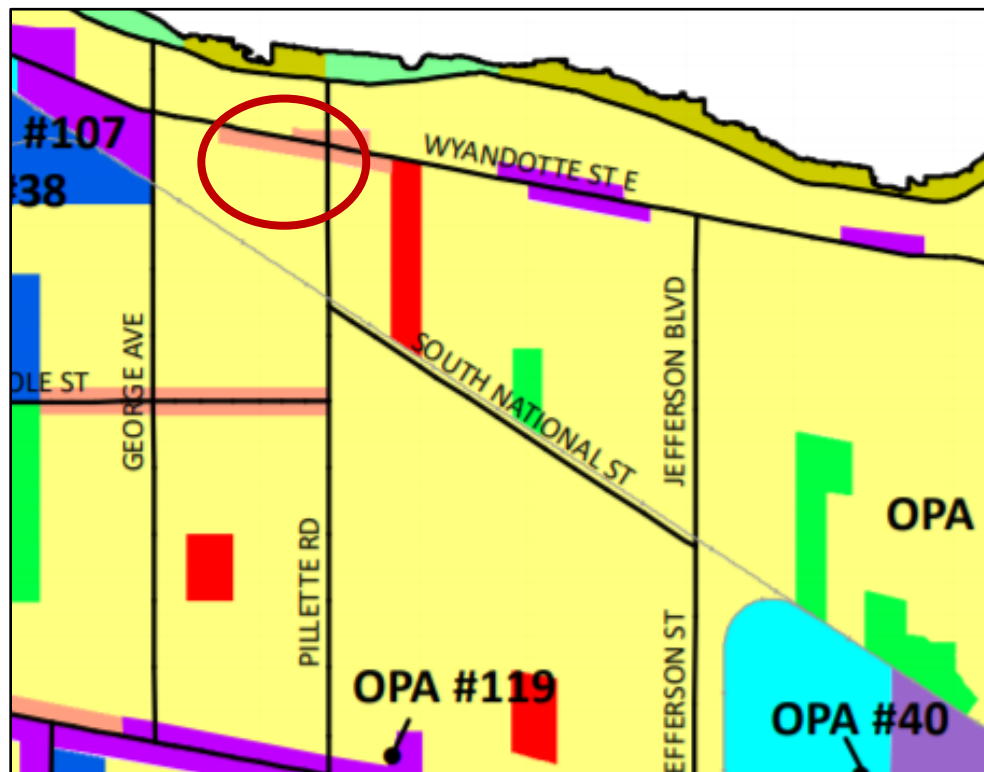


Figure 3 – OP

The Site is also subject to the following:

- adjacent to a Proposed Recreationway (namely Wyandotte Street East) on Schedule B: Greenway System,
- within 1,000.0 metres of a Rail Yard (namely George Avenue Rail Yard) on Schedule C - Development Constraints,
- adjacent to a Class II Arterial Road (namely Wyandotte Street East) on Schedule F: Roads and Bikeways,
- adjacent to a Mainstreet (namely Wyandotte Street East) on Schedule G - Civic Image, and
- within proximity to a Neighbourhood Node on Schedule J - Urban Structure Plan.

The following provides a summary of the key policy considerations of the OP as it relates to the proposed development.

OP Policy #	Policy	Response
3.2.1.2	Encouraging a range of housing types will ensure that people have an opportunity to live in their neighbourhoods as they pass through the various stages of their lives.	The proposed residential development supports one of the City's overall development strategies of providing for a range of housing types.
4.0 – Healthy Community	The implementing healthy community policies are interwoven throughout the remainder of the Plan, particularly within the Environment, Land Use, Infrastructure and Urban Design chapters, to ensure their consideration and application as a part of the planning process.	<p>The proposed development will support the City's goal of promoting a healthy community in order to live, work, and play.</p> <p>The proposed development is close to nearby transit, employment, shopping, local amenities, and parks.</p>
6.0 - Preamble	A healthy and livable city is one in which people can enjoy a vibrant economy and a sustainable healthy environment in safe, caring and diverse neighbourhoods. In order to ensure that Windsor is such a city, Council will manage development through an approach which balances environmental, social and economic considerations.	The proposed development supports the policy set out in the OP as it is suited for addressing the residential needs of the City.
6.1 - Goals	<p>In keeping with the Strategic Directions, Council's land use goals are to achieve:</p> <p>6.1.1 Safe, caring and diverse neighbourhoods.</p> <p>6.1.3 Housing suited to the needs of Windsor's residents.</p> <p>6.1.10 Pedestrian oriented clusters of residential, commercial, employment and institutional uses.</p>	<p>The proposed development supports the goals set out in the OP.</p> <p>The proposed residential use will provide a new housing choice in an existing built-up area.</p>

OP Policy #	Policy	Response
6.2.1.2 – General Policies, Types of Development Profile	<p>For the purpose of this Plan, Development Profile refers to the height of a building or structure. Accordingly, the following Development Profiles apply to all land use designations on Schedule D: Land Use unless specifically provided elsewhere in this Plan:</p> <p>(a) Low Profile developments are buildings or structures generally no greater than three (3) storeys in height;</p> <p>(b) Medium Profile developments are buildings or structures generally no greater than six (6) storeys in height; and</p> <p>(c) High Profile developments are buildings or structures generally no greater than fourteen (14) storeys in height.</p>	<p>The proposed development is considered a low profile development as it is proposed to have 2 storeys.</p>
6.5.3.1 – Mixed Use Corridor (previously Commercial Corridor, prior to OPA 159)	<p>Uses permitted in the Mixed Use Corridor land use designation are primarily retail, wholesale store and service oriented uses and, to a lesser extent, office uses.</p> <p>Medium and High Profile residential uses either as standalone buildings or part of a commercial-residential mixed use buildings shall be throughout the Corridors.</p>	<p>Low profile is proposed based on the size of the Site rather than the medium and high profile buildings.</p> <p>The proposed development is similar to a development located to the west of the Site.</p> <p>The proposed height and massing of the building is appropriate for the area.</p> <p>The development will be designed with a pedestrian orientation and foster a distinctive and attractive area identity.</p>

OP Policy #	Policy	Response
6.5.3.3 - Street Presence	<p>Council will encourage Mixed-Use Corridor development to provide a continuous street frontage and presence. Accordingly, development along a Mixed-Use Corridor shall be:</p> <p>a) no more than four storeys in height, except on lands at an intersection of any combination of the following roads: Class I Arterial Road, Class II Arterial Road, Class I Collector, or Class II Collector Road. The height of buildings shall generally not exceed the width of the road right-of-way abutting the development site; and b) Notwithstanding the identified maximum building height, the Council may consider additional height, where the Council is satisfied that the proposed height achieves compatible development, and where appropriate transitions to abutting lower scale development are established. Appropriate transitions may be achieved through the implementation of regulatory techniques including, but not limited to new height limitations, enhanced building setbacks and step backs, enhanced landscape buffers and planting requirements and/or the implementation of an angular plane. Permissions for taller buildings may be established through a site specific Zoning Bylaw Amendment. c) encouraged</p>	<p>The Site is adjacent to a Class II Arterial Road (namely Wyandotte Street East).</p> <p>The proposed height will be 2 storeys.</p> <p>Parking is to be located at the front of the proposed building, similar to the development located to the west of the Site.</p> <p>The design will address compatibility. It will take into consideration a transition between land uses using an appropriate amount of setbacks and buffering.</p>

OP Policy #	Policy	Response
	to locate the buildings at the street frontage lot line with parking accommodated at the rear of the Site.	
6.5.3.4 – Infill & Consolidations	Council shall promote the infilling and consolidation of existing Mixed Use Corridors.	<p>The proposed building is a form of infill development.</p> <p>The current RD2.2 permits semi-detached dwellings with ADUs.</p>
6.5.3.6 – Location Criteria	Mixed Use Corridor development shall be located where: (a) there is access to Class I or Class II Arterial Roads or Class I Collector Roads; (b) full municipal physical services can be provided; and (c) commercial related traffic can be directed away from residential areas.	<p>Access will only be from Wyandotte Street East with 2 new driveways.</p> <p>Full municipal services are available, which is the preferred type of servicing.</p>
6.5.3.7 – Evaluation Criteria	At the time of submission, the proponent shall demonstrate to the satisfaction of the Municipality that a proposed commercial development is: (a) feasible having regard to the other provisions of this Plan, provincial legislation, policies and appropriate guidelines and support studies for uses: (i) within or adjacent to any area identified on Schedule C: Development Constraint Areas and described in the Environment chapter of this Plan; (ii) within a site of potential or known contamination; (iii) where traffic generation and distribution is a provincial or municipal concern; and (iv) adjacent to sensitive land uses and/or heritage resources. (b) in keeping with the goals,	<p>This PRR has addressed the provisions of the OP and provincial legislation.</p> <p>There are no development constraint areas.</p> <p>The required support studies have been provided to address services.</p> <p>There are no secondary plans that impact the Site.</p> <p>The proposed development will include pedestrian connections, landscaping, and amenity space.</p> <p>Amenity space is provided, including private balconies, and outdoor seating areas.</p>

OP Policy #	Policy	Response
	<p>objectives and policies of any secondary plan or guideline plan affecting the surrounding area; (c) capable of being provided with full municipal physical services and emergency services; (d) provided with adequate off-street parking; (e) compatible with the surrounding area in terms of scale, massing, height, siting, orientation, setbacks, parking and landscaped areas; and (f) acceptable in terms of the proposal's market impacts on other commercial areas (see Procedures chapter).</p>	<p>The proposed scale and massing do not cause any negative impact on the enjoyment of abutting properties.</p> <p>The proposed building will provide an appropriate transition between uses, including an appropriate amount of setbacks.</p>
6.5.3.8 – Design Guidelines	<p>The following guidelines shall be considered when evaluating the proposed design of a Mixed Use Corridor development: (a) the ability to achieve the associated policies as outlined in the Urban Design chapter of this Plan; (b) the provision of appropriate landscaping or other buffers to enhance: (i) all parking lots, and outdoor loading and service areas; and (ii) the separation between the use and adjacent sensitive uses, where appropriate; (c) as a general rule, the height of buildings are consistent with the height of buildings which characterize the Mixed Use Corridor. Where Council deems it desirable that higher profile development be permitted in an existing Mixed Corridor, the development should be built at a human scale by utilizing one</p>	<p>The design and style of the proposed building will blend well with the scale and massing of the surrounding area.</p> <p>There is a similar building to the west of the Site.</p> <p>The Site will have pedestrian connections.</p> <p>The building will face Wyandotte Street East.</p> <p>Parking will be located at the rear of the Site. Parking cannot be provided at the rear of the Site.</p> <p>The proposed development will blend with the existing character of the surrounding area.</p>

OP Policy #	Policy	Response
	or both of the following measures: (i) treatment of the lower floors of building(s) to provide continuity; and/or (ii) setting back the upper floors of building(s) from the street to avoid overpowering effects at-grade; (d) where possible, parking is located in the rear of the property to encourage continuous building facades adjacent to the street; (e) measures are taken in site design which provide for ease of access for pedestrians between the public sidewalk and building main entrances in a manner which is distinguishable from access provided for vehicles; and (f) Council will adopt Design Guidelines that will assist in the design and review of development applications in a manner that will ensure implementation of these policies.	
7.0 - Infrastructure	The provision of proper infrastructure provides a safe, healthy and efficient living environment. In order to accommodate transportation and physical service needs in Windsor, Council is committed to ensuring that infrastructure is provided in a sustainable, orderly and coordinated fashion.	The proposed development is close to nearby transit, off a major roadway, and has access to full municipal services.
8.1 – Urban Design	A memorable, attractive and liveable city is one where people feel comfortable and are inspired by their surroundings. The physical systems and built form of the	The final design of the proposed building will be addressed as part of Building Permit approval.

OP Policy #	Policy	Response
	city are also designed to protect, maintain and improve the quality of life for present and future generations by integrating the principles of sustainability and place making. In order for Windsor to be such a city, Council is committed to urban design principles that enhance the enjoyment and image of Windsor and its people.	
8.7.2.3 – Built Form, infill development	<p>Council will ensure that proposed development within an established neighbourhood is designed to function as an integral and complementary part of that area's existing development pattern by having regard for:</p> <ul style="list-style-type: none"> (a) massing; (b) building height; (c) architectural proportion; (d) volumes of defined space; (e) lot size; (f) position relative to the road; (g) building area to site area ratios; (h) the pattern, scale and character of existing development; (i) exterior building appearance; and (j) Council adopted Design Guidelines that will assist in the design and review of applications for development in accordance with the policies noted above 	<p>The proposed development will be a natural integration of the established area.</p> <p>The proposed building will provide an appropriate transition.</p> <p>Massing – the proposed building will be limited to 2 storeys, which will blend well with the low profile scale and massing of the existing surrounding area.</p> <p>Building height – there are no impacts on privacy or shadowing on abutting properties based on the proposed building height.</p> <p>Architectural proportion – the proposed visual effect of the relationship of the proposed development will blend well with the immediate area.</p> <p>Volume of defined space – the proposed design and layout of the development includes appropriate setbacks and lot coverage.</p>

OP Policy #	Policy	Response
		<p>The parking area will be constructed according to City standards.</p> <p>Lot size – the existing parcel is appropriate for the development.</p> <p>Building area – appropriate lot coverage is proposed. The proposed building will not negatively impact the private use and enjoyment of area residents.</p> <p>Pattern, scale, and character – the style of development will blend well with the scale and massing of the existing low profile surrounding area.</p> <p>Exterior building appearance – the proposed building will be designed professionally and aesthetically pleasing.</p>

Therefore, the proposed development will conform with the purpose and intent of the City of Windsor OP.

5.1.3 Zoning By-law

The City of Windsor Zoning By-law (ZBL) #8600 was passed by Council on July 8, 2002, and then a further Ontario Municipal Board (OMB) decision was issued on January 14, 2003.

A ZBL implements the PPS and the City OP by regulating the specific use of property and provide for its day-to-day administration.

According to Map 10 attached to the ZBL the Site is currently zoned Residential District 2.2 (RD2.2) category (see Figures 4 – ZBL).

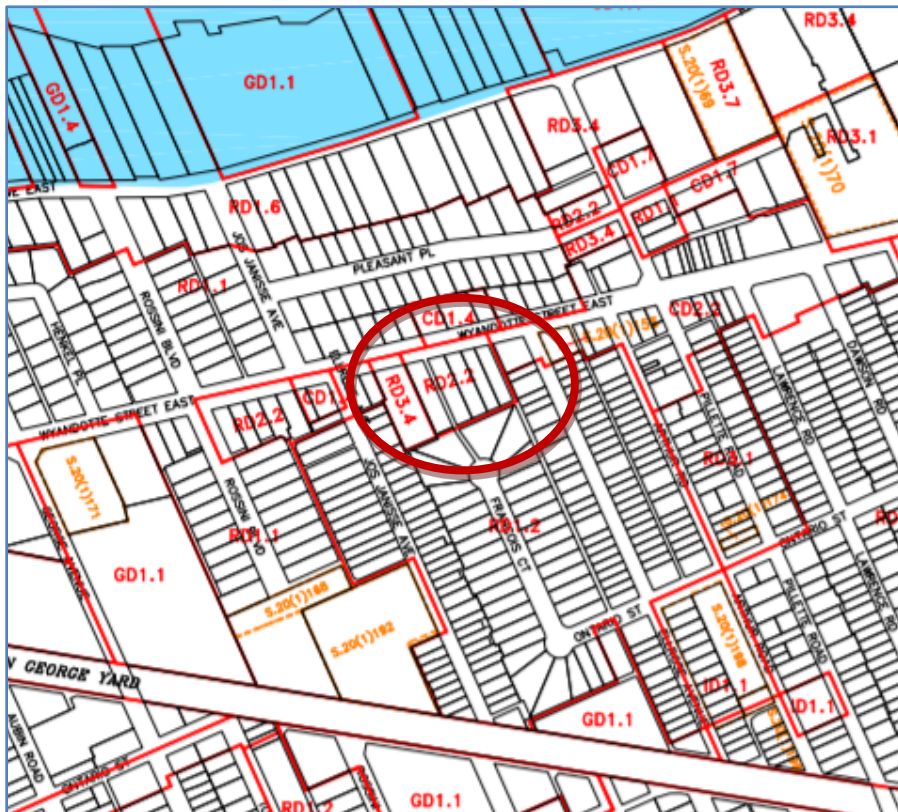


Figure 4 – ZBL

A site-specific ZBA is required for the proposed development to permit a semi-detached dwelling with relief.

SEMI-DETACHED DWELLING means one dwelling divided vertically into two dwelling units by a common interior wall having a minimum area above grade of 10.0 sq. m., and may include, where permitted by Section 5.99.80, up to two additional dwelling units.

An additional dwelling unit is a separate residential dwelling unit consisting of a separate access, kitchen, washroom, and living space that is located within a single detached, semi-detached, duplex dwelling, or rowhouse dwelling (i.e. the primary dwelling unit) or a building accessory to the primary dwelling unit located on the same lot (OP Policy 6.3.2.22).

It is proposed to further amend the zoning to Residential District 2.2 (RD2.2 - S.20(1)(XXX)) category to permit a semi-detached dwelling with relief.

A review of the RD2.2 zone provisions, as set out in Section 11.2 of the ZBL is as follows:

Zone Regulations	Required RD2.2 Zone	Proposed	Compliance and/or Relief Requested Justification with
Permitted Uses	One Double Duplex Dwelling One Duplex Dwelling One Multiple Dwelling containing a maximum of four dwelling units One Semi-Detached Dwelling One Single Unit Dwelling Townhome Dwelling Any use accessory to any of the preceding uses	Semi-detached dwelling	Complies
Additional Dwelling Units (Section 5.99.80.1)	For any zoning district that permits a single unit dwelling, semi-detached dwelling , duplex dwelling, or townhome dwelling, the following additional provisions shall apply: .1 Additional Permitted Uses a) Two additional dwelling units shall be permitted on a parcel of urban residential land. This may be either: i. Two additional dwelling units within the primary dwelling unit located in the main building, or ii. One additional dwelling unit in the primary dwelling unit located in the main building and one	2 ADUs within each semi detached dwelling	Complies

Zone Regulations	Required RD2.2 Zone	Proposed	Compliance and/or Relief Requested Justification with
	additional dwelling unit in a building accessory to said dwelling.		
Lot Width – minimum	15.0 m	13.7 m 12.9 metres + 14.4 metres = 27.3 metres / 2 = 13.7 metres	Relief required. Relief is considered minor. The Site is existing and does not change the lot pattern of the area. There is a variety of lot frontages/widths along Wyandotte St E. The Site is very long and narrow and can accommodate the proposed development adequately. No other relief is being requested, including lot coverage or setbacks. <i>LOT WIDTH means the perpendicular distance in metres between the side lot lines. Where the side lot lines are not parallel, the lot width shall be the average distance in metres between the side lot lines.</i>
Lot Area – minimum	450.0 m ²	873.57 m ²	Complies
Lot Coverage – maximum	45.0%	37.3 %	Complies
Main Building Height - maximum	9.0 m	8.94 m (2 storeys)	Complies

Zone Regulations	Required RD2.2 Zone	Proposed	Compliance and/or Relief Requested Justification with
Front Yard Depth – minimum	6.0 m	21.0 m	Complies
Rear Yard Depth – minimum	7.50 m	7.50 m	Complies
Side Yard Width – minimum	1.20 m	1.3 m east side 1.3 m west side	Complies
Gross Floor Area – Main Building– maximum	400 m2	851.6 m2	<p>Relief required.</p> <p>The Site is very long and narrow and can accommodate the proposed GFA.</p> <p>GFA includes the main floor, second floor and cellar.</p> <p>The Site is located in the 'Mixed Use Corridor' designation of the OP which does support larger scale developments.</p> <p>No other relief is being requested, including lot coverage or setbacks.</p> <p><i>This provision was added to Zoning By-law 8600 by By-law 101-2022, on July 11, 2022. By-law 101-2022 amended Zoning By-law 8600 for the purpose of implementing the recommendations coming out of the Multi-Residential Interim Control By-law Study Background Report, dated April 20, 2022.</i></p>

Zone Regulations	Required RD2.2 Zone	Proposed	Compliance and/or Relief Requested with Justification
Dwelling – Semi-Detached & Townhomes - Additional Provisions (Section 5.23.1)	For a dwelling unit in a semi-detached dwelling or in a townhome dwelling, a door that opens to the rear yard shall be located a minimum of 1.20 m from the centreline of the common wall between the dwelling units.	N/A – no door that opens to the rear yard	Complies
Dwelling – Semi-Detached & Townhomes - Additional Provisions (Section 5.23.5)	When a lot on which a semi-detached dwelling or townhome dwelling has been erected and is subsequently severed by a common interior lot line that separates the dwelling units, for each dwelling unit the following additional provisions shall apply: 1 Lot Width – minimum – equal to the width of the dwelling unit plus any exterior side yard as existing at the time of the lot severance 2 Lot Area – minimum – as existing at the time of the lot severance	TBD	Complies

Zone Regulations	Required RD2.2 Zone	Proposed	Compliance and/or Relief Requested with Justification
	<p>3 Lot Coverage – Total – maximum – 50% of lot area</p> <p>7 An interior side yard shall not be required along the common interior lot line for that part of the dwelling unit lawfully existing at the time of the lot severance</p>		
Parking Requirements - minimum 24.20.5.1	<p>Semi-detached - 1 for each dwelling unit = 2</p> <p>ADU – 1 for the first dwelling unit and 1 for the second and 0 for the third = 2</p>	4	Complies

Therefore, the proposed development will comply with all zone provisions set out in the RD2.2 Zone except for the following, which requires site-specific relief:

1. *decrease the minimum lot width from 15.0 m to 13.7 m, and*
2. *increase the maximum gross floor area from 400 m² to 851.6 m².*

6.0 SUMMARY AND CONCLUSION

6.1 Context and Site Suitability Summary

6.1.1 Site Suitability

The Site is ideally suited for residential development for the following reasons:

- The land area is sufficient to accommodate the proposed development,
- The Site is generally level,
- The Site will be able to accommodate municipal water, storm and sewer systems,
- There are no anticipated traffic or parking concerns, and
- The location of the proposed development is appropriate.

6.1.2 Compatibility of Design

The proposed development will be limited to a 2 storey, low profile building, which is a compatible density for the Site and with the surrounding area.

The proposed residential use will provide a new housing choice in an existing built-up area.

The design of the proposed building will address compatibility.

The Site is capable of accommodating the proposed development in terms of scale, massing, height, and siting.

Parking, amenity areas, and landscaping will be provided.

6.1.3 Good Planning

The proposal represents good planning as it addresses the need for the development of a parcel of land, which contributes to affordability and intensification requirements.

Residential use on the Site represents an efficient development pattern that optimizes the use of land.

There is a similar building to the west of the Site.

6.1.4 Environment Impacts

The proposal does not have any negative impact on the natural environment.

6.1.5 Municipal Services Impacts

There will be no negative impacts on the municipal system as the proposed use will not add to the capacity in a significant way.

6.1.6 Social, Cultural, and Economic Conditions

The proposed development does not negatively affect the social environment as the Site is in close proximity to major transportation corridors and nearby amenities.

Infilling in an existing built-up area of the City contributes toward the goal of 'live, work and play' where citizens share a strong sense of belonging and a collective pride of place.

The proposed development promotes efficient development and land use patterns which sustains the financial well-being of the Municipality.

The proposal does not cause any public health and safety concerns.

The proposal represents a cost-effective development pattern that minimizes land consumption and servicing costs.

There will be no urban sprawl as the proposed development is within the existing settlement area and is an ideal infilling opportunity.

There are no cultural heritage resources that impact the Site.

6.2 Conclusion

In summary, it would be appropriate for the City of Windsor to approve the ZBA application to permit the proposed development on the Site.

This PRR has shown that the proposed development is consistent with the PPS, conforms with the intent and purpose of the OP and ZBL and represents good planning.

The report components for this PRR have set out the following, as required under the City of Windsor OP:

10.2.13.2 Where a Planning Rationale Report is required, such a study should:

- (a) Include a description of the proposal and the approvals required;*
- (b) Describe the Site's previous development approval history;*
- (c) Describe major physical features or attributes of the Site including current land uses(s) and surrounding land uses, built form and contextual considerations;*
- (d) Describe whether the proposal is consistent with the provincial policy statements issued under the Planning Act;*

- (e) Describe the way in which relevant Official Plan policies will be addressed, including both general policies and site-specific land use designations and policies;*
- (f) Describe whether the proposal addresses the Community Strategic Plan;*
- (g) Describe the Suitability of the Site and indicate reasons why the proposal is appropriate for this Site and will function well to meet the needs of the intended future users;*
- (h) Provide an analysis of the compatibility of the design and massing of the proposed developments and land use designations;*
- (i) Provide an analysis and opinion as to why the proposal represents good planning, including the details of any methods that are used to mitigate potential negative impacts;*
- (j) Describe the impact on the natural environment;*
- (k) Describe the impact on municipal services;*
- (l) Describe how the proposal will affect the social and/or economic conditions using demographic information and current trends; and,*
- (m) Describe areas of compliance and non-compliance with the Zoning By-law.*

Planner's Certificate:

I hereby certify that this report was prepared by Tracey Pilon-Abbs, a Registered Professional Planner, within the meaning of the Ontario Professional Planners Institute Act, 1994.


Tracey Pilon-Abbs, RPP
Principal Planner



**4461 Wyandotte St E
Windsor, Ontario**

Stormwater Management Report

March 28, 2024

Prepared for:

N.A.J.M ENGINEERING LTD

4461 Wyandotte St E

4461 Wyandotte St E Windsor, Ontario

Stormwater Management Report

Prepared For:

4461 Wyandotte St E

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N.A.J.M ENGINEERING LTD

PROJECT # 23196

March 28, 2024

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1.0 INTRODUCTION

N.A.J.M. Engineering Ltd. has been retained by the owner of 4461 Wyandotte St E to prepare a Stormwater Management (SWM) report in support of the application for the proposed development of residential building located at 4461 Wyandotte St E in the City of Windsor.

2.0 BACKGROUND

2.1 Existing Condition

The overall site area is 0.0895 hectares, and the site is bounded by existing residential buildings along Wyandotte St E to the North, existing residential developments to the West and South and existing auto repair development to the East. Refer Appendix A for Legal Survey.

The existing site consists of a residential building, which will be demolished. New residential building, infrastructure services, parking lots, driveways, and landscape regions to be constructed are discussed in detail in this report.

Refer to

Figure 2.1 for the subject site's location and aerial view of the existing condition.



Figure 2.1 – Site Location and Existing Aerial Plan

2.2 Soil

Soil information was obtained from Essex Region Conservation Authority Interactive Mapping. The site consists of - Brookston Clay Loam soil of Hydrologic soil group 'D'. Refer Appendix D for soil map.

2.3 Proposed development

The subject development area (0.0895ha) will consist of a new residential building. The proposed development will also have new driveway, parking lot and landscape areas. Refer Appendix E for site plan.

2.4 Background and Resource Information

In preparing this report, the following information was obtained and reviewed:

- City of Windsor - MappMyCity - online Interactive mapping.
- Consultation with the City of Windsor.
- The Windsor/Essex Region Stormwater Management Standards Manual, Version 1, Dec. 6/18. (WERSWM)
- ERCA Interactive mapping
- Topographic survey of Lot 27, Registered plan 1492 in the City of Windsor, County of Essex, Ontario by Verhaegen Land Surveyors – A division of J.D. Barnes Limited dated February 02,2024. (See Appendix A).

3.0 SERVICING INVESTIGATION

Information with respect to existing municipal services and utilities was determined from topographic survey and online interactive mapping.

3.1 Storm Servicing

3.1.1 Existing Storm Sewers and Servicing.

There is an existing 450mm municipal storm sewer in the Wyandotte St E Right of way adjacent to the site. Proposed Storm Connection

The proposed SWM system will be restricted to the 2-year storm event and the discharge to the sewer will be limited to the allowable rate as outlined in Section 4.2.2.

The storm outlet to the sewer will convey controlled runoff from the on-site SWM system which will be employed to meet the Windsor Essex Region Stormwater Management

Standards Manual and the City of Windsor discharge requirements outlined in section 4.1 of this report.

A storm sewer design calculation is provided in Appendix D.

4.0 STORMWATER MANAGEMENT PLAN

4.1 Storm Drainage Criteria

The following general SWM criteria would apply to the Proposed Development:

- Water Quality: Provide long-term average removal of 70% of Total Suspended Solids of fine particle size distribution on an annual loading basis from the post-development site.
- Water Quantity (Rate Control): Control flows from the site during all design storm events (2-year through 100-year storm) to a rate no greater than the peak run off rate that would be generated on the pre-developed site in a 2-year storm event.
- Run-off generated on the entire site, in all storm events, up to and including the 100-year event, shall be contained on-site; and
- Maintain existing drainage patterns, ensuring adjacent properties are not adversely affected.

4.2 Proposed Stormwater Management

4.2.1 Pre-Development Condition

The subject site consists of an existing residential building and concrete and grassed areas. The subject site is assessed to the 450mm municipal storm sewer along Wyandotte St E.

The pre-development flow calculations were performed based on the existing conditions of the site ($C=0.45$) and the allowable release rate will be calculated using the 2-year storm event.

4.2.2 Allowable Release Rate

The stormwater management for this site has been prepared in accordance with the Windsor/Essex Region Stormwater Management Standards Manual, Version 1, Dec. 6/18 (WERSWM) and the City of Windsor guidelines.

As discussed in section 4.2.1, the site is assessed to the 450mm municipal storm sewer; and the allowable release rate shall be restricted to the 2-year flow. The Stormwater calculations was carried out using rational method.

Following rainfall parameters from Windsor/Essex Region Stormwater Management Standards Manual were used to determine the allowable release rate using rational method.

$a = 854$; $b = 7.0$; $c = 0.818$

The allowable peak discharge rate from the site can be calculated using the rational method:

Allowable release rate $Q = 2.78 CiA$

Q = Discharge in L/s

C = Runoff Coefficient

I = Rainfall Intensity

A = Area in hectares

Allowable release rate $Q = 7 \text{ L/s}$

Refer to **Appendix B** for calculations design sheet.

4.2.3 Post Development Conditions

The Stormwater analysis for post development condition was carried out using the rational method. The post-development flows from the proposed development up to the 100-year storm event shall be restricted to the allowable release rate of $0.007\text{m}^3/\text{s}$. On-site quantity control is to be provided for storm events leading up to the 100-yr storm event while maintaining the allowable release rate.

The Stormwater quantity analysis was carried out using the IDF Curve Parameters from WERSWM Standards Manual. Runoff coefficient of $C=0.69$ was used for the post development condition of the site during the 2-year and 5-year storm events. The runoff coefficient was increased by 25% for the 100-year storm event ($C=0.86$). Calculations and details of catchment area and their parameters are provided in Appendix B.

Table 4.1 – Post development SWM Calculation - Results

Storm Event	Allowable Discharge Rate (m ³ /s)	Storage Volume Provided (m ³)	Storage Volume Required (m ³)	High Water Level (m)
2-Yr Storm	0.007	31.50	4.50	179.043
5-Yr Storm	0.007	31.50	7.40	180.555
100-Yr Storm	0.007	31.50	24.80	180.704

- 5.12 m³ of underground pipe storage and 26.4 m³ of surface storage has been provided in the stormwater management design.
- The required storage during the 2-year storm event is **4.50 m³** and the provided underground pipe storage (**5.12m³**) is greater. Hence, no ponding is observed during the 2-year and the 32mm storm events.
- The lowest rim elevation of Catch basin is **180.450m** in the parking lot.
- The Maximum storage provided within the site up to the elevation of **180.750m** is **31.50m³**.
- The maximum storage required during the 100-year storm event is **24.80 m³** and the HWL is **180.704m**. Hence, the HWL within the site contained within the site.
- The maximum surface ponding depth during the 100-year storm event is 0.26m, which satisfies the requirement of WERSWM Standards Manual.
- The minimum finished elevation of the proposed residential building will be **181.100m**, thus providing 0.39m freeboard during the 100-year storm event.
- The overflow from site will be directed towards Wyandotte St E.

4.2.4 Quantity Control

The site will be connected to the existing 450mm storm sewer along Wyandotte St E through the proposed 150mm service connection. To ensure the maximum discharge rate from the SWM system is restricted at or below 7 L/s (0.007m³/s) up to the 100-year storm event, a tempest Inlet control device (LMF) will be installed at the outlet of MH#02.

By providing an underground PIPE storage system with a volume of 5.12m³ and surface storage of 24.80m³ in the parking lot/ landscape areas, the post-developed site will achieve controlled discharge equivalent to the allowable release rate of 7 L/s from the 2-year up to 1:100-year storm events.

4.2.5 Quality Control

Storm runoff from the proposed parking lot will require water quality control. Runoff from the parking lot will be directed towards CBMH#01. To achieve the quality control target - 70% TSS removal, ADS Flexstorm pure inlet filter will be installed at CBMH#01 to provide the adequate quality control required for the development. (Refer Appendix D for details).

4.3 Maintenance

The stormwater management and drainage system for the site does require regular maintenance to ensure that it functions as intended and continues to meet the by-law requirements of the Municipality. Key components of the system and applicable maintenance issues are as follows:

Parking Lot: The CB sump should be inspected regularly and cleaned out when sediment accumulates at the bottom.

Roof Drains: Area drains, and roof drains should be inspected and maintained semi-annually to ensure that they are free of debris that may clog them.

Water Quality Filter: The Water Quality filter should be inspected regularly and maintained per manufacturer's guidelines to ensure optimum operation of the structure.

5.0 EROSION AND SEDIMENT CONTROL

Measures are to be taken during construction to ensure that erosion and/or transportation of sediments off-site is controlled. Mitigation measures include:

- Erection of sediment control fence prior to construction, and maintenance throughout construction activities.
- Construction of a clear-stone "mud-mat" at construction site exists to control the tracking of sediments off-site from the tires of vehicles.
- Use of watering for dust control.

6.0 CONCLUSION

With respect to the proposed development at 4461 Wyandotte St E, the stormwater management system will address the requirements of the City of Windsor, such that:

Storm Servicing

- Proposed development flows will be discharged to the proposed 150mm storm service connection and connected to the existing 450mm municipal storm sewer along Wyandotte St E.
- The total post-development allowable storm discharge rate up to 100-year storm event from the site is 7 L/s ($0.007\text{m}^3/\text{s}$), based on the 2-year storm event to the Municipal storm sewer.

Stormwater Management

- Rooftop runoff will be captured by roof drains and will be connected to the proposed storm sewer system. The parking lot will drain to catch basin CBMH#01.
- For the proposed development, the flows leaving the site will be restricted to 7 L/s up to the 100-year storm event, by the Tempest Inlet Control device (LMF) installed at the outlet of MH#02.
- 2-year storm event is stored within the storm sewer pipes and surface ponding during the 100-year storm event meets the WERSWM Standards criteria.
- Water quality treatment unit is required for the proposed asphalt parking lot. The total runoff from the parking lot will be treated by the ADS Flexstorm Pure Inlet Filters installed in CBMH#01.

We trust that this report satisfies the requirements of the City of Windsor with respect to the subject development. Should you have any questions, please do not hesitate to contact the undersigned.

N.A.J.M Engineering Ltd

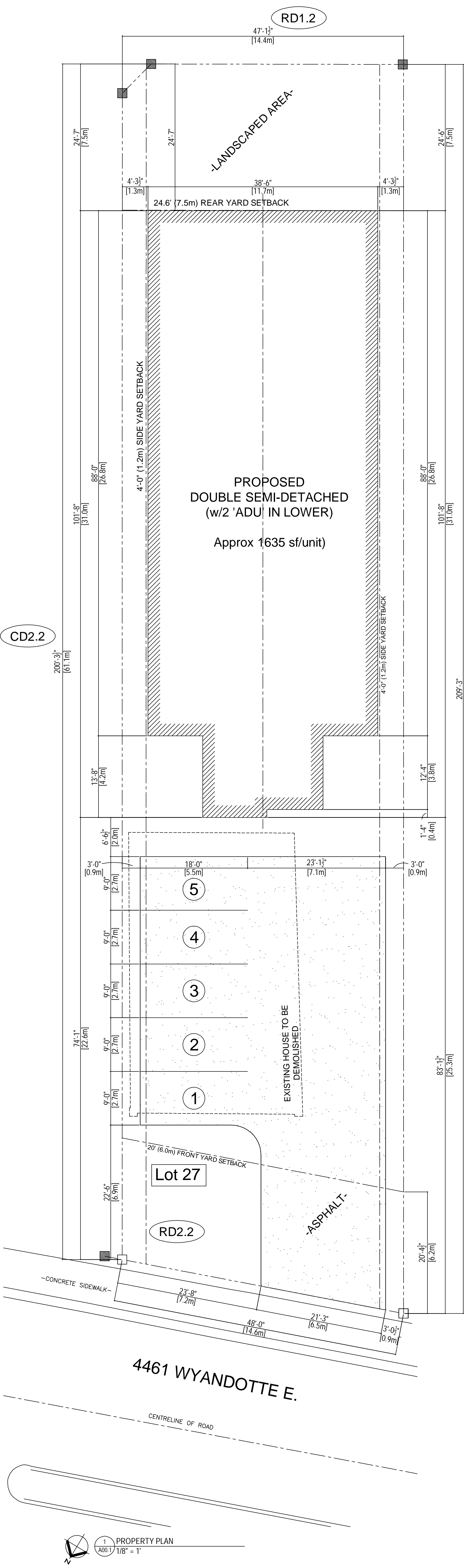


Nadim Mrad, P. Eng.

Project Manager

APPENDIX A

ARCHITECTURAL PLAN



ADDRESS:	LOT 27 4461 WYANDOTTE E. WINDSOR, ON
ZONE:	RD2.2
SITE AREA:	9640 sf
COVERAGE ALLOWANCE:	45% (4338 sf)
PROPOSED COVERAGE:	38% (3651) sf
SETBACKS:	as shown
AREA:	
UNIT 'A':	UNIT 'B':
LOWER (ADU):	1634 sf
Grade Ent:	128 sf
MAIN:	1634 sf
BALCONY:	50 sf
UPPER:	1634 sf
BALCONY:	50 sf
FRONT PORCH:	69 sf
LOWER (ADU):	1634 sf
Grade Ent:	128 sf
MAIN:	1634 sf
BALCONY:	69 sf
UPPER:	1634 sf
BALCONY:	69 sf
FRONT PORCH:	55 sf
GFA:	
ALLOWABLE:	(400 m²) 4305 sf
REQUIRED:	(607 m²) 6536 sf
HARD SURFACE:	
PARKING LOT & DRIVEWAY:	(178 m²) 1916 sf
PERCENTAGE OF LOT COVERAGE:	19.84 %
TOTAL UNITS:	6
PARKING SPACES:	5

NOTE:
SITE INFORMATION BASED ON SITE PLAN INFORMATION ON mappmycity.com

NOTE:
THIS DESIGNER IS NOT RESPONSIBLE FOR ERCA, EASEMENTS, SITE PLAN CONTROL, APPROVALS AND DEVELOPMENTAL SITE CONDITIONS, AND LOT RESTRICTIONS.

THIS PLAN IS NOT A LEGAL SURVEY. ALL DIMENSIONS SHOULD BE VERIFIED BY AN O.L.S. THIS SITE PLAN IS PROVIDED AS A GUIDE TO LOCATE THE BUILDING

--- SETBACKS
----- PROPERTY LINES

4461 Wyandotte E.
A00 - Site
.1 - Property Plan, Front Elevation
.2 - Main Floor Plan

NOTE:
THESE PRINTS ARE THE PROPERTY AND DESIGN OF DESIGNER. THE DESIGNER RESERVES THE RIGHT TO REPRODUCE THE SAME HOUSE AT NO BENEFIT TO THE PURCHASER OF THIS HOME.

NOTE:
THIS DESIGNER IS NOT RESPONSIBLE FOR ERCA, EASEMENTS, SITE PLAN CONTROL, APPROVALS AND DEVELOPMENTAL SITE CONDITIONS, AND LOT RESTRICTIONS.

1
A00.1 1/8" = 1'

PROPERTY PLAN

APPENDIX B

STORMWATER MANAGEMENT CALCULATIONS

N.A.J.M ENGINEERING LTD

PROJECT NAME:	4461 Wyandotte E , Windsor
PROJECT NO	23196
DATE	3/21/2024

MUNICIPALITY OF CHATHAM KENT IDF

Return Period	A	B	C
(Year)			
2	854	7	0.818
5	1259	8.8	0.838
100	2375	11	0.861

I =	$A / ((T+B)^C)$
I=	Intensity
T=	Duration of Rainfall
A,B,C =	Coefflicients

PRE-DEVELOPMENT FLOW CALCULATIONS

Storm event =	2 year
A =	854
B =	7
C=	0.818

Intensity I =	$A/(T+B)^C$	mm/hr
Discharge / Flow Q =	$2.78 \cdot A \cdot C$	L/s

SITE / CATCHMENT PARAMETERS		
CATCHMENT ID =	EXISTING	
Runoff Coefficient 'C' =	0.45	
Area A =	0.0895	ha

Time of Concentration Calculation		
Flow Length L	62	m
Runoff Coefficient C =	0.45	
Slope S =	0.5	%
Time of Concentration T =	21	min.

Time (t)	INTENSITY (I)	A x C	Q (pre)
(min.)	(mm/hr)	(ha)	(L/s)
5	112	0.0403	13
10	84	0.0403	9
19	59	0.0403	7
20	58	0.0403	6

Pre-development Release Rate = 7 L/s

POST-DEVELOPMENT FLOW CALCULATIONS

Storm event =	2 year
A =	854
B =	7
C =	0.818

Intensity I =	$A/(T+B)^C$	mm/hr
Discharge / Flow Q =	$2.78 \cdot A^C$	L/s

SITE / CATCHMENT PARAMETERS

CATCHMENT ID =	PROPOSED	
Runoff Coefficient 'C' =	0.69	
Area A =	0.0895	ha

Time (t) (min.)	INTENSITY (I) (mm/hr)	A x C (ha)	Q (post) (L/s)	STORM VOL. (L)	RELEASE RATE (L/s)	RELEASE VOL. (L)	REQUIRED STORAGE (L)	REQUIRED STORAGE m ³
1	156	0.062	27	1605.5	7.00	420	1185.5	1.2
2	142	0.062	24	2916.0	7.00	840	2076.0	2.1
3	130	0.062	22	4012.8	7.00	1260	2752.8	2.8
4	120	0.062	21	4949.1	7.00	1680	3269.1	3.3
5	112	0.062	19	5761.4	7.00	2100	3661.4	3.7
6	105	0.062	18	6475.5	7.00	2520	3955.5	4.0
7	99	0.062	17	7110.4	7.00	2940	4170.4	4.2
8	93	0.062	16	7680.2	7.00	3360	4320.2	4.3
9	88	0.062	15	8195.9	7.00	3780	4415.9	4.4
10	84	0.062	14	8666.0	7.00	4200	4466.0	4.5
11	80	0.062	14	9097.2	7.00	4620	4477.2	4.5
12	77	0.062	13	9494.8	7.00	5040	4454.8	4.5
13	74	0.062	13	9863.4	7.00	5460	4403.4	4.4
14	71	0.062	12	10206.5	7.00	5880	4326.5	4.3
15	68	0.062	12	10527.3	7.00	6300	4227.3	4.2
20	58	0.062	10	11871.4	7.00	8400	3471.4	3.5

TOTAL ALLOWABLE RELEASE RATE

7 L/s

TOTAL STORAGE REQUIRED - 2YR

4.5 cu.m

POST-DEVELOPMENT FLOW CALCULATIONS

Storm event =	5 year
A =	1259
B =	8.8
C =	0.838

Intensity I =	$A/(T+B)^C$	mm/hr
Discharge / Flow Q =	$2.78 \cdot A^C$	L/s

SITE / CATCHMENT PARAMETERS

CATCHMENT ID =	PROPOSED	
Runoff Coefficient 'C' =	0.69	
Area A =	0.0895	ha

Time (t) (min.)	INTENSITY (I) (mm/hr)	A x C (ha)	Q (post) (L/s)	STORM VOL. (L)	RELEASE RATE (L/s)	RELEASE VOL. (L)	REQUIRED STORAGE (L)	REQUIRED STORAGE m ³
1	186	0.062	32	1915.3	7.00	420	1495.3	1.5
2	171	0.062	29	3531.1	7.00	840	2691.1	2.7
3	159	0.062	27	4917.9	7.00	1260	3657.9	3.7
4	149	0.062	26	6125.1	7.00	1680	4445.1	4.4
5	140	0.062	24	7188.6	7.00	2100	5088.6	5.1
6	132	0.062	23	8135.2	7.00	2520	5615.2	5.6
7	125	0.062	21	8985.0	7.00	2940	6045.0	6.0
8	118	0.062	20	9753.8	7.00	3360	6393.8	6.4
9	113	0.062	19	10454.0	7.00	3780	6674.0	6.7
10	108	0.062	18	11095.6	7.00	4200	6895.6	6.9
11	103	0.062	18	11686.4	7.00	4620	7066.4	7.1
12	99	0.062	17	12233.1	7.00	5040	7193.1	7.2
13	95	0.062	16	12741.2	7.00	5460	7281.2	7.3
14	92	0.062	16	13215.2	7.00	5880	7335.2	7.3
15	88	0.062	15	13658.8	7.00	6300	7358.8	7.4
16	85	0.062	15	14075.5	7.00	6720	7355.5	7.4
17	83	0.062	14	14467.9	7.00	7140	7327.9	7.3
18	80	0.062	14	14838.5	7.00	7560	7278.5	7.3
19	78	0.062	13	15189.3	7.00	7980	7209.3	7.2
20	75	0.062	13	15522.2	7.00	8400	7122.2	7.1

TOTAL ALLOWABLE RELEASE RATE

7 L/s

TOTAL STORAGE REQUIRED - 5 YR

7.4 cu.m

POST-DEVELOPMENT FLOW CALCULATIONS

Storm event =	100 year
A =	2375
B =	11
C =	0.861

Intensity I =	$A/(T+B)^C$	mm/hr
Discharge / Flow Q =	$2.78 \cdot A^C$	L/s

SITE / CATCHMENT PARAMETERS

CATCHMENT ID =	PROPOSED
Runoff Coefficient 'C' =	0.86 (Increased by 25%)
Area A =	0.0895 ha

Time (t) (min.)	INTENSITY (I) (mm/hr)	A x C (ha)	Q (post) (L/s)	STORM VOL. (L)	RELEASE RATE (L/s)	RELEASE VOL. (L)	REQUIRED STORAGE (L)	REQUIRED STORAGE m ³
5	218	0.077	47	14008.9	7.00	2100	11908.9	11.9
10	173	0.077	37	22169.2	7.00	4200	17969.2	18.0
20	123	0.077	26	31706.5	7.00	8400	23306.5	23.3
30	97	0.077	21	37384.8	7.00	12600	24784.8	24.8
40	80	0.077	17	41307.0	7.00	16800	24507.0	24.5
50	69	0.077	15	44257.0	7.00	21000	23257.0	23.3
60	60	0.077	13	46601.4	7.00	25200	21401.4	21.4
70	54	0.077	12	48537.1	7.00	29400	19137.1	19.1

TOTAL ALLOWABLE RELEASE RATE

7 L/s

TOTAL STORAGE REQUIRED - 2YR

24.8 cu.m

STORM WATER MANAGEMENT FACILITY CALCULATIONS**Storm sewer Storage****200mm Pipe**

Length L =	50.3 m
Pipe Diameter =	0.2 m
Volume V =	1.58 m ³

600mm Pipe

Length L =	12.52 m
Pipe Diameter =	0.6 m
Volume V =	3.54 m ³

Underground Storage in

Storm Pipes = 5.12 m³

(Storage provided is greater than storage required for 2-year storm)

Surface Storage Calculation

$$\text{Vol} = \text{frustum of pyramid}$$

$$\text{Vol} = h/3(A_1 + A_2 + (A_1 A_2)^{0.5})$$

$$\text{Storage Vol} = \text{m}^3$$

$$h = \text{Depth}$$

$$A_1 = \text{Bottom Contour Area}$$

$$A_2 = \text{Top Contour Area}$$

$$\text{m}$$

$$\text{m}^2$$

$$\text{m}^2$$

CBMH#1

Elevation	Depth (m)	Contour Area A1 (sq.m)	Contour Area A2 (sq.m)	Storage Volume (cu.m)
180.450	0.00	0.36	0.36	0.0
180.550	0.10	0.36	47	1.7
180.650	0.20	0.36	158	11.1
180.700	0.25	0.36	180	15.7
180.750	0.30	0.36	190	19.9

RCB

Elevation	Depth (m)	Contour Area A1 (sq.m)	Contour Area A2 (sq.m)	Storage Volume (cu.m)
180.500	0.00	0.36	0.36	0.0
180.550	0.05	0.36	3	0.1
180.650	0.15	0.36	26	1.5
180.700	0.20	0.36	47	3.4
180.750	0.25	0.36	73	6.5

Total Surface Storage (Including Storage in Underground Pipes)

Elevation	Storage Volume (cu.m)
178.690	0
179.090	5.1
180.450	5.1
180.550	6.9
180.650	17.6
180.700	24.2
180.750	31.5

2 YR VOL= 4.5cu.m; HWL - 179.043 m

5 YR VOL= 7.4 cu.m; HWL - 180.555 m

100 YR VOL = 24.8 cu.m ; HWL - 180.704 m

Total Storage Provided within
the Site for SWM =31.5 m³

(Storage provided is greater storage required for 100-year storm)

APPENDIX C

LEGAL SURVEY

LEGEND

○ MHH	DENOTES HYDRO MANHOLE	✚ FH	DENOTES FIRE HYDRANT
○ MHS	DENOTES SEWER MANHOLE	✚ WM	DENOTES WATER METER
○ MHT	DENOTES TELEPHONE MANHOLE	✚ WVS	DENOTES WATER VALVE (Service)
○ MHTR	DENOTES TRAFFIC MANHOLE	✚ WVM	DENOTES WATER VALVE (Main)
○ MHW	DENOTES WATER MANHOLE	✚ GM	DENOTES GAS METER
CB	DENOTES CATCH BASIN	✚ GV	DENOTES GAS VALVE
DCB	DENOTES DOUBLE CATCH BASIN	HM	DENOTES HYDRO METER
LSc	DENOTES LIGHT STANDARD CONCRETE	PedT	DENOTES TELEPHONE PEDESTAL
LSs	DENOTES LIGHT STANDARD STEEL	PedCTV	DENOTES CABLE TV PEDESTAL
LSw	DENOTES LIGHT STANDARD WOOD	TRs	DENOTES TRAFFIC SIGN
UPc	DENOTES UTILITY POLE CONCRETE	TRsg	DENOTES TRAFFIC SIGNAL
UPs	DENOTES UTILITY POLE STEEL	TRsb	DENOTES TRAFFIC SIGNAL BOX
UPw	DENOTES UTILITY POLE WOOD	TH	DENOTES TESTHOLE
GP	DENOTES GUY POLE	BM	DENOTES BENCH MARK
GW	DENOTES GUY WIRE	HCP	DENOTES HORIZONTAL CONTROL POINT
Bol	DENOTES BOLLARD	VCP	DENOTES VERTICAL CONTROL POINT
PM	DENOTES PARKING METER	SHR	DENOTES SHRUB
TOC	DENOTES TOP OF CURB	SC	DENOTES SEWER CLEANOUT
BOC	DENOTES BOTTOM OF CURB	INV	DENOTES INVERT

DECIDUOUS AND CONIFEROUS TREES ARE DENOTED DT AND CT RESPECTIVELY. A PREFIX TO THE DESCRIPTION DESIGNATES THE NUMBER OF TREE TRUNKS WHEN TREES ARE CLUMPED TOGETHER AND A SUFFIX DENOTES THE TREE DIAMETER OR (NTS) NOT TO SCALE.

C	C	C	DENOTES OVERHEAD CABLE TV LINE
G	(pipe size)	G	DENOTES GAS LINE
H	H	H	DENOTES OVERHEAD HYDRO LINE
CS	(pipe size)	CS	DENOTES COMBINED SEWER
SA	(pipe size)	SA	DENOTES SANITARY SEWER
ST	(pipe size)	ST	DENOTES STORM SEWER
T	T	T	DENOTES OVERHEAD TELEPHONE LINE
W	(pipe size)	W	DENOTES WATER LINE

UNDERGROUND CABLE, HYDRO OR TELEPHONE LINES ARE PREFIXED WITH THE LETTER "U" (CABLE = uC HYDRO = uH TELEPHONE = uT)

CAUTION

UNDERGROUND UTILITIES AND SERVICES SHOWN ON THIS PLAN ARE APPROXIMATE AND MUST BE VERIFIED BEFORE CONSTRUCTION

INVERTS

INVERTS ARE DERIVED FROM CITY OF WINDSOR SEWER ATLAS (PLATE M3) AND SHOULD BE VERIFIED BEFORE CONSTRUCTION.

"IMPERIAL" DISTANCES AND COORDINATES SHOWN ON THIS PLAN ARE IN FEET AND CAN BE CONVERTED TO METRES BY MULTIPLYING BY 0.3048

INTEGRATION DATA

COORDINATES ARE DERIVED FROM GRID OBSERVATIONS USING THE CAN-NET NETWORK SERVICE AND ARE REFERRED TO UTM ZONE 17 (81° WEST LONGITUDE) NAD83 (CSRS) (2010.0). COORDINATE VALUES ARE TO AN URBAN ACCURACY IN ACCORDANCE WITH SECTION 14(2) O.REG 216/10

POINT ID	NORTHING	EASTING
ORP-A	N15380104.35	E1104572.20
ORP-B	N15380069.50	E1104423.46

COORDINATES CANNOT, IN THEMSELVES, BE USED TO RE-ESTABLISH CORNERS OR BOUNDARIES SHOWN ON THIS PLAN.

FOR BEARING COMPARISON, A ROTATION OF 1° 27' 20" CLOCKWISE WAS APPLIED TO (P1) TO CONVERT TO GRID BEARINGS.

TOPOGRAPHIC SURVEY

OF LOT 27, REGISTERED PLAN 1492 IN THE CITY OF WINDSOR COUNTY OF ESSEX, ONTARIO

© VERHAEGEN LAND SURVEYORS – A DIVISION OF J. D. BARNES LIMITED.

SCALE : 1"=20'



LEGEND AND NOTES

BEARINGS ARE UTM GRID DERIVED FROM OBSERVED REFERENCE POINTS 'A' AND 'B' BY REAL TIME NETWORK OBSERVATIONS AND ARE REFERRED TO UTM ZONE 17 (81° WEST LONGITUDE) NAD83 (CSRS) (2010.0).

DISTANCES ON THIS PLAN ARE GROUND AND CAN BE CONVERTED TO GRID BY MULTIPLYING BY THE COMBINED SCALE FACTOR OF 0.999905

ALL SET SSIB AND PB MONUMENTS WERE USED DUE TO LACK OF OVERBURDEN AND/OR PROXIMITY OF UNDERGROUND UTILITIES IN ACCORDANCE WITH SECTION 11 (4) OF O.REG. 525/91.

■	DENOTES SURVEY MONUMENT FOUND
□	DENOTES SURVEY MONUMENT SET
SSIB	DENOTES STANDARD IRON BAR
SSIB	DENOTES SHORT STANDARD IRON BAR
IB	DENOTES IRON BAR
PB	DENOTES PLASTIC BAR
WIT	DENOTES WITNESS
M	DENOTES MEASURED
S	DENOTES SET
L	DENOTES PERPENDICULAR
D	DENOTES DEED
ORP	DENOTES ORIGIN UNKNOWN
ORP	DENOTES OBSERVED REFERENCE POINT
(P)	DENOTES REGISTERED PLAN 1492
(P1)	DENOTES PLAN 12R-19356
(JDB)	DENOTES J.D. BARNES LIMITED, O.L.S.
(1744)	DENOTES VERHAEGEN LAND SURVEYORS, O.L.S.
(691)	DENOTES HOWARD B. HEAD, O.L.S.
(1341)	DENOTES RICHARD W. MURRAY, O.L.S.

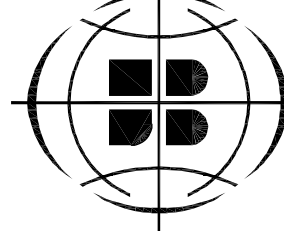
SURVEYOR'S CERTIFICATE

I CERTIFY THAT:

- THIS SURVEY AND PLAN ARE CORRECT AND IN ACCORDANCE WITH THE SURVEYS ACT, THE SURVEYORS ACT AND THE REGULATIONS MADE UNDER THEM.
- THIS SURVEY WAS COMPLETED ON THE 5th DAY OF JANUARY, 2024.

DATE FEBRUARY 2, 2024

ALEC S. MANTHA
ONTARIO LAND SURVEYOR



VERHAEGEN
LAND SURVEYORS
A DIVISION OF J.D. BARNES LTD.

SURVEYING
MAPPING
GIS

944 OTTAWA STREET, WINDSOR, ON, N8X 2E1
T: (519) 258-1772 F: (519) 258-1791 www.jdbarnes.com

DRAWN BY: A.J.M.	CHECKED BY: K.H./A.M.	REFERENCE NO.: 23-47-687-00
FILE: 23-47-687-00.dwg	E-1492-2	CAD Date: January 11th, 2024 CAD File: 23-47-687-00.dwg

APPENDIX D

SUPPLEMENTARY REPORTS AND INFORMATION

N.A.J.M Engineering Ltd						STORM SEWER DESIGN CALCULATIONS FOR 4461 WYANDOTTE, WINDSOR																	
<div>Project No.23196</div> <div>Date:3/28/2024</div> <div>Design byG.S.</div> <div>Reviewed byN.M.</div>						<div>Q = 2.78.peak flow (l/s)</div> <div>A = Area in hectares (ha)</div> <div>I = Rainfall intensity (mm/hr)</div> <div>R = Runoff Co-efficient</div> <div>T= Time of Concentration in minutes</div> <div>I= a/ (T+b)^c</div> <div>a= 1259</div> <div>b= 8.8</div> <div>c= 0.838</div>																	
Location						Indiv. 2.78AR	Accum. 2.78AR	Time of Conc. (T)	Rainfall Intensity (I) (mm/hr)	Peak Flow Q(i) (L/s)	Proposed Sewer							Capacity Full (%)	Ground Elev	UpStream Inv	DnStream Inv	Cover to Top	Cover to springline
Catchment ID	from	to	AREAS (ha)	Composite Runoff Coefficient C=	CxA						Length (m)	Pipe Size (mm)	Type of Pipe	Grade %	Capacity (L/s) n=0.013	Full Flow Velocity (m/s)	Time of Flow (min)						
A1	RCB	CBMH#01	0.0299	0.59	0.018	0.05	0.05	15.00	88.40	4.34	50.3	200	PVC	0.60	25.41	0.81	1.037	17.06	180.500	179.090	178.790	1.2	1.3
A2	CBMH#01	MH#02	0.0596	0.74	0.044	0.12	0.17	16.04	85.30	14.64	12.52	600	HDPE	0.30	336.31	1.19	0.175	4.35	180.450	178.730	178.550	1.1	1.4
TEMPEST INLET CONTROL DEVICE (LMF) INSTALLED AT THE OUTLET OF MH #02 LIMITS FLOW TO 7 L/s																							
	MH#02	OUTLET				-	-			7.00	9.37	150	PVC	1.50	18.65	1.06	0.148	37.53	180.529	178.690	178.400	1.7	1.8

Volume III: TEMPEST INLET CONTROL DEVICES

Municipal Technical
Manual Series



SECOND EDITION

LMF (Low to Medium Flow) ICD

HF (High Flow) ICD

MHF (Medium to High Flow) ICD



IPEX Tempest™ Inlet Control Devices

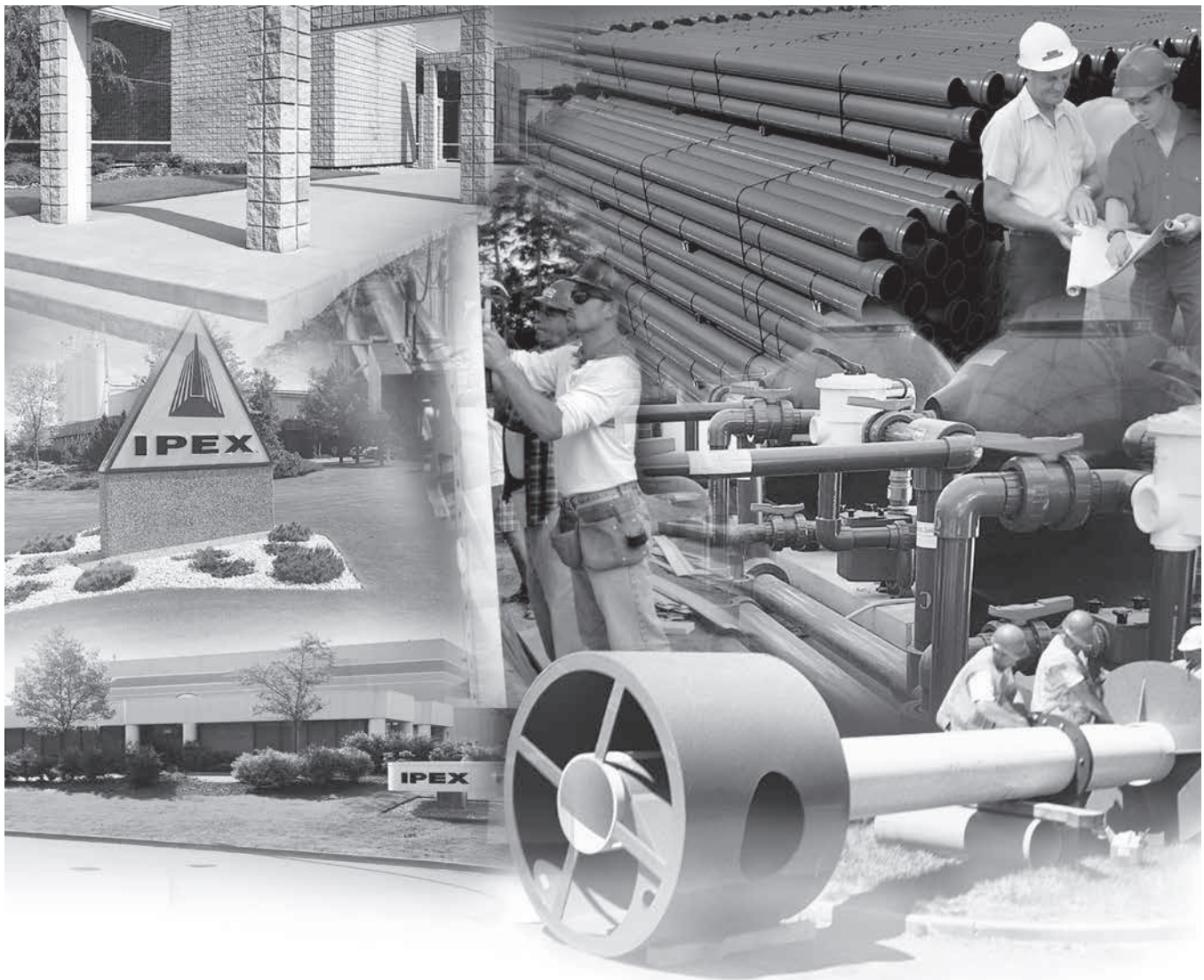
Municipal Technical Manual Series

Vol. I, 2nd Edition

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ABOUT IPEX

At IPEX, we have been manufacturing non-metallic pipe and fittings since 1951. We formulate our own compounds and maintain strict quality control during production. Our products are made available for customers thanks to a network of regional stocking locations throughout North America. We offer a wide variety of systems including complete lines of piping, fittings, valves and custom-fabricated items.

More importantly, we are committed to meeting our customers' needs. As a leader in the plastic piping industry, IPEX continually develops new products, modernizes manufacturing facilities and acquires innovative process technology. In addition, our staff take pride in their work, making available to customers their extensive thermoplastic knowledge and field experience. IPEX personnel are committed to improving the safety, reliability and performance of thermoplastic materials. We are involved in several standards committees and are members of and/or comply with the organizations listed on this page.

For specific details about any IPEX product, contact our customer service department.

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TEMPEST INLET CONTROL DEVICES Technical Manual

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PRODUCT INFORMATION: TEMPEST LOW, MEDIUM FLOW (LMF) ICD

Purpose

To control the amount of storm water runoff entering a sewer system by allowing a specified flow volume out of a catch basin or manhole at a specified head. This approach conserves pipe capacity so that catch basins downstream do not become uncontrollably surcharged, which can lead to basement floods, flash floods and combined sewer overflows.

Product Description

Our LMF ICD is designed to accommodate catch basins or manholes with sewer outlet pipes 6" in diameter and larger. Any storm sewer larger than 12" may require custom modification. However, IPEX can custom build a TEMPEST device to accommodate virtually any storm sewer size.

Available in 14 preset flow curves, the LMF ICD has the ability to provide flow rates: 2lps – 17lps (31gpm – 270gpm)

Product Function

The LMF ICD vortex flow action allows the LMF ICD to provide a narrower flow curve using a larger orifice than a conventional orifice plate ICD, making it less likely to clog. When comparing flows at the same head level, the LMF ICD has the ability to restrict more flow than a conventional ICD during a rain event, preserving greater sewer capacity.

Product Construction

Constructed from durable PVC, the LMF ICD is light weight 8.9 Kg (19.7 lbs).

Product Applications

Will accommodate both square and round applications:

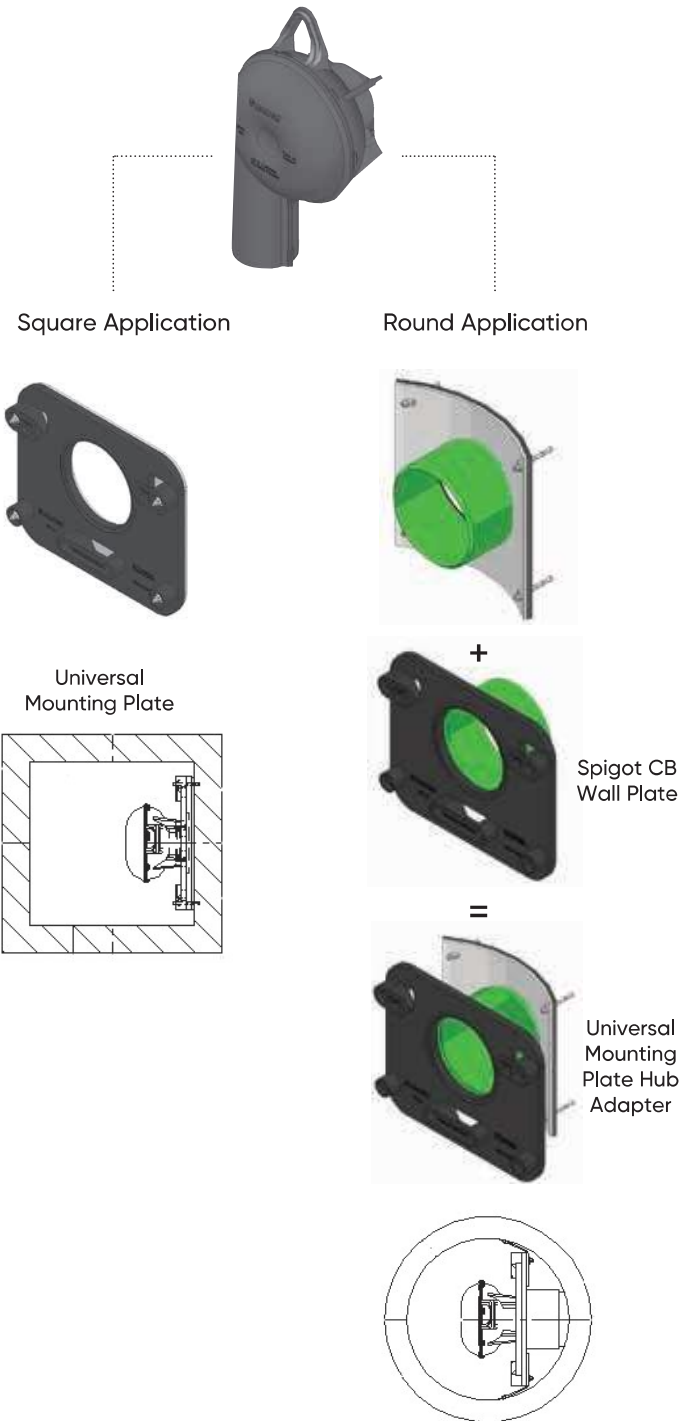


Chart 1: LMF 14 Preset Flow Curves

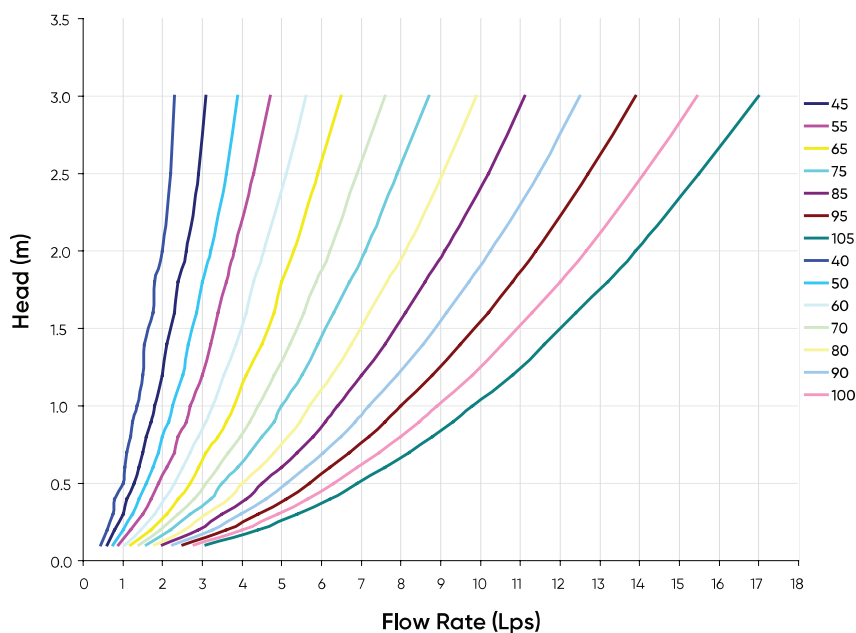
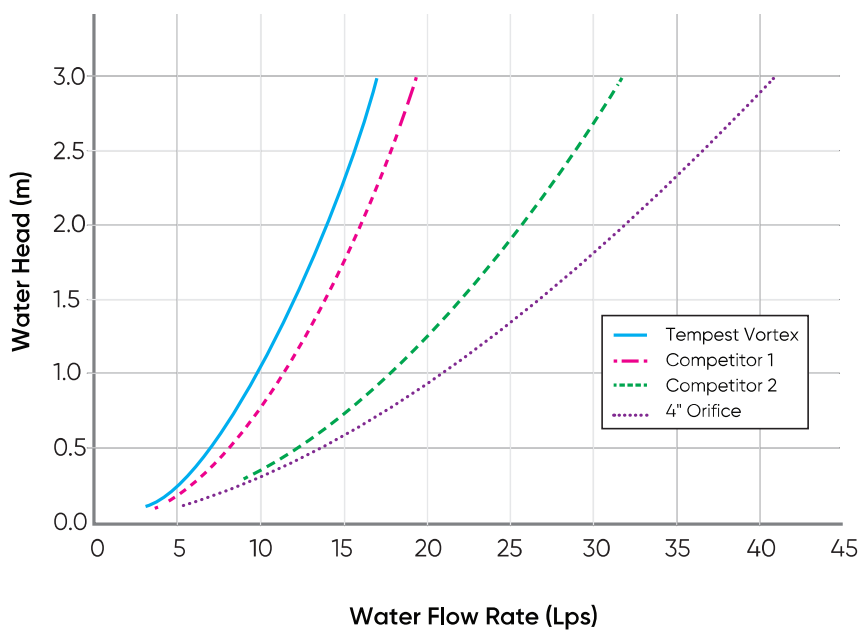


Chart 2: LMF Flow vs. ICD Alternatives



PRODUCT INSTALLATION

Instructions to assemble a TEMPEST LMF ICD into a Square Catch Basin:

STEPS:

1. Materials and tooling verification:
 - Tooling: impact drill, 3/8" concrete bit, torque wrench for 9/16" nut, hand hammer, level, and marker.
 - Material: (4) concrete anchor 3/8 x 3-1/2, (4) washers, (4) nuts, universal mounting plate, ICD device.
2. Use the mounting wall plate to locate and mark the hole (4) pattern on the catch basin wall. You should use a level to ensure that the plate is at the horizontal.
3. Use an impact drill with a 3/8" concrete bit to make the four holes at a minimum of 1-1/2" depth up to 2-1/2". Clean the concrete dust from the holes.
4. Install the anchors (4) in the holes by using a hammer. Thread the nuts on the top of the anchors to protect the threads when you hit the anchors with the hammer. Remove the nuts from the ends of the anchors.
5. Install the universal mounting plate on the anchors and screw the 4 nuts in place with a maximum torque of 40 N.m (30 lbf-ft). There should be no gap between the wall mounting plate and the catch basin wall.
6. From the ground above using a reach bar, lower the ICD device by hooking the end of the reach bar to the handle of the ICD device. Align the triangular plate portion into the mounting wall plate. Push down the device to be sure it has centered in to the universal mounting plate and has created a seal.



WARNING

- Verify that the outlet pipe doesn't protrude into the catch basin. If it does, cut down the pipe flush to the catch basin wall.
- Call your IPEX representative for more information or if you have any questions about our products.

Instructions to assemble a TEMPEST LMF ICD into a Round Catch Basin:

STEPS:

1. Materials and tooling verification.
 - Tooling: impact drill, 3/8" concrete bit, torque wrench for 9/16" nut, hand hammer, level and marker.
 - Material: (4) concrete anchor 3/8 x 3-1/2, (4) washers and (4) nuts, spigot CB wall plate, universal mounting plate hub adapter, ICD device.
2. Use the spigot catch basin wall plate to locate and mark the hole (4) pattern on the catch basin wall. You should use a level to ensure that the plate is at the horizontal.
3. Use an impact drill with a 3/8" concrete bit to make the four holes at a depth between 1-1/2" to 2-1/2". Clean the concrete dust from the holes.
4. Install the anchors (4) in the holes by using a hammer. Thread the nuts on the top of the anchors to protect the threads when you hit the anchors with the hammer. Remove the nuts from the ends of the anchors.
5. Install the CB spigot wall plate on the anchors and screw the 4 nuts in place with a maximum torque of 40 N.m (30 lbf-ft). There should be no gap between the spigot wall plate and the catch basin wall.
6. Apply solvent cement on the hub of the universal mounting plate, hub adapter and the spigot of the CB wall plate, then slide the hub over the spigot. Make sure the universal mounting plate is at the horizontal and its hub is completely inserted onto the spigot. Normally, the corners of the universal mounting plate hub adapter should touch the catch basin wall.
7. From ground above using a reach bar, lower the ICD device by hooking the end of the reach bar to the handle of the ICD device. Align the triangular plate portion into the mounting wall plate. Push down the device to be sure it has centered in to the mounting plate and has created a seal.



WARNING

- Verify that the outlet pipe doesn't protrude into the catch basin. If it does, cut back the pipe flush to the catch basin wall.
- The solvent cement which is used in this installation is to be approved for PVC.
- The solvent cement should not be used below 0°C (32°F) or in a high humidity environment. Refer to the IPEX solvent cement guide to confirm the required curing time or visit the IPEX Online Solvent Cement Training Course available at ipexna.com.
- Call your IPEX representative for more information or if you have any questions about our products.

PRODUCT TECHNICAL SPECIFICATION

General

Inlet control devices (ICD's) are designed to provide flow control at a specified rate for a given water head level and also provide odour and floatable control. All ICD's will be IPEX Tempest or approved equal.

All devices shall be removable from a universal mounting plate. An operator from street level using only a T-bar with a hook will be able to retrieve the device while leaving the universal mounting plate secured to the catch basin wall face. The removal of the TEMPEST devices listed above must not require any unbolting or special manipulation or any special tools.

High Flow (HF) Sump devices will consist of a removable threaded cap which can be accessible from street level with out entry into the catchbasin (CB). The removal of the threaded cap shall not require any special tools other than the operator's hand.

ICD's shall have no moving parts.

Materials

ICD's are to be manufactured from Polyvinyl Chloride (PVC) or Polyurethane material, designed to be durable enough to withstand multiple freeze-thaw cycles and exposure to harsh elements.

The inner ring seal will be manufactured using a Buna or Nitrile material with hardness between Duro 50 and Duro 70.

The wall seal is to be comprised of a 3/8" thick Neoprene Closed Cell Sponge gasket which is attached to the back of the wall plate.

All hardware will be made from 304 stainless steel.

Dimensioning

The Low Medium Flow (LMF), High Flow (HF) and the High Flow (HF) Sump shall allow for a minimum outlet pipe diameter of 200mm with a 600mm deep Catch Basin sump.

Installation

Contractor shall be responsible for securing, supporting and connecting the ICD's to the existing influent pipe and catchbasin/manhole structure as specified and designed by the Engineer.

APPENDIX E

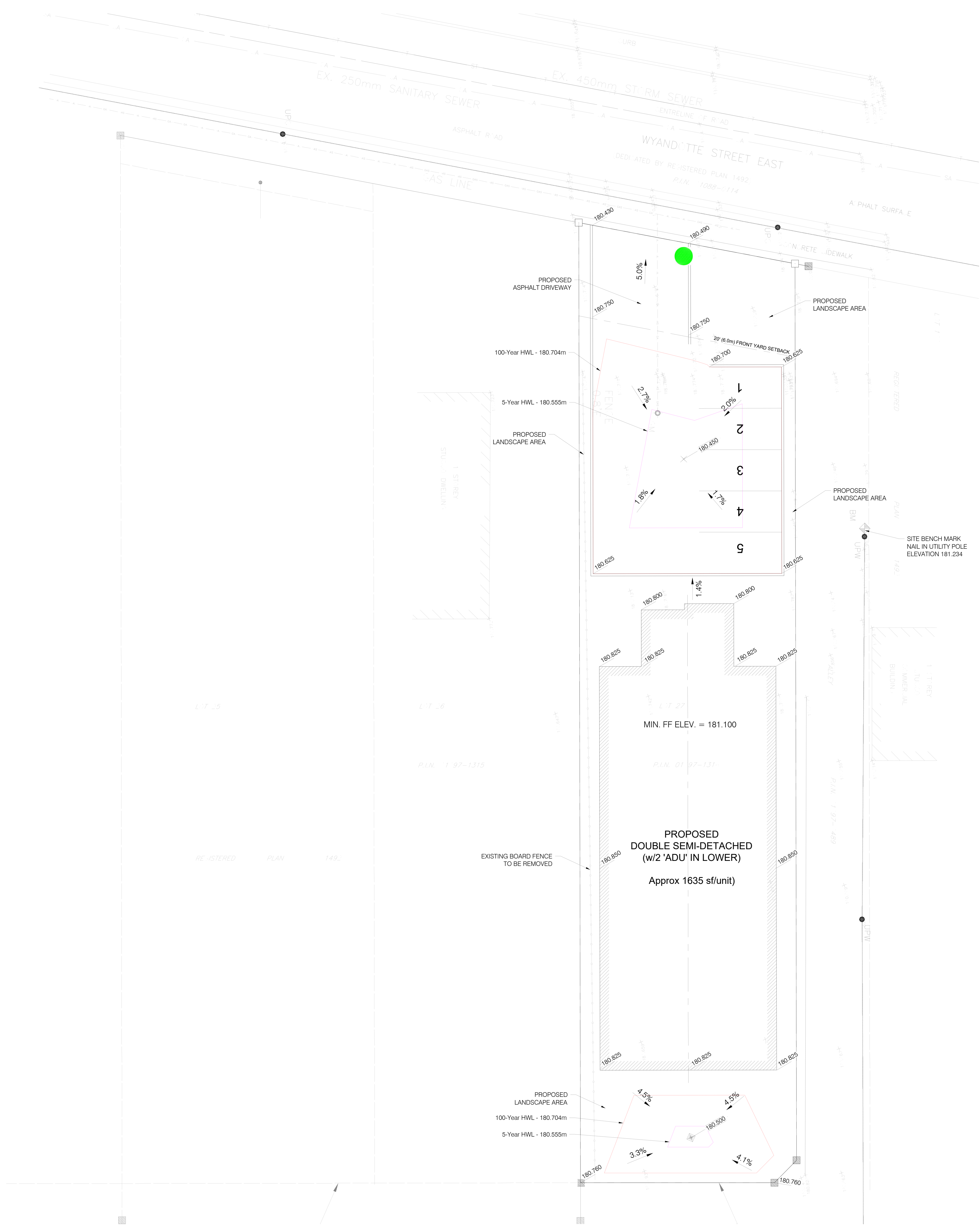
CIVIL DRAWINGS

461 WYANDOTTE ST E, WINDSOR, ON

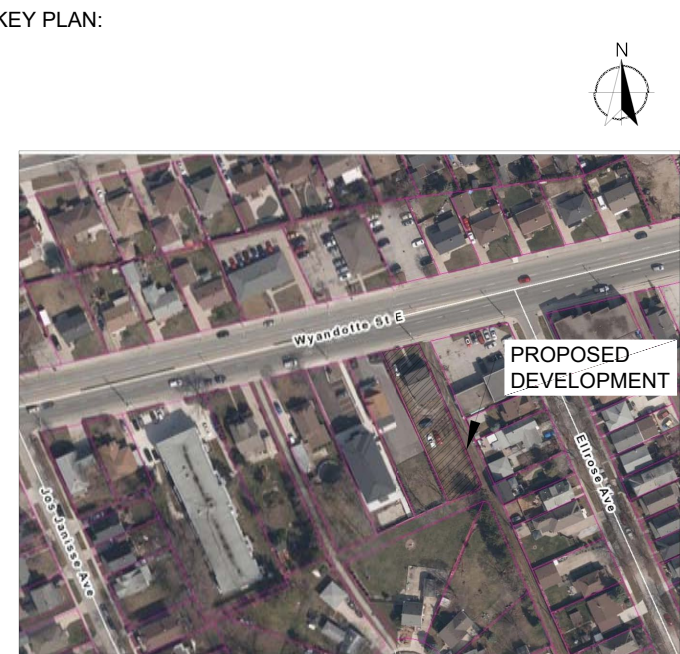
INDEX	
No.	SHEET DESCRIPTION
01	GRADING PLAN
02	SERVICING PLAN
03	STORM DRAINAGE AREA PLAN
04	DETAILS

BENCHMARK

NAIL IN UTILITY POLE (EAST OF SUBJECT PROPERTY) - 181.234m



DISCLAIMER:
1. THE CONTRACTOR IS RESPONSIBLE TO VERIFY THE ACCURACY OF THE SURVEY AND UTILITIES IN THE SITE AND MUST INFORM
2. THE DRAWING IS NOT TO BE SCALED.
3. CONTRACTOR IS RESPONSIBLE TO PERFORM ALL CONSTRUCTION PER APPROPRIATE STANDARDS OF THE CITY OF WINDSOR/ OPSD.



KEY PLAN:

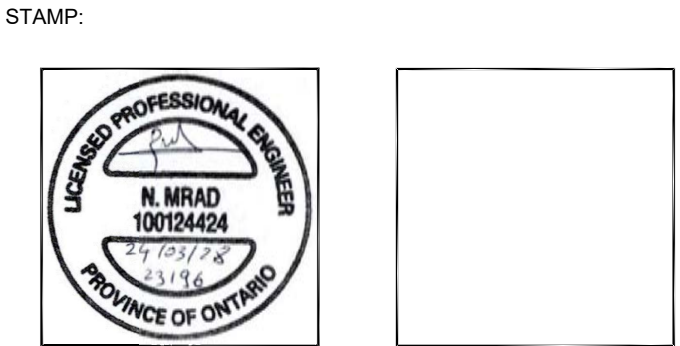
SCALE= N.T.S

LEGEND:

176.28	EX. ELEVATION
176.176	PROP. ELEVATION
176.176	PROP. TOP OF CB/CBMH
PROP. STORM SEWER	PROP. STORM SEWER
PROP. STORM MANHOLE	PROP. STORM MANHOLE
PROP. CATCH BASIN MANHOLE	PROP. CATCH BASIN MANHOLE
PROP. REAR YARD CB	PROP. REAR YARD CB
PROP. STORM CATCHMENT AREA	PROP. STORM CATCHMENT AREA
EX. WATER SERVICE	EX. WATER SERVICE
EX. SANITARY SERVICE	EX. SANITARY SERVICE
ST	EX STORM SEWER
SA	EX SANITARY SEWER
4.8%	PROP. SLOPE ELEVATION
EX. FIRE HYDRANT	EX. FIRE HYDRANT
EX. GAS LINE	EX. GAS LINE
EX. OVERHEAD LINE	EX. OVERHEAD LINE
PROPOSED PROPERTY LINE	PROPOSED PROPERTY LINE
5 - Year HWL - 180.550 m	5 - Year HWL - 180.550 m
100 - Year HWL - 180.704m	100 - Year HWL - 180.704m

REVISIONS

No.	DATE	DESCRIPTION
01	MAR 28, 2024	ISSUED FOR SPA



ENGINEER:
N.A.J.M. ENGINEERING LTD.
9000 CATER DRIVE, UNIT 4
WINDSOR, ON N9S 1C3 +1-519-333-1715

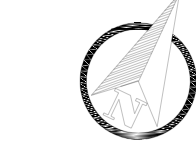
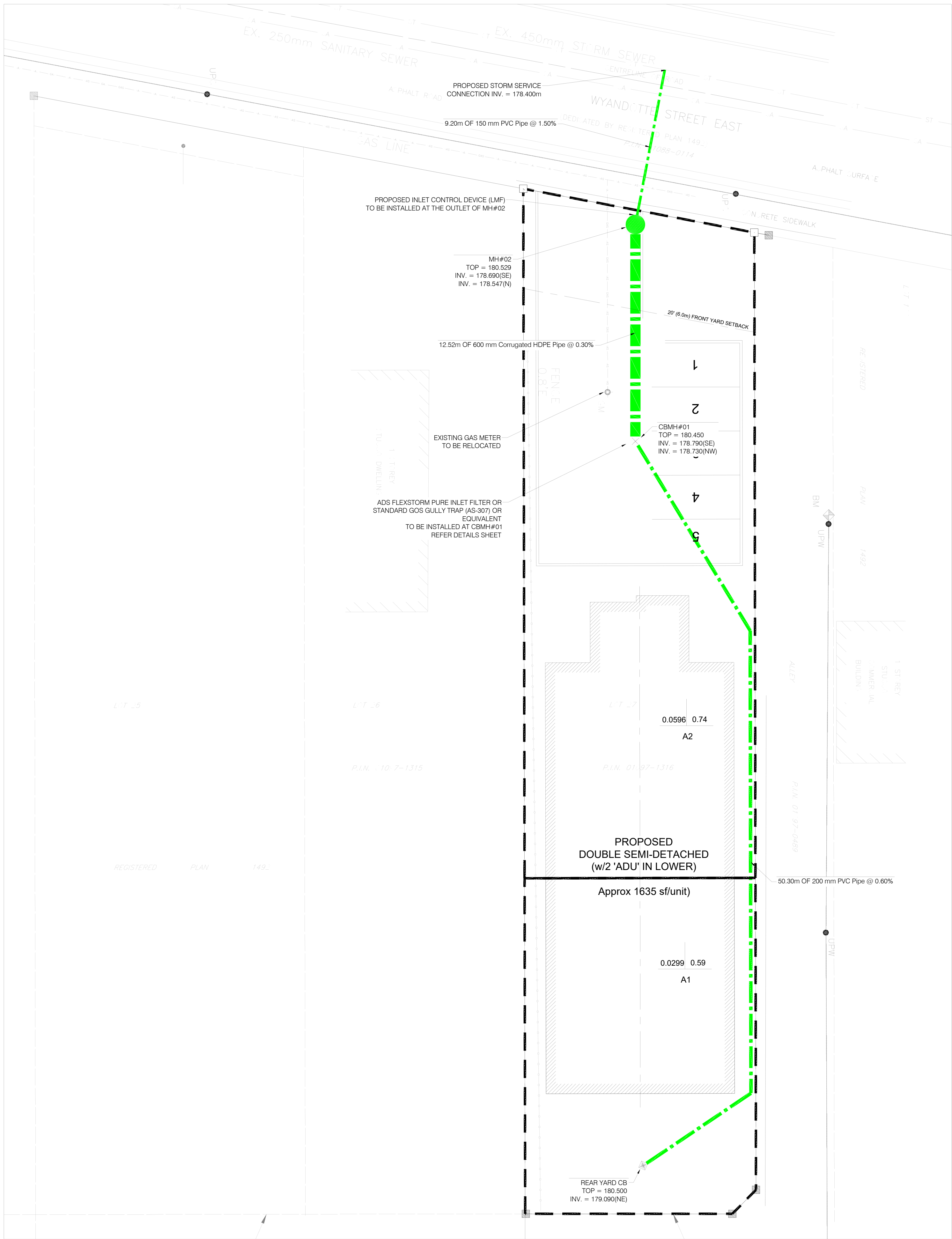
PROJECT:
4461 WYANDOTTE ST E

CLIENT:

SHEET TITLE:
GRADING PLAN

ADDRESS: 4461 WYANDOTTE ST E, WINDSOR, ON

DRAWN BY:	G.S.	SCALE:	1:150
DESIGNED BY:	G.S.	DATE:	MAR-28-2024
CHECKED BY:	N.M.	JOB #:	23196
APPROVED BY:	N.M.	DRAWING #:	01



N.A.J.M Engineering Ltd

Project No.	23196
Date:	3/28/2024
Design by	G.S.
Reviewed by	N.M.

STORM SEWER DESIGN CALCULATIONS FOR 4461 WYANDOTTE, WINDSOR

Q = 2.78 peak flow (l/s)
A = Area in hectares (ha)
I = Rainfall intensity (mm/hr)
R = Runoff Co-efficient
T= Time of Concentration in minutes

$I = a / (T+b)^c$
a = 1259
b = 8.8
c = 0.838

Catchment ID	Location			Composite Runoff Coefficient C _w	Cx _A	Indiv. 2.78AR	Accum. 2.78AR	Time of Conc. (T)	Rainfall Intensity (I) (mm/hr)	Peak Flow Q(I) (L/s)	Proposed Sewer						Capacity Full (%)	Ground Elev	UpStream Inv	DnStream Inv	Cover to Top	Cover to springline	
	from	to	AREAS (ha)								Length (m)	Pipe Size (mm)	Type of Pipe	Grade %	Capacity (L/s) n=0.013	Full Flow Velocity (m/s)							Time of Flow (min)
A1	RCB	CBMH#01	0.0299	0.59	0.018	0.05	0.05	15.00	88.40	4.34	50.3	200	PVC	0.60	25.41	0.81	1.037	17.06	180.500	179.090	178.790	1.2	1.3
A2	CBMH#01	MH#02	0.0596	0.74	0.044	0.12	0.17	16.04	85.30	14.64	12.52	600	HDPE	0.30	336.31	1.19	0.175	4.35	180.450	178.730	178.550	1.1	1.4
TEMPEST INLET CONTROL DEVICE (LMF) INSTALLED AT THE OUTLET OF MH #02 LIMITS FLOW TO 7 L/s																							
	MH#02	OUTLET				-	-			7.00	9.37	150	PVC	1.50	18.65	1.06	0.148	37.53	180.529	178.690	178.400	1.7	1.8

DISCLAIMER:
1. THE CONTRACTOR IS RESPONSIBLE TO VERIFY THE ACCURACY OF THE SURVEY AND UTILITIES IN THE SITE AND MUST INFORM N.A.J.M ENGINEERING LTD IN THE EVENT OF ANY DISCREPANCY.
2. THE DRAWING IS NOT TO BE SCALED.
3. CONTRACTOR IS RESPONSIBLE TO PERFORM ALL CONSTRUCTION PER APPROPRIATE STANDARDS OF THE CITY OF WINDSOR/ OPSD.



- KEY PLAN:
- SCALE= N.T.S
- LEGEND:
- EX. ELEVATION
 - PROP. ELEVATION
 - PROP. TOP OF CB/CBMH
 - PROP. STORM SEWER
 - PROP. STORM MANHOLE
 - PROP. CATCH BASIN MANHOLE
 - PROP. REAR YARD CB
 - PROP. STORM CATCHMENT AREA
 - EX. WATER SERVICE
 - EX. SANITARY SERVICE
 - EX STORM SEWER
 - EX SANITARY SEWER
 - PROP. SLOPE ELEVATION
 - EX. FIRE HYDRANT
 - EX. GAS LINE
 - EX. OVERHEAD LINE
 - PROPOSED PROPERTY LINE
 - 5 - Year HWL - 180.550 m
 - 100 - Year HWL - 180.704m

REVISIONS		
No.	DATE	DESCRIPTION
01	MAR 28, 2024	ISSUED FOR SPA



ENGINEER:
N.A.J.M. ENGINEERING LTD.
901 CATER DRIVE, UNIT 4
LD ACTLE, N N9S 1J3 +1-519-333-1755

PROJECT:
4461 WYANDOTTE ST E

CLIENT:

SHEET TITLE:	
STORM DRAINAGE AREA PLAN	
ADDRESS: 4461 WYANDOTTE ST E, WINDSOR, ON	
DRAWN BY:	SCALE: 1:150
DESIGNED BY:	DATE: MAR-28-2024
CHECKED BY:	JOB #: 23196
APPROVED BY:	DRAWING #: 03

ROUND INLET FILTER

Clear Opening Size	Style P/N	Minimum Bypass Flow Rate (CFS)
Small: 10" - 16" Dia.	62SHDR	1.6
Medium: 17" - 24" Dia.	62MHDR	2.7
Large: 25" - 36" Dia.	62LHDR	3.8

CURB OPEN THROAT INLET FILTER

Basin Width Size	Style P/N	Minimum Bypass Flow Rate (CFS)
Up to 4' Width (1 Piece Set)	62HDMW1	2.4
4' - 8' Width (2 Piece Set)	62HDMW2	3.5
8' - 12' Width (3 Piece Set)	62HDMW3	5.0
12' - 16' Width (4 Piece Set)	62HDMW4	7.2

SQUARE/RECTANGULAR INLET FILTER

Clear Opening Size	Style P/N	Maximum Bypass Flow Rate (CFS)
Small: Up to 64" Perimeter	62SHD	2.4
Medium: 65" - 96" Perimeter	62MHD	3.5
Large: 97" - 120" Perimeter	62LHD	5.0
Extra-Large: 121" or Greater Perimeter	62XLHD	7.2

SPECIFICATIONS BY NOMINAL SIZE RANGE (MIN. VALUES)

Nominal Bag Size	Solids Storage (Cuft)	Flow Rate (CFS)*			Oil Retention (Qt)**	
		FX/FPX	PCP	FXP	PCP	
Small	1.6	1.2	0.8	89	168	
Medium	2.1	1.7	1.2	89	204	
Large	3.8	2.7	1.8	89	262	
Extra Large	4.2	3.6	2.4	178	319	

TSS Removal Rate

82%

N/A

Large scale 3rd party testing per ASTM D 7353 using 7% concentration USDA Sandy Loam

TSS Removal Rate

NA

99%

Large Scale testing at 90 GPM using US Silica OK-10 sand at 1750 mg/L measuring TSS per 3M 25400.

TPH Removal Rate

NA

97%

Large Scale testing at 90 GPM with used motor oil at 243 mg/L measuring per EPA Method 1664A.

*Filter bag at 50% max solids storage capacity

**Filter bag at 50% oil capacity and oil skimmer pouch at 100% oil capacity

INSTALLATION INSTRUCTIONS:

1. REMOVE GRATE

2. CLEAN GRATE LEDGE

3. SET INLET FILTER ON LOAD BEARING LEDGE OF STRUCTURE

4. REPLACE GRATE

ALL PRODUCTS MANUFACTURED BY ADVANCED DRAINAGE SYSTEMS

WWW.ADSPIPE.COM

PH. 1-800-821-6710

ADS

For residential use only

SIZE DATE DOWNS

C 02/06/2023 ADS FLEXSTORM PURE

SCALE 1"=1'-0" 1/2"=1'-0"

NOTES:

1. ALL FRAMING IS CONSTRUCTED OF 304 STAINLESS STEEL.

2. TOTAL BYPASS CAPACITY WILL VARY WITH EACH SIZE DRAINAGE STRUCTURE. ADS DESIGNS FRAMING BYPASS TO MEET OR EXCEED THE DESIGN FLOW OF THE PARTICULAR DRAINAGE STRUCTURE.

3. UPON ORDERING, CONFIRMATION OF THE INLET SPECIFICATION, PRECAST/FOUNDRY CASTING MAKE AND MODEL, OR DETAILED DIMENSIONAL FORMS MUST BE PROVIDED TO CONFIGURE AND ASSEMBLE AN INLET FILTER.

4. ALL FILTERS MEET ASTM D8057 SPECIFICATIONS.

5. FOR WRITTEN SPECIFICATIONS AND MAINTENANCE GUIDELINES VISIT WWW.ADSPIPE.COM.

NOTES:

1 The sump is measured from the lowest invert.

A Granular backfill shall be placed to a minimum thickness of 300mm all around the maintenance hole.

B Precast concrete components shall be according to OPSD 701.030, 701.031, or 701.032.

C Structure exceeding 5.0m in depth shall include safety platform according to OPSD 404.020.

D Pipe support according to OPSD 708.020.

E For benching and pipe opening details, see OPSD 701.021.

F For adjustment unit and frame installation, see OPSD 704.010.

G All dimensions are nominal.

H All dimensions are in millimetres unless otherwise shown.

ONTARIO PROVINCIAL STANDARD DRAWING

Nov 2014 Rev 5

PRECAST CONCRETE MAINTENANCE HOLE

1200mm DIAMETER

OPSD 701.010

NOTES:

1 When sidewalk is continuously adjacent, the dropped curb at entrances shall be reduced to 75mm.

2 For slipforming procedure a 5% batter is acceptable.

A Treatment at entrances shall be according to OPSD 351.010.

B Outlet treatment shall be according to the OPSD 610 Series.

C The transition from one curb type to another shall be a minimum length of 3.0m, except in conjunction with guide rail where it shall be according to the OPSD 900 Series.

D All dimensions are in millimetres unless otherwise shown.

ONTARIO PROVINCIAL STANDARD DRAWING

Nov 2012 Rev 2

CONCRETE BARRIER CURB

OPSD 600.110

KEY PLAN:

SCALE= N.T.S

LEGEND:

EX. ELEVATION

PROP. ELEVATION

PROP. TOP OF CB/CBMH

PROP. STORM SEWER

PROP. STORM MANHOLE

PROP. CATCH BASIN MANHOLE

PROP. REAR YARD CB

PROP. STORM CATCHMENT AREA

EX. WATER SERVICE

EX. SANITARY SERVICE

EX STORM SEWER

EX SANITARY SEWER

PROP. SLOPE ELEVATION

EX. FIRE HYDRANT

EX. GAS LINE

EX. OVERHEAD LINE

PROPOSED PROPERTY LINE

5 - Year HWL - 180.550 m

100 - Year HWL - 180.704m

REVISIONS

No.	DATE	DESCRIPTION
01	MAR 28, 2024	ISSUED FOR SPA

STAMP:

LICENSED PROFESSIONAL ENGINEER

N. MRAD

100124424

24/03/2024

11/96

PROVINCE OF ONTARIO

ENGINEER:

N.A.J.M. ENGINEERING LTD.

9, CATER DRIVE, UNIT 4

LD. ACTLE, ON N9S 1J3 +1-519-312-1715

PROJECT:

4461 WYANDOTTE ST E

CLIENT:

SHEET TITLE:

DETAILS

ADDRESS: 4461 WYANDOTTE ST E, WINDSOR, ON

DRAWN BY: G.S. SCALE:

DESIGNED BY: G.S. DATE: MAR-28-2024

CHECKED BY: N.M. JOB #: 23196

APPROVED BY: N.M. DRAWING #: 04

PRODUCT INFORMATION: TEMPEST LOW, MEDIUM FLOW (LMF) ICD

Purpose

To control the amount of storm water runoff entering a sewer system by allowing a specified flow volume out of a catch basin or manhole at a specified head. This approach conserves pipe capacity so that catch basins downstream do not become uncontrollably surcharged, which can lead to basement floods, flash floods and combined sewer overflows.

Product Description

Our LMF ICD is designed to accommodate catch basins or manholes with sewer outlet pipes 6" in diameter and larger. Any storm sewer larger than 12" may require custom modification. However, IPK can custom build a TEMPEST device to accommodate virtually any storm sewer size.

Available in 1/4 preset flow curves, the LMF ICD has the ability to provide flow rates: 20ps - 175ps (33gpm - 270gpm)

Product Function

The LMF ICD varies flow action allows the LMF ICD to provide a narrower flow curve using a larger orifice than a conventional orifice plate ICD, making it less likely to clog. When comparing flows at the same head level, the LMF ICD has the ability to restrict more flow than a conventional ICD during a rain event, preserving greater sewer capacity.

Product Construction

Constructed from durable PVC, the LMF ICD is light weight 8.9 Kg (19.7 lbs).

Product Applications

Will accommodate both square and round applications:

Square Application

Round Application

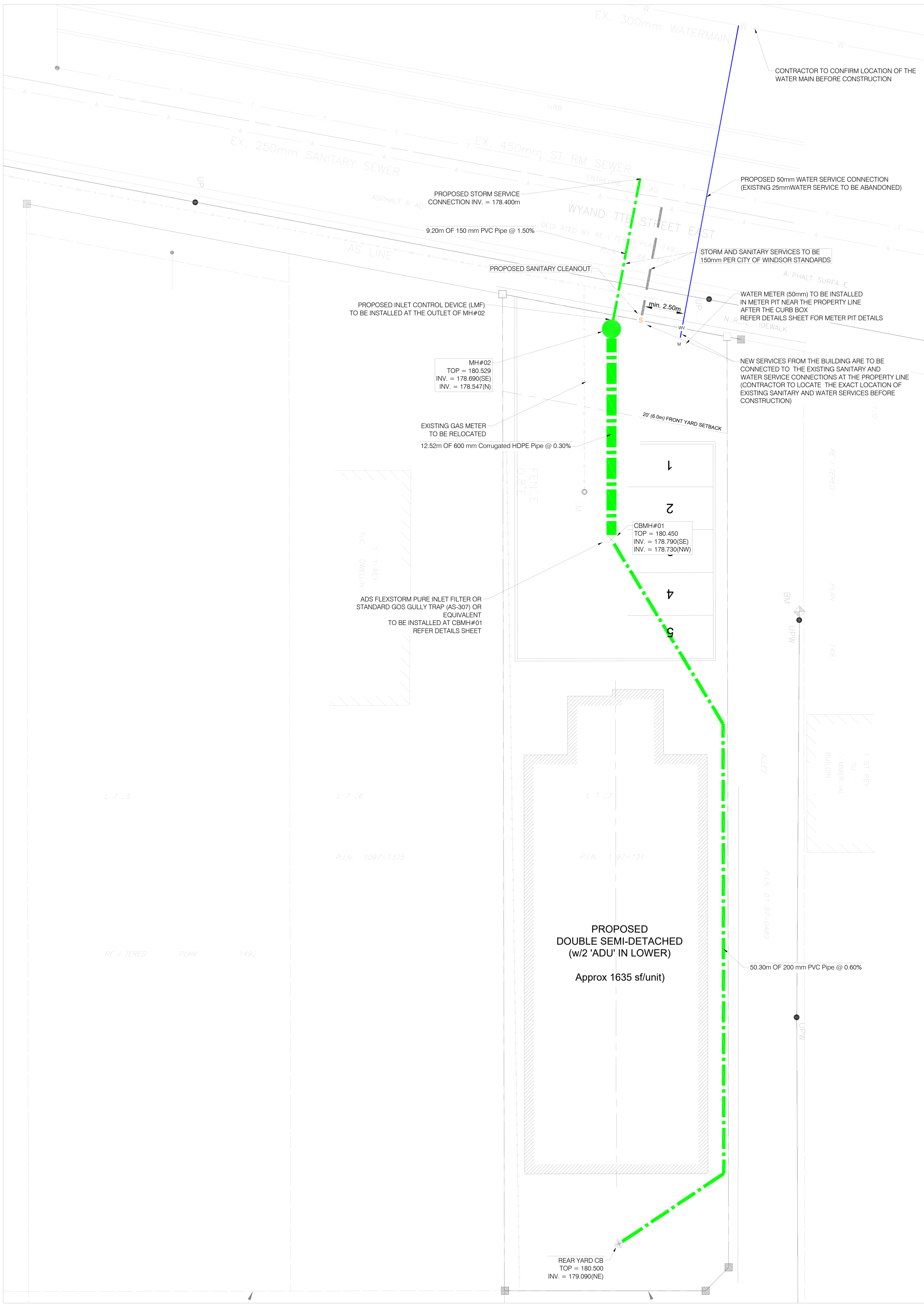
Universal Mounting Plate

Spigot CB Wall Plate

Universal Mounting Plate Hub Adapter

NOTE: Do not use or test the products in this manual with compressed air or other gases including air-over-water boosters

Chart 1: LMF 1/4 Preset Flow Curves



SERVICING NOTES:
1. IF THE EXISTING SERVICES ARE IN BAD CONDITION, NEW SERVICES ARE TO BE INSTALLED PER CITY OF WINDSOR STANDARDS.

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SCALE= N.T.S

LEGEND:

176.28	EX. ELEVATION
176.176	PROP. ELEVATION
176.176	PROP. TOP OF CB/CBMH
[Green dashed line]	PROP. STORM SEWER
[Green circle]	PROP. STORM MANHOLE
[Green 'X']	PROP. CATCH BASIN MANHOLE
[Green square]	PROP. REAR YARD CB
[Blue dashed line]	PROP. STORM CATCHMENT AREA
[Blue solid line]	PROP. WATER SERVICE
[Grey dashed line]	EX. SANITARY SERVICE
[Grey solid line]	EX STORM SEWER
[Grey solid line]	EX SANITARY SEWER
[Grey solid line]	EX WATERMAIN
4.8%	PROP. SLOPE ELEVATION
[Green circle]	EX. FIRE HYDRANT
[Grey solid line]	EX. GAS LINE
[Grey solid line]	EX. OVERHEAD LINE
[Red dashed line]	PROPOSED PROPERTY LINE
[Red solid line]	5 - Year HWL - 180.550 m
[Red solid line]	100 - Year HWL - 180.704m

R E V I S I O N S		
No.	DATE	DESCRIPTION
01	MAR 28, 2024	ISSUED FOR SPA

STAMP:



ENGINEER:
N.A.J.M. ENGINEERING LTD.
900 CATER DRIVE, UNIT 4
WINDSOR, ON N9A 6C3 TEL: 519.333-1715

PROJECT:
4461 WYANDOTTE ST E

CLIENT:

SHEET TITLE:	
SERVICING PLAN	
ADDRESS: 4461 WYANDOTTE ST E, WINDSOR, ON	
DRAWN BY: G.S.	SCALE: 1:150
DESIGNED BY: G.S.	DATE: MAR-28-2024
CHECKED BY: N.M.	JOB #: 23196
APPROVED BY: N.M.	DRAWING #: 02



Council Report: S 131/2024

Subject: Zoning By-Law Amendment Z001-24 (ZNG/7164) - Alta Nota Construction – 0 Windsor Ave & 1140 Goyeau St, Ward 3

Reference:

Date to Council: October 7, 2024
Author: Diana Radulescu
T. (519) 255-6543 x 6918
E. dradulescu@citywindsor.ca
Planning & Building Services
Report Date: September 23, 2024
Clerk's File #: Z/14850

To: Mayor and Members of City Council

Recommendation:

- I. THAT Zoning By-law 8600 **BE AMENDED** by changing the zoning of Lots 46 to 51, Plan 447 (identified as Lot 1A on Appendix A to Report S 131/2024 and known municipally as 0 Windsor Avenue) from Commercial District 1.5 (CD1.5) to Commercial District 1.7 (CD1.7).
- II. THAT Zoning By-law 8600 **BE FURTHER AMENDED** by changing the zoning of Lot 183 and Part of Lot 182, Plan 1303 (identified as Lot 1B on Appendix A to Report S 131/2024 and known municipally as 1140 Goyeau Street) from Commercial District 1.5 (CD1.5) to Residential District 3.3 (RD3.3).
- III. THAT Zoning By-law 8600 **BE FURTHER AMENDED** by deleting and replacing Section 20(1)52 as follows:

52. EAST SIDE OF GOYEAU STREET AND WEST SIDE OF WINDSOR AVENUE, NORTH OF GILES BOULEVARD EAST

- A. For the lands comprising of LOT 46 TO LOT 51, PLAN 447 for an existing parking area, the following additional provisions shall apply:
 - 1) Section 25.5.10.3 shall not apply.
 - 2) Section 25.5.20 shall not apply.
 - 3) Section 25.5.40.6 and 25.5.40.7 shall not apply.
 - 4) Notwithstanding Section 25.5.50, the minimum *parking aisle* width shall be as existing.
- B. For the lands comprising of LOT 183 & PT LOT 182, PLAN 1303, the following additional provisions shall apply:

- 1) Lot Area – minimum 1,728 m²
- 2) Lot Coverage – maximum 43.5%
- 3) Main Building height – maximum 24.0 m
- 4) Landscaped Open Space Yard – minimum 28%
- 5) Section 12.3.5.13 shall not apply
- 6) All required *parking spaces* (excluding accessible parking spaces and *loading spaces*) shall be located entirely on LOT 46 TO LOT 51, PLAN 447.
- 7) Notwithstanding Section 25.5.20.1.6, where a *building* is located on the same *lot* as the *parking area*, for that portion of a *building* wall not containing a *habitable room window* within 4.0 m of the *ground*, the minimum parking area separation from that portion of the *building* wall shall be 3.1 m.

(ZDM 7)

IV. THAT, when Site Plan Control is applicable:

- A. Prior to the submission of an application for site plan approval, at the discretion of the City Planner, Deputy City Planner, or Site Plan Approval Officer:
 - 1) Those documents submitted in support of the application for amendment to the Zoning By-law 8600 **BE UPDATED** to reflect the site plan for which approval is being sought, and any comments from municipal departments and external agencies included in Appendix E to Report S 131/2024.
- B. The Site Plan Approval Officer **BE DIRECTED** to incorporate the following, subject to any updated information, into an approved site plan and an executed and registered site plan agreement:
 - 1) Vibration Impact Assessment as requested by the City of Windsor Heritage Planner in Appendix E to Report S 131/2024.
 - 2) Requirements of the City of Windsor – Engineering Department (subject to the approval of the City Engineer), City of Windsor – Parks Development, in Appendix E of Report S 131/2024.
- C. The Site Plan Approval Officer **CONSIDER** all other comments contained in Appendix E of Report S 131/2024 and all recommendations in the documents submitted in support of the application for an amendment to Zoning By-law 8600.

Executive Summary:

N/A

Background:

Application Information

Municipal Address: 1140 Goyeau St and 0 Windsor Ave

Ward: 3 **Planning District:** South Central **Zoning District Map:** 7

Applicant/Agent: Lassaline Planning Consultants (Jackie Lassaline)

Owner: Alta Nota Construction, Mariusz Buchcic

Submitted Materials: Attached to Report S 131/2024 as an Appendix:

Appendix A - Conceptual Site Plan

Appendix B - Conceptual Elevations

Appendix C - Conceptual Floor Plans

Appendix D - Planning Rationale Report

Appendix E – Comments

Appendix F – Site Photos

Proposal:

The development consists of two parcels – Lot A fronting on Windsor Avenue and Lot B known as 1140 Goyeau Street.

The applicant is proposing to construct a multiple dwelling with a total of 54 units in a building with a maximum height of 22.9 metres containing 6 floors on the 1140 Goyeau Street parcel (Lot B). Parking is to be provided over Lot A (80 existing parking spaces) and Lot B (4 accessible parking spaces) for a total of 84 parking spaces. The proposal includes 4 bicycle parking spaces and one loading space on Lot B. An open and travelled north-south alley divides the parcels. Vehicular access is from Goyeau Avenue and the alley.

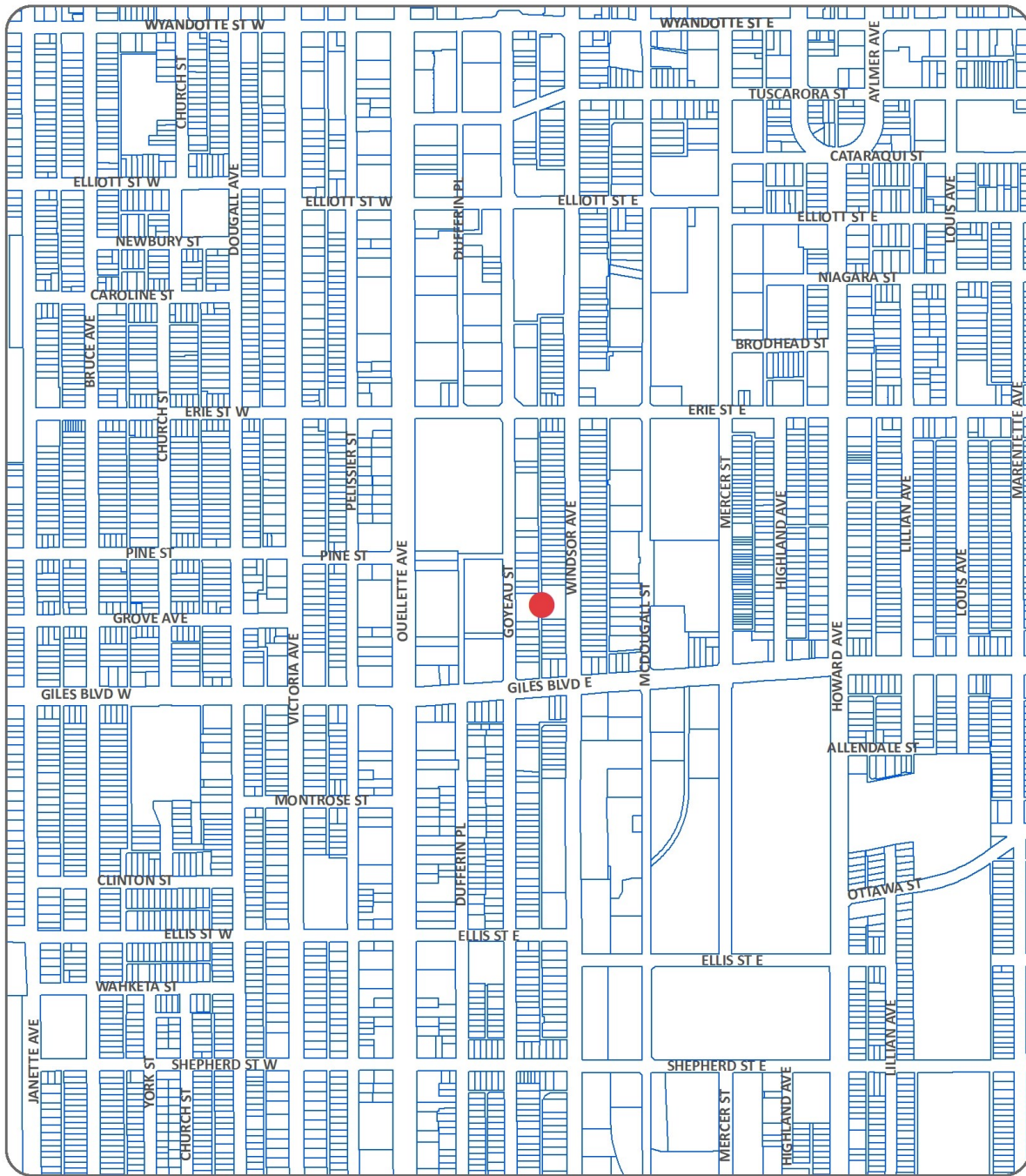
The applicant is requesting an amendment to Zoning By-law 8600 to rezone the subject properties to Residential District 3.3 (RD 3.3) to allow for the proposed residential use. A site-specific exception has also been requested for recognition of a reduced total landscaped area of 28% and a provision to connect the parking area with the residential building lot.

The development as proposed is subject to Site Plan Control.

Site Information

OFFICIAL PLAN	ZONING	CURRENT USE	PREVIOUS USE
Residential	Commercial District 1.5, S 20(1)52	Lot A & B: Parking Area	Lot A: Single Detached Dwelling, Parking Area Lot B: Club
LOT FRONTAGE	LOT DEPTH	LOT AREA	LOT SHAPE
Lot A: 55.1 m Lot B: 46.0 m	Lot A: 39.2 m Lot B: 36.4 m	Lot A: 2,159 m ² Lot B: 1,656 m ²	Lot A: rectangular Lot B: rectangular
<i>All measurements are provided by the applicant and are approximate.</i>			

Figure 1: Key Map



KEY MAP - Z-001/24, ZNG/7164



● SUBJECT LANDS

Figure 2: Neighbourhood Map



NEIGHBOURHOOD MAP - Z-001/24, ZNG/7164



SUBJECT LANDS

Neighbourhood:

Site images are provided in Appendix F. Section 2.2 in the Planning Rationale Report (see Appendix D) provide details including site photos.

The subject parcels are located within an established mixed-use neighbourhood with low to medium profile residential uses, commercial and institutional uses (a hospital, school and religious institutions) as well as public parks.

To the north are a mix of low and medium profile residential uses and commercial uses. To the east are low profile residential and institutional uses as well as a public park. To the south are several religious institutions, the Windsor Grove Cemetery and commercial uses. Further west is Ouellette Avenue which is recognized as a Mixed-Use Corridor as per *Schedule D – Land Use* and a Traditional Commercial Street as per *Schedule A-1 - Special Policy Areas* of the Windsor Official Plan.

The Canadian Pacific Railway is located approximately 500m to the west.

There are several schools nearby including James L. Dunn Public School 280 m and St. Angela Catholic Elementary School 960 m to the east. There are several parks nearby including Wigle Park 200 m and Alton C. Parker Park 580 m to the northeast and Mitchell Park 560 m to the west. The nearest library is Windsor Public Library's Central Branch 1.6 km to the north.

The neighbourhood includes many community services and centres. The subject parcels are located near a cluster of medical uses that are centred on the Ouellette Campus of the Windsor Regional Hospital.

Goyeau Street is classified as a Local Road per *Schedule F: Roads and Bikeways* and has two travel lanes with curbs and sidewalks on both sides with street parking on the east side. Windsor Avenue is classified as a Local Road and has two travel lanes with curbs and sidewalks on both sides of the road with alternate side on-street parking.

Active transportation infrastructure in the form of bikes lanes is available along McDougall Street to the east. The City of Windsor Active Transportation Master Plan identifies Windsor Avenue as a "High Priority" on the Proposed Bicycle Network.

Public transit is available via the Ottawa 4 route along Giles Boulevard East and the Central 3 route along Erie Street East. The closest existing bus stop to the subject property is located on Giles at the southeast corner of Goyeau Street (Ottawa 4 route). This bus stop is approximately 140 m from the subject property which falls within Transit Windsor's 400 m walking distance guidelines to a bus stop. This service will continue to be maintained with Transit Windsor's City Council-approved Transit Master Plan.

The site is serviced by a 200 mm vitrified clay combined sewer and a 525 mm concrete storm sewer located within the Goyeau Street right-of-way.

Figure 3: Subject Parcel – Rezoning



PART OF ZONING DISTRICT MAP 7

N.T.S.

REQUESTED ZONING AMENDMENT

Applicant: 2312205 Ontario Ltd.



SUBJECT LANDS

PLANNING & BUILDING DEPARTMENT



DATE : AUGUST, 2024
FILE NO. : Z-001/24, ZNG/7164

Discussion:

Planning Rationale Report (Lassaline Planning Consultants Inc. – July 28, 2024)

The Planning Rationale Report (PRR), attached as Appendix D to Report S 131/2024, notes that *“the new development is proposed within a mixed-use neighbourhood that is supported by municipal transit, by active transportation (paved sidewalks and separated bike lanes) and is within close walking distance to many commercial and institutional uses.”* It states that *“the proposed development is a residential intensification and an infill development on an underutilized site”* and that *“the residential use is compatible with the residential uses and the mixed-use profile of the neighbourhood.”*

The PRR further notes that *“the use of the vacant lot for the new multi-unit building and the use of the adjacent vacant lands for parking is a coordinated, sound planning approach to the infill development.”*

The PRR concludes that *“the [Zoning Bylaw Amendment] provides a good solution for the provision of needed alternative residential accommodation and compatible development within an existing neighbourhood, while supporting a diversity of housing tenures and styles within the municipality on major municipal route.”* The Planning Department generally concurs with the PRR.

Urban Design Brief (Section 5.0, Planning Rationale Report, Lassaline Planning Consultants – July 28, 2024)

The Urban Design Brief included in Section 5 of the PRR noted that *“the proposed development will encourage future development with a pedestrian orientation by increasing the density of the neighbourhood in a manner that is supported and sustainable.”* It concluded that *“the building will be compatible in size, scale, and massing with the existing neighbourhood.”* The Planning Department generally concurs with the Urban Design Brief.

Sewer Assessment Memo (Dillon Consulting Ltd. – November 28, 2023)

The Sewer Assessment Memo confirms that the existing combined sewer on Goyeau Street will effectively accommodate the site's sewer servicing needs. The memo demonstrates that the municipal combined sewer has adequate capacity, and no adverse impacts are expected on the surrounding areas as a result of the proposed development. The Sanitary Sewer Study has been deemed acceptable, and the proposed sanitary servicing strategy is supported by the Planning and Engineering Development departments.

Stormwater Management Report (Ambashi Engineering and Management Inc. – June 2024)

The Stormwater Management Report notes that the proposed Stormwater drainage system will address the Stormwater management requirements of the city of Windsor. A revised study will be required for Site Plan Control to address Engineering Department comments included in Appendix E.

Shadow Study (Z Square Consulting Inc. – October 10, 2023)

The Shadow Study notes that the proposed development will have minimal impact to surrounding residences with exception to loss of early morning sunlight to 1128 and 1124 Goyeau Street from Fall Equinox to Spring Equinox and through the winter months. No other property except the proposed parking area at 0 Windsor Avenue appears to be affected.

Tree Survey (Bezaire Partners – September 2023)

The Tree Survey noted that there are three trees in poor condition, one tree in fair condition and two trees that are in good and very good conditions. The Tree Survey recommended removing the three trees in poor and fair condition while retaining and trimming the trees in good and very good condition. A landscape plan will be a requirement of Site Plan Control.

Provincial Policy Statement (2020)

The PPS provides direction on matters of provincial interest related to land use planning and development and sets the policy foundation for regulating the development and use of land in Ontario.

The proposed development of a multiple dwelling with 54 units represents an efficient development and land use pattern that will have no adverse impact on the financial well-being of the City of Windsor, land consumption and servicing costs, accommodates an appropriate range of residential uses, and optimizes existing investments in transit and infrastructure. There are no anticipated environmental or public health and safety concerns with the proposed use of land.

The requested zoning amendment is consistent with the “*Healthy, liveable and safe communities*” policies in Section 1.1.1 of the PPS.

The lot is located within a settlement area. The zoning amendment promotes a land use – a multiple dwelling with 54 units - that makes efficient use of land and existing infrastructure. The proposed rezoning is a residential infill and intensification project, thereby not requiring settlement area expansion. Active transportation options and transit services are located near the subject lands and include planned improvements. The zoning amendment is consistent with the “Settlement Areas” policies in Sections 1.1.3.1 and 1.1.3.2.

The proposed development will provide a form of housing that is appropriate in terms of the range and mix of the surrounding neighbourhood context and is located where municipal infrastructure and public service facilities are already available. The subject amendment is consistent with the housing policies in Section 1.4.3 of the PPS.

The subject proposal is on lands within an area that is serviced by municipal sewage and water services. The recommended zoning by-law amendment is consistent with the Infrastructure and Public Service Facilities policies in Section 1.6.6.2 of the PPS.

The agent indicates that the objectives of the PPS have been considered and have informed their professional planning opinion and concept design for the project site. These objectives are discussed in the Planning Rationale Report (Appendix D). Planning & Development Services generally concurs with this and is of the opinion that the proposed amendment to Zoning Bylaw 8600 is consistent with the PPS 2020.

City of Windsor Official Plan

The subject property is located within the South Central Planning District in *Schedule A – Planning Districts & Policy Areas* of Volume I of the Official Plan.

The subject parcels have a Residential land use designation in *Schedule D – Land Use Plan*. Section 6.3.2.1 stipulates that low profile and medium profile dwelling units are permitted within the Residential land use designation. The proposed development of a multiple dwelling with 54 units is classified as a Medium Profile development under Section 6.2.1.2 (b) and is compatible and complementary with the surrounding land uses (Section 6.3.2.5 (c)). The development is of a scale that is compatible with the profile and uses of the surrounding neighbourhood. No deficiencies in municipal physical and emergency services have been identified (Section 6.3.2.5 (e)). The proposed rezoning conforms to the policies in Sections 6.3.2 of the Official Plan.

The Official Plan has policies that encourage redevelopment and intensification. Objective 6.3.1.1 supports a complementary range of housing forms and tenures in all neighbourhoods. Objective 6.3.1.2 seeks to promote compact neighbourhoods and balanced transportation systems. Objective 6.3.1.3 seeks to promote selective residential redevelopment, infill and intensification initiatives. The proposed medium profile multiple dwelling with 54 units represents a complementary and compact form of housing, redevelopment, and intensification that is located near active and public transportation. The proposed rezoning satisfies the objectives set out in Section 6.3.1 of the Official Plan.

The proposed amendment to Zoning By-law 8600 will conform to the general policy direction of the Official Plan. Planning and Development Services generally concurs with the Official Plan analysis in Section 5 of the PRR submitted by the Applicant.

City of Windsor Intensification Guidelines

The City of Windsor Intensification Guidelines provide direction for infill and intensification within existing neighbourhood patterns. The intent is to guide new development to become distinctive, while relating harmoniously to the use, scale, architecture, streetscapes, and neighbourhoods of Windsor, as well as meeting the needs of its citizens and visitors.

The subject parcel is located within an established neighbourhood area but not within a defined Mature Neighbourhood or a Mixed-Use Corridor as per the Official Plan. The PRR notes that “there are existing buildings in the neighbourhood of a similar size, scale, and massing to the proposed development.”

Planning and Development Services is of the opinion that the proposed rezoning amendment is consistent with the general direction of the Windsor Intensification Guidelines.

Zoning By-Law 8600

The subject lands are zoned “Commercial District 1.5 (CD1.5)” which only permits a Club. Lot B was the site of the previous Windsor Manor Club & Banquet Hall (which was demolished in 2008).

The Applicant is requesting to rezone both parcels to Residential District 3.3 (RD3.3) to allow for a multiple dwelling to be built on Lot B and for the parking lot to continue to operate on Lot

A. Planning & Development Services supports the requested rezoning amendment in principle, but recommends the following:

- Changing the zoning designation of Lot A from CD1.5 to CD1.7 which includes a parking area as a permitted use. This zoning designation is most appropriate since Lot A will continue to be used as a parking area;
- Changing the zoning designation of Lot B from CD1.5 to RD3.3 to allow for a multiple dwelling as a permitted use; and
- A revised site-specific provision (Section 20(1)52) as outlined below.

Notwithstanding the site-specific provisions noted below, the subject lands have been considered as a single development lot or area in this report.

Parking: A total of 67 parking spaces are required per Zoning Bylaw 8600. 80 parking spaces are proposed for Lot A, 4 accessible spaces and 1 loading space are proposed for Lot B as shown on the Conceptual Site Plan (Appendix A). Site-specific provision B.6) notes that all required parking spaces (excluding accessible parking spaces and loading spaces) shall be located on the 0 Windsor Avenue parcel and will serve the parking requirements of the 1140 Goyeau Street parcel.

The proposed development proposes a new access from Windsor Avenue. Recommendation III removes the previous prohibition of an access from Windsor Avenue. The proposal includes reconfiguring the existing access from the alleyway to be moved further south in response to comments from the Transportation Planning department.

Site-specific provisions A.1-4) address the proposed parking area configuration of Lot A that deviate from requirements in Zoning Bylaw 8600 for: providing a curb separation between parking areas and landscaped open spaces, parking aisle widths and parking area separations. This is primarily to address the new proposed access area off Windsor Avenue and adjusted access area from the alleyway. The remainder of the parking area is as existing.

Site-specific provision B.7) addresses a gap in the application of minimum parking separations with respect to habitable windows within section 25.5.20.1 in Zoning Bylaw 8600.

Landscaped Open Space Area:

Site-specific provision B.4) recommends a minimum landscaped open space yard of 28% for Lot B. Currently, Lot B exists as a parking area with minimal landscaping. The proposed development, while changing the use to a multiple dwelling, will include designated landscaped areas as noted on the Conceptual Site Plan (Appendix A).

Density:

Site specific provision B.5) provides relief from the maximum dwelling unit density in section 12.3.5.13 of Zoning Bylaw 8600. The proposed development of a multiple dwelling with 54 units on Lot B creates a density that is above the maximum stipulated for the specific RD 3.3 zone but is compatible and complementary to existing multiple dwellings and directly adjacent higher order uses within the neighbourhood context. As discussed previously in this report, the proposed development conforms to the policy direction of the relevant medium-profile residential sections of the Official Plan.

Site Plan Control

All plans, drawings and elevations are conceptual and subject to change. The development as proposed is subject to site plan control. Site Plan Control will be the primary planning tool to implement the direction of the PPS and the Official Plan, the provisions of Zoning By-law 8600, and the requirements and recommendations of municipal departments and external agencies. Recommendation IV provides direction and guidance for the Site Plan Control process.

Risk Analysis:

N/A

Climate Change Risks

Climate Change Mitigation:

The subject lands are located within an existing neighbourhood on existing municipal services, therefore reducing the impacts of climate change by locating within the existing built-up area. In general, residential intensification minimizes the impact on community greenhouse gas emissions as these developments create complete communities and neighbourhoods while using available infrastructure such as sewers, sidewalks, and public transit.

Climate Change Adaptation:

The proposed development of a medium profile multiple dwelling provides an opportunity to increase resiliency for the development and surrounding area through supporting a complementary and compact form of housing, redevelopment, and intensification that is near existing and future transit and active transportation options.

Financial Matters:

N/A

Consultations:

An in-person Open House was held on October 26, 2023. Notification of this meeting was distributed to residents and property owners within 120m of the subject site. There were no attendees. Details of the Open House are summarized in Section 3.2 of the PRR (Appendix D).

Comments received from municipal departments and external agencies are attached as Appendix E. Statutory notice was advertised in the Windsor Star. A courtesy notice was mailed to property owners within 200m of the subject lands. Submitted documents were posted on the City of Windsor website.

Conclusion:

The *Planning Act* requires that a decision of Council in respect of the exercise of any authority that affects a planning matter, “*shall be consistent with*” Provincial Policy Statement 2020. Based on the supporting documents submitted by the Applicant and the analysis in this report, it is my opinion that the requested amendment to Zoning By-law 8600 is consistent with the PPS 2020 and is in conformity with the City of Windsor Official Plan.

The proposed amendment permits a medium profile multiple dwelling with 54 units which is compatible with existing uses in the surrounding neighbourhood. The proposed development represents an incremental increase in density and provides an opportunity for residential intensification, while also supporting a complementary form of housing located near various transportation options and institutional uses.

The recommendation to amend Zoning By-law 8600 constitutes good planning. Staff recommend approval.

Planning Act Matters:

Diana Radulescu

Planner II – Development Review

I concur with the above comments and opinion of the Registered Professional Planner.

Greg Atkinson, MCIP, RPP

Neil Robertson, MCIP, RPP

Deputy City Planner - Development

City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP JM

Approvals:

Name	Title
Diana Radulescu	Planning & Building Services
Greg Atkinson	Deputy City Planner - Development
Neil Robertson	City Planner
Aaron Farough	Senior Legal Counsel
Jelena Payne	Commissioner of Economic Development
Joe Mancina	Chief Administrative Officer

Notifications:

Name	Address	Email
Alta Nota Construction Attn: Mariusz Buchcic	888 Old Tecumseh Road, Belle River, N0R 1A0	altanota@live.com
Lassaline Planning Consultants Attn: Jackie Lassaline	P.O. Box 52, 1632 County Road 31, St. Joachim, ON N0R 1S0	jackie@lassalineplan.ca
Councillor Renaldo Agostino (Ward 3)		
Property owners and tenants within 200 m of the subject lands		

Appendices:

- 1 Appendix A - Conceptual Site Plan
- 2 Appendix B - Conceptual Elevations
- 3 Appendix C - Conceptual Floor Plans
- 4 Appendix D - Planning Rationale Report
- 5 Appendix E - Comments
- 6 Appendix F - Site Photos

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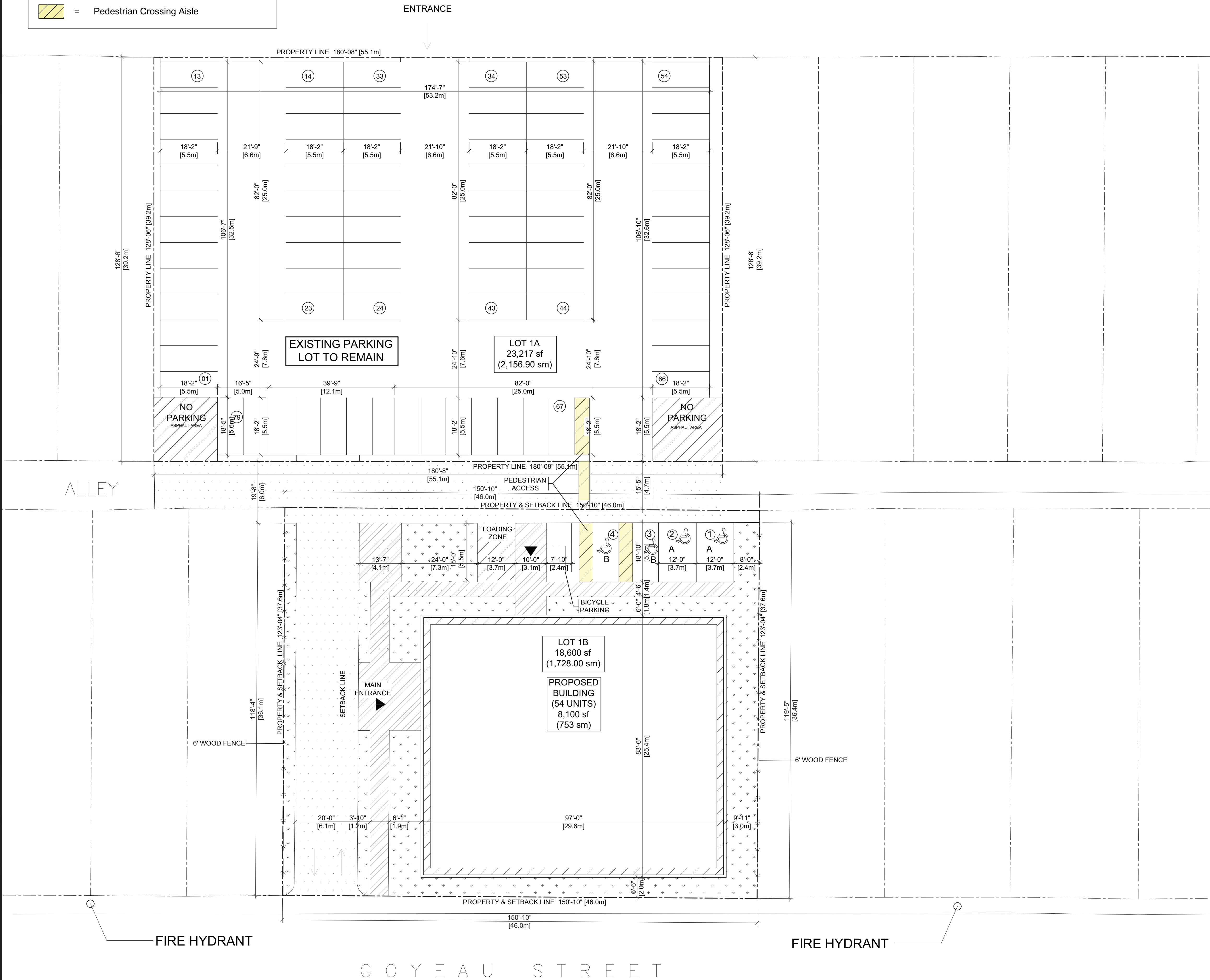
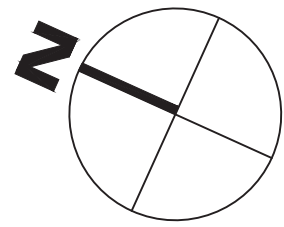
New Asphalt Pavement

Existing Asphalt Pavement

Pedestrian Concrete Pavement

Landscaping

Pedestrian Crossing Aisle



1 FIRST FLOOR PLAN
SP-1R1 3/16" = 1'-0"

Item	Site Data Matrix Lot (A)			OBC Reference
1	Project Description: 1140 GOYEAU ST. & 0 WINDSOR AVE. ON Canada		New Addition Alteration Change of Use	Part 3 Part 9 Part 11
2	Zoning Proposed :	Residential District RD 3.3		
3	Existing Land Use:	Parking Lot		
4	Major Occupancy(s):	Parking Lot		
5	Building Classification:	N/A		
6	Site Area	Building Area	Gross Area	
	Existing: 2,156.9sq.m	Existing: 0.0 sq.m	Existing:	0.0 sq.m
	Proposed: 0.0 sq.m	Proposed: 0.0 sq.m	Proposed:	0.0 sq.m
	Total: 2,156.9sq.m	Total: 0.0sq.m	Total:	0.0 sq.m
7	Lot Coverage	Minimum Lot Width	Building Height	
	Maximum: 35%	Required: 45m	Maximum:	N/A
	Provided: N/A	Provided: 55.1m	Provided:	N/A
	Minimum Front Yard Depth	Minimum Rear yard Depth	Minimum Side Yard Depth	
8	Required: N/A	Required: N/A	Required:	N/A
	Provided: N/A	Provided: N/A	Provided:	N/A
	Parking	Bicycle Spaces	Loading Spaces	
	Use Classification: 1.25 /per unit	Existing: 0	Existing:	0
9	Existing: 79	Proposed: 0	Proposed:	0
	Proposed: 0	Required: 0	Required:	0
	Required: 67	Total: 0	Total:	0
	Total: 79			
10	Landscaped Area	Curbing Length	Screening Fence Length	
	Existing: 188sq.m	Existing: EXISTING	Existing:	EXISTING
	Proposed: 0.0	Proposed: EXISTING	Proposed:	EXISTING
	Total: 188sq.f	Total: EXISTING	Total:	EXISTING

Item	Site Data Matrix Lot (B)		OBC Reference
1	Project Description: 1140 GOYEAU ST. & 0 WINDSOR AVE ON Canada		X Part 3 Part 9 Part 11
2	Zoning Proposed :	Residential District RD 3.3	
3	Existing Land Use:	Vacant	
4	Major Occupancy(s):	Residential	
5	Building Classification:	Group (C) 3.2.2. (48) up to 6 Storeys, Sprinklered, Noncombustible	
6	Site Area	Building Area	Gross Area
	Existing: 1,728.0 sq.m	Existing: 0.0 sq.m	Existing: 0.0 sq.m
	Proposed: 0.0sq.m	Proposed: 753 sq.m	Proposed: 4,518 sq.m
	Total: 1,728.0sq.m	Total: 753sq.m	Total: 4,518 sq.m
7	Lot Coverage	Minimum Lot Width	Building Height
	Maximum: 35%	Required: 45m	Maximum: 6 storeys
	Provided: 43.5%	Provided: 46m	Provided: 22.9 (6 storeys)
8	Minimum Front Yard Depth	Minimum Rear yard Depth	Minimum Side Yard Depth
	Required: N/A	Required: N/A	Required: N/A
	Provided: 6.1m	Provided: 6.1m	Provided: 5.0m
9	Parking	Bicycle Spaces	Loading Spaces
	Use Classification: 1.25 /per unit	Existing: 0	Existing: 0
	Existing: 0	Proposed: 4	Proposed: 1
	Proposed: 4	Required: 4	Required: 1
	Required: 67	Total: 4	Total: 1
Total: 4			
10	Landscaped Area	Curbing Length	Screening Fence Length
	Existing: 0.0sq.m	Existing: 0.0 m	Existing: 0.0m
	Proposed: 489 sq.m (28%)	Proposed: 57 m	Proposed: 58m
	Total: 489 sq.m (28%)	Total: 57m	Total: 58m

Item	Site Data Matrix Lot (A&B)		OBC Reference
1	Project Description: 1140 GOYEAU ST. & 0 WINDSOR AVE. ON Canada		X Part 3 Part 9 Part 11
2	Zoning Proposed :	Residential District RD 3.3	
3	Existing Land Use:	Vacant & Parking Lot	
4	Major Occupancy(s):	Residential	
5	Building Classification:	Group (C) 3.2.2. (48) up to 6 Storeys, Sprinklered, Noncombustible	
6	Site Area	Building Area	Gross Area
	Existing: 3,884.9 sq.m	Existing: 0.0 sq.m	Existing: 0.0 sq.m
	Proposed: 0.0sq.m	Proposed: 753 sq.m	Proposed: 4,518 sq.m
	Total: 3,884.9sq.m	Total: 753 sq.m	Total: 4,518 sq.m
7	Lot Coverage	Minimum Lot Width	Building Height
	Maximum: 35%	Required: 45m	Maximum: 6 storeys
	Provided: 19.3%	Provided: 46m	Provided: 22.9 (6 storeys)
8	Minimum Front Yard Depth	Minimum Rear yard Depth	Minimum Side Yard Depth
	Required: N/A	Required: N/A	Required: N/A
	Provided: 6.1m	Provided: 6.1m	Provided: 5.0m
9	Parking	Bicycle Spaces	Loading Spaces
	Use Classification: 1.25 /per unit	Existing: 0	Existing: 0
	Existing: 0.0	Proposed: 4	Proposed: 1
	Proposed: 83	Required: 4	Required: 1
	Required: 67	Total: 4	Total: 1
	Total: 83		
10	Landscaped Area	Curbing Length	Screening Fence Length
	Existing: 0.0sq.m	Existing: 0.0 m	Existing: EXISTING
	Proposed: 677 sq.m	Proposed: 57 m	Proposed: EXISTING
	Total: 677 sq.m	Total: 57 m	Total: EXISTING

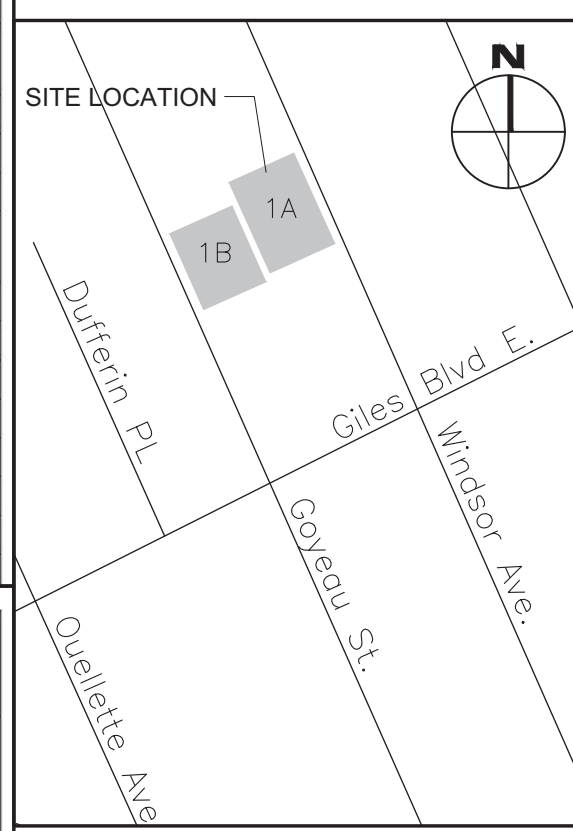
Should archaeological deposits be found during construction activities, all work in the area must stop immediately and the City's Manager of Cultural Affairs and the Ontario Ministry of Tourism, Culture and Sport must be notified, and clearance given by the Ministry.

In the event that human remains are encountered during construction, all work in that area must be stopped immediately and the site secured. The local police or coroner must be contacted to determine whether or not the skeletal remains are human, and whether the remains constitute a part of a crime scene. The Ontario Ministry of Tourism, Culture and Sport and the Registrar of the Cemeteries Regulation Unit of the Ministry of Consumer Services must then be notified, and clearance be given by the Ministry of Tourism.

NuFusion

& ASSOCIATES

1179 LAUZON RD. WINDSOR, ONTARIO, N8S 3M9
E mail: ap.nufusion@gmail.com. Cell: 519-890-9614



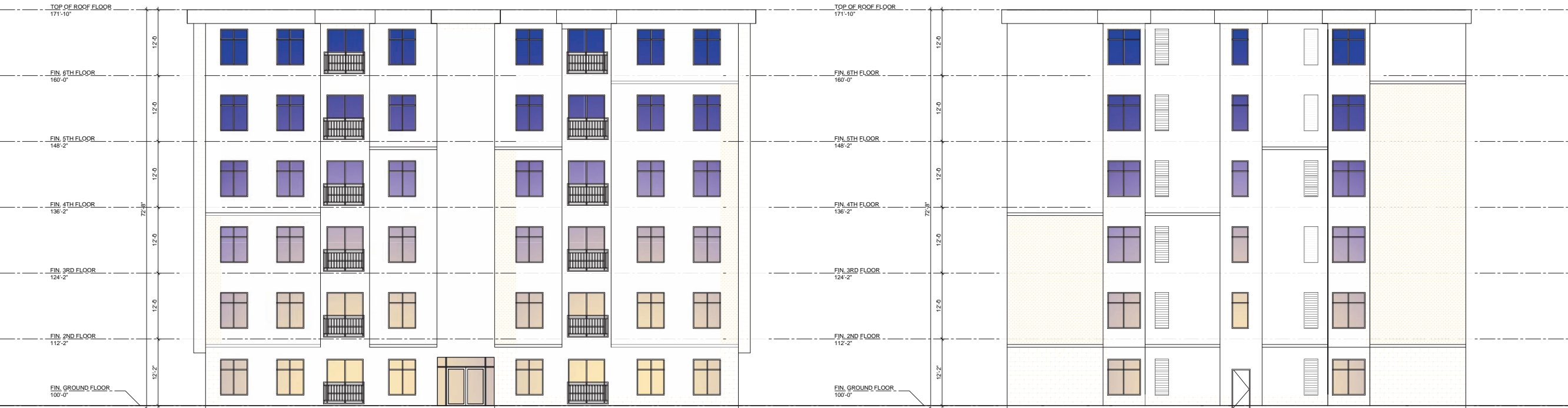
KEY PLAN		
N.T.S.		
10 MAY '24		PRE-APPLICATION
20 MAY '23		PRE-APPLICATION
16 JUN. '22		PRE-APPLICATION
16 NOV. '21		PRE-APPLICATION
29 MAY. '21		CLIENT REVIEW
DATE	NO.	ISSUED FOR

PROJECT	
NEW RESIDENTIAL BUILDING COMPLEX	
1140 GOYEAU ST. WINDSOR, ONTARIO N9A 1J1	
DWG. TITLE PROPOSED PROPERTY SITE PLAN	
DATE	: MAY 2021
SCALE	: AS NOTED
DESIGNED BY	: JF
DRAWN BY	: JF
CHECKED BY	: AP
APPROVED BY	:
PROJECT NO.	: 20-147
DWG. NO.	:

S-1R1

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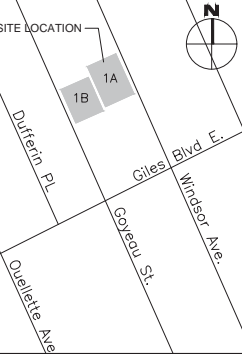
2 REAR AND SIDE ELEVATIONS
A-3 NTS



1 FRONT AND SIDE ELEVATIONS
A-3 NTS



850 Ouellette Ave. WINDSOR, ONTARIO, N9A 4M9
E mail: ap.nufusion@gmail.com. Cell: 519-890-9614



KEY PLAN
N.T.S.

DATE	NO.	ISSUED FOR
20 MAY '23		PRE-APPLICATION
29 MAY '22		PRE-APPLICATION
16 NOV. '21		PRE-APPLICATION
17 OCT. '21		CLIENT REVIEW

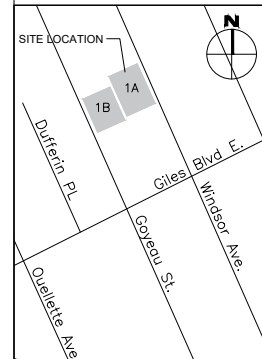
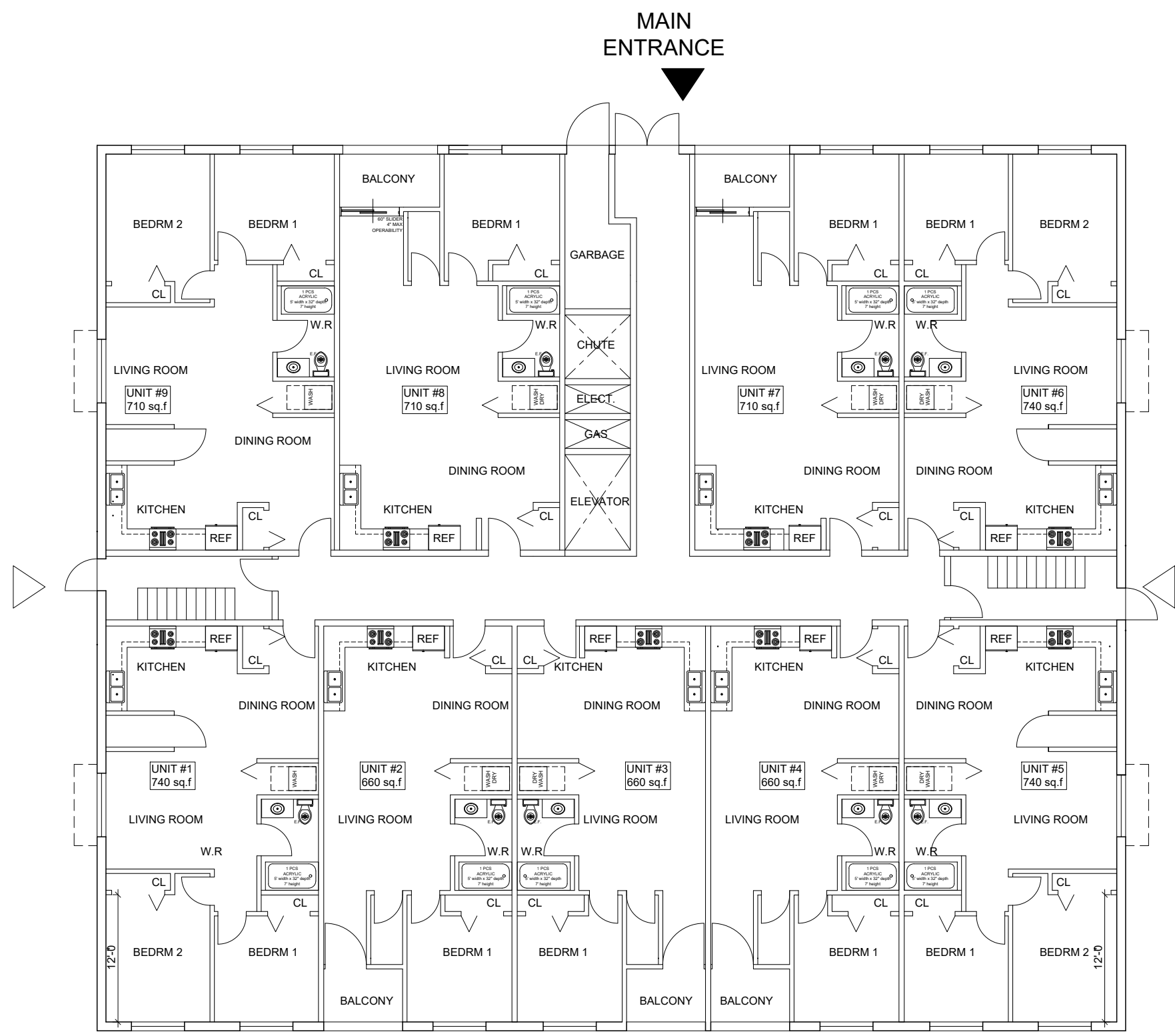
PROJECT
NEW RESIDENTIAL
BUILDING COMPLEX

1140 GOYEAU ST.
WINDSOR, ONTARIO N9A 1J1

DWG. TITLE
PRELIMINARY ELEVATIONS
47 UNITS TOTAL

DATE	: OCT 2021
SCALE	: AS NOTED
DESIGNED BY	: JF
DRAWN BY	: JF
CHECKED BY	: AP
APPROVED BY	:
PROJECT NO.	: 20-147
DWG. NO.	:

A-3



KEY PLAN
N.T.S.

DATE	NO.	ISSUED FOR
20 MAY '23		PRE-APPLICATION
01 JUN. '22		PRE-APPLICATION
16 NOV. '21		PRE-APPLICATION
15 JUNE. '21		CLIENT REVIEW

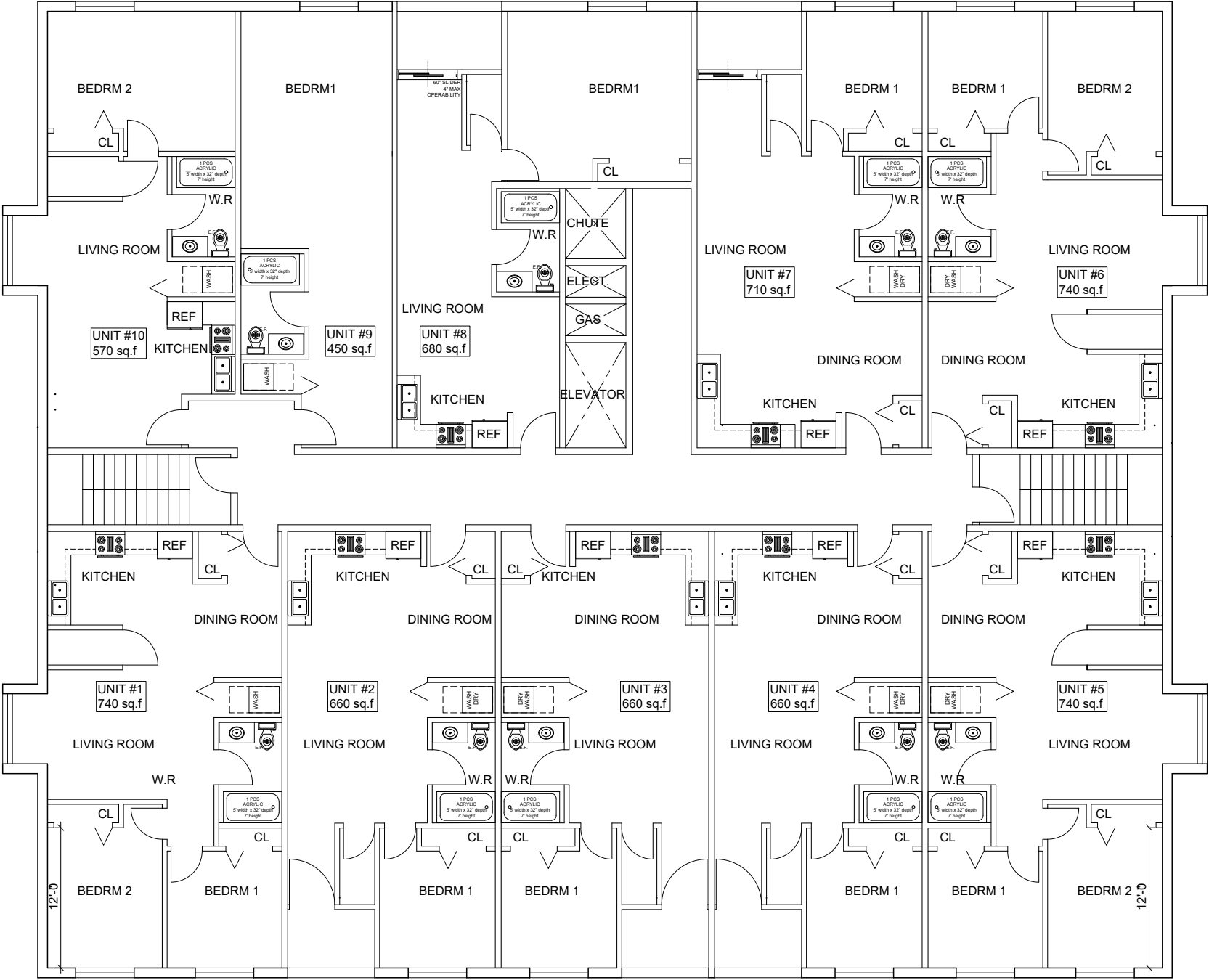
PROJECT
**NEW RESIDENTIAL
BUILDING COMPLEX**

1140 GOYEAU ST./1140 WINDSOR AVE.
WINDSOR, ONTARIO N9A 1J1

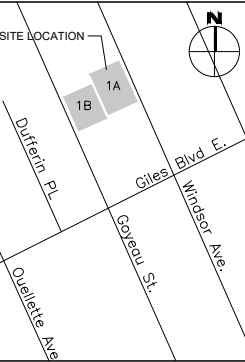
DWG. TITLE
PRELIMINARY FLOOR PLANS
FIRST FLOOR

DATE	: MAY 2021
SCALE	: AS NOTED
DESIGNED BY	: JF
DRAWN BY	: JF
CHECKED BY	: AP
APPROVED BY	:
PROJECT NO.	: 20-147
DWG. NO.	:

A-1



850 Ouellette Ave. WINDSOR, ONTARIO, N9A 4M9
E mail: ap.nufusion@gmail.com Cell: 519-890-9614



KEY PLAN
N.T.S.

DATE	NO.	ISSUED FOR
20 MAY '23		PRE-APPLICATION
01 JUN. '22		PRE-APPLICATION
16 NOV. '21		PRE-APPLICATION
15 JUNE. '21		CLIENT REVIEW

PROJECT
NEW RESIDENTIAL
BUILDING COMPLEX

1140 GOYEAU ST./1140 WINDSOR AVE.
WINDSOR, ONTARIO N9A 1J1

DWG. TITLE
PRELIMINARY FLOOR PLANS
2nd-6th FLOORS PLAN

DATE	: MAY 2021
SCALE	: AS NOTED
DESIGNED BY	: JF
DRAWN BY	: JF
CHECKED BY	: AP
APPROVED BY	:
PROJECT NO.	: 20-147
DWG. NO.	:

A-2



REPORT: **PLANNING RATIONALE REPORT (PRR)**

MUNICIPALITY: **CITY OF WINDSOR**

MUNICIPAL ADDRESS: **1140 GOYEAU and 0 WINDSOR AVE**

DEVELOPMENT: **ZBA**

DATE: **JULY 28, 2024 (REV.)**

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APPENDIX A - 'RESIDENTIAL DISTRICT 3.3 (RD3.3)'

APPENDIX B - 'SHADOW STUDY'

1.0 INTRODUCTION

Lassaline Planning Consultants (LPC) has been retained to undertake a Planning Rationale Report (PRR) regarding the feasibility of a site-specific Zoning By-law Amendment (ZBA) to support the development of a medium profile, 6 storey (22.9 m height) residential building. The redevelopment consists of a 6 storey building containing 54 residential units and 80 parking spaces. The development sites consist of a conjoined parcel comprising **Lot B** (1140 Goyeau Street) and **Lot A** (0 Windsor Ave) with the building to be located on **Lot B**.

The subject property is presently designated 'Residential' on Schedule D of the in the Official Plan for the City of Windsor and is zoned 'Commercial District (CD1.5)' and S. 20(1) in the Comprehensive Zoning Bylaw (CZB) 8600 for the City of Windsor.

A site-specific ZBA has been requested to rezone the lands from 'Commercial District (CD1.5)' to a site specific 'Residential District (RD3.3 #)' to allow for a proposed residential building. The proposed building is considered a medium profile height at 23 m and with 54 residential dwelling units in the 6 storey multi-unit building. The proposal includes 80 car parking spaces and 4 bicycle parking spaces.

A pre-consultation was held with the City of Windsor, Planning Department with a response letter under file PS 032-22 to Anthony Pipilo, Nufusion. Lassaline Planning Consultants has prepared this planning rationale report to support, explain and justify the Zoning Bylaw Amendment (ZBA) application.

1.1 PURPOSE OF THE REPORT

With the request to develop the site for a residential rental multi-unit building in a mixed use neighbourhood, a Zoning Bylaw Amendment (ZBA) is required to recognize the proposed land use and the site characteristics.

The proposed residential medium density development, in my professional opinion and as discussed and examined in this PRR, conforms with relevant policies of the Official Plan for the City of Windsor. Policies in place supports the proposal in the 'Residential' designation and it is my professional opinion an Official Plan Amendment is not required.

This planning rationale report will demonstrate the consistency of the proposal with the Provincial Policy Statement (PPS) 2020 and how the development is consistent with provincial housing policies, provides for healthy community initiatives, provides for healthy, walkable community policies of the province. Examining the proposed new building within the policy framework of the City of Windsor's Official Plan. Analysing the site and development characteristics in context of the Comprehensive Zoning Bylaw (CZB) 8600 for the city and determining compliance of the ZBA with the CZB regulatory framework.

The subject lands are zoned 'Commercial District (CD1.5)' and Section 20(1)52 in the CZB 8600 for the City of Windsor. A residential multi-unit building is not a permitted use in the present Bylaw 8600.

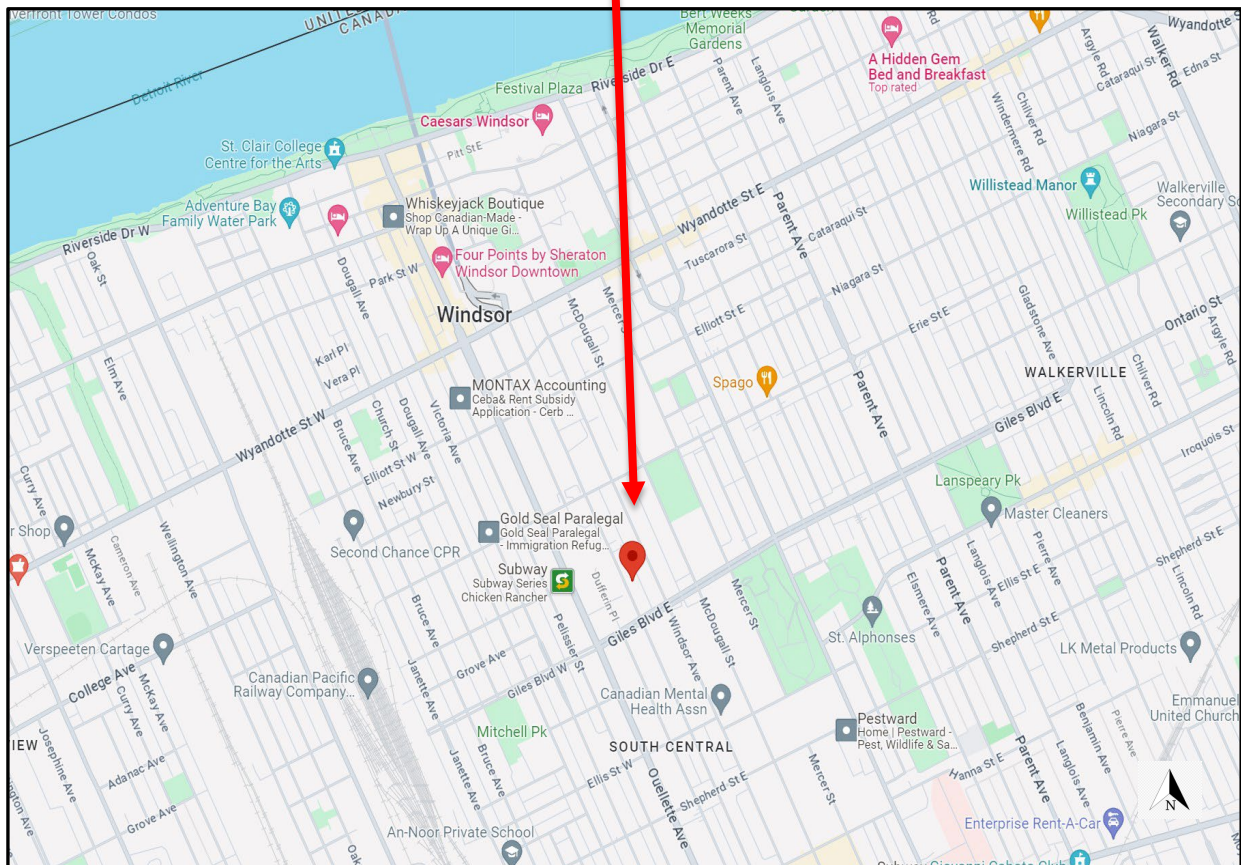
A site-specific Zoning By-law Amendment (ZBA) is requested with the application of a 'Residential District (RD3.3 #)' regulatory framework and a site specific provision to address landscaped open space and a provision to recognize the residential building on **Lot B** and the majority of the parking on **Lot A**. The ZBA will establish a consistent regulatory framework for the site to support the proposed development of a 6 storey (23 m height) building with 54 residential rental apartment units, 80 residential parking spaces, and 4 bike parking spaces.

This report provides the rationale and support for the requested site specific Zoning Bylaw Amendment (ZBA) that will rezone the lands from 'Commercial District 1.5 (CD 1.5)' to a site specific 'Residential District (RD3.3 #)' regulatory framework.

2.0 SITE LAND USES

The applicant proposes to develop the subject property municipally known as 1140 Goyeau Street with a multi-unit residential building consisting of 54 residential units in a 6 storey, medium profile building in a mix used neighbourhood.

FIGURE 1 – LOCATIONAL MAP: 1140 GOYEAU ST and O WINDSOR AVE



The site is in the South Central neighbourhood of Windsor in Ward 3. The site is not within a Secondary Plan or Special Policy Area. The subject site is not in the defined City Centre but is located on the periphery of the City Center.

As noted on **FIGURE 2 – SCHEDULE 'A' PLANNING DISTRICTS & POLICY AREA** the subject property is designated 'Residential' on Schedule D of the Official Plan for the City.

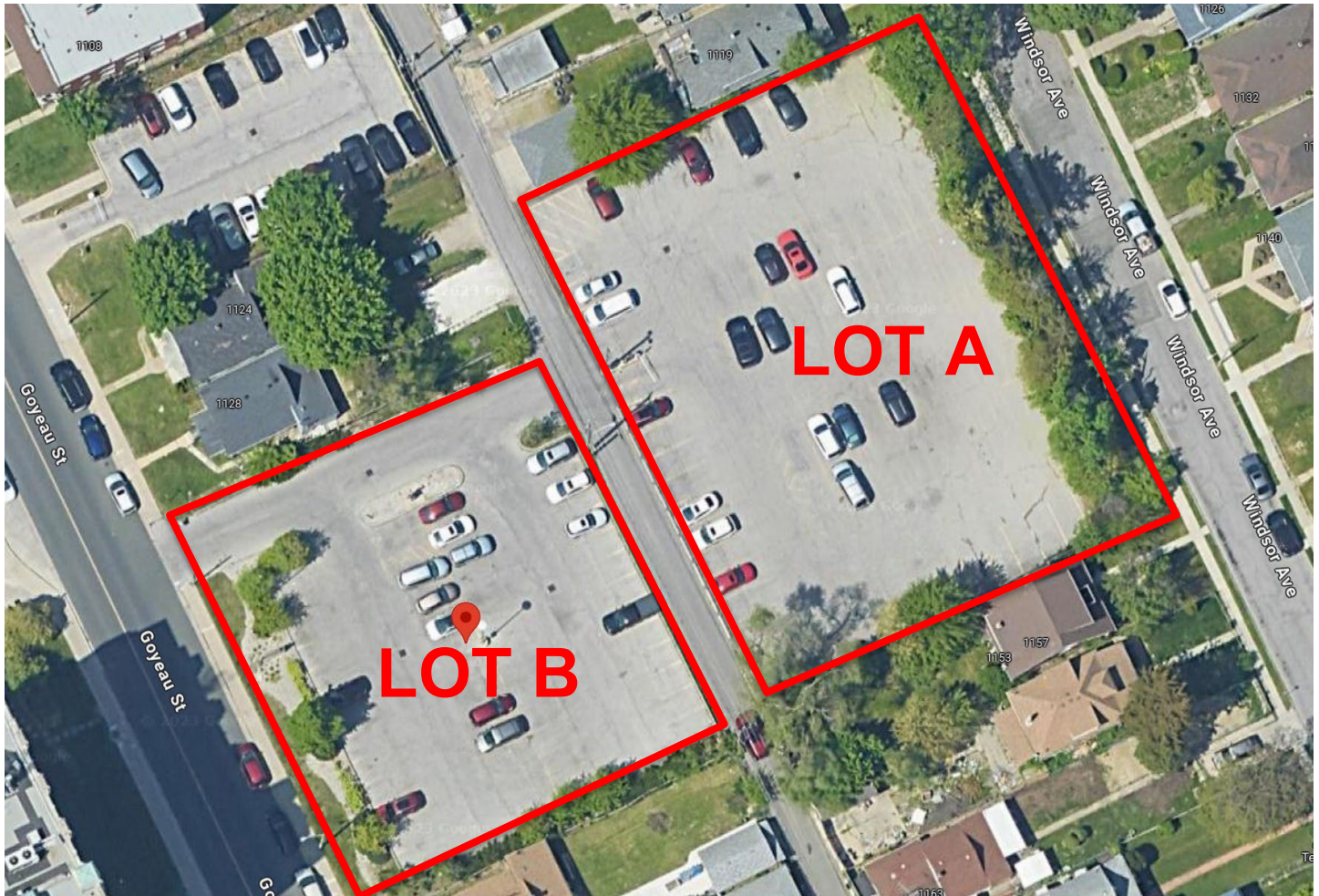
A ZBA to recognize the site for a medium profile, residential multi-unit building within the mixed use neighbourhood with a 'Residential District (RD3.3 #)' is proposed to establish a regulatory framework that is consistent with the neighbourhood and the City of Windsor Official Plan.

2.1 SIZE AND SITE DIMENSION

Lot A (0 Windsor Ave) – has a lot frontage of 55 m along the alleyway and a lot area of 2,159 m². Lot A is to remain vacant and will be used for 80 parking spaces.

Lot B (1140 Goyeau Street) – has a lot frontage of 46 m on Goyeau and a lot area of 1,656 m².

FIGURE 4 – EXISTING VACANT LOTS



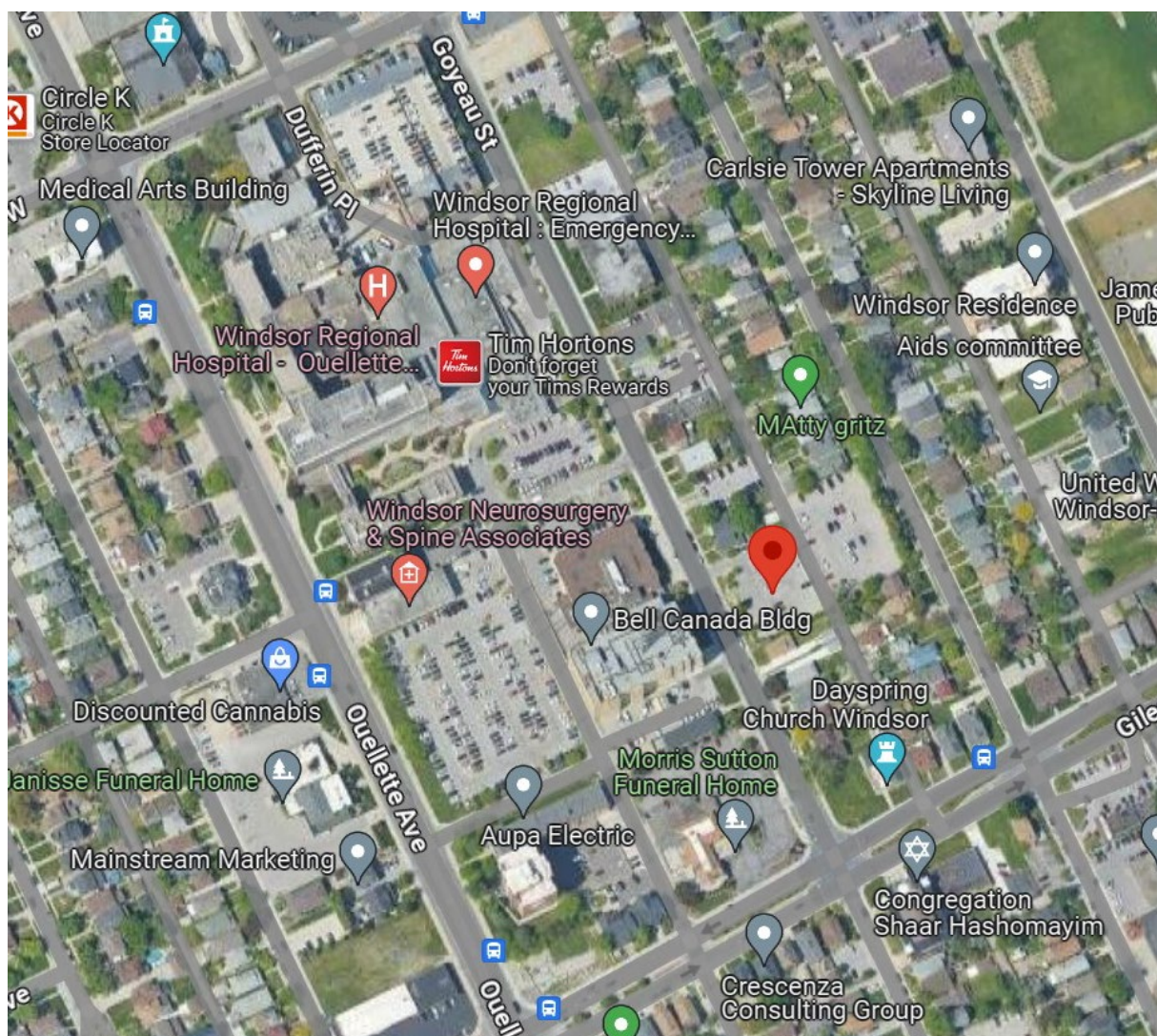
2.2 SURROUNDING LAND USES

The neighbourhood is a mix of uses including residential, commercial and major instructional with Windsor Regional Hospital, Ouellette campus across the street from the subject lands. The proposed

Ouellette Street is a major arterial road 1 block to the west of the subject lands. Ouellette Street is a major bus route for the City,

The new development is proposed within a mixed-use neighbourhood that is supported by municipal transit, by active transportation (paved sidewalks and separated bike lanes) and is within close walking distance to many commercial and institutional uses.

FIGURE 5 – NEIGHBOURHOOD USES



- a) North –** residential homes, community services (Windsor Residence Aids Committee),
- b) East –** United Way offices, residential homes.
- c) South –** Dayspring Church, Morris Sutton Funeral Home, Jewish Temple.
- d) West –** Bell Canada Building, Windsor Neurosurgery, Windsor Regional Hospital, Ouellette campus.

FIGURE 6 – NEIGHBOURHOOD AERIAL



3.0 DEVELOPMENT PROPOSAL

The applicant is proposing to develop the subject lands with a 6 storey, medium profile, residential multi-unit building containing 54 rental apartment units. The proposed development is a residential intensification and an infill development on an underutilized site. The use of the vacant lot for the new multi-unit building and the use of the adjacent vacant lands for parking is a coordinated, sound planning approach to the infill development.

The site is located in the immediate periphery to the CBD of the city with all amenities within close proximity. The site is located in close proximity to the main transit line for the city. The residential use is compatible with the residential uses and the mixed use profile of the neighbourhood. The residential building is well suited for the mixed use neighbourhood and will provide needed rental accommodation for staff associated with the hospital and other community agencies and office uses within proximity. The multi-unit building will provide for older neighbours in the adjacent residences to 'age in place'.

The proposed residential rental apartment units will provide an alternative housing form and tenure from the single detached houses more commonly available in Windsor. These units will address the historically low vacancy rate of 1.8% for rental units in the City by providing additional renting housing stock.

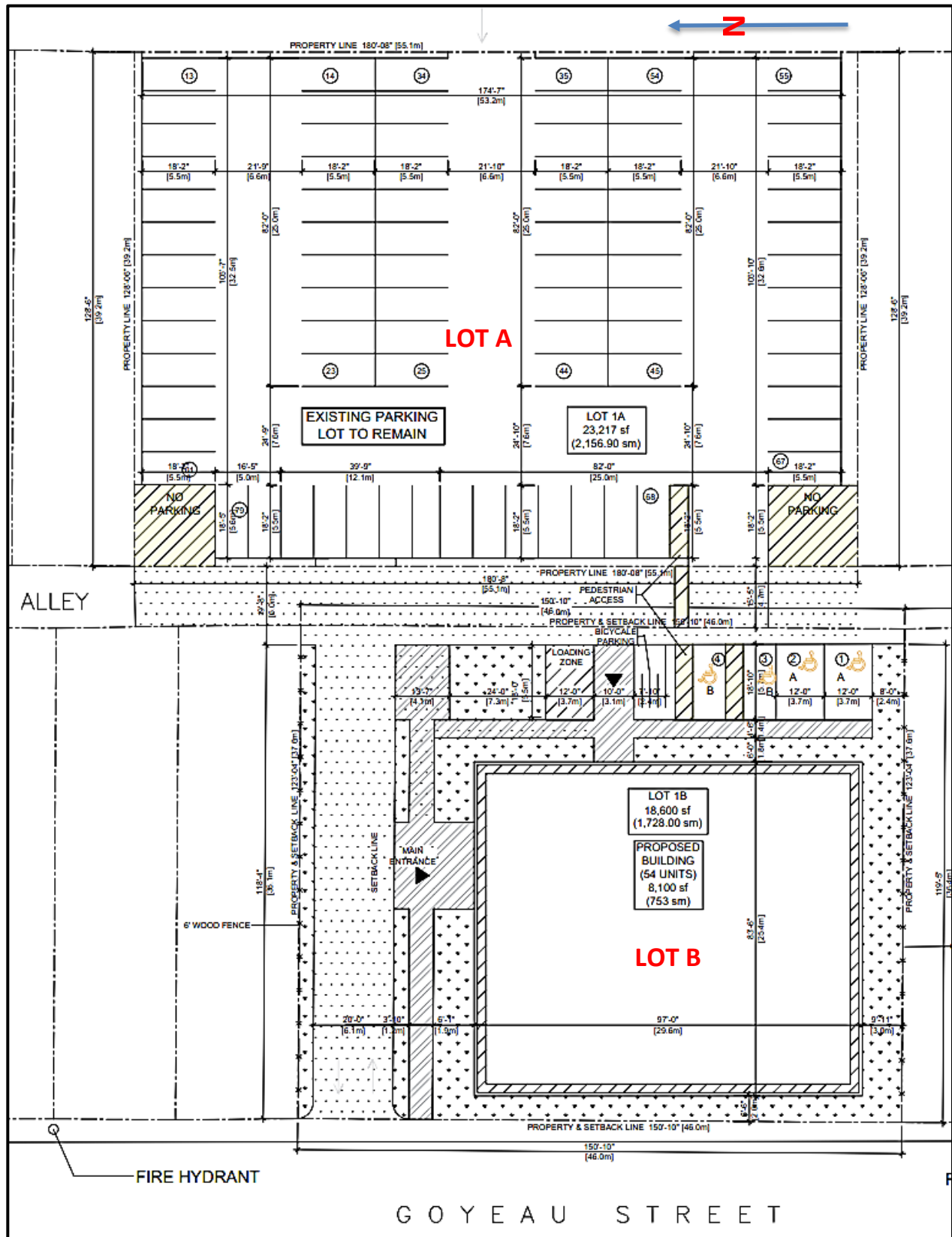
There are like and similar buildings with multiple storeys and other medium profile residential buildings within the immediate neighbourhood making the proposed building comparable and compatible with the neighbourhood.

The applicant is **requesting a ZBA** that will change the zoning from 'Commercial District 1.5 (CD1.5)' to '**Residential District (RD3.3 #)**' to support the construction of the proposed medium profile, 6 storey building with 54 residential units in a multi-unit building. One site specific provision is being requested to recognize landscaped open space at 28%. A second site specific provision has been requested to conjoin 1140 Goyeau Street (Lot B) and 0 Windsor Avenue (Lot A) with the building on Lot B and the parking lot on Lot A.

There will be 80 total parking spaces; 4 bike parking spaces, and 4 barrier free proposed to be located on site to be associated with the multi-unit building and the residential units.

3.1 SITE PLAN

FIGURE 7 – SITE PLAN LOTS A AND B



3.2 OPEN HOUSE:

240 Notices were mailed out to neighbours of the site. An Open House was held October 26, 2023 from 5:30-7:30. No one attended.

FIGURE 8 – NEIGHBOURHOOD USES

OPEN HOUSE

OCTOBER 26, 2023 | 5:30 P.M. – 7:30 P.M.

850 OUELLETTE AVE., WINDSOR



We are excited to introduce the community to our new development at 1140 Goyeau Street. This project is conveniently centrally located near local amenities and features a multi- residential building with a total of 59 units and 80 parking spaces.

We invite you to come and meet us and attend the Public Open House to learn more about the development. Our project team will be there to welcome guests and discuss comments and questions.

**FOR MORE INFORMATION
REGARDING THE OPEN HOUSE**

PHONE:
519-818-1633

EMAIL:
altanota@live.com
OR
Jackie Lassaline:
jackie@lassalineplan.ca



3.3 SEWER CAPACITY

Sewer Capacity Study was completed by Dillon Consulting in Windsor ON with the following conclusions:

“Dillon was retained by Valdez Engineering to complete a sewer capacity assessment for the combined sewers on Goyeau Street for the proposed multi-storied residential development on 1140 Goyeau Street. The City has confirmed through correspondence with the client that stormwater flows from the proposed development will outlet to the existing storm sewer along Goyeau Street, flowing south towards Giles Boulevard. The existing combined sewer on Goyeau Street, flowing north towards Erie Street East, will be the outlet for only sanitary flows from the proposed development.

The proposed development at 1140 Goyeau Street is proposed to consist of a total of 59 residential units and estimated to result in a peak sanitary flow rate of approximately 4.79 L/s from the drainage area containing the development area. The additional flow can be accommodated within the existing sanitary sewer along Goyeau Street without a significant increase in risk of basement flooding due to sanitary sewer surcharging.

The existing conditions combined sewer HGL along Goyeau St is at a minimum depth of 0.12 m below the existing ground surface for the 25 year boundary conditions and 0.51 m above the existing ground surface for the 100 year boundary conditions. There is no increase in HGL in the combined sewers along Goyeau St within the study area during the 25 year and 100 year event simulations due to the relatively minor flows contributed by the proposed development.

Accordingly, we are of the opinion that the increased sanitary sewer flows from the proposed development can be accommodated with negligible impacts to the existing sanitary sewer system and risk of basement flooding. The sanitary flows from the site are recommended to be monitored so as to not exceed design flows during wet-weather events.”

3.4 STORM WATER MANAGEMENT REPORT

Ambashi Engineering, 1080 Tapscott Road, Unit 24, Scarborough ON M1X 1E7 prepared the attached SWM Report and Plan.

“7.0 CONCLUSION

Concerning the development of the property (1140 Goyeau Street) within the Town of Windsor, the proposed Stormwater drainage system will address the Stormwater management requirements of the city of Windsor in that:

- The proposed storm sewer system is adequate for the peak discharge rate from the site directly discharging to the Walker Road storm sewer.
- Maximum site allowable stormwater release rate: 25.18 L/s (based on a 0.078 Ha site).
- For a one-in-five-year storm, the maximum runoff rate increases from 18.10 L/s (pre-development) to 25.18 L/s (post-development), a 39.2% increase.
- For a one-in-100-year storm, the maximum runoff rate increases from 29.01 L/s (pre-development) to 40.37 L/s (post-development), a 39.1% increase.
- A maximum volume of 6.63 cubic meters is required during the governing 1:5-year event.
- Employing an Oil-Grit Separator will provide the removal of 91.3% Total Suspended Solids (TSS) and >90% Volume Treatment for the site which will discharge to the Calderwood Avenue storm sewer.
- The total capacity of ponding at the parking lot is 41.75 cum, which is significantly more than the required storage of 6.63 cum for a 1:5 Year Storm Event as well as a Required 17.51 cum for a 1:100 Year Storm Event. The proposed Ponding has adequate capacity for the required storage during the governing 1:5-year as well as 1:100-year storm events.
- The report also covers erosion and sediment control measures to be taken by sections B and C of Guidelines on Erosion and Sedimentation Control for Urban Construction Sites, published by the Government of Ontario.”

3.5 RENDERINGS

FIGURE 9 – RENDERING LOOKING NORTH EAST

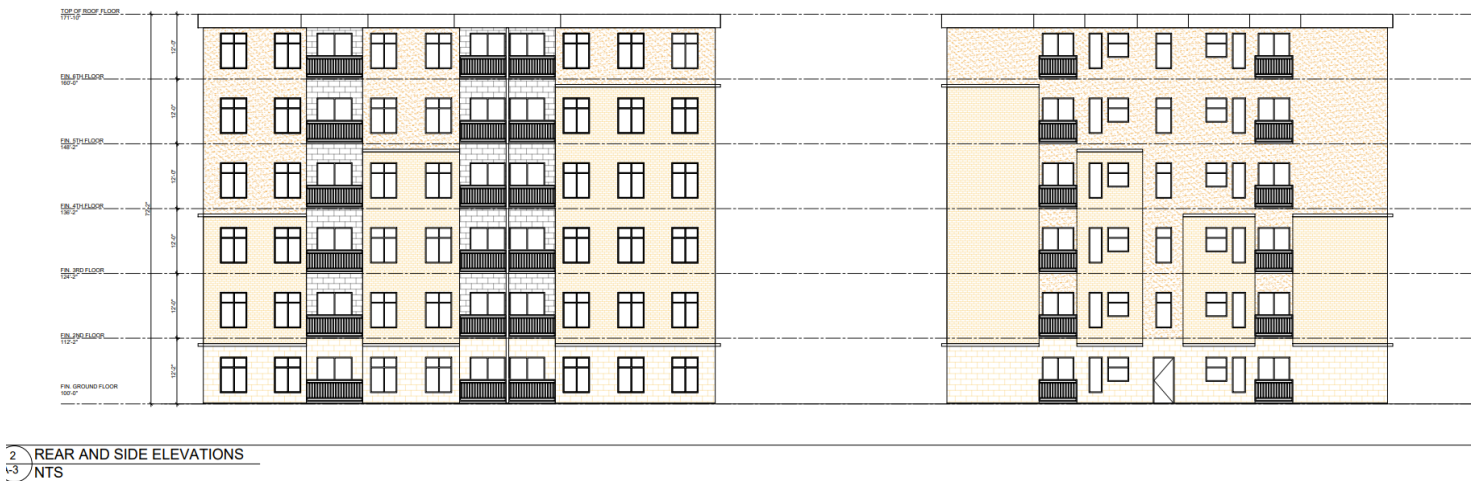


FIGURE 10 – RENDERING LOOKING NORTH



3.6 ELEVATIONS

FIGURE 11 – ELEVATIONS



3.7 FLOOR PLANS

FIGURE 12 – MAIN FLOOR PLANS

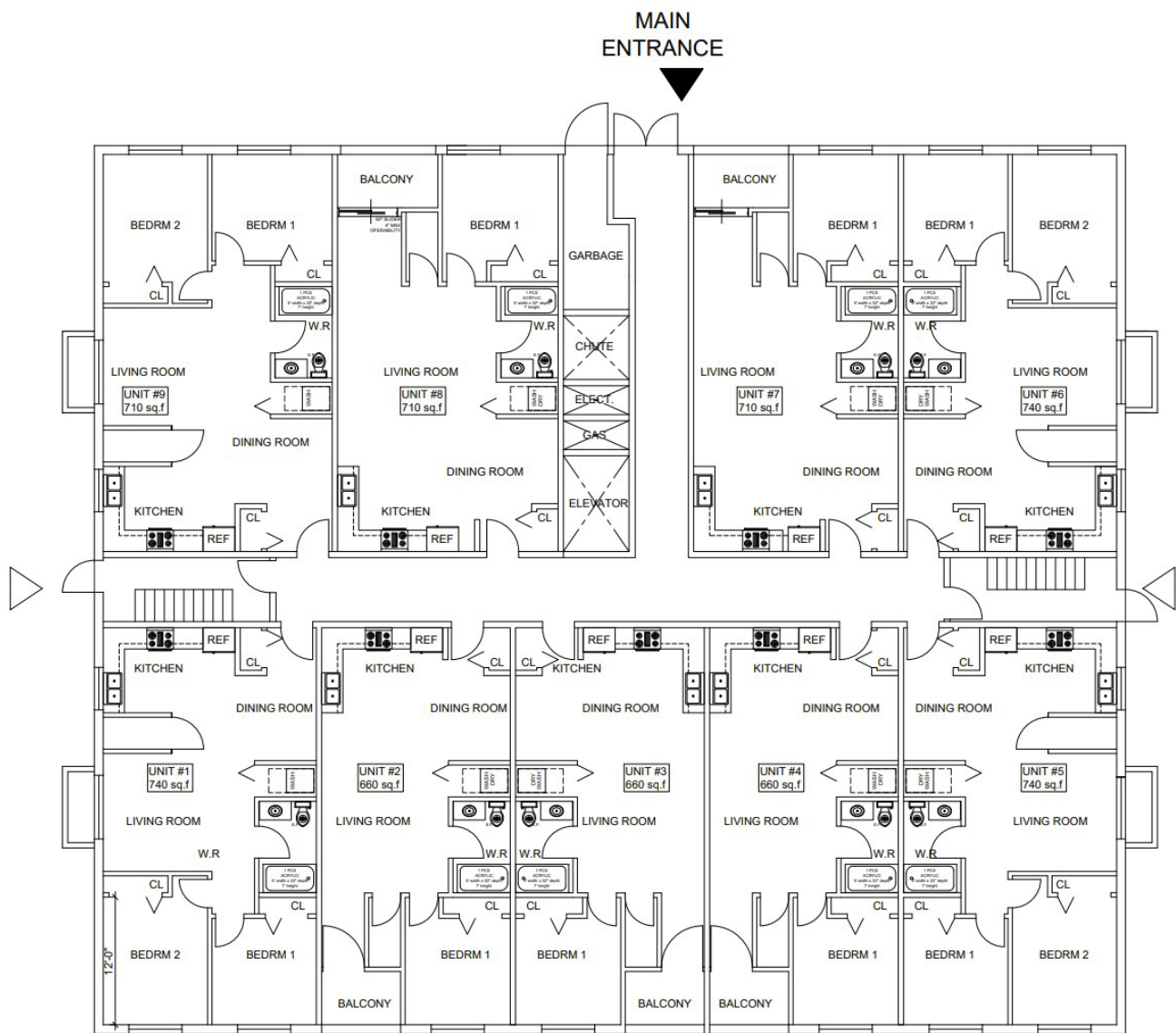
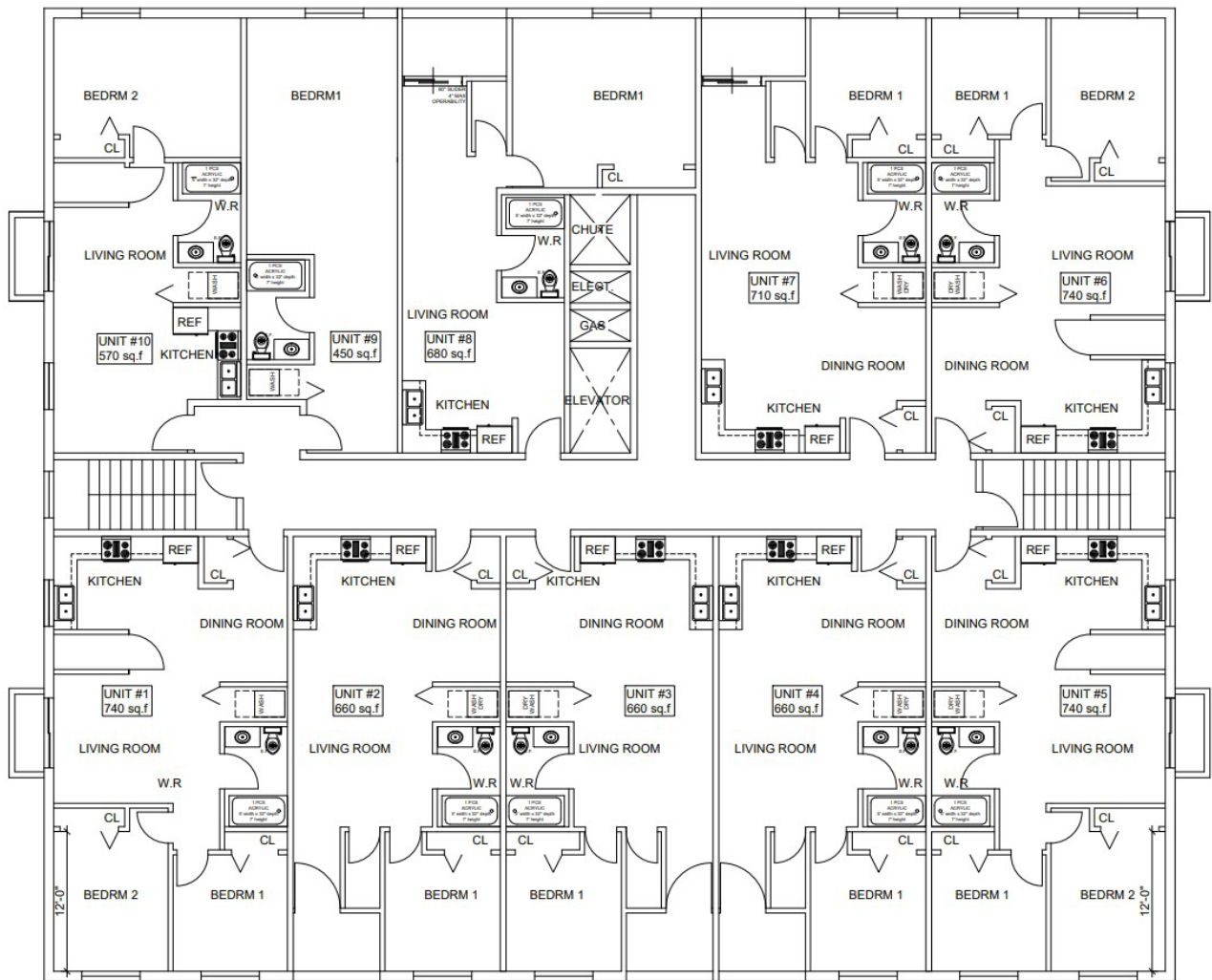


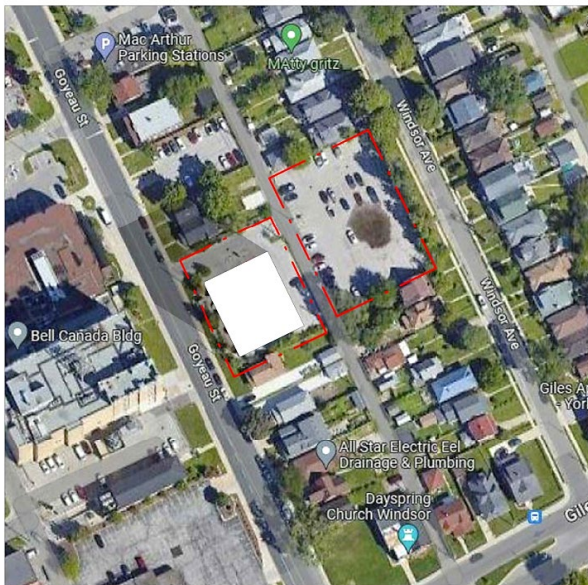
FIGURE 13 – UPPER FLOOR PLANS



3.8 SHADOW STUDY

The shadow study shows the 5 storey residential building and the shadow cast at the four seasons: March 21 (Spring Equinox); October 21 (Autumnal Equinox); June 21 (Summer Solstice); and December 21 (Winter Solstice).

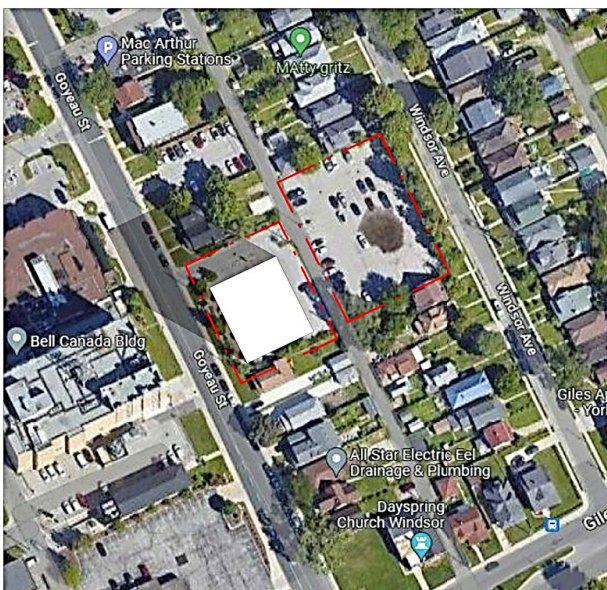
FIGURE 14 – SHADOW STUDY SAMPLES



1 SEPTEMBER 21 09 18 AM
1 : 1000

Spring Equinox :

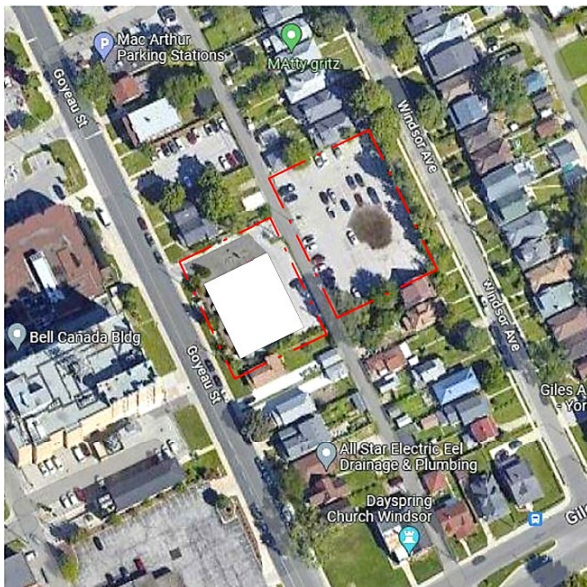
The morning starts with a minor shade on the residence to the north at 9:00 am however this quickly moves and can be considered, in my professional opinion, as negligible as the shadow dissipates by noon..



1 MARCH 21 09 18 AM
1 : 1000

Fall Equinox:

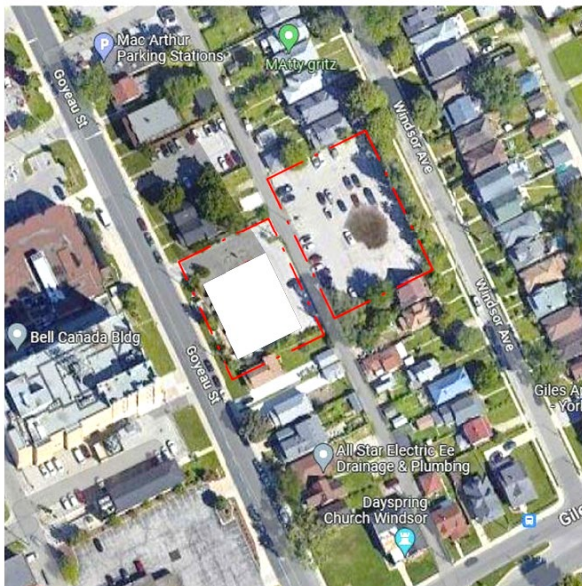
The morning starts with a minor shade on the residence to the north at 9:00 am however this quickly moves and can be considered, in my professional opinion, as negligible as the shadow dissipates by noon.



3 JUNE 21 11 18 AM
1 : 1000

Summer Solistic:

During the summer period, the sun is directly overhead of the building and does not result in a shadow on the neighbourhood of the 6 storey building.



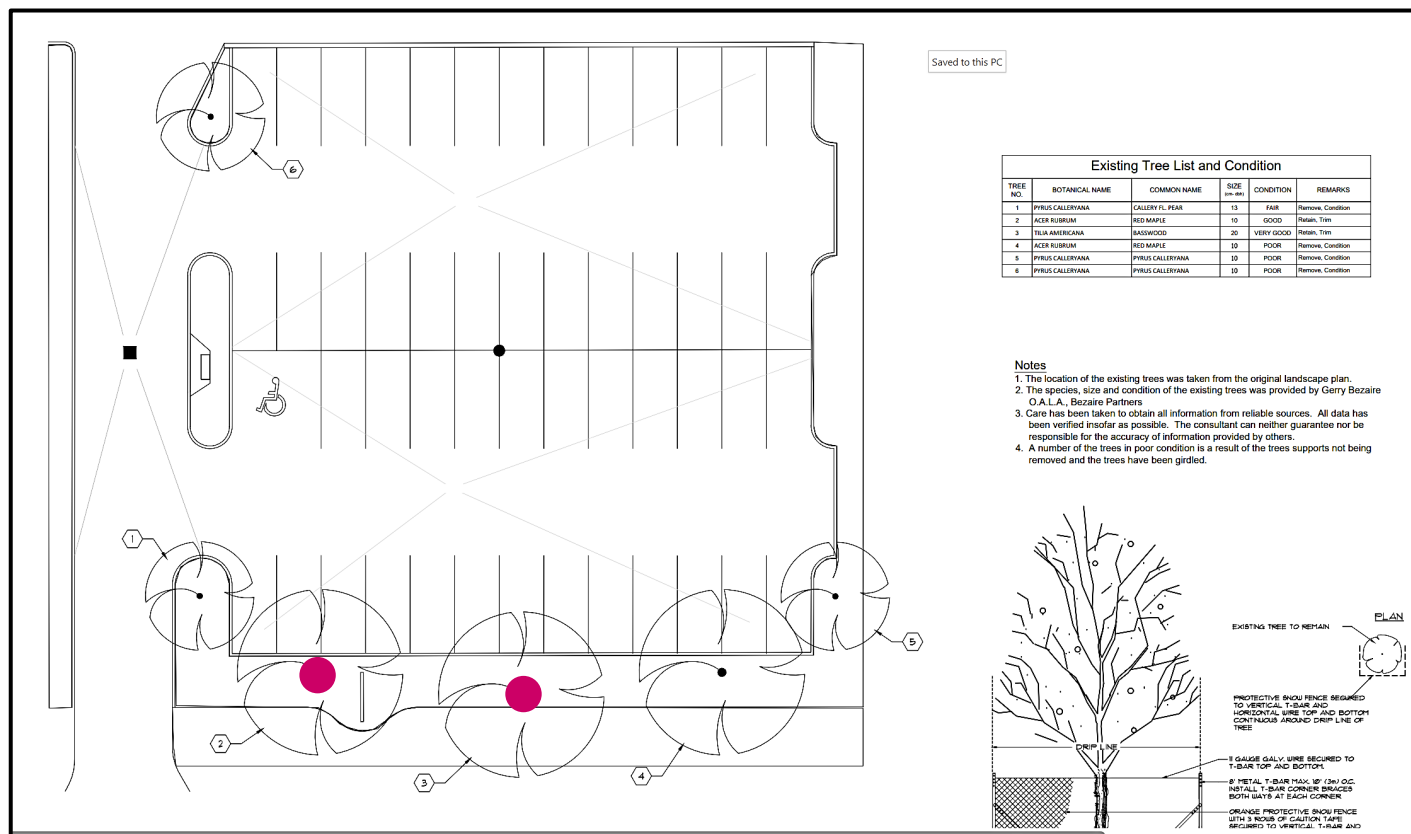
2 DECEMBER 21 10 18 AM
1 : 1000

Winter Solistic: There is shadow on the northern lots to the north during the winter months however, similar to the spring and fall equinox, the shadow dissipates by noon and, in my professional opinion, does not have a significant impact on the neighbourhood.

3.9 TREE SURVEY

The tree survey was completed and shows that the **trees 1, 4, 5 and 6 are to be removed** due to their poor condition while efforts will be made to retain trees **2 and 3** ●.

FIGURE 15 – TREE SURVEY



4.0 PROVINCIAL POLICY STATEMENT (PPS)

The *Planning Act*, R.S.O. 1990, c.P. 13, as amended, requires that the Council of a local Municipality shall make decisions on development applications which are consistent with the Provincial Policy Statement (PPS), 2020.

The PPS was issued by the Ministry of Municipal Affairs and Housing under the *Planning Act* and provides direction on matters of provincial interest primarily related to land use planning and development. The policies within the PPS apply province-wide and are an integral part of the Ontario's policy led planning system.

The PPS generally aims to encourage the wise use and management of land and other resources, promote the development of healthy and prosperous communities, protect public health and safety, and protect the natural environment. Specifically, the primary directives of the PPS include:

“Section 1.1.1 Healthy, liveable and safe communities are sustained by:

- (a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;”*

COMMENT:

In my professional opinion, the proposed ZBA to support the development of the site with the new 6 storey, 54 unit residential building will create an efficient and effective use suited and compatible with the existing neighbourhood.

- (b) accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;”*

COMMENT:

The medium profile residential building with 54 apartments will, in my professional opinion, provide for an alternative style and tenure of housing to the standard single detached residential housing. The site-specific ZBA will facilitate the provision of a variety and diversity of housing Windsor needs to support a healthy community.

The building is located in a neighbourhood of a mix of uses including Windsor Regional Hospital across Goyeau Street from the site. The new apartment building will provide for housing for staff within the hospital and other supportive community offices within the neighbourhood.

In my professional opinion, the proposed ZBA conforms with the relevant policy of the OP.

“(c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;”

COMMENT:

There are no public health, environmental or safety concerns associated with the development of the property.

“(d) avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;”

COMMENT:

The subject lands are located within the urban settlement area of the City of Windsor. The new building creates an infilling development on an underdeveloped lot within an established mixed-use neighbourhood. The medium profile building provides a housing tenure and style diversification that is compatible with the neighbourhood. In my professional opinion, the development will support the efficient and effective establishment of an appropriate alternative residential housing in an appropriate mixed-use area.

“(e) promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;”

COMMENT:

The site is adjacent to Ouellette, a major bus route in the Windsor Transit. The property is located adjacent to the city centre and within a short walking distance to all the institutional and commercial amenities available within the downtown of Windsor. Bike racks will be provided to support the utilization of municipal trails.

Municipal sanitary and piped water services are available to the site. The location will allow for an efficient and effective development while providing for a cost-effective utilization of existing municipal infrastructure. As noted in the Engineering report, there is municipal capacity to accommodate the proposed land use and the development will not result in an expansion of municipal infrastructure.

The proposed development of a medium profile building mixed-use building is an appropriate infill within the mixed use neighbourhood.

In my professional opinion, the proposal is consistent with and supports the establishment of alternative housing tenure and style; supports the cost-effective use of the property while minimizing land consumption; and supports the efficient and effective utilization of municipal infrastructure while being neighbourhood compatible.

“(f) improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;”

COMMENT:

Interior elevators will enhance the accessibility of the building for older persons and persons with disabilities. Four Barrier Free parking spaces will be provided. Building accessibility will be established in compliance with the OBC for all residential units.

“(g) ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs;”

COMMENT:

As noted in the Civil Works report there is sufficient capacity available in the municipal infrastructure to accommodate the proposed 54 residential dwelling units. The proposed development, in my professional opinion, is considered an efficient and effective utilization of municipal infrastructure and is consistent with these PPS policies.

“(h) promoting development and land use patterns that conserve biodiversity; and”

COMMENT:

The proposed mixed-use building with 54 multi-unit building will further intensify the use of an existing built-up area.

The infill development and residential intensification on an underused lot minimizes the consumption of land resources by maximizing the efficient use of a small footprint of land.

The proposed building relieves development pressure on areas of high-quality habitat by infilling and intensifying an underused lot which holds no habitat suitable for Species At Risk, thereby conserving biodiversity.

“j) preparing for the regional and local impacts of a changing climate.”

COMMENT:

In my professional opinion, the proposed development is consistent with the PPS in preparing for impacts of a changing climate.

The development will reduce carbon emissions through providing residents with:

- opportunities for a live-work community in a mixed-use neighbourhood, reducing the need to commute long distances to work
- paved sidewalks
- bike racks
- nearby supportive commercial amenities, reducing the need to travel outside of the neighbourhood
- access to nearby bus routes.

The proposed development will feature appropriate stormwater mitigation strategies to address a 1 in 100 year storm.

It is my professional opinion that the multiple dwelling design of the building will conserve energy used for heating and cooling.

“Section 1.1.3.2 Settlement Areas

Land use patterns within settlement areas shall be based on densities and a mix of land uses which:

- (a) efficiently use land and resources;*
- (b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
- (c) minimize negative impacts to air quality and climate change, and promote energy efficiency;*
- (d) prepare for the impacts of a changing climate;*
- (e) support active transportation;*
- (f) are transit-supportive, where transit is planned, exists or may be developed; and*
- (g) are freight-supportive.*

Land use patterns within settlement areas shall also be based on a range of uses and opportunities for intensification and redevelopment in accordance with the criteria in policy 1.1.3.3, where this can be accommodated.”

COMMENT:

The proposed development is an efficient use of the existing municipal services and can be considered an appropriate use of the subject lands. There is no need to expand municipal services to accommodate the proposed development.

The proposal supports active transportation and municipal transit.

In my professional opinion, the proposed residential development is an efficient, effective development for the site and supportive of the PPS policies ensuring compatible new development within the existing neighbourhood.

“Section 1.1.3.3 Settlement Areas

Planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.”

COMMENT:

The subject lands are within a short walking distance to supportive commercial amenities, including a grocery store, pharmacies, and an Urgent Care centre. A public transit line and a separated bike path are within a short walk.

The ZBA will support the evolution of an underused property for the establishment of a new medium profile, mixed use building with retail space on the ground floor and 54 residential rental units that will provide needed residential accommodation as alternative tenure and style of housing.

The proposed building will be within a couple lots of a similarly zoned ‘Commercial District (CD3.10)’ property that is under construction for a 6 storey, mixed use building considered a medium profile mixed-use building.

It is my professional opinion that the proposed is consistent with this policy of the PPS.

COMMENT:

In my professional opinion, the proposed residential development is an efficient, effective development that provides for an alternative housing tenure and style needed within the neighbourhood. The location will support the 'live work' concept that will allow residents to reside in the same neighbourhood as many employment opportunities in the nearby retail shops, restaurants, and commercial businesses. As well, the rental multi-unit building will support existing residents to age in place and remain in their neighbourhood.

"Section 1.1.3.4 Settlement Areas

Appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety."

COMMENT:

In my professional opinion, the proposed development creates an appropriate residential intensification and infill on an existing parcel that does not pose any health and safety concerns consistent with the PPS policies.

"Section 1.4.1 Housing

To provide for an appropriate range and mix of housing options and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall:

- (a) maintain at all times the ability to accommodate residential growth for a minimum of 15 years through residential intensification and redevelopment and, if necessary, lands which are designated and available for residential development; and*

- (b) *maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned to facilitate residential intensification and redevelopment, and land in draft approved and registered plans. Upper-tier and single-tier municipalities may choose to maintain land with servicing capacity sufficient to provide at least a five-year supply of residential units available through lands suitably zoned to facilitate residential intensification and redevelopment, and land in draft approved and registered plans.”*

COMMENT:

In my professional opinion the proposed 6 storey, medium profile rental housing will effectively respond to the demand for rental housing in the City of Windsor, consistent with the PPS policies. Windsor presently has a historically low rental vacancy rate of 1.8%, and the population of the City is expected to grow. The proposed development will contribute much needed stock of high quality, purpose built rental housing.

“Section 1.4.3 Housing

Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs of current and future residents of the regional market area by:

- (a) *establishing and implementing minimum targets for the provision of housing which is affordable to low and moderate income households and which aligns with applicable housing and homelessness plans. However, where planning is conducted by an upper-tier municipality, the upper-tier municipality in consultation with the lower-tier municipalities may identify a higher target(s) which shall represent the minimum target(s) for these lower-tier municipalities;*
- (b) *permitting and facilitating:*
- 1. all housing options required to meet the social, health, economic and well-being requirements of current and future residents, including special needs requirements and needs arising from demographic changes and employment opportunities; and*
 - 2. all types of residential intensification, including additional residential units, and redevelopment in accordance with policy 1.1.3.3;*
- (c) *directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;*

- (d) *promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed;*
- (e) *requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations; and*
- (f) *establishing development standards for residential intensification, redevelopment and new residential development which minimize the cost of housing and facilitate compact form, while maintaining appropriate levels of public health and safety.”*

COMMENT:

In my professional opinion, the proposed development of a six-storey mixed-use building with 54 units of rental apartments will provide for an alternative range and tenure of housing within the city, consistent with PPS policies.

The increased density of the proposed development will benefit existing public transit and active transportation networks while utilizing existing municipal infrastructure in a cost-effective manner.

The development of the site for a 6 storey medium density building will provide for a more efficient and effective use of the property than the present existing vacant use.

“Section 1.8.1 Energy Conservation, Air Quality and Climate Change

Planning authorities shall support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and preparing for the impacts of a changing climate through land use and development patterns which:

- (a) *promote compact form and a structure of nodes and corridors;*
- (b) *promote the use of active transportation and transit in and between residential, employment (including commercial and industrial) and institutional uses and other areas;*
- (c) *focus major employment, commercial and other travel-intensive land uses on sites which are well served by transit where this exists or is to be developed, or designing these to facilitate the establishment of transit in the future;*

- (d) *focus freight-intensive land uses to areas well served by major highways, airports, rail facilities and marine facilities;*
- (e) *encourage transit-supportive development and intensification to improve the mix of employment and housing uses to shorten commute journeys and decrease transportation congestion;*
- (f) *promote design and orientation which maximizes energy efficiency and conservation, and considers the mitigating effects of vegetation and green infrastructure; and*
- (g) *maximize vegetation within settlement areas, where feasible.”*

COMMENT:

In my professional opinion, the proposed development is consistent with PPS policies by:

- Creating an efficient and compact building structure on a small footprint of land that is underutilized;
- supporting a pedestrian friendly mixed-use neighbourhood;
- promotes active transportation and its proximity to supportive commercial and institutional amenities;
- Supports existing public transit;
- Demonstrates a pedestrian friendly streetscape with visually attractive building design and landscaping.

COMMENT:

In my professional opinion, the requested ZBA is consistent with the 2020 PPS by supporting the sound and efficient managed intensification and growth associated with the mixed use development of the land use for the subject site. The proposed ZBA regulatory framework supports the development of these lands as a medium profile, mixed-use building as a compatible infilling development for the community. The proposed development, in my professional opinion, is consistent with the Provincial Policy Statement (PPS).

5.0 CITY OF WINDSOR OFFICIAL PLAN

The Official Plan for the City of Windsor is applicable to planning matters for the City. The subject property is located within the mixed use neighbourhood in the periphery to the city center. The lands are designated as 'Residential' in the City's Official Plan under OPA No. 159. The following sections review City policies as they relate to the proposed development:

“Section 1.8.1 Energy Conservation, Air Quality and Climate Change

Planning authorities shall support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and preparing for the impacts of a changing climate through land use and development patterns which:

(a) promote compact form and a structure of nodes and corridors;

COMMENT:

The new building will be designed in compliance with the OBC and energy efficiencies where possible. The nature of the medium profile building supports energy conservation by providing for moderate density on site. EV charging stations will be included in the design. In my professional opinion the requested ZBA will support an environmentally sound development and conforms with OP policies.

“3.2.1.2 Neighbourhood Housing Variety

Encouraging a range of housing types will ensure that people have an opportunity to live in their neighbourhoods as they pass through the various stages of their lives. Residents will have a voice in how this new housing fits within their neighbourhood. As the city grows, more housing opportunities will mean less sprawl onto agricultural and natural lands.”

COMMENT:

The proposed 54 unit residential multi-unit building will be located within a mix of uses neighbourhood. The physical location of the building is an infill development and residential intensification on an under utilized lot and within a neighbourhood of compatible and comparable development.

In my professional opinion, the proposed development will reduce urban sprawl by providing for simultaneous retail and residential uses in a medium profile building of 6 storeys on a relatively small footprint of land.

In my professional opinion, the proposed apartment accommodation will support the live work scenario with the ability for tenants to be located within walking distance or located on a major bus route to work.

The proposed development conforms with the relevant policies of the OP.

“3.2.1.4 Community Design

The design of buildings and spaces will respect and enhance the character of their surroundings, incorporating natural features and creating interesting and comfortable places. Streets, open spaces and the greenway system will serve as public amenities connecting and defining neighbourhoods and contributing to Windsor’s image. New development in Windsor will accommodate the needs of pedestrians, cyclists and other recreational activities.”

COMMENT:

The building has been designed to emphasize the pedestrian. There is a minimal setback from Goyeau Street, with pedestrian entrances fronting on Goyeau and both the east and west sides of the building. The orientation of the building at the front of the lot, with parking behind the building, provides for an aesthetically appealing building. Bike racks will be located at the side of the building to promote cycling. There will be extensive landscaping. In my professional opinion, the proposed development conforms with the OP policy.

“3.2.3.5 Energy Efficiency

Windsor will encourage the design and construction of energy efficient buildings and landscapes to reduce air, water and land pollution and environmental impacts of energy production and consumption.”

COMMENT:

The new building will be designed in compliance with the OBC and energy efficiencies where possible. The nature of the medium profile building supports energy conservation by providing for moderate density on the site. In my professional opinion the requested ZBA will support an environmentally sound development that conforms with OP policies.

“3.3.2 Corridors

Corridors represent the backbones of the urban network structure. Neighbourhoods gravitate towards these corridors to serve their everyday needs or to connect with larger nodes, commercial centres and employment centres to access a wider range of services and opportunities. Corridors have opportunities for intensification that would provide a wider range of services and opportunities for adjacent neighbourhoods but also more opportunities to live and work in the area.”

COMMENT:

The ZBA will allow for the inclusion of a mixed-use development in the peripheral area of the City Center. The building will front on Goyeau, a local road and is connected to Ouellette Ave, a major arterial road. The residential apartment will provide for alternative housing tenure and style while accommodating live/work initiative and aging in place.

In my professional opinion, the ZBA supports an appropriate development that conforms with this policy of the OP.

“4.2.1 Healthy and Liveable City – Planning & Design

4.2.1.1 To consider community health in the planning and design of Windsor and its neighbourhoods.

4.2.1.2 To provide for activities and facilities which will foster an active lifestyle to improve community health.

4.2.1.4 To protect against climate change and its possible adverse effects on human health, the physical environment, economy and quality of life.

4.2.1.5 To encourage a mix of housing types and services to allow people to remain in their neighbourhoods as they age.

4.2.1.6 To provide for pedestrian scale neighbourhood centres that serve the day-to-day needs of the local residents.”

COMMENT:

The requested ZBA will support a healthy lifestyle for residents by encouraging walking and cycling with its proximity to goods and services that residents will need on a regular basis (grocery store, bank, pharmacies, restaurants); its proximity to a major institutional (health unit offices, hospital); and its inclusion of 6 spaces of bike parking on site. A live-work lifestyle is supported by the nature of the mixed-use neighbourhood which has many commercial businesses in the same area as residential dwellings, reducing the need for residents to travel to work by car.

The 54 residential apartment units offers a housing style and tenure that addresses the current housing crisis occurring in Windsor, evidenced by the historically low rental vacancy rate of 1.8% in 2022. The proposed development will provide needed stock of alternative housing.

In my professional opinion, the proposed development conforms with the healthy community initiative of the City of Windsor Official Plan policies.

“4.2.3 Quality of Life

4.2.3.1 To encourage a mix of uses.

4.2.3.2 To encourage the location of basic goods and services close to where people live and work.

4.2.3.3 To recognize the needs of the community in terms of shelter, support services, accessibility and mobility.

4.2.3.4 To accommodate the appropriate range and mix of housing.”

COMMENT:

The proposed development is located in short distance to a municipal transit route and will support the use of the municipal bike trail system. The residential apartment will provide for alternative housing style and tenure in a medium rise, 6 storey building that is compatible in height and massing with the neighbourhood. The multi-unit building will provide the existing residents access to health facilities, institutional and commercial uses including retail stores, restaurants, and commercial businesses.

In my professional opinion the requested site-specific ZBA will conform with the ability to provide the neighbourhood and City alternative residential tenure and style of housing.

“5.3.7.2 Atmospheric Air Quality Policies

Council will contribute to the reduction of air pollution by using the following land use planning approaches:

- (a) increasing opportunities for non-automotive transportation modes including walking, cycling and public transportation in accordance with the Infrastructure chapter of this Plan;*
- (b) regulating development which has the potential to increase atmospheric pollution in accordance with the Land Use chapter of this Plan;*
- (c) improving energy conservation in accordance with the Urban Design chapter of this Plan;*
- (d) locating compatible residential, commercial and employment uses in a manner that reduces distance and vehicle trips as outlined in the Land Use chapter of this Plan.”*

COMMENT:

The location of the proposed residential building within a mixed use neighbourhood, located in close proximity to a municipal bus route and in close proximity to alternative trails supports alternative methods of transportation. Bike racks supporting alternative transportation will be provided.

The design of the medium profile, mixed-use building supports live-work initiative by establishing in a mixed-use community with many employment opportunities. In my professional opinion, the location and design of the proposed development will reduce carbon emissions by reducing the need for trips by private vehicle and supports the OP policies.

“6.1.14 Residential Intensification

To direct residential intensification to those areas of the City where transportation, municipal services, community facilities and goods and services are readily available.”

COMMENT:

The proposed medium profile building with 54 residential units provides for alternative housing style and tenure in close proximity to municipal transit and will be on full municipal services. The site has excellent access to transit and active transportation routes and many commercial amenities are available in the neighbourhood. In my professional opinion, the proposed development is an appropriate addition of residential intensification to this mixed-use neighbourhood and conforms with relevant policies of the OP for Windsor.

“6.2.1.2 Types of Development Profile

For the purpose of this Plan, Development Profile refers to the height of a building or structure. Accordingly, the following Development Profiles apply to all land use designations on Schedule D: Land Use unless specifically provided elsewhere in this Plan:

- (a) Low Profile developments are buildings or structures generally no greater than three (3) storeys in height;*
- (b) Medium Profile developments are buildings or structures generally no greater than six (6) storeys in height, and;*
- (c) High Profile developments are buildings or structures generally no greater than fourteen (14) storeys in height.”*

COMMENT:

In my professional opinion, the proposed six-storey development has a Medium Profile.

“6.3.1 Residential: Range of Forms & Tenures

6.3.1.1 To support a complementary range of housing forms and tenures in all neighbourhoods.

6.3.1.2 To promote compact neighbourhoods which encourage a balanced transportation system.

6.3.1.3 To promote residential redevelopment, infill and intensification initiatives in locations in accordance with this plan.

6.3.1.5 To provide for complementary services and amenities which enhance the quality of residential areas.”

COMMENT:

The proposal is a residential intensification and infill on a lot that is presently underdeveloped. The construction of 54 high quality residential apartment units provides for an alternative style and tenure of housing that is needed within the City. The is a compact and efficient use of a small footprint of land.

The mixed-use neighbourhood provides residents with a variety of employment opportunities and supportive goods and services.

Across Goyeau is the Regional Hospital and within the neighbourhood are supportive institutional offices and facilities.

In my professional opinion, the proposed ZBA provides for a compact and efficient development that is well suited to the existing mixed-use neighbourhood, in conformity with the OP.

“6.3.2.1 Permitted Uses

Uses permitted in the Residential land use designation identified on Schedule D: Land Use include Low Profile, and Medium Profile dwelling units. High Profile Residential Buildings shall be directed to locate in the City Centre, Mixed Use Centres and Mixed-Use Corridors.”

COMMENT:

A medium profile residential building is a permitted land use in ‘Residential’ designation. The proposed medium profile, mixed-use building provides for an alternative housing style and tenure needed in the City. The proposed building is compatible with the neighbourhood in design, scale and massing and will provide an appropriate increase of density without overburdening the existing neighbourhood and infrastructure. Municipal transit services are available within a 5 minute walk of the site. Municipal water and sanitary services are available to the site without need for extension. The proposed development in my opinion conforms with the relevant policy of the Official Plan.

“6.3.2.4 Locational Criteria

Residential intensification shall be directed to the Mixed Use Nodes and areas in proximity to those Nodes. Within these areas Medium Profile buildings, up to 4 storeys in height shall be permitted. These taller buildings shall be designed to provide a transition in height and massing from low-profile areas. New residential development and intensification shall be located where:

- a) there is access to a collector or arterial road;*
- b) full municipal physical services can be provided;*
- c) adequate community services and open spaces are available or are planned; and*
- d) public transportation service can be provided.”*

COMMENT:

The subject lands front on Goyeau Street, in close proximity to a major arterial road, and has full access to municipal sanitary sewers and piped water. The subject site is situated in a neighbourhood with a variety of goods and services including a grocery store, a regional hospital, pharmacies, etc. Multiple outdoor recreational areas are located within a 15-minute walk. Transit bus stops are within a 5-minute walk. In my professional opinion, the proposed residential intensification is appropriately located and is compatible with the surrounding neighbourhood and conforms with these policies of the OP.

“6.3.2.5 Evaluation Criteria for a Neighbourhood Development Pattern

At the time of submission, the proponent shall demonstrate to the satisfaction of the Municipality that a proposed residential development within an area having a Neighbourhood development pattern is:

- (a) feasible having regard to the other provisions of this Plan, provincial legislation, policies and appropriate guidelines and support studies for uses:
 - (i) within or adjacent to any area identified on Schedule C: Development Constraint Areas and described in the Environment chapter of this Plan;*
 - (ii) adjacent to sources of nuisance, such as noise, odour, vibration and dust;*
 - (iii) within a site of potential or known contamination;*
 - (iv) where traffic generation and distribution is a provincial or municipal concern; and*
 - (v) adjacent to heritage resources.**

- (b) *In keeping with the goals, objectives and policies of any secondary plan or guideline plan affecting the surrounding area;*
- (c) *In existing neighbourhoods, compatible with the surrounding area in terms of scale, massing, height, siting, orientation, setbacks, parking and amenity areas. In Mature Neighbourhoods as shown on Schedule A-1, compatible with the surrounding area, as noted above, and consistent with the streetscape, architectural style and materials, landscape character and setback between the buildings and streets;*
- (d) *provided with adequate off street parking;*
- (e) *capable of being provided with full municipal physical services and emergency services; and*
- (f) *facilitating a gradual transition from Low Profile residential development to Medium and/or High-Profile development and vice versa, where appropriate, in accordance with Design Guidelines approved by Council.”*

COMMENT:

The subject site, as discussed above, is an appropriate location for the inclusion of a new medium profile residential building:

- The subject lands are not within a development constraint area or within a natural heritage area;
- The 54 residential rental apartments will provide for needed alternative housing with access to nearby institutional, stores, restaurants, and businesses;
- The proposed development is compatible in size, scale and massing with the existing buildings in the neighbourhood.
- There are 80 parking spaces, more than sufficient parking on the adjacent site to accommodate the proposed 54 residential units;
- The site is serviced by municipal water and sewer and there is existing capacity available for the proposed development.

The development, in my professional opinion, conforms with the policy of the OP.

“6.5.1.8 Objectives: Residential Intensification

To promote residential intensification with Medium and High-Profile buildings to meet the housing needs of the City in appropriate areas in proximity to municipal services, transit and employment areas.”

COMMENT:

In my professional opinion the proposed 6-storey, mixed-use building satisfies the OP objective of residential intensification in an appropriate area with proximity to municipal services, transit, and employment opportunities.

“6.5.3.1 Permitted Uses

Uses permitted in the Mixed-Use Corridor land use designation are primarily retail, wholesale store (added by OPA 58, 24 07 2006) and service oriented uses and, to a lesser extent, office uses. Medium and High Profile residential uses either as stand-alone buildings or part of a commercial-residential mixed use buildings shall be throughout the Corridors.”

COMMENT:

In my professional opinion, the proposed 6 storey medium profile residential multiple unit building conforms to the permitted uses of the OP.

6.5.3.3 Street Presence

Council will encourage Mixed-Use Corridor development to provide a continuous street frontage and presence. Accordingly, development along a Mixed-Use Corridor shall be:

- (a) no more than four storeys in height, except on lands at an intersection of any combination of the following roads: Class I Arterial Road, Class II Arterial Road, Class I Collector, or Class II Collector Road. The height of buildings shall generally not exceed the width of the road right-of-way abutting the development site; and*
- (b) Notwithstanding the identified maximum building height, the Council may consider additional height, where the Council is satisfied that the proposed height achieves compatible development, and where appropriate transitions to abutting lower scale development are established. Appropriate transitions may be achieved through the implementation of regulatory techniques including, but not limited to new height limitations, enhanced building setbacks and step backs, enhanced*

landscape buffers and planting requirements and/or the implementation of an angular plane. Permissions for taller buildings may be established through a site-specific Zoning By-law Amendment.

- (c) encouraged to locate the buildings at the street frontage lot line with parking accommodated at the rear of the site.”*

COMMENT:

The proposed building will be located at 6 m setback from Goyeau Street and with a 6 m setback from each side yard and rear yard providing for landscaping. There are existing buildings in the neighbourhood of a similar size, scale, and massing to the proposed development. The site will be extensively landscaped.

In my professional opinion, the new building will be an attractive and appropriate addition to a pedestrian oriented, mixed-use neighbourhood.

“6.5.3.8 Design Guidelines

The following guidelines shall be considered when evaluating the proposed design of a Mixed-Use Corridor development:

- (a) the ability to achieve the associated policies as outlined in the Urban Design chapter of this Plan;*
- (b) the provision of appropriate landscaping or other buffers to enhance:*
 - (i) all parking lots, and outdoor loading and service areas; and*
 - (ii) the separation between the use and adjacent sensitive uses, where appropriate;*
- (c) as a general rule, the height of buildings are consistent with the height of buildings which characterize the Mixed-Use Corridor. Where Council deems it desirable that higher profile development be permitted in an existing Mixed-Use Corridor, the development should be built at a human scale by utilizing one or both of the following measures:*
 - (i) treatment of the lower floors of building(s) to provide continuity; and/or*
 - (ii) setting back the upper floors of building(s) from the street to avoid overpowering effects at-grade;*
- (d) where possible, parking is located in the rear of the property to encourage continuous building facades adjacent to the street; and*

- (e) measures are taken in site design which provide for ease of access for pedestrians between the public sidewalk and building main entrances in a manner which is distinguishable from access provided for vehicles.*
- (f) Council will adopt Design Guidelines that will assist in the design and review of development applications in a manner that will ensure implementation of these policies.”*

COMMENT:

The medium profile, 6-storey building will be of a similar size and massing to buildings already existing within the neighbourhood. The building will be extensively landscaped to create an attractive street presence. Pedestrian entrances will be located at both the east and west sides of the building with access to the public sidewalk along Goyeau. In my professional opinion, the proposed development will be an attractive, pedestrian oriented building that will be an asset to the neighbourhood while providing for needed alternative housing. The proposed ZBA and development, in my professional opinion, conforms with the OP.

“10.2.20 Design Briefs

A Design Brief identifies the character of a street or neighbourhood over a smaller area than an Urban Design Study. Where this Plan requires the preparation of a Design Brief the following steps shall be taken:

“10.2.20.1 The Urban Design Brief should provide a description and analysis of the site and surrounding context. Photographs and a context map showing the subject site in relation to the existing neighbourhood should be included.”

COMMENT:

The proposed design and build is compatible with the neighbourhood and will be a positive aspect of the neighbourhood. In my professional opinion the building is high level design and will be an asset to the neighbourhood and conforms with the OP policies.

“10.2.20.2 The Urban Design Brief should provide an analysis of the design rationale for the building, landscape, and site design elements of the proposed development and explain why the proposed development represents the optimum design solution. Discussion should consider the following:

- i) *How the design of the proposed development meets the intent of the City's applicable urban design guidelines and policies;*
- ii) *How the design addresses existing site conditions and constraints such as lot size, grading, or natural heritage features;*
- iii) *How the design of the proposed development integrates with the existing neighbourhood and enhances its function and aesthetics; and,*
- iv) *How the design of the proposed development will influence and integrate with future development in the neighbourhood."*

COMMENT:

The design of the proposed building is pedestrian oriented, with 3 pedestrian entrances on either side of the building and at the front as well as well landscaped. The proposed development will encourage future development with a pedestrian orientation by increasing the density of the neighbourhood in a manner that is supported and sustainable.

"10.2.20.3 The Urban Design Brief should include a written description, plans, elevations, diagrams, and/or photographs to illustrate the design choices of the proposed development and site design. Depending on the scale of the development proposal explain how the applicable design considerations have been addressed:

- i) *Street and block pattern (e.g., connectivity, pedestrian access);*
- ii) *Lot sizes;*
- iii) *Building orientation and site layout;*
- iv) *Built form, height, scale, and massing;*
- v) *Building articulation and detailing;*
- vi) *Building materials;*
- vii) *Setbacks from adjacent properties and the street;*
- viii) *Building step back (if applicable);*
- ix) *Building transition to adjacent neighbourhoods;*
- x) *Heritage considerations (if applicable);*
- xi) *Location of parking (surface or underground), driveways, ramps, drop-off areas;*
- xii) *Access to transit;*
- xiii) *Bicycle parking/storage;*
- xiv) *Location of servicing, garbage, organics, and recycling storage and collection, and loading areas;*

- xv) *Streetscape elements (e.g., boulevard design, landscaping, street furniture, public art, signage, lighting, etc.); and,*
- xvi) *On-site landscaping and buffering.”*

COMMENT:

The proposed 6 storey building has a 6 m setback from Goyeau Ave. There are pedestrian entrances on the north side on both the west and east sides of the building, which will connect with the paved sidewalks along Goyeau Ave. A bike rack with 6 spaces in front of the building will promote cycling. The building is in close walking distance to municipal bus stops and to a major north/south arterial road.

The building will be compatible in size, scale, and massing with the existing neighbourhood.

Private balconies will add to the attractiveness of the building. The lot will be extensively landscaped.

The building will have indoor amenity areas with balconies on each of the six residential floors. The accessibility of the building will be enhanced by placement of elevators on both the north and south sides of the building. There will also be two staircases.

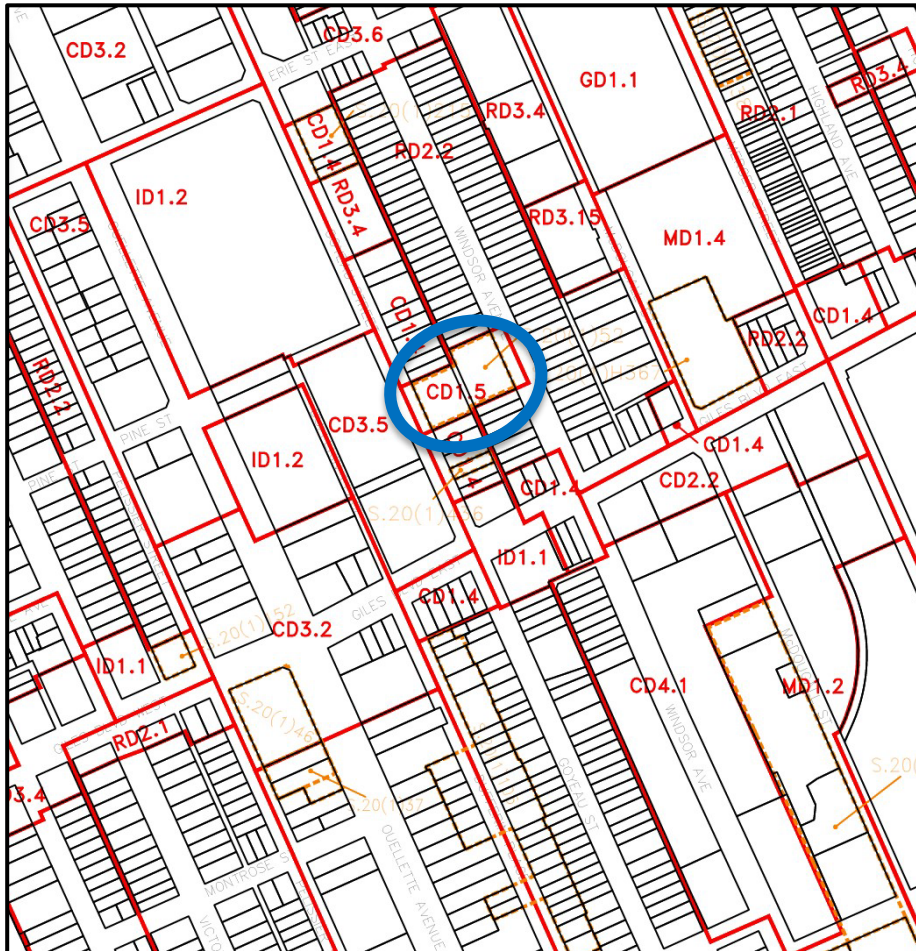
In my professional opinion, the proposed multi-unit building is an attractive and environmentally responsible design that is compatible with its neighbourhood.

COMMENT:

In my professional opinion, the requested site-specific ZBA conforms with the relevant policies of the Official Plan for the City of Windsor based on the evaluation noted above. The proposed development is consistent with the intent of the OP.

6.0 CITY OF WINDSOR ZONING BYLAW

The subject lands are zoned 'Commercial District 1.5 (CD1.5)' in the CZB 8600 for the City of Windsor. The proposed multi-unit residential building is not a permitted use under the present (CD1.5) zone.



The neighbourhood is a mix of zone categories. There are a number of (RD3)'Residential District 3 zoned properties within the neighbourhood. The proposed (RD3.3 #) is compatible in height, density and use with the mix of uses and densities within the neighbourhood.

A site specific provision has been requested that associates the Residential building at 1140 Goyeau Street and the parking lot across the alleyway at 0 Windsor Ave.

PROVISIONS	RESIDENTIAL DISTRICT (DC3.3)	PROPOSED DEVELOPMENT
PERMITTED USES	MUTLI-UNIT BUILDING	MULTI-UNIT BUILDING
LOT AREA	3,400m ²	<p>LOT A (0 Windsor Ave) –lot area of 2,156 m². Lot A to remain vacant and will be used for 79 parking spaces.</p> <p>LOT B (1140 Goyeau Street) – lot area of 1,728 m². Lot B is site of new building.</p> <p>Total area of 3,885 m2</p>
BUILDNG HEIGHT	24 m max	23 m
DWELLING UNIT DENSITY	180 units/hectare	54 units/.3885 ha = 151 units/ha
LANDSCAPED OPEN SPACE	35% minimum	<p>LOT A (PARKING LOT) = 9 %</p> <p>LOT B (RESIDENTIAL LOT) = 28%</p>
PARKING	1.25 X 54 units = 67 spaces	80 spaces
BARRIER FREE PARKING	<p>Type A 2% of parking spaces = 1 spaces</p> <p>Type B 2% of parking spaces = 1 spaces</p>	<p>2 spaces Type A</p> <p>2 spaces Type B</p>
LOADING SPACES	1 space	1 space
BIKE PARKING	<p>20 or more parking spaces: 2 for first 19 spaces plus 1 for each additional 20 spaces = 5 spaces</p>	5 spaces

There is a site specific provision that presently applies to the subject parking area to the west of Giles. This site specific provision can continue to remain applicable to the property without ramifications to the proposed land use.

“Section 20(1)(52) Site Specific Provisions:

For any lands located on the west side of Windsor Avenue between Giles Boulevard East and Erie Street East, which are zoned CD1.5 on Zoning District Map 7, an access area to Windsor Avenue is prohibited.”

COMMENT:

After review and evaluation of the CZB for the City of Windsor, it is my professional opinion that the proposed site development complies with the intent of the ‘Residential District 3.3 (RD3.3) regulations with modification to recognize a site specific provision.

A site specific provision to allow for 28% landscaped open space maintains the intent of the CZB to support a mixed use development and provides for a compatible building within a mixed use neighbourhood.

A site specific provision that identifies Lot A as the parking associated with Lot B the 54 unit building.

It is therefore my professional opinion that a ZBA to establish the (CD 3.10) zone regulatory framework for the subject lands meets the intent of the CZB for the City of Windsor.

7.0 SUMMARY AND CONCLUSIONS

Given the foregoing assessment and my evaluation of the proposal in relation to the PPS 2020, the City of Windsor Official Plan and the Comprehensive Zoning By-law 8600, in my professional opinion the proposed site-specific By-law Amendment (ZBA) is consistent with policies of the PPS, OP, and the regulations found in the Zoning By-law.

The requested site specific ZBA to provide a site specific regulatory framework that will support the development of the site for 54 residential apartment rental units and 80 parking spaces makes sound planning.

In addition, it is my professional opinion that the proposed site-specific Zoning By-law Amendment (ZBA) is appropriate and desirable within this policy framework as it will facilitate development of site while also implementing the proposals included in this Planning Justification Report dated December 14, 2023.

In summation, the proposal complies with the Zoning Bylaw Amendment (ZBA) that will appropriately establish a site-specific regulatory framework under the 'Residential District 3.3 (RD33.)' zone. The ZBA provides a good solution for the provision of needed alternative residential accommodation and compatible development within an existing neighbourhood, while supporting a diversity of housing tenures and styles within the municipality on major municipal route.

In my professional opinion the requested ZBA:

- 1) is consistent with the policies of the 2020 Provincial Policy Statements;
- 2) maintains the intent of the relevant policies of the City of Windsor Official Plan;
- 3) maintains the intent of City of Windsor CZB 8600 and when the ZBA is passed, it will establish the regulatory framework required for the development to comply with the CZB;
- 4) makes sound planning.

I hereby certify that this report was prepared by Jackie Lassaline RPP MCIP, a Registered Professional Planner within the meaning of the Ontario Professional Planners Institute Act, 1994.

Lassaline Planning Consultants Inc.

Jacqueline Lassaline

Jackie Lassaline BA MCIP RPP

Principal Planner

APPENDIX A – ‘RESIDENTIAL DISTRICT (RD3.3)’

12.3 RESIDENTIAL DISTRICT 3.3 (RD3.3)

12.3.1 PERMITTED USES

Lodging House

Multiple Dwelling

Religious Residence

Residential Care Facility

Any of the following *existing* dwellings:

Double Duplex Dwelling

Duplex Dwelling

Semi-Detached Dwelling

Single Unit Dwelling

Any use accessory to any of the preceding uses

12.3.5 PROVISIONS

- | | | |
|-----|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------------|
| .1 | Lot Frontage – minimum | 45.0 m |
| .2 | Lot Area – minimum | |
| | For a <i>corner lot</i> having a minimum <i>lot frontage</i> of 45.0 m on each of the <i>exterior lot lines</i> : | |
| | a) For the first 23 <i>dwelling units</i> | 1,825.0 m ² |
| | b) For each additional <i>dwelling unit</i> | 37.0 m ² per unit |
| | For any other <i>lot</i> : | |
| | c) For the first 19 <i>dwelling units</i> | 1,825.0 m ² |
| | d) For each additional <i>dwelling unit</i> | 45.0 m ² per unit |
| .3 | Lot Coverage – maximum | 35.0% |
| .4 | Main Building Height – maximum | |
| | <i>Corner Lot</i> | 30.0 m |
| | <i>Interior Lot</i> | 24.0 m |
| .8 | Landscaped Open Space Yard – minimum | 35.0% of <i>lot area</i> |
| .13 | Dwelling Unit Density – <i>dwelling units</i> per hectare – maximum | |
| | For a <i>corner lot</i> having a minimum <i>lot frontage</i> of 45.0 m on each of the <i>exterior lot lines</i> | 225 units per ha |
| | For any other <i>lot</i> | 180 units per ha |
| .50 | A <i>Lodging House</i> for the accommodation of 10 persons or less, and any use accessory thereto, shall comply with the <i>Single Unit Dwelling</i> provisions of Section 10.1.5 and further, the whole of the <i>building</i> shall be used for a <i>Lodging House</i> , including any <i>accessory use</i> . [ZNG/5630] | |

APPENDIX B – SHADOW STUDY

APPENDIX E – CONSULTATION COMMENTS

ENVIRONMENTAL SERVICES – ANNE-MARIE ALBIDONE

No Concerns from Environmental Services.

TRANSIT WINDSOR – JASON SCOTT

Transit Windsor has no objections to this development. The closest existing transit route to this property is with the Ottawa 4. The closest existing bus stop to this property is located on Giles at Goyeau Southeast Corner. This bus stop is approximately 140 metres from this property falling within Transit Windsor's walking distance guidelines of 400 metres to a bus stop. This will be maintained with Transit Windsor's City Council approved Transit Master Plan.

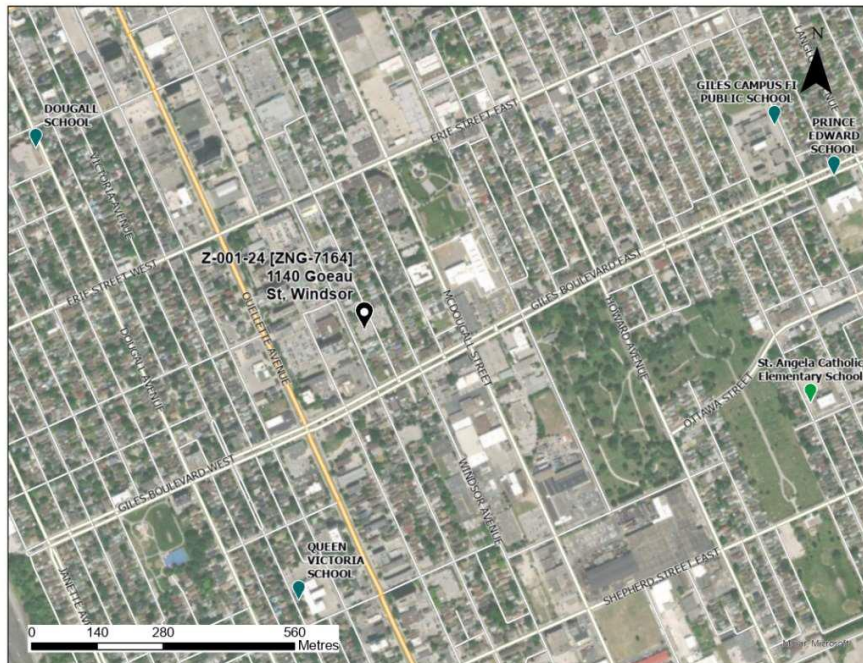
SITE PLAN CONTROL

The development proposal is subject to Site Plan Control pursuant to the Planning Act and City of Windsor By-law 1-2004. Where preceding development applications are required, inclusive of Official Plan and Zoning By-law Amendments, request for Site Plan Control Pre-Consultation Stage 1 may be made following completion of the requisite Development and Heritage Standing Committee meeting at <https://ca.cloudpermit.com/login>.

GREATER ESSEX COUNTY SCHOOL BOARD & WINDSOR ESSEX CATHOLIC DISTRICT SCHOOL BOARD – JORDAN COOK

On behalf of the Greater Essex County District School Board and the Windsor Essex Catholic District School Board, we confirm receipt of the Zoning By-Law Amendment noted above and dated August 23, 2024. A total of 58 units are proposed for this development. With respect to this application, the following comments are submitted:

Location Map:



This development falls within the attendance boundaries of:

Greater Essex County District School Board:

☐ Queen Victoria Public School

☐ Kennedy Secondary School

At this time, sufficient space exists within the local elementary schools to accommodate additional students from the development as proposed.

Although the impact of this development will be minimal, the local secondary school is operating near capacity and may not accommodate all students from new development. Interim accommodation measures such as portables or a holding area are being utilized to accommodate existing students. Additional measures may be required to address future student accommodation.

Windsor Essex Catholic District School:

- ☐ St Angela Catholic Elementary School
- ☐ Catholic Central High School

At this time, sufficient space exists within the local elementary and secondary schools to accommodate additional students from the development as proposed.

General Comments:

Please note, student transportation providers may not travel on privately owned or maintained right-of-ways and roads not yet assumed by the municipality to pick-up/drop-off students. A congregated bus stop may be located outside of the development area.

If there are any questions or comments regarding the Board's response, please contact vw@watsoncon.ca.

ENWIN

HYDRO ENGINEERING: Zachary Mancini

No Objection, provided adequate clearances are achieved and maintained.

ENWIN has existing overhead pole lines with 27,600 volt primary, 347/600 volt secondary, and 120/240 volt secondary hydro distribution along the alley between 1140 Goyeau Street and 0 Windsor Avenue.

ENWIN has existing overhead 120/240 volt secondary hydro services crossing the North-West portion of 0 Windsor Avenue for its respective parking lot and 1119 Windsor Avenue. There is also an overhead 120/240 volt secondary hydro service crossing the North-East portion of the 1140 Goyeau Street property to 1128 Goyeau Street. Lastly there is an overhead 120/240 volt secondary hydro service to 1154 Goyeau Street along the property line neighbouring this development.

There are City of Windsor streetlight poles with overhead 120 volt secondary hydro distribution along Goyeau Street, West of the development property.

Prior to working in these areas, we would suggest notifying your contractor and referring to the Occupational Health and Safety Act and Regulations for Construction Projects to confirm clearance requirements during construction.

Also, we suggest referring to the Ontario Building Code for permanent required clearances for New Building Construction.

WATER ENGINEERING: Bruce Ogg

ENWIN Water has no objections on the rezoning.

TRANSPORTATION PLANNING – ELARA MEHRILOU

- The Official Plan classifies Goyeau St as a Local Road with a required right-of-way width of 20 meters. The current right-of-way is sufficient; therefore, no conveyance is required. Confirmation required from Right-of-Way.
- The Official Plan classifies Windsor Ave as a Local Road with a required right-of-way width of 20 meters. The current right-of-way is sufficient; therefore, no conveyance is required. Confirmation required from Right-of-Way.
- All parking must comply with ZBL 8600.
 - Loading space shall have a minimum width of 3.m, minimum length of 7.5m and a minimum height clearance of 3.5m throughout its length and width.
- All new accesses shall conform to the TAC Geometric Design Guide for Canadian Roads and the City of Windsor Standard Engineering Drawings.
 - All proposed driveways must be 7-9 metres total at the property line (minimum 3.5m/lane, maximum 4.5m/lane).
- All exterior paths of travel must meet the requirements of the Accessibility for Ontarians with Disabilities Act (AODA).
 - A pedestrian walkway to Windsor Ave must be indicated on the revised site plan to connect pedestrians from the building to the existing sidewalks.
- With regards to the proposed alley access, here are Transportation's thoughts:
 - Transportation has concerns related to both vehicles and pedestrians crossing the alley.
 - The alleyway is not maintained to the same level as a City sidewalk and therefore, there is a significant liability in using the alley as a continued pedestrian access
 - The volume of vehicles that the proposed development will create is higher than normal for an alley. Proper controls and traffic signage must be installed and maintained by the owner of the properties; the City will not maintain these.
 - Transportation Planning does not support this layout of a parking lot on the other side of a City-owned alleyway due to safety concerns and liability to the City.

ENGINEERING RIGHT-OF-WAY – MARK SCHAFFHAUSER

Required Drawing Revisions:

1. **Driveway Approaches** – Do not conform to City of Windsor Standards, which must be constructed with straight flares and no raised curbs within the right-of-way.

- o Modify as per Standard Engineering Drawing AS-204.
- o Owner to create proper curb cut off of Goyeau St to fit existing driveway approach.
- o Proposed location of driveway approach off of Windsor Ave has trees in the right-of-way. City Forester to be contacted for fees pertaining to removal of any trees in the right-of-way

2. **Sewer Connections** – All existing and proposed storm, sanitary and water services must be identified on the drawings, as well as the associated mainline sewers/water mains.

- o Modify drawings to include all sewer connections and water services.
- o Identify any redundant connections to be abandoned in accordance with Engineering Best Practice BP1.3.3.

3. **Encroachment Agreement** – There are existing landscaping items encroaching into the right-of-way, which require either removal or an encroachment agreement to legalize said items.

- o Modify drawings to remove encroaching items or identify that an encroachment agreement with the City of Windsor will be required for the landscaping items.

4. **Alley Paving** – Proposal includes access off existing alley.

- o Modify drawings to include alley paving in proposal

Special Provisions (to be included in Site Plan Control Agreement)

The following special provisions will be required prior to submitting a building permit application:

Site Plan Control Agreement – The applicant enter into an agreement with the City of Windsor for all requirements under the General Provisions of the Site Plan Control Agreement for the Engineering Department.

Alley Paving – The owner shall agree to drain and pave at his entire expense, the alley abutting the subject lands. The minimum acceptable cross-section will be 300 mm Granular “A” and 100 mm base and surface course asphalt in accordance with Standard City of Windsor Specifications, Selected Granular Base Course (S4) and Hot Mix, Hot Laid Asphaltic Concrete (S-10). The geometrics of the pavement shall comply with City of Windsor Standard Drawing AS-201. All work shall be to the satisfaction of the City Engineer.

Encroachment Agreement – The owner agrees to submit application for and execute an agreement with the Corporation for the existing encroachments into the right-of-way (landscaping items along Goyeau St frontage) to the satisfaction of the City Engineer.

If you have any further questions or concerns, please contact Mark Schaffhauser, of this department at mschaffhauser@citywindsor.ca

DEVELOPMENT ENGINEERING – JUAN PARAMO

We have reviewed the subject Rezoning application and have the following comments:

Sewers The site may be serviced by a 200mm vitrified clay combined sewer and a 525mm concrete storm sewer located within Goyeau Street right-of-way. If possible existing connections should be utilized. Any redundant connections shall be abandoned in accordance with the City of Windsor Engineering Best Practice B.P 1.3.3.

A Sanitary Sewer Study, dated November 2023 by Dillon Consulting, has been received and reviewed. The applicant's consultant has confirmed that the existing combined sewer on Goyeau Street will effectively accommodate the site's sewer servicing needs. The study demonstrates that the municipal combined sewer have adequate capacity, and no adverse impacts are expected on the surrounding areas as a result of the proposed development.

The Sanitary Sewer Study has been deemed acceptable, and the proposed sanitary servicing strategy is supported by the Engineering Development department.

A Stormwater Management Report dated June 2024 by Ambashi Engineering has been received and reviewed. A revised study will be **required for Site Plan Control** to address comments provided separately in Appendix A.

Right-of-Way

Goyeau Street is classified as a Local Residential Road according to the Official Plan requiring a right-of-way width of 20.1m; the current right-of-way is 20.1m, therefore, no conveyance is required at this time.

There are currently landscaping encroachments in the Goyeau Street right-of-way. Encroaching items are to be removed at the owner's expense or the Owner may apply for an Encroachment Agreement.

At the time of site plan control, the applicant will be required to submit a full grading plan of the alley between the two subject sites to demonstrate existing drainage patterns, and to confirm that proposed alley reconstruction will continue to accommodate or improve drainage.

In summary we have no objection to the proposed development, subject to the following requirements:

Site Plan Control Agreement – The applicant enters into an agreement with the City of Windsor for all requirements under the General Provisions of the Site Plan Control Agreement for the Engineering Department.

Encroachment Agreement – The owner agrees to submit application for and execute an agreement with the Corporation for the existing encroachments into the Goyeau Street right-of-way to the satisfaction of the City Engineer or have the encroaching items removed.

Appendix A: Stormwater Management Report Comments

- Stormwater Management Report dated June 2024 must be stamped and signed prior to acceptance and approval.
- Section 3.2 states that the surface ponding in the proposed parking lot can provide storage of 41.75 cubic meters. Please demonstrate via a figure or drawings the location of this storage and confirm that this storage is within in the asphalt surface of the 0.173 ha site that is being assessed in the report.
- Section 4.3.2 – Consultant to calculate a weighted C value based on the proposed development areas, which will be used for the 1:5 post-development calculations. The C value of 0.35 is only to be used in pre-development calculations related to the allowable release rate.
This post-development C value should then be adjusted according to the WERSM adjustment factor for the 1:100 event and used in the 1:100 event calculations.
- Section 4.7 contains a table of surface ponding calculations. Please include additional information in the calculations, including parking lot slopes and a visual representation of the corresponding storage depth/elevations via a figure or in the drawings.
- The SWM report must demonstrate that the subject site has capacity in the form of underground storage to accommodate the 1:5 year event, though pipes, catch basins, MHs, or chambers. No ponding will be permitted for the 1:5 year event.
- Consultant to state in the report the 1:100 year surface ponding elevation for the site and required minimum finished floor/building opening elevation for the site
- Please provide a clear outline and elevations of the ponding area for the 1:100 year storm event within the 0.173 ha site that is being assessed in the report on

the drawing set. Include elevation for proposed building Finished Floor Elevation (FFE) and/or minimum building opening. Building FFE to be a minimum 0.30m above 1:100 year surface ponding level on the site

- Label MH IDs, invert, and pipe sizes on the drawing set.
- Site is to be graded to fully contain the 1:100 year storm event flows and ensure no overland flow runoff onto adjacent properties. Provide a grading plan.
- The report needs to provide additional information on the orifice flow restrictor regarding location of installation. The plans must also show the flow restrictor.

If you have any further questions or concerns, please contact Daniel Lopez, of this department at dlopez@citywindsor.ca

ENBRIDGE - Sandro Aversa

After reviewing the provided information, and consulting our mapping system, please note that Enbridge Gas has active infrastructure at 1140 Goyeau Ave. A PDF drawing have been attached for reference.

Please Note:

- 1. The shown piping locations are approximate and for information purposes only**
- 2. The drawings are not to scale**
- 3. This drawing does not replace field locates. Please contact Ontario One Call for onsite locates prior to excavating, digging, etc**

Enbridge Gas requires a minimum separation of 0.6m horizontal and 0.3m vertical from all our plant less than NPS 16 and a minimum separation 1.0m horizontal and 0.6m vertical between any CER-regulated and vital pipelines. For all pipelines (including vital pipelines), when drilling parallel to the pipeline, a minimum horizontal clearance measured from the edge of the pipeline to the edge of the final bore hole of 1 m (3.3 ft) is required. Please ensure that this minimum separation requirement is maintained, and that the contractor obtains locates prior to performing any work and utilizes safe excavation practices while performing any work in the vicinity.

Also, please note the following should you find any abandoned infrastructure in the area:

- Any pipe that is excavated, please assume that it is live.
- If during any job, any pipe is found that is not on the locate sheet and conflicts with your work, please call our emergency number (1-877-969-0999), and one of our Enbridge representatives will respond to determine if that plant is in fact live or dead.
- Please note that our Enbridge Gas representative will respond to the live or dead call within 1-4 hours, so please plan your work accordingly.

Please contact me if you have any further questions or concerns.



HERITAGE PLANNING – TRACY TANG

City of Windsor – Planning Department – Heritage Planner

Contact: Tracy Tang, Planner III – Heritage (Acting), ttang@citywindsor.ca

Built Heritage

1140 Goyeau Street is located adjacent to (across the street from) a property that is recognized on the Municipal Heritage Register.

R | 1149 Goyeau St | Bell Canada | 1929 | Art Deco | Core

Please consider designing the proposed development to be compatible with the adjacent property with respects to materials and colour selections.

Vibration Impact Assessment (SPC) - The subject properties are located within 50 metres of a heritage resource, therefore a Vibration Impact Assessment is required as part of any Site Plan Control application to evaluate and mitigate potential structural impacts to nearby heritage properties.

The vibration limits for historic buildings/structures on and off-site is to be determined based on the pre-construction documentation and analysis. The assessment must be conducted (or have the input of) a professional with proven experience with historic conservation, preferably one with CAHP credentials. The Construction Vibration Control Assessment is required to be completed by a qualified vibration engineer and structural engineer with a P.Eng., as a condition of SPC approvals, and to the satisfaction of City Administration prior to any building permit issuance, to include:

- a) Analysis of all construction activities potentially causing vibration impacts on the heritage resources
- b) Establishment of more stringent vibration criterion for heritage resource based on the potential for architectural and structural damage
- c) Background vibration measurements of the site and surrounding areas
- d) Predict extent of vibration impacts and identify all heritage structures within the vibration zone of influence

- e) Conduct pre-condition survey (including photographic documentation) to establish condition of existing heritage structures within the vibration zone of influence
- f) Recommend vibration mitigation and monitoring program with establishment of "do-not-exceed" threshold levels, and a construction vibration control plan
- g) Complete inspection (including photographic documentation) post construction of the heritage structures within the vibration zone of influence to ensure no damage has occurred from the construction activities

Archaeology

The subject properties are not located within an Archaeological Potential Zone (APZ). Nevertheless, the Applicant should be notified of the following archaeological precaution.

1. Should archaeological resources be found during grading, construction or soil removal activities, all work in the area must stop immediately and the City's Planning & Building Department, the City's Manager of Culture and Events, and the Ontario Ministry of Citizenship and Multiculturalism must be notified and confirm satisfaction of any archaeological requirements before work can recommence.
2. In the event that human remains are encountered during grading, construction or soil removal activities, all work in that area must be stopped immediately and the site secured. The local police or coroner must be contacted to determine whether or not the skeletal remains are human, and whether the remains constitute a part of a crime scene. The Local police or coroner will then notify the Ontario Ministry of Citizenship and Multiculturalism and the Registrar at the Ministry of Government and Consumer Services if needed, and notification and satisfactory confirmation be given by the Ministry of Citizenship and Multiculturalism.

Contacts:

Windsor Planning & Building Department:

519-255-6543 x6449, ttang@citywindsor.ca, planningdept@citywindsor.ca

Windsor Manager of Culture and Events (A):

Michelle Staadegaard, (O) 519-253-2300x2726, (C) 519-816-0711, mstaadegaard@citywindsor.ca

Ontario Ministry of Citizenship and Multiculturalism

Archaeology Programs Unit, 1-416-212-8886, Archaeology@ontario.ca

Windsor Police: 911

Ontario Ministry of Government & Consumer Services

A/Registrar of Burial Sites, War Graves, Abandoned Cemeteries and Cemetery Closures, 1-416-212-7499, Crystal.Forrest@ontario.ca

PARKS DEVELOPMENT DIVISION

Yemi Adeyeye, Forestry:

According to the tree assessment provided, 4 of 6 poor condition trees are to be removed and 2 good condition trees are to be preserved. There is no detail provided in the Site plan for new tree plantings to compensate for Lost Tree Canopy cover.

City Forestry requires Compensation for the lost City tree canopy:

Canopy replacement costs associated with the lost trees, based on 40cm of diameter replacement, would be ...

\$ 5,440.00 plus removal costs.

The site plan indicates a distance of 2 meters between the property line on Goyeau St. and the façade of the new building. The 2 good trees identified in this area as "*To Be Preserved*" require a minimum of 2.5 meters Root Protection zone (based on 20cm

diameter). These trees are already 1-2 meters inside the property line; there is insufficient room to create a viable tree protection zone during construction. These trees cannot be successfully protected on-site.

Canopy Replacement cost for these 2 additional trees would be...

\$4,080.00 plus removal costs.

Total Replacement costs...

\$9,520.00 plus removal costs.

If there are plans as part of the site landscaping following construction, please provide details to Forestry for consideration as on-site compensation for tree removals.

Forestry requests the opportunity to review landscape plans as related to tree planting in order review species selection and establishment procedures.

APPENDIX F – SITE PHOTOS



View along Windsor Avenue towards Lot A



View along Windsor Avenue to the north



View south along Windsor Avenue



View along alleyway to the north. Lot A (0 Windsor Ave) is on the left and Lot B (1140 Goyeau Street) is on the right.



View to the west of Lot B (1140 Goyeau Street) from the alleyway



View to the south of existing accesses to Lot A (right) and Lot B (left) from the alleyway



View looking north from the alleyway



View onto Lot B from Goyeau Street (looking southeast).



View looking north along Goyeau Street.



View looking south along Goyeau Street with Lot B on the left.



Subject: Brownfield Redevelopment Community Improvement Plan (CIP) application submitted by Farhi Holdings Corporation for 0 Riverside Drive West at Janette Avenue (Ward 3)

Reference:

Date to Council: October 7, 2024
Author: Tracy Tang, MCIP, RPP
Planner III – Economic Development
ttang@citywindsor.ca
519-255-6543 x 6449
Planning & Building Services
Report Date: September 12, 2024
Clerk's File #: Z/14427

To: Mayor and Members of City Council

Recommendation:

- I. THAT the request made by Farhi Holdings Corporation to participate in the Environmental Site Assessment Grant Program **BE APPROVED** for the completion of a Phase II Environmental Site Assessment Study and Remedial Work Plan for the property located at 0 Riverside Drive West at Janette Avenue pursuant to the City of Windsor Brownfield Redevelopment Community Improvement Plan; and,
- II. THAT the City Treasurer **BE AUTHORIZED** to issue payment up to a maximum of \$17,100 based upon the completion and submission of a Phase II Environmental Site Assessment Study and Remedial Work Plan completed in a form acceptable to the City Planner and City Solicitor; and,
- III. THAT the grant funds in the amount of \$17,100 under the Environmental Site Assessment Grant Program **BE TRANSFERRED** from the CIP Reserve Fund 226 to Brownfield Strategy Remediation (project 7069003) when the eligible work is completed to the satisfaction of the City Planner; and,
- IV. THAT should the Phase II Environmental Site Assessment Study and Remedial Work Plan not be completed within two (2) years of Council approval, the approval **BE RESCINDED** and the funds be uncommitted and made available for other applications.

Executive Summary:

N/A

Background:

Brownfield Redevelopment Community Improvement Plan (CIP)

Brownfield sites are properties that may be contaminated due to previous industrial or commercial uses such as a manufacturing facility or gas station. City Council approved a Brownfield Redevelopment CIP at its April 19, 2010 meeting for the purpose of encouraging the study, clean-up, and redevelopment of contaminated properties. The approval of the CIP was the result of nearly five years of study and consultation, which began in October 2005.

Importance of Brownfield Redevelopment

Historically, there has been little interest in redeveloping brownfield sites due to the uncertainty surrounding the extent of contamination and the potential cost of clean-up. The Brownfield Redevelopment CIP was adopted in 2010 and provides financial incentives to undertake the necessary studies and remedial work necessary to redevelop brownfield sites and reduce the potential negative impacts to the City's environment and neighbourhoods.

The benefits associated with brownfield redevelopment go far beyond the boundaries of the property. For example, they are often strategically located within existing built up areas of the City where services and other infrastructure, such as roads, schools, community facilities and public transit are already available, therefore additional infrastructure costs are not incurred to service these areas. The redevelopment of these sites also remove the negative stigma often associated with brownfield properties, which increases the value of the subject property and adjacent properties.

Site Background

The subject site at 0 Riverside Drive West at Janette Avenue consists of one property parcel located on the south-west intersection of Riverside Drive West and Janette Avenue. The property is 0.22 hectares (or 0.54 acres) in size and irregularly shaped. It is currently vacant with remnants of a parking area. The site is designated "Mixed Use (City Centre)" in the Official Plan, and is zoned Commercial District CD3.1 with special provision 20 (1) 489, which permits a multiple dwelling.

Originally, the property was occupied by a hotel and other buildings, including a grocery store. Between 1988 and 1995, the buildings on the property were demolished. In some time between 1995 and 2000, the property began operating as a parking lot and continued the use to present day. The Potentially Contaminating Activities (PCAs)

identified on the subject site include fill material of unknown quality following demolition.

The principal owner of Farhi Holdings Corporation is Shmuel Farhi. Farhi Holdings Corporation intends to redevelop the property for residential use, thus a Record of Site Condition (RSC) is required under Ontario Regulation 153/04. In recent years, Farhi Holdings Corporation has submitted Planning Act applications to facilitate the redevelopment proposal, including a Zoning By-law Amendment application (Z-017/22, ZNG/14427 and associated By-law 118-2023 passed by City Council on September 5, 2023). Through the Zoning By-law Amendment application, the requirement for an RSC was noted. The owner has now submitted the grant application and will be incurring the eligible Phase II ESA and Remedial Work Plan costs and, should the application be approved, would receive the grant payment.

Discussion:

Environmental Site Assessment Grant Program

The ESA Grant Program offers a matching grant to property owners of brownfield sites to conduct environmental studies that provide information on the type and extent of contamination and potential remediation costs. The program offers 50% of the cost of an eligible study up to a maximum of \$15,000. If two studies are required, an additional \$10,000 is available for a maximum total grant value of \$25,000.

The applicant proposes to redevelop 0 Riverside Drive West at Janette Avenue for residential use, consisting of a 28-storey, 166-unit residential building with 187 parking spaces. They require a Phase II ESA study as part of their application for an RSC. The applicant has completed a Phase I ESA, which identifies areas of potential environmental concern, and recommends that a Phase II ESA study be completed to assess the existing soil and groundwater conditions at the site. The applicant is in the process of undertaking the Phase II ESA, and is also undertaking a Remedial Work Plan which is an eligible second study under the ESA Grant Program. The findings from the Phase II ESA sampling analysis revealed that contamination is present on the site, thus remediation is required. Upon completion, the City would retain a copy of the final Phase II ESA study report.

CIP Goals

City staff is supportive of the application as it meets all of the eligibility requirements specified within the Brownfield Redevelopment CIP. The study and remediation of the subject site also supports the following CIP goals:

- To promote the remediation, rehabilitation, adaptive re-use and redevelopment of brownfield sites throughout the City of Windsor in a fiscally responsible and sustainable manner over the long term;

- Improve the physical and visual quality of brownfield sites;
- Improve environmental health and public safety;
- Provide opportunities for new housing, employment uses, and commercial uses;
- Increase tax assessment and property tax revenues;
- Promote Smart Growth, including the reduction of urban sprawl and its related costs;
- Increase community awareness of the economic, environmental and social benefits of brownfield redevelopment; and
- Utilize public sector investment to leverage significant private sector investment in brownfield remediation, rehabilitation, adaptive re-use, and redevelopment.

Policy Support

The clean up, redevelopment, and intensification of the site is supported by numerous policies within the 2020 Provincial Policy Statement, the City's Official Plan, and the City's Environmental Master Plan.

Risk Analysis:

As with all brownfield sites, there is a degree of risk associated related to the potential presence of contamination. In this case, there is also a risk of the property remaining in a vacant state, which negatively affects the surrounding properties. The studies and remediation will assist in mitigating these risks. The City would retain a copy of the studies for future reference.

Climate Change Risks

Climate Change Mitigation:

The proposed residential redevelopment implements Environmental Master Plan Objective C1: Encourage in-fill and higher density in existing built areas. In particular, the redevelopment would implement the action that supports the existing Brownfields Redevelopment Strategy and achieve its work plan.

Climate Change Adaptation:

The redevelopment of the existing vacant property may be affected by climate change, in particular with respect to extreme precipitation and an increase in days above 30 degrees. While not the subject of this report, any new construction would be required to

meet the current provisions of the Building Code, which would be implemented through the building permit process.

Financial Matters:

The cost estimate (excluding HST) for completing the Remedial Work Plan is \$24,100. The cost estimate (excluding HST) for completing the Phase II ESA study is \$10,100. If approved, the maximum grant would total \$17,100 (\$12,050 for the first study and \$5,050 for the second study). Should the actual costs of the studies be less than what has been estimated, the grant payments would be based on the lower amount.

If approved, the grant would be paid from the Brownfield Strategy Remediation Fund (Project #7069003). The funds would be transferred from CIP reserve fund 226 for payment when the eligible studies are complete. The current uncommitted balance of the CIP reserve fund is \$299,916 however this balance does not account for other CIP grant requests that are currently being considered by the standing committee or have been endorsed by the standing committee and are not yet approved by City Council.

The applicant has also expressed interest in applying for incentives under the Downtown Windsor CIP, specifically the Building/Property Improvement Tax Increment Grant Program and the New Residential Development Grant Program, in the near future.

Consultations:

The development and approval of the Brownfield Redevelopment CIP was subject to extensive stakeholder and public consultation, which sought input from a wide range of stakeholders and internal City departments.

Planning staff have consulted with the applicant's agent from Dillon Consulting prior to accepting the Brownfield Redevelopment CIP application. Greg Atkinson, Manager of Development, Planning and Building Services Department; Josie Gualtieri, Financial Planning Administrator, Finance Department; and Kate Tracey, Senior Legal Counsel, Legal Department were consulted in the preparation of this report.

Conclusion:

City Staff recommend Council approve the request from Farhi Holdings Corporation to participate in the Environmental Site Assessment Grant Program. In the opinion of planning staff, the study of this brownfield site conforms to the Brownfield Redevelopment CIP and assists the City in the achievement of a number of CIP, Official Plan, Community Energy Plan, and Environmental Master Plan goals.

Planning Act Matters:

N/A

Approvals:

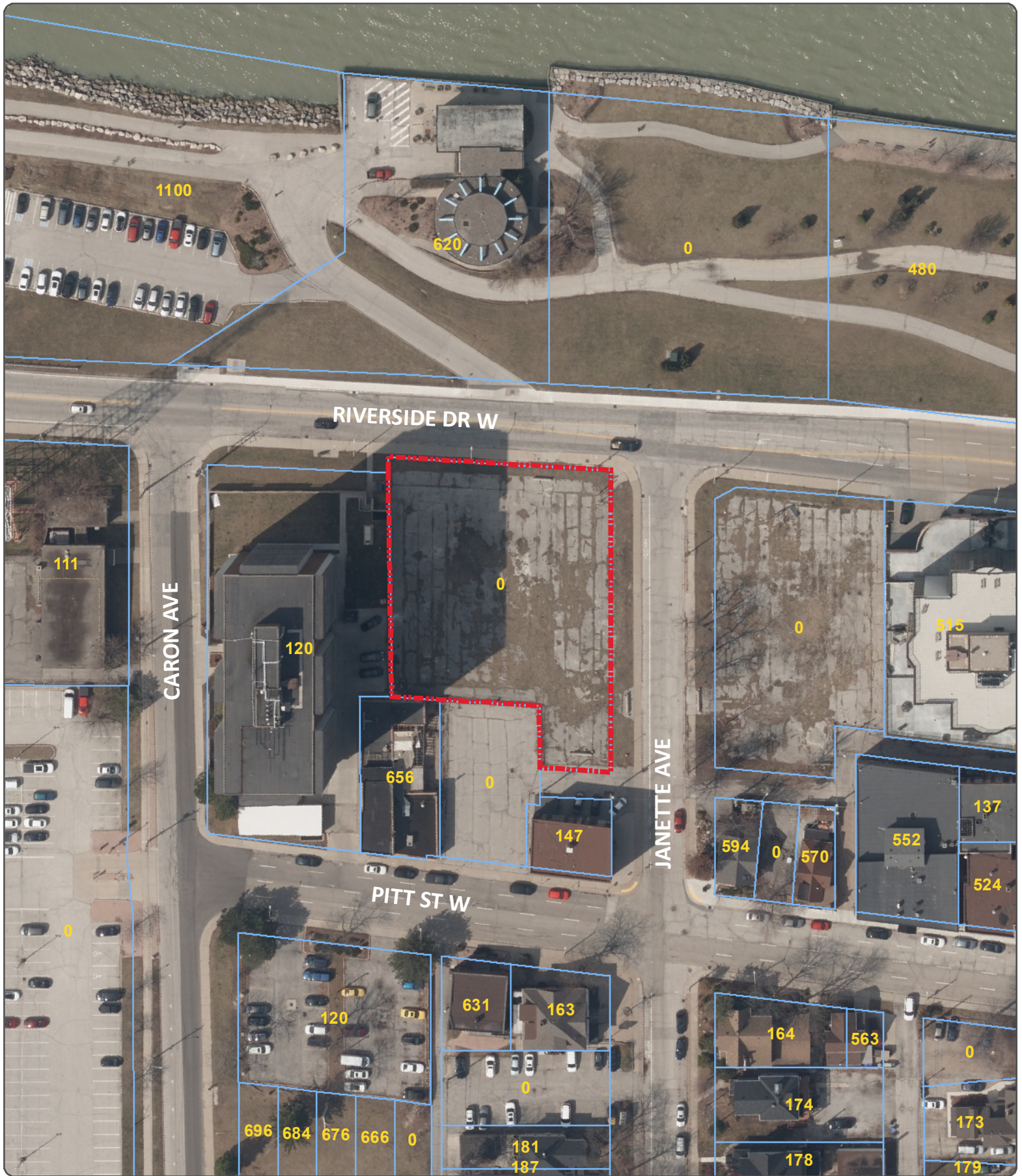
Name	Title
Emilie Dunnigan	Manager Development Revenue & Financial Administration
Jason Campigotto	Deputy City Planner – Growth (Acting)
Neil Robertson	City Planner
Kate Tracey	Senior Legal Counsel, Legal Services & Real Estate
Janice Guthrie	Commissioner, Finance & City Treasurer
Jelena Payne	Commissioner, Economic Development
Joe Mancina	Chief Administrative Officer

Notifications:

Name	Address	Email
Theresa O'Neill		
Matthew Antaya		

Appendices:

- 1 Appendix A - Location Map



LOCATION MAP : 0 RIVERSIDE DRIVE WEST



SUBJECT PROPERTY





Council Report: S 124/2024

**Subject: Economic Revitalization Community Improvement Plan (CIP)
application submitted by Agri-Box Inc. for 3324 Marentette Avenue
(Ward 9)**

Reference:

Date to Council: October 7, 2024
Author: Tracy Tang, MCIP, RPP
Planner III – Heritage (A)
ttang@citywindsor.ca
519-255-6543 x 6449
Planning & Building Services
Report Date: September 12, 2024
Clerk's File #: SPL2024

To: Mayor and Members of City Council

Recommendation:

- I. THAT the request made by Agri-Box Inc. to participate in the Small Business Investment Grant Program **BE APPROVED** for 100% of the municipal portion of the tax increment resulting from the proposed development of eligible uses located at 3324 Marentette Avenue for up to 10 years or until 100% of the eligible costs are repaid pursuant to the City of Windsor Economic Revitalization Community Improvement Plan.
- II. THAT Administration **BE DIRECTED** to prepare an agreement between the City and Agri-Box Inc. to implement the Small Business Investment Grant Program in accordance with all applicable policies, requirements, and provisions contained within the Economic Revitalization Community Improvement Plan to the satisfaction of the City Planner for content, the City Solicitor as to form, and the City Treasurer as to financial content.
- III. THAT the CAO and City Clerk **BE AUTHORIZED** to sign the Small Business Investment Grant Agreement.

- IV. THAT the approval to participate in the Small Business Investment Grant Program **EXPIRE** if the grant agreement is not signed by the applicant within two years following Council approval.

Executive Summary: N/A

Background:

City Council approved the Economic Revitalization Community Improvement Plan (CIP) at its January 31, 2011, meeting via CR 50/2011. The adopting By-law 30-2011 was passed by Council at its February 14, 2011, meeting.

The Economic Revitalization CIP provides financial incentives to encourage new investment in targeted economic sectors for the purposes of diversifying the local economy and creating/retaining jobs. The CIP allows the City to take a variety of measures to further the objectives of the Economic Revitalization CIP that would otherwise be prohibited by Ontario's *Municipal Act*. This includes the acquisition and preparation of land; construction, repair, rehabilitation or improvement of buildings; the sale, lease or disposal of land and buildings; and the provision of grants to owners or tenants of land—all of which must conform with the objectives and policies contained within the CIP.

To date, City Council has approved a number of applications made under the CIP representing a range of targeted economic sectors including manufacturing, research and development, creative industries, logistics, health & life sciences, and tourism.

Agri-Box Inc. (principal owner Tony Liu) has applied for financial incentives under the Small Business Investment Grant Program for the property located at 3324 Marentette Avenue (see Appendix 'A' - Location Map). Agri-Box Inc. also owns and operates the adjacent properties located at 3350 Devon Drive (vacant) and 0 Marentette Avenue (vacant).

The subject property is 3.29 hectares (8.14 acres) in size, designated 'Industrial' in the City's Official Plan Schedule D: Land Use, and zoned Manufacturing District MD1.1, which permits a range of light industrial and service commercial uses. Currently there is a vacant two-storey +200,000 square foot industrial building with truck loading docks and asphalt parking lots on the subject property. The building has remained vacant for almost a decade.

Discussion:

Small Business Investment Grant Program

The Small Business Investment Grant Program is intended to stimulate investment in targeted economic sectors by small businesses that create or retain jobs in Windsor. Manufacturing businesses with less than 50 employees are eligible to apply—provided an investment results in an increase of at least \$25,000 to the assessed value of a property.

Successful applicants are eligible to receive an annual grant for up to 100% of the municipal property tax increase created by an investment in development or redevelopment of a building or property—provided it conforms with the Economic Revitalization CIP. The annual grants may continue, at Council's discretion, for up to 10 years or until up to 100% of the eligible investment costs are repaid.

Proposed Redevelopment

The applicant proposes to refurbish and revitalize the entire existing vacant building for the purpose of manufacturing packaging for the agri-food industry. The entire +200,000 square foot building is intended to be occupied, with a combination of office area, warehousing area, and manufacturing area. The refurbishment consists of significant interior property upgrades including a new fire sprinkler system, HVAC, plumbing, electrical, lighting, roofing, and floors. The applicant also plans to improve the existing exterior façade by having some portions cleaned, repaired, and painted and some portions covered with new cladding materials. All exterior windows and doors will be replaced with new. The grant calculations are based on the gross floor area which the eligible use is proposed to occupy.

Eligible Sector

The proposed manufacturing use of 3324 Marentette Avenue falls under the eligible Manufacturing sector, which is defined as:

Manufacturing

Companies engaged in the fabricating, processing, assembling, packaging, producing or making goods or commodities, including ancillary repair, storage, wholesaling or office uses.

Employment

According to the CIP application, Agri-Box Inc. will retain their 5 current employees and hire 80 new full-time employees.

CIP Objectives

The proposed refurbishment and revitalization of the vacant industrial building located at 3324 Marentette Avenue and recommended Small Business Investment Grant supports the following CIP objectives:

- Encourage investment that results in the productive use of lands and/or buildings for the purposes of establishing or maintaining a business enterprise, or the expansion of existing businesses to realize more effective use of the land's potential;
- Encourage capital investments that create new and/or maintain existing permanent jobs, as well as short-term construction jobs that contribute to the reduction of the unemployment rate;

- Attract investment based on the community's strengths and competitive advantages;
- Support investments in specified high potential economic sectors that contribute to the diversification of the local economy;
- Facilitate the development of the City's vacant employment lands and other areas that have the potential to be new employment areas;
- Provide financial incentive programs that are attractive to potential investors and corporate decision-makers, but are balanced with expectations of City taxpayers and the City's ability to fund the financial incentive programs; and
- Support investment and development that results in an increase in property assessment and grows the non-residential municipal tax base over the long-term.

Risk Analysis:

There is little risk associated with the approval of the CIP application. Staff resources are required for the upfront administration of the grant program and finalization of the legal agreement. Limited staff resources related to on-going monitoring of the eligible employment uses and issuance of annual grants will also be required over the next ten years. Should Council refuse the CIP request, there is a risk that Agri-Box Inc. may not proceed with the proposed revitalization project and the building will remain vacant or may be demolished.

Climate Change Risks

Climate Change Mitigation:

The proposed refurbishment and revitalization of the existing vacant industrial building implements Environmental Master Plan Objective C1: Encourage in-fill and higher density in existing built areas.

Climate Change Adaptation:

The proposed refurbishment and revitalization of the existing vacant industrial building may be affected by climate change, in particular with respect to extreme precipitation and an increase in days above 30 degrees. While not the subject of this report, any new construction would be required to meet the current provisions of the Building Code, which would be implemented through the building permit process. The site would also be required to incorporate storm water management best practices. Any site plan control application will be reviewed for opportunities to enhance resiliency.

Financial Matters:

Small Business Investment Grant Program

The tax increment portion of the Small Business Investment Grant is not calculated or paid out until all eligible work is completed and the property is reassessed by MPAC.

Reassessment of the property must result in an increase of at least \$25,000 in assessment value. The grant amount is recalculated annually based on the actual assessed property value, tax class, and municipal tax rate.

Summary of Potential Financial Incentives

The applicant proposes to spend a total of approximately \$6,000,000 on the project, with \$3,220,000 being eligible costs under the CIP program. The current assessment value for the property is \$2,381,000 and the annual property taxes are \$111,095 with the municipal share being \$90,143.

City staff anticipate the post-development assessment value of the building to be approximately \$7,000,000. Total annual property taxes on the increased assessment value would be \$331,966 – an increase of \$220,871. The post-development annual municipal tax levy would be \$270,366 – an increase of \$180,223. This would result in a total grant value of \$1,802,230 over the lifespan of the 10-year grant program and would offset approximately 56% of the eligible investment proposed by Agri-Box Inc.

Because the Small Business Investment Grant Program does not cancel taxes, the applicant must pay the full amount of property taxes annually and will subsequently receive a grant for up to 100% of the difference between the pre- and post-development municipal taxes. The City will retain the amount of pre-development (base) municipal taxes of \$90,143 per annum throughout the lifespan of the grant program, however, will be foregoing any incremental property taxes which could otherwise be used to offset future budget pressures.

Consultations:

The Economic Revitalization CIP was subject to extensive stakeholder and public consultation as part of the approval process, including two public open houses, a statutory public meeting of Council and circulation among internal City staff and the Province.

Planning staff have consulted with the applicant, the applicant's agent, and the applicant's architect prior to accepting the application for the Small Business Investment Grant Program. Greg Atkinson, Manager of Development, Planning and Building Services Department; Jose Mejalli, Assessment Management Officer, Taxation & Financial Projects; Carolyn Nelson, Manager of Property Valuation & Administration, Taxation & Financial Projects; and Kate Tracey, Senior Legal Counsel, Legal Department were consulted in the preparation of this report.

Conclusion:

Administration recommends that Council approve the request made by Agri-Box Inc. to participate in the Small Business Investment Grant Program. Specifically, that 100% of the municipal portion of the tax increment resulting from the proposed refurbishment and revitalization located at 3324 Marentette Avenue be provided as an annual grant for

up to 10 years or until 100% of the eligible costs are repaid pursuant to the City of Windsor Economic Revitalization CIP.

It is also recommended that approval to participate in the CIP expire if the grant agreement is not signed within two years following Council approval. The planned refurbishment and revitalization conforms with the Economic Revitalization CIP and assists the City in the achievement of a number of the CIP objectives.

Planning Act Matters: N/A

Approvals:

Name	Title
Jason Campigotto	Deputy City Planner – Growth (ACTING)
Neil Robertson	City Planner
Kate Tracey	Senior Legal Counsel, Legal Services & Real Estate
Lorie Gregg	Deputy Treasurer, Taxation & Financial Projects
Lorie Gregg	On behalf of Commissioner, Finance & City Treasurer
Jelena Payne	Commissioner, Economic Development
Joe Mancina	Chief Administrative Officer

Notifications:

Name	Address	Email
Tony Liu, Agri-Box Inc.		
Juan Tao		
Bill MacArthur		
James Thoman		

Appendices:

- 1 Appendix A - Location Map
- 2 Appendix B - Application Overview



LOCATION MAP: 3324 MARENTETTE AVENUE



Ms. Tracy Tang, Planner II – Revitalization & Policy Initiatives
Planning & Building Services
350 City Hall Square West | Suite 320 | Windsor, ON | N9A 6S1
July 24, 2024

Re.: Agri-Box Inc. Application for Economic Revitalization CIP Grant

Dear Tracy,

I write on behalf of Agri-Box Inc. as an authorized agent regarding the application for an Economic Revitalization CIP Grant. We are requesting that Agri-Box obtains a freeze on property tax increases over the next 10 years to help offset the cost of renovating the firm's building at 3324 Marentette Ave. in Windsor.

The Business Proposal

Agri-Box purchased this vacant building with the intent to refurbish and re-purpose for manufacturing of packaging for the agri-food industry using recycled plastics and paper. Products will include clamshells, trays and bowls, and paper cups. Approximately, 45% of the area of the building will be used for manufacturing and 40% for warehousing. The remaining 15% will be used for administration and logistics. Target customers include the local greenhouse industry and agri-food firms in the United States. The location close to existing border crossings and the new Gordie Howe bridge will facilitate exports to the US as well as internal Canadian sales.

The City's financial assistance will facilitate the hiring of 50+ new employees in manufacturing and support operations. Most of the new employees will be production operators and support staff. The firm will have engineering, logistics, and accounting personnel as well.

The Planned Refurbishment

This building has been abandoned for approximately 10 years and has deteriorated badly from lack of maintenance. It has also been the recipient of vandalism in various forms. Significant property upgrades are required and are composed of two parts: the interior and exterior. The interior improvements will facilitate ongoing business. This entails major changes and replacements to the fire sprinkler system, HVAC, plumbing, and electrical systems, a new roof, lighting, adding fibre optic cables, as well as floor upgrades and more to facilitate manufacturing. Exterior improvements including improvements to the façade and painting as well as items detailed by drawings provided by Architect Stuart Miller will reverse the air of decay that this industrial park now faces. The firm will be building offices for managerial and support staff. Total project estimates are \$6,000,000 of which we believe \$3,220,000 are eligible costs. These include \$3,000,000 for Rehabilitation and Renovation work, \$60,000 for demolition work, \$50,000 for

building and related fees, \$20,000 for upgrading onsite infrastructure, and \$50,000 for upgrading offsite infrastructure.

Effect of Refurbishment

The firm is making a major investment in revitalizing an essentially abandoned building which has been underutilized and neglected for an extended period, resulting in significant deferred maintenance and infrastructure deficiencies. The required upgrades and renovations incur substantial costs that exceed the current financial capacity of the project stakeholders. Without Agri-Box's purchase and investment this building most likely would continue to deteriorate and eventually would have to be demolished. Agri-Box's investment and the City of Windsor's requested investment will ensure that the building is refurbished and remains viable for use for decades to come. Not only that, but a large, refurbished building with curb appeal will make the area more attractive to other businesses, provide the area with a sense of renewal, and in turn encourage other businesses to locate, renovate and remain. Ultimately, it will strengthen the tax base by solidifying the industrial area, the local merchants, and the local residential areas.

Conclusion

The refurbishment of 3324 Marentette by Agri-Box should be encouraged by the City of Windsor since it will help revitalize a building and an area in need of renewal, create spin-off benefits from the 50+ jobs created by Agri-Box, and encourage other firms to move to the industrial park or stay and renovate. We humbly request that the CIP application be approved as submitted.

Please consider our application favourably and contact us if you have any questions.

Best regards,

William MacArthur, B. Eng., B. Comm.
President
M-TEAMS
1583923 Ontario Inc.
1695 Gladstone Avenue
Windsor, Ontario N8W 2N2 Canada
Mailing Addr: 1695 Gladstone Ave
Telephone: (519) 567-0918
Email: billmac@mteams.net



Council Report: S 117/2024

Subject: Amendment to CR58/2021, as amended by CR285/2023 for Closure of east/west alley located between Matthew Brady Boulevard and east limit of 7730 St. Rose Avenue, Ward 6, SAA-5947

Reference:

Date to Council: October 7, 2024
Author: Brian Nagata, MCIP, RPP
Planner II - Development Review
(519) 255-6543 ext. 6181

Planning & Building Services
Report Date: September 10, 2024
Clerk's File #: SAA2024

To: Mayor and Members of City Council

Recommendation:

- I. THAT CR58/2021, adopted on February 1, 2021, as amended by CR285/2023, adopted on July 10, 2023, **BE AMENDED** as follows:

By **DELETING** the following from Resolution IV in its entirety:

“, subject to the following:

- a. Easement, subject to there being accepted in the City's standard form and in accordance with the City's standard practice, be granted to:
 - i. Owner of the property known municipally as 7720 St. Rose Avenue (legally described as Lot 50, Plan 1181) to enter, construct, maintain, inspect, alter, repair, remove, replace, reconstruct and enlarge the existing private sanitary sewer; and
 - ii. Owner of the property known municipally as 7730 St. Rose Avenue (legally described as Lot 49, Plan 1181) to enter, construct, maintain, inspect, alter, repair, remove, replace, reconstruct and enlarge the existing private sanitary sewer.”

Executive Summary:

N/A



STREET & ALLEY CLOSING (SAA/5947)

APPLICANT : P. THWAITES

 SUBJECT LAND

PLANNING DEPARTMENT - DEVELOPMENT DIVISION

DATE: OCTOBER, 2020

1:500



Background:

CR58/2021 was adopted by Council on February 1, 2021, directing administration to undertake the necessary steps to assume, close and convey the following alley:

- 4.86-metre-wide east/west alley located between Matthew Brady Boulevard and east limit of 7730 St. Rose Avenue (the alley).

CR285/2023 was adopted by Council on July 10, 2023, to amend CR58/2021 to add a condition requiring an easement to be granted in favour of 7720 St. Rose Avenue and 7730 St. Rose Avenue for access to maintain and repair the existing private sanitary sewer within the alley before its closure and conveyance.

The Planning Department emailed the abutting three property owners (884 Matthew Brady Boulevard, 7720 St. Rose Avenue and 7730 St. Rose Avenue) on August 15, 2024, requesting their lawyer's information for executing the said easement. The owner of 884 Matthew Brady Boulevard confirmed that they are not retaining a lawyer. The Planning Department advised the abutting property owners that the execution of the easement is dependent on all parties retaining a lawyer. The Planning Department further advised the abutting property owners that without an easement, all matters pertaining to the private sanitary sewer will become a civil matter after the alley is closed and conveyed. The owner of 884 Matthew Brady Boulevard accepted responsibility for granting access to the other two property owners for maintenance and repair of the private sanitary sewer.

Discussion:

CR58/2021, as amended by CR285/2023, must be further amended to remove the said easement to allow for the closure and conveyance of the alley

Risk Analysis:

The recommended amendment to CR58/2021, as amended by CR285/2023 poses no known risk to the City.

Climate Change Risks**Climate Change Mitigation:**

N/A

Climate Change Adaptation:

N/A

Financial Matters:

The recommended amendment to CR58/2021, as amended by CR285/2023 does not impact the conveyance cost.

Consultations:

N/A

Conclusion:

The Planning Department recommends that CR285/2023 be amended to remove the said easement as in Recommendation I of this report.

Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Greg Atkinson, MCIP, RPP
Manager of Development

Neil Robertson, MCIP, RPP
City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

Approvals:

Name	Title
Brian Nagata	Planner II - Development Review
Greg Atkinson	Manager of Development/Deputy City Planner
Neil Robertson	City Planner / Executive Director, Planning & Development Services
Kate Tracey	Senior Legal Counsel, Legal Services & Real Estate
Jelena Payne	Commissioner, Economic Development
Joe Mancina	Chief Administration Officer

Notifications:

Name	Address	Email
Ward 6 Councillor Jo-Anne Gignac	350 City Hall Square West, Suite 220 Windsor, Ontario N9A 6S1	joagignac@citywindsor.ca
List of mailing labels for property owners abutting the alley issued to Clerks office		

Appendices:



Council Report: S 118/2024

Subject: Amendment to CR419/2023 for Closure of east/west alley between Dieppe Street and Genevieve Avenue, Ward 6, SAA-6844

Reference:

Date to Council: October 7, 2024
Author: Brian Nagata, MCIP, RPP
Planner II - Development Review
(519) 255-6543 ext. 6181

Planning & Building Services
Report Date: September 10, 2024
Clerk's File #: SAA2024

To: Mayor and Members of City Council

Recommendation:

I. THAT CR419/2023, adopted on October 16, 2023, **BE AMENDED** as follows:

By **DELETING** the following from Resolution V:

“, subject to the following:

- a. Easement over that portion of the subject east/west alley abutting 8415 Riverside Drive East, subject to there being accepted in the City's standard form and in accordance with the City's standard practice, be granted to:
 - i. The owner of 8415 Riverside Drive East for access to maintain their hedgerow bordering the said portion of the subject east/west alley”

Executive Summary:

N/A



STREET & ALLEY CLOSING (SAA/6844)

1:1,000

APPLICANT : CLAIRE OFNER AND JACK TOBIN

 SUBJECT LANDS

PLANNING DEPARTMENT - PLANNING POLICY

DATE: AUGUST, 2022



Background:

CR419/2023 was adopted by Council on October 16, 2023, directing administration to undertake the necessary steps to assume, close and convey the following alley:

- 4.88-metre-wide east/west alley located between Dieppe Street and Genevieve Avenue (the alley).

CR419/2023 included a condition requiring an easement to be granted in favour of 8415 Riverside Drive East for access to maintain the hedgerow bordering the alley. The condition was included at the request of the owner of 8415 Riverside Drive East.

The Planning Department emailed and telephoned the abutting two property owners (244 Dieppe Street & 8415 Riverside Drive East, respectively) on August 14, 2024, requesting their lawyer's information for executing the said easement. The owner of 8415 Riverside Drive East confirmed that they are not retaining a lawyer. The Planning Department advised the abutting property owners that the execution of the easement is dependent on all parties retaining a lawyer. The Planning Department further advised the abutting property owners that without an easement, all matters pertaining to the hedgerow become a civil matter after the alley is closed and conveyed. The owner of 8415 Riverside Drive East maintained their position and confirmed that they are now satisfied with 244 Dieppe Street's original unofficial verbal agreement to allow them access to maintain their hedgerow.

Discussion:

CR419/2023 must be amended to remove the said easement to allow for the closure and conveyance of the alley.

Risk Analysis:

The recommended amendment to CR419/2023 poses no known risk to the City.

Climate Change Risks**Climate Change Mitigation:**

N/A

Climate Change Adaptation:

N/A

Financial Matters:

The recommended amendment to CR419/2023 does not impact the conveyance cost.

Consultations:

N/A

Conclusion:

The Planning Department recommends that CR419/2023 be amended to remove the said easement as in Recommendation I of this report.

Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Greg Atkinson, MCIP, RPP
Manager of Development

Neil Robertson, MCIP, RPP
City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP

JM

Approvals:

Name	Title
Brian Nagata	Planner II - Development Review
Greg Atkinson	Manager of Development/Deputy City Planner
Neil Robertson	City Planner / Executive Director, Planning & Development Services
Kate Tracey	Senior Legal Counsel, Legal Services & Real Estate
Jelena Payne	Commissioner, Economic Development
Joe Mancina	Chief Administration Officer

Notifications:

Name	Address	Email
Ward 6 Councillor Jo-Anne Gignac	350 City Hall Square West, Suite 220 Windsor, Ontario N9A 6S1	joagignac@citywindsor.ca
List of mailing labels for property owners abutting the alley issued to Clerks office		

Appendices:



Council Report: S 119/2024

Subject: Amendment to CR437/2023 for Closure of north/south alley located between Taylor Avenue and 1124-1224 Campbell Avenue, Ward 2, SAA-6848

Reference:

Date to Council: October 7, 2024
Author: Brian Nagata, MCIP, RPP
Planner II - Development Review
(519) 255-6543 ext. 6181

Planning & Building Services
Report Date: September 10, 2024
Clerk's File #: SAA2024

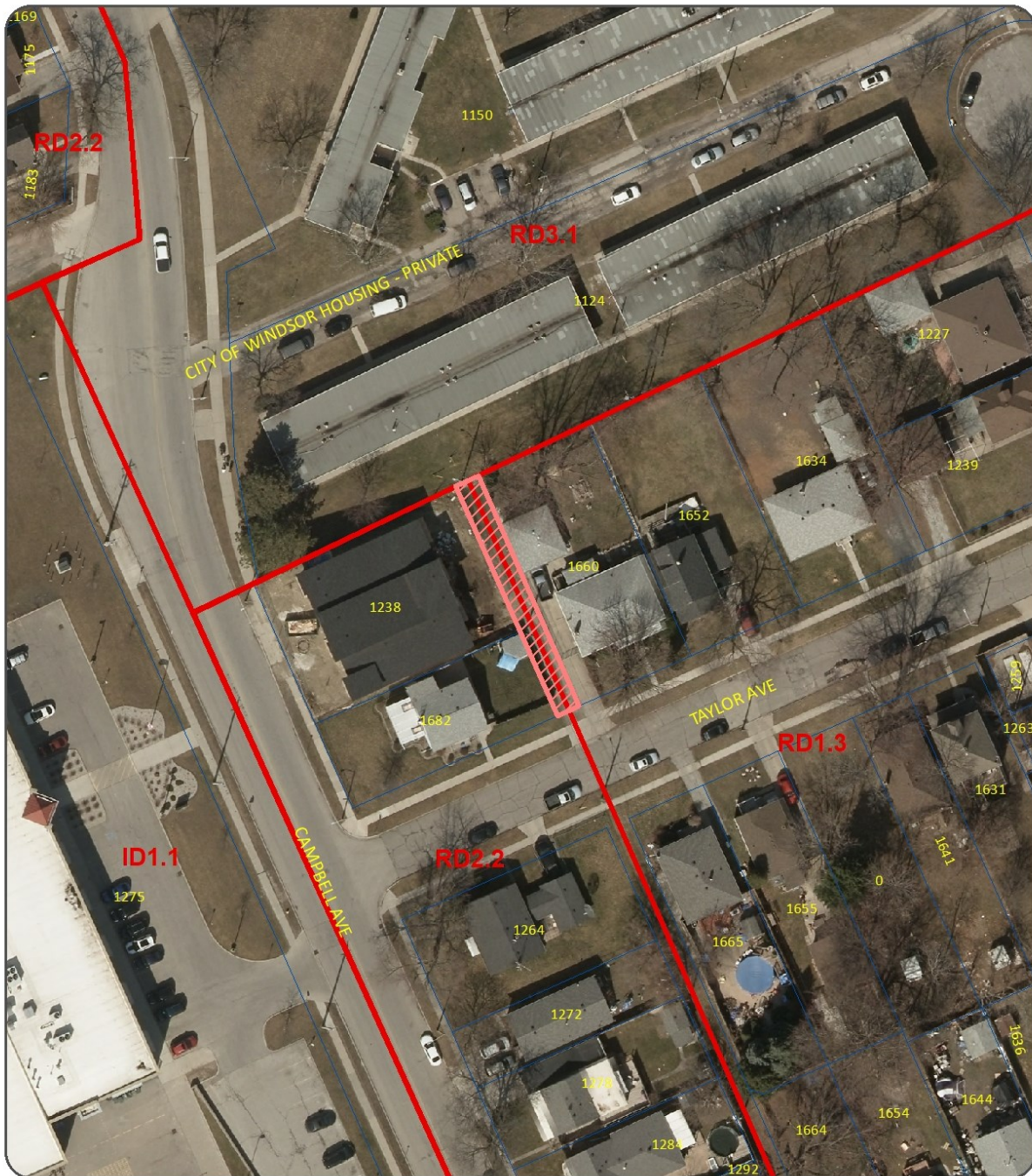
To: Mayor and Members of City Council

Recommendation:

- I. THAT CR437/2023, adopted on October 30, 2023, **BE AMENDED** by **DELETING** subsection b) of Resolution II in its entirety.

Executive Summary:

N/A



STREET & ALLEY CLOSING (SAA/6848)

1:750

APPLICANT : DAVID TRAN & KENG FU

 SUBJECT LANDS

PLANNING DEPARTMENT - PLANNING POLICY

DATE: JULY, 2023



Background:

CR437/2023 was adopted by Council on October 30, 2023, directing administration to undertake the necessary steps to assume, close and convey the following alley:

- 3.66-metre-wide north/south alley located between Taylor Avenue and 1124-1224 Campbell Avenue (the alley).

CR437/2023 included a condition requiring an easement to be granted in favour of 1660 Taylor Avenue for access to repair and maintain the west face of the existing detached garage bordering the alley.

Administration has since concluded that easements in favour of private property owners should not be included as a condition of closure and conveyance of an alley.

The City should not be concerned about private property within an alley unless there is an encroachment agreement in place. Further, the City should not be concerned about buildings or structures bordering an alley unless they meet one of the criteria under the City's *Classification of Alleys and Suitability for Closure* guideline document and were established legally by a Building Permit.

A property owner can submit a [Right-of-Entry Permit Application](#) to the Building Department to request access to an abutting property to maintain or repair a building or structure when denied such access by the adjoining property owner.

Discussion:

CR437/2023 must be amended to remove the said easement to allow for the closure and conveyance of the alley.

Risk Analysis:

The recommended amendment to CR437/2023 poses no known risk to the City.

Climate Change Risks**Climate Change Mitigation:**

N/A

Climate Change Adaptation:

N/A

Financial Matters:

The recommended amendment to CR437/2023 does not impact the conveyance cost.

Consultations:

N/A

Conclusion:

The Planning Department recommends that CR437/2023 be amended to remove the said easement as in Recommendation I of this report.

Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Greg Atkinson, MCIP, RPP
Manager of Development

Neil Robertson, MCIP, RPP
City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP

JM

Approvals:

Name	Title
Brian Nagata	Planner II - Development Review
Greg Atkinson	Manager of Development/Deputy City Planner
Neil Robertson	City Planner / Executive Director, Planning & Development Services
Kate Tracey	Senior Legal Counsel, Legal Services & Real Estate
Jelena Payne	Commissioner, Economic Development
Joe Mancina	Chief Administration Officer

Notifications:

Name	Address	Email
Ward 2 Councillor Fabio Costante	350 City Hall Square West, Suite 220 Windsor, Ontario N9A 6S1	fcostante@citywindsor.ca
List of mailing labels for property owners abutting the alley issued to Clerks office		

Appendices:



Council Report: S 120/2024

Subject: Amendment to CR26/2024 for Conveyance of north/south alley located between Closed Manitoba Street right-of-way and 1954 Huron Church Road, Ward 10, Closed by Judge's Order No. 2970/87

Reference:

Date to Council: October 7, 2024
Author: Brian Nagata, MCIP, RPP
Planner II - Development Review
(519) 255-6543 ext. 6181

Planning & Building Services
Report Date: September 10, 2024
Clerk's File #: SAA2024

To: Mayor and Members of City Council

Recommendation:

- I. THAT CR26/2024, adopted on January 15, 2024, **BE AMENDED** by **DELETING** subparagraphs ii), iii) and iv) under subsection a) of Resolution I in their entirety.

Executive Summary:

N/A



STREET & ALLEY CLOSING - 1800/1900 BLOCK BETWEEN HURON CHURCH ROAD & DAYTONA AVENUE

1:750

PROPOSED CONVEYANCE (ALLEY)

APPLICANT: CITY OF WINDSOR

PLANNING DEPARTMENT - DEVELOPMENT DIVISION

DATE: OCTOBER, 2023



Background:

CR26/2024 was adopted by Council on January 15, 2024, directing administration to undertake the necessary steps to convey the following alley:

- 4.27-metre-wide north/south alley located between the Closed Manitoba Street right-of-way and 1954 Huron Church Road (the alley).

CR26/2024 included a condition requiring the following easements to be granted in favour of the abutting properties:

- 0 Huron Church Road (legally described as Lot 3 and Part of Lot 4 & Closed Alley, Plan 997; Parts 1, 3 & 4, RP 12R-17003) to accommodate the privately owned southerly utility pole and overhead wires serving the standalone billboard on the said property;
- 0 Huron Church Road (legally described as Lot 11, Plan 948) to accommodate the privately owned northerly utility pole and overhead wires serving the two (2) standalone billboards on the said property; and
- 1954 Huron Church Road to accommodate the aforesaid southerly utility pole and overhead wires serving the motel (Bestway Motel) on the said property.

Administration has since concluded that easements in favour of private property owners should not be included as a condition of closure and conveyance of an alley.

The City should not be concerned about private property within an alley unless there is an encroachment agreement in place.

A property owner can submit a [Right-of-Entry Permit Application](#) to the Building Department to request access to an abutting property to maintain or repair privately owned utility infrastructure when denied such access by the adjoining property owner.

Discussion:

CR26/2024 must be amended to remove the said easements to allow for the conveyance of the alley.

Risk Analysis:

The recommended amendment to CR26/2024 poses no known risk to the City.

Climate Change Risks**Climate Change Mitigation:**

N/A

Climate Change Adaptation:

N/A

Financial Matters:

The recommended amendment to CR26/2024 does not impact the conveyance cost.

Consultations:

N/A

Conclusion:

The Planning Department recommends that CR26/2024 be amended to remove the said easements as in Recommendation I of this report.

Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Greg Atkinson, MCIP, RPP
Manager of Development

Neil Robertson, MCIP, RPP
City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP

JM

Approvals:

Name	Title
Brian Nagata	Planner II - Development Review
Greg Atkinson	Manager of Development/Deputy City Planner
Neil Robertson	City Planner / Executive Director, Planning & Development Services
Kate Tracey	Senior Legal Counsel, Legal Services & Real Estate
Jelena Payne	Commissioner, Economic Development
Joe Mancina	Chief Administration Officer

Notifications:

Name	Address	Email
Ward 10 Councillor Jim Morrison	350 City Hall Square West, Suite 220 Windsor, Ontario N9A 6S1	jmorrisson@citywindsor.ca
List of mailing labels for property owners abutting the alley issued to Clerks office		

Appendices:



Council Report: S 121/2024

Subject: Closure of east/west alley located between Alexis Road and Chandler Road, Ward 5, SAA-7197

Reference:

Date to Council: October 7, 2024
Author: Brian Nagata, MCIP, RPP
Planner II - Development Review
(519) 255-6543 ext. 6181

Planning & Building Services
Report Date: September 10, 2024
Clerk's File #: SAA2024

To: Mayor and Members of City Council

Recommendation:

- I. THAT the 5.59-metre-wide east/west alley located between Alexis Road and Chandler Road, and shown on Drawing No. CC-1851 (attached hereto as Appendix "A"), and hereinafter referred to as the "subject alley", **BE ASSUMED** for subsequent closure;
- II. THAT the subject alley **BE CLOSED AND CONVEYED** in **as is condition** to the abutting property owners and as necessary, in a manner deemed appropriate by the City Planner, subject to the following:
 - a. Easements, subject to being accepted in the City's standard form and in accordance with the City's standard practice, being granted to:
 - i. Bell Canada to accommodate existing aboveground infrastructure;
 - ii. Cogeco Connexion Inc. to accommodate existing aboveground infrastructure;
 - iii. ENWIN Utilities Ltd. to accommodate the existing overhead hydro distribution;
 - iv. Managed Network System Inc. (MNSi.) to accommodate existing aboveground infrastructure;
 - v. The Corporation of the City of Windsor to accommodate existing 250-millimetre PVC sanitary sewer.
- III. THAT Conveyance Cost **BE SET** as follows:
 - a. For alley conveyed to abutting lands zoned CD2.1, \$20.00 per square foot without easements plus HST (if applicable) and \$10.00 per square foot with easements plus HST (if applicable).

- b. For alley conveyed to abutting lands zoned RD1.2, \$1.00 plus HST (if applicable) and deed preparation fee.
- IV. THAT The City Planner **BE REQUESTED** to supply the appropriate legal description, in accordance with Drawing No. CC-1851.
- V. THAT The City Solicitor **BE REQUESTED** to prepare the necessary by-law(s).
- VI. THAT The Chief Administrative Officer and City Clerk **BE AUTHORIZED** to sign all necessary documents approved as to form and content satisfactory to the City Solicitor.
- VII. THAT the matter **BE COMPLETED** electronically pursuant to By-law Number 366-2003

Executive Summary:

N/A

Background:

The Operations Department applied to close 5.59-metre-wide east/west alley located between Alexis Road and Chandler Road (the alley) and shown on Drawing No. CC-1851 attached hereto as Appendix “A” and shown on the aerial photo attached hereto as Appendix “B”.

The Operations Department wishes to close the alley due to its maintenance demand being very high and outside the allotted budget. The Operations Department notes that it will be more cost effective for the City to dispose of the alley.

The alley is unmaintained and composed primarily of gravel bordered partially with grass and trees. The alley contains a catch basin, curb cuts off Alexis Road and Chandler Road, overhead hydro and utility lines, 250-millimetre PVC sanitary sewer and utility poles with guy wires and anchors. The alley is bordered by four (4) privately owned properties under common ownership (the subject property). The subject property utilizes the alley for pedestrian and vehicular access as described in the Discussion section of this report. The alley has been encroached on over the years by the subject property with chain-link fences. There are no Encroachment Agreements on record for the use of the alley.

The City has budgeted money to grade its gravel alleys twice a year, which is the extent of maintenance provided. The Operations Department receives several complaints annually from the owner of the subject property around such issues as grading, accessibility, etc. The subject property's use of the alley is the sole contributor to the high maintenance demand. The requested closure will give the owner the opportunity to purchase the alley and develop all of their parcels as one contiguous parcel to suit their needs.

The alley was established by Registered Plan of Subdivision No. 1140, registered on September 3, 1924.

Discussion:

The decision to recommend closure of an alley is derived from the City's *Classification of Alleys and Suitability for Closure* guideline document (the document), attached hereto as Appendix “E”. The document includes the following four classifications of alleys based on their usefulness and provides the following corresponding criteria for determining their suitability for closure. The use of the document is referenced under Part I of CR146/2005.

Classification of Public Right-of-Ways

1. Alley that is indispensable.
 - a. Does the alley serve commercial properties?
 - i. The alley serves the commercial property known municipally as 3099 Tecumseh Road East as a secondary vehicular means of access to a gravel parking area.

- ii. Notwithstanding this fact, the parking area was not legally established after the former gas station on the site was demolished in 1996 and as such shall have no bearing on this alley closure.
 - b. Does the alley serve properties fronting on heavily traveled streets i.e. major arterial routes?
 - i. Same response as provided for 1.a. above.
 - c. Does the alley contain sewers, and must the alley remain accessible for servicing?
 - i. The alley contains a 250-millimetre PVC sanitary sewer.
 - ii. The Engineering - Right-of-Way Department has no objection to the closure on the condition that an easement is granted to the City for access to repair and maintain the said sanitary sewer.
 - d. Does the alley serve as the only vehicular means of access to rear parking areas and garages where the property has insufficient lot width for a side drive?
 - i. The alley does not serve as the only vehicular means of access to rear parking areas and garages where the property has insufficient lot width for a side drive.
 - ii. 2238 Alexis Road has a gravel driveway off the alley, however it also has a driveway off Alexis Road.
 - e. Does the alley contain Fire Department connections that are deemed to be necessary for firefighting access?
 - i. The alley does not contain any Fire Department connections.
2. Alleys that, have some usefulness, are nevertheless dispensable and may or may not be a complete liability.
- a. The Engineering - Right-of-Way Department has deemed the alley to have no usefulness.
3. Alleys that appear to serve no useful purpose, either now, or anticipated. Such alleys are in residential areas and locations where generally the lots are wide enough for side drives, or those alleys abutting parks and other parcels of land that do not require any servicing from the alley. Remnant or stub-end streets which are dead-ended and do not serve as access to other streets.
- a. The Engineering - Right-of-Way Department has deemed the alley to serve no useful purpose.
4. Alley lying in Holding zones and other similar undeveloped areas where the alley system is clearly obsolete and has never been developed, but where the City needs to keep its options open until new area plans are prepared and development is imminent.

- a. The alley does not lie within a Holding zone or similar undeveloped area.

Suitability for Closing

1. Indispensable alleys should not be closed, conveyed, reduced or otherwise jeopardized through minority interests unless a suitable substitute alley is opened in lieu thereof.
2. Alley having some usefulness should be considered for closing only upon request of abutting owners rather than by encouragement of the City.
3. Alleys that serve no useful purpose should be closed if possible, and in fact the owners abutting thereon should be encouraged to accept conveyance.
4. Alleys that are clearly obsolete should not be closed unless there is a municipal need or specific development proposals acceptable to the City are submitted.

Based on the above, the Planning Department deems the alley dispensable and supports the requested closure.

It is our recommendation that, upon closure, the abutting property owners be given the chance to acquire the alley in the manner described in the Recommendation section herein. Hence, the recommendation is to close and convey the alley in, **as is condition** to the abutting property owners, which is the standard manner of conveyance.

Risk Analysis:

The recommended closure will divest the City of associated liability risks and maintenance costs. The recommended closure poses no known risk to City.

Climate Change Risks

Climate Change Mitigation:

N/A

Climate Change Adaptation:

N/A

Financial Matters:

The rate for an alley conveyed to abutting lands zoned CD2.1 is assessed at \$20.00 per square foot without easements plus HST (if applicable), and \$10.00 per square foot with easements plus HST (if applicable).

The rate for an alley conveyed to abutting lands zoned RD1.2 is assessed at \$1.00 plus HST (if applicable) and deed preparation fee.

It should be noted that all the abutting properties are under the ownership of LucasHB Enterprises Inc. This factor will allow the City to transfer the alley by PIN parcel to one of LucasHB Enterprises Inc.'s properties, thus avoiding the need for a Reference Plan.

Consultations:

Consultations were held with Municipal Departments and Utility Companies, which resulted in the information found in attached hereto as Appendix "C".

Notice of this application was issued to property owners abutting the alley by regular mail, with no objections being received as of the date of writing this report.

Notice of Development & Heritage Standing Committee meeting and Council meeting are published in the Windsor Star prior to each of the meetings. In addition, notice of each of the public meetings will be mailed to the abutting/affected property owners prior to the meetings.

Conclusion:

The Planning Department recommends closure of the alley shown on attached Appendix "A", subject to easements in favour of Bell Canada, Cogeco Connexion Inc., ENWIN Utilities Ltd., Managed Network System Inc. (MNSi.) and The Corporation of the City of Windsor as in Recommendation II.

The closed alley is to be conveyed in, **as is condition** to the abutting property owners as in Recommendation II report.

Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Greg Atkinson, MCIP, RPP
Manager of Development

Neil Robertson, MCIP, RPP
City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP *JM*

Approvals:

Name	Title
Brian Nagata	Planner II - Development Review
Greg Atkinson	Manager of Development/Deputy City Planner
Neil Robertson	City Planner / Executive Director, Planning & Development Services
Kate Tracey	Senior Legal Council, Legal Services & Real Estate
Jelena Payne	Commissioner, Economic Development
Joe Mancina	Chief Administration Officer

Notifications:

Name	Address	Email
Ward 5 Councillor Ed Sleiman	350 City Hall Square West, Suite 220 Windsor, Ontario	esleiman@citywindsor.ca

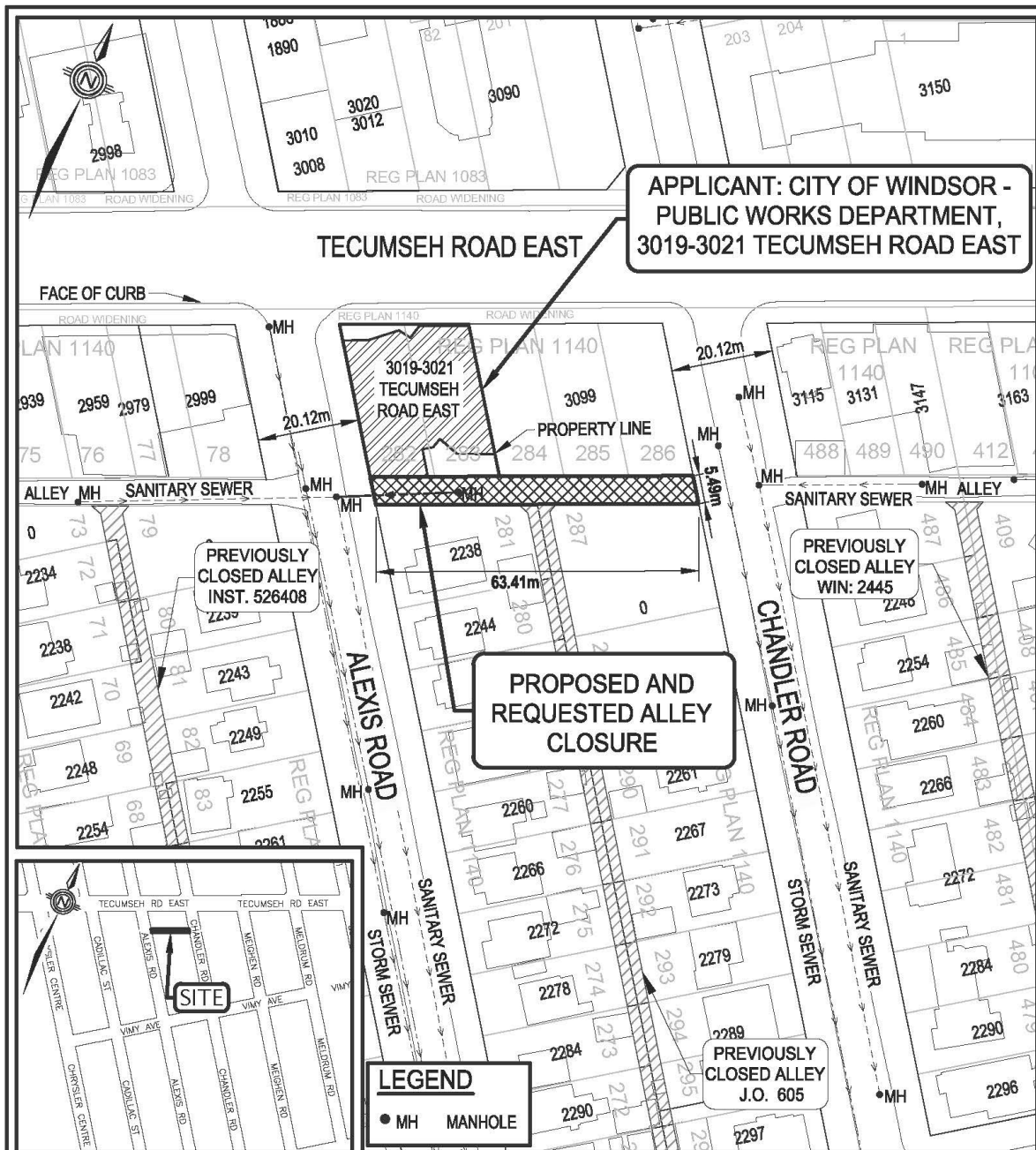
	N9A 6S1	
Roberta Harrison Coordinator Maintenance Operations Operations Department	1531 Crawford Avenue Windsor, Ontario N8X 0A2	roharrison@citywindsor.ca
List of mailing labels for property owners abutting the alley issued to Clerks office		

Appendices:

- 1 Appendix A - Drawing No. CC-1851
- 2 Appendix B - EIS Drawing - Aerial Photo
- 3 Appendix C - Consultations with Municipal Departments & Utility Companies
- 4 Appendix D - Site Photos
- 5 Appendix E - Classification of Alleys and Suitability for Closure

APPENDIX "A"

Drawing No. CC-1851



THE CORPORATION OF THE CITY OF WINDSOR - ENGINEERING DEPARTMENT

Proposed Closure of East/West Alley Located Between Alexis Road and Chandler Road, South of Tecumseh Road East

Kirk Tamm

Kirk Tamm, Manager of Geomatics

SCALE: 1:1000

DATE: AUG 2024

REVISED: -

DWG. NO.

DWN BY: AZ

CHKD BY: JB / SB

REVISION NO.: -

CC-1851

APPENDIX "B"

EIS Drawing - Aerial Photo



STREET & ALLEY CLOSING (SAA/7197)

1:750

APPLICANT : CITY OF WINDSOR PUBLIC WORKS DEPARTMENT

 REQUEST FOR CLOSURE

PLANNING DEPARTMENT - PLANNING POLICY

DATE: JUNE, 2024



APPENDIX “C”

Consultations with Municipal Departments and Utility Companies

BELL CANADA

No comments provided

COGECO CONNEXION INC.

Cogeco is attached to EnWin/Bell poles and will need an easement for the highlighted area.

[Daniel Haggins - Lead OSP Engineering Windsor and Essex]

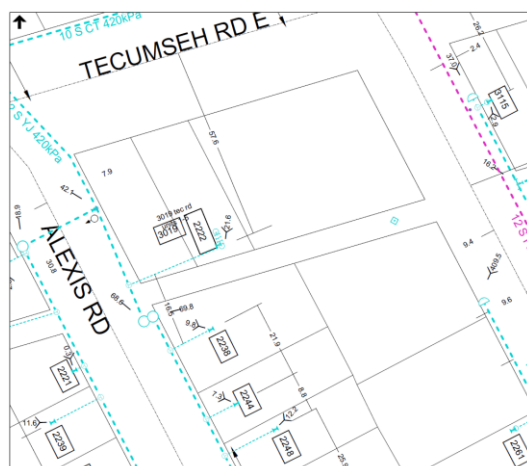
ENBRIDGE GAS INC.

After reviewing the provided drawing at 3019 Tecumseh Rd E and consulting our mapping system, please note that Enbridge Gas has no active infrastructure in the proposed area. A PDF drawing has been attached for reference.

Also, please note the following should you find any abandoned infrastructure in the area:

- Any pipe that is excavated, please assume that it is live
- If during any job, any pipe is found that is not on the locate sheet and conflicts with your work, please call our emergency number (1-877-969-0999), and one of our Union Gas representatives will respond to determine if that plant is in fact live or dead
- Please note that our Enbridge Gas representative will respond to the live or dead call within 1-4 hours, so please plan your work accordingly

Please contact me if you have any further questions or concerns.



[Jose Dellosa - Drafter Estimator]

ENGINEERING (DEVELOPMENT & ROW)

The proposed closure is approximately 63 m long and 5.5 m wide and made of gravel. There is a municipal sanitary sewer located in the western portion of the alley; an easement would be required for the sanitary sewer. There are hydro poles, guy wires and overhead wires in this alley; an easement would be required for utilities. There is a concrete driveway approach on the west side and a gravel approach on the east side of the proposed closure. The alley appears to be used by 3021 Tecumseh Road East and 2238 Alexis Road. Should the abutting owners use this access, they will be required to obtain a permit to maintain the approach as per AS-204 for the commercial property and AS-221 or AS-222 for the residential property. Otherwise, the driveway approaches to the alley will be the responsibility of the city to remove in the future when funds exist. This subject alley has no usefulness by CR146/2005; therefore, Public Works has no objections to the alley closure.

[Adam Pillon - Manager of Right-of-Way]

ENGINEERING (OPERATIONS)

No comments provided

ENVIRONMENTAL SERVICES

No concerns from Environmental Services.

[Anne-Marie Albidone - Manager, Environmental Services]

ENWIN UTILITIES LTD. (HYDRO)

No Objection provided clearances are maintained from our distribution plant. However, an easement named to ENWIN Utilities Ltd., will be required for East to West limit of the purposed alley closure to accommodate for the existing overhead hydro distribution.

ENWINs easement guidelines:

Overhead

A 3 m (10 ft) Easement is required for a straight pole line 1.5 m (5 ft) (on each side). This takes into consideration a 0.3 m (1 ft) pole diameter, 0.46 m (1.5 ft) primary insulator, and 0.9 m (3 ft) clearance from any nearby structure.

Guy and Anchor

The easement for the guy and anchor is 3 m (10 ft) wide easement is required into private property. This easement should extend 1 m (3.3 ft) into the property.

Underground Cable

A 0.6 m (2 ft) easement on either side of a duct bank is required for underground installations.

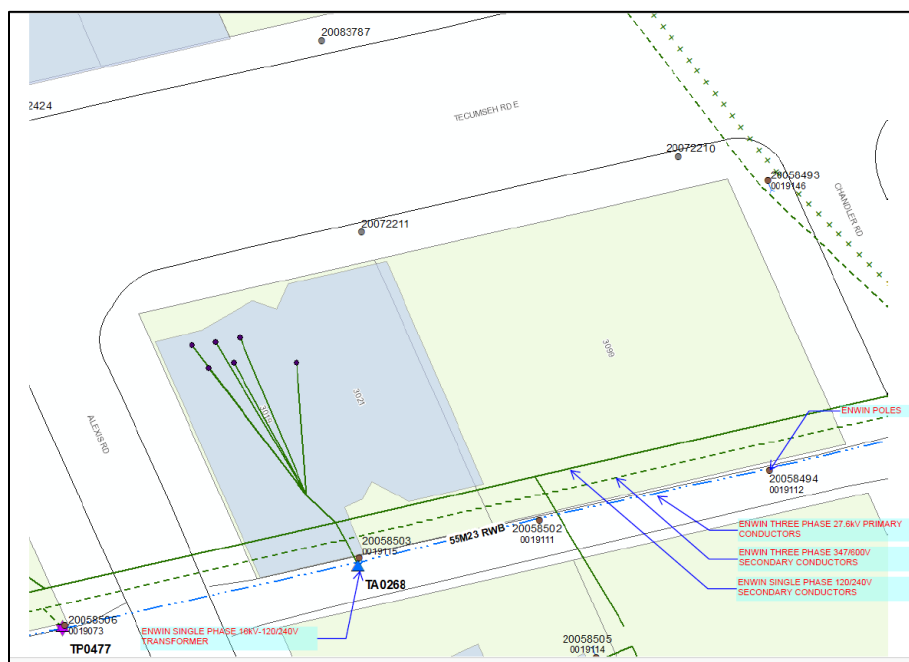
Please note the following.

1. ENWIN has overhead service poles throughout the alley to service each property.
2. ENWIN has 120/240V overhead secondary conductors along the back of the property.
3. ENWIN has 16kV overhead secondary conductors along the back of the property.

Prior to working in these areas, we would suggest notifying your contractor and referring to the *Occupational Health and Safety Act* and Regulations for Construction Projects to confirm clearance requirements during construction.

Also, we suggest referring to the *Ontario Building Code* for permanent required clearances for New Building Construction.

[Keegan Morency Kendall - Hydro Engineering Technologist]



ENWIN UTILITIES LTD. (WATER)

ENWIN Water has no objections.

[Bruce Ogg - Water Project Review Officer]

LEGAL & REAL ESTATE SERVICES

The same price would apply to these lands as well: \$20/sq foot without easements and \$10/sq foot with easements.

[Stephanie Santos - Coordinator Real Estate Services]

MANAGED NETWORK SYSTEM INC. (MNSi)

MNSi will require an aerial easement through the subject properties please.

[Dave Hartleib - Outside Plant Manager]

PARKS

No comments provided

PLANNING (DEVELOPMENT)

No comments provided

[Jim Abbs - Planner III - Development]

PLANNING (LANDSCAPE)

There are no objections from a landscape architectural or urban design perspective. There is no rationale provided by the applicant. However, while the proposed closure is surrounded by 4 properties owned by the same entity, the residence at 2238 Alexis Rd utilizes the alley for parking access. The applicant is to provide more detail as to how this property will be serviced for parking should the closure be approved.

[Stefan Fediuk - Planner III - Senior Urban Designer]

ROGERS

No comments provided

TELECON (TELUS)

TELUS has no infrastructure between Pavement Centerline & ROW line on the same side as the proposal.

Consent expires six (6) months from approval date. If the location of your proposed design changes, it will be necessary to re-apply.

[Frederic Sua - Design Specialist II - Access Engineering]

TRANSPORTATION PLANNING

It appears 2238 Alexis Rd has rear parking and a side entrance accessed from the alley. Also, 0 Chandler Rd parking lot users use the alley.

[Elara Mehrilou - Transportation Planner I]

APPENDIX “D”
Site Photos (Google Street View - November 2023)



Figure 1 - Looking east towards alley from Alexis Road (2238 Alexis Road on right)



Figure 2 - Looking west towards alley from Chandler Road (3099 Tecumseh Road East on right)



Figure 3 - Looking east towards Chandler Road from midpoint of alley (3099 Tecumseh Road East on left)



Figure 4 - Looking west towards Alexis Road from midpoint of alley (2238 Alexis Road on left)

APPENDIX “E”

Classification of Alleys and Suitability for Closure

Classification of Public Rights-of-Ways:

Currently streets and alleys fall into four classifications on the basis of their usefulness:

1. Alleys that are indispensable. These would be alleys serving commercial properties and properties fronting on heavily traveled streets i.e. major arterial routes and alleys which contain sewers and must remain accessible for servicing; alleys or streets which serve as the only vehicular means of access to rear parking areas and garages where the property has insufficient lot width for a side drive; and, alleys which contain Fire Department connections that are deemed to be necessary for firefighting access.
2. Alleys that, have some usefulness, are nevertheless dispensable and may or may not be a complete liability.
3. Alleys that appear to serve no useful purpose, either now, or anticipated. Such alleys are in residential areas and locations where generally the lots are wide enough for side drives, or those alleys abutting parks and other parcels of land that do not require any servicing from the alley. Remnant or stub-end streets which are dead-ended and do not serve as access to other streets.
4. Alleys lying in Holding zones and other similar undeveloped areas where the alley system is clearly obsolete and has never been developed, but where the City needs to keep its options open until new area plans are prepared and development is imminent.

Suitability for Closing:

Following are the criteria and suitability for closing alleys in each of the above classifications:

1. Indispensable alleys should not be closed, conveyed, reduced or otherwise jeopardized through minority interests unless a suitable substitute alley is opened in lieu thereof. They are essential from the viewpoint of fire protection, police protection, emergency services (i.e. ambulance) and loading or unloading of goods, refuse collection, servicing of blocked sewers and utility services. Without such alleys, the above noted services would at least be more costly if not impossible to complete or adequately access; and would noticeably interfere with street traffic, thereby reducing the access capacity of the adjacent arterial, collector, or street for business.
2. Alleys having some usefulness should be considered for closing only upon request of abutting owners rather than by encouragement of the City.
3. Alleys that serve no useful purpose should be closed if at all possible, and in fact the owners abutting thereon should be encouraged to accept conveyance.
4. Alleys that are clearly obsolete should not be closed unless there is a municipal need or specific development proposals acceptable to the City are submitted.



Council Report: S 111/2024

**Subject: Amendment to Sign By-law 250-04 for 9250 Tecumseh Rd E,
File No. SGN-002/24 (BILLBOARD) - Ward #7**

Reference:

Date to Council: October 7, 2024
Author: Stefan Fediuk, OALA FCSLA
Sr. Urban Designer
519-255-6543 ext.6025
sfediuk@citywindsor.ca

Kevin Alexander, MCIP RPP
Senior Planner--Special Projects
519-255-6543 ext.6732
kalexander@citywindsor.ca

Planning & Building Services
Report Date: August 27, 2024
Clerk's File #: SPL2024

To: Mayor and Members of City Council

Recommendation:

- I. THAT the application for a Site Specific Amendment to By-law 250-2004 (the "Sign By-law"), to allow for the installation of an Electronic Changing Copy Ground Sign at 9250 Tecumseh Road East, **BE APPROVED**;
- II. THAT Schedule "E" – "Special Provisions for Individual Signs" of the Sign By-law **BE AMENDED** by adding the following as a new section E. 23:

E. 23 Despite the provisions of this By-law, that an Electronic Changing Copy Sign, may be constructed at 9250 Tecumseh Road East situated on the north side of Tecumseh Road East, and immediately south of the Little River Corridor, subject to the following conditions:

- a. That the sign does not include flashing illumination, and that changing of the advertisement does not occur at intervals of less than 10 seconds;
- b. That the manufacturer of the sign confirm compliance with the lighting restrictions in accordance with Section 3 for Electronic Changing Copy Signs;

- c. That the sign does not have any animation including scrolling letters, television or video message, or any moving sign message objects;
 - d. That the owner complies with the illumination regulations in subsection 3.3.1.(c) of this By-law, and that the brightness of the sign be automatically controlled with an ambient light photo-sensor; and
 - e. That the sign may only be illuminated between the hours of 7am and 11pm.
- III. THAT the Chief Building Official **BE DIRECTED** to require, as a condition of approval of the installation of an Electronic Changing Copy Ground Sign at 9250 Tecumseh Road East, Pattison Outdoor Advertising apply for a Permit for Demolition to remove an existing quad-faced billboard ground sign located at 3124 Jefferson Avenue.; and,
- IV. THAT the City Solicitor **BE DIRECTED** to prepare the by-law to amend the Sign By-law.

Executive Summary:

N/A.

Background:

An application for a site-specific amendment to the Sign By-law was received by the Building and Planning Department, seeking relief from Section 6.3.17.iii Prohibition for Electronic Change Copy Billboards within 300.0m of a sensitive use. While minor variances may be considered through the Committee of Adjustment, prohibitions can only be approved by Council through a site-specific Sign By-law Amendment.

The proposed site, located at 9250 Tecumseh Rd East, is situated on the north side of Tecumseh Road East, and contains an automotive sales business and associated parking lot. The subject property has a "HCD 3.3" Zoning designation, which is "General Commercial - Warehouse". There is an existing ground sign on the property, which would be removed and re-located further east on the property as part of this project.

This location, as depicted in Appendix "A," is subject to the City of Windsor Sign By-law 250-2004. The applicant is proposing installation of a new Billboard Ground Sign utilizing Electronic Changing Copy Signs, facing east and west, on the property in the current ground sign location (Appendix "B"). The sign is regulated by Section 6.3: Regulations for Billboard Ground and Wall Signs. The proposed sign falls within the permitted locations for Billboard Signs as outlined in Sign Bylaw Section 6.3.2 Tecumseh Road East, between Lauzon Parkway and the eastern City Boundary. Per Section 6.6.3 of the Sign Bylaw, two ground signs are permitted and the owner is proposing that one of the allowed Ground Signs be a Billboard Sign.

Discussion:

The applicant is seeking a site-specific amendment to the Sign Bylaw related to the westerly-facing electronic change copy sign face.

Section 6.3.17.iii of the Sign By-law prohibits the erection of any Electronic Change Copy Billboard Ground or Wall Sign within 300 metres of any residential or sensitive use where the sign structure or face would be visible.

The applicant is proposing to install the billboard sign approximately 218 metres from residences to the south and 215 metres from residences to the east of the site. Notably, there is significant commercial development, including a car dealership and a Canadian Tire store, located on the south side of Tecumseh Road East between the proposed sign and the sensitive land uses. This commercial development will block the visibility of the proposed sign. The applicant has also provided evidence that existing vegetation along the Little River Corridor, combined with the commercial development, will further screen the billboard.

To mitigate potential impacts, the applicant has proposed that the billboard will not be illuminated or operational between 11:00 PM and 7:00 AM. A light analysis summary has been provided, showing that the maximum increase in ambient light for the nearest residential properties will be 0.3 lux, equivalent to the light output of a 60-watt bulb positioned 14 metres away. The sign will be continuously monitored to ensure that illumination levels remain within the permitted intensity.

As the amendment to the Sign Bylaw (CR 93-2024) was intended to reduce conflicts related to health and safety and minimize the visual impact of concentrated signage, the applicant has also proposed removing an existing quad-faced billboard ground sign at 3124 Jefferson Avenue. This addresses the benefit of an electronic change copy billboard, which can display multiple messages in one location, compared to traditional poster billboards, which display a single message for extended periods.

Risk Analysis:

There are no significant risks identified with approval of the proposed Sign By-law amendment. Potential impact to sensitive land uses has been mitigated in accordance with the Recommendations of this report.

Climate Change Risks

Climate Change Mitigation:

Light pollution is a contributing factor to climate change and light levels for LED display signs are regulated by the Sign Bylaw. Automatic brightness controls even out the illumination levels related to the ambient light surrounding the signage. Currently, the Sign By-law does regulate the brightness of illuminated signs in Section 3.3 Illumination Regulations, however the very nature of Illuminated Electronic Change Copy Billboard Signs will create light pollution as they cannot be full cut-off as per CR228/200 Lighting Intensity Standards Study.

In addition, the applicant has agreed to restrict the illuminated hours of operation from 7am to 11pm to accommodate Dark Sky Friendly practices.

Climate Change Adaptation:

LED technology has proven more energy efficient than traditional static lighting. With climate change and increasing strain on natural resources, limiting carbon footprints is essential. Electronic Change Copy Billboard Signs help to reduce the amount of poster waste entering our landfills and recycling plants, but however have other risks from climate perspectives.

Financial Matters:

There are no direct matters of financial consequence to the Corporation of the City of Windsor arising from the recommendations of this application for an amendment.

Consultations:

Several municipal departments were circulated for consultation and comments, including: Transportation Planning, Traffic Operations, Engineering, Planning and Building Services, Windsor Police Services, and the Legal Department, to address the variances, restrictions and prohibitions, related to this proposal.

Conclusion:

The applicant has worked with City Administration since the original application in May 2022, to provide a rational proposal that reduced the number of non-compliances. It is Administration's opinion that applicant has demonstrated that the impact to the residential community will be minimized through the technology being utilized and the restricted hours that this Electronic Billboard Sign will be operated.

Therefore, it is Administration's opinion that Council should approve the proposed site-specific amendments related to the proximity to the residential uses south of Tecumseh Rd East and the Little River Corridor, limited to static messages only, which change at a rate of 10 second intervals, illumination regulations Subsection 3.3. of the Sign Bylaw, and will only be illuminated between the hours of 7am and 11pm daily.

Planning Act Matters:

N/A

Approvals:

Name	Title
Stefan Fediuk	Senior Urban Designer / Landscape Architect
Kevin Alexander	Senior Planner / Special Projects
Jason Campigotto	Deputy City Planner - Growth (Acting)
Neil Robertson	City Planner / Executive Director - Planning & Development
Kate Tracey	Senior Legal Counsel

Name	Title
Wira Vendrasco	City Solicitor & Commissioner, Legal and Legislative Services
Jelena Payne	Commissioner of Economic Development & Innovation
Joe Mancina	Chief Administrative Officer

Notifications:

Name	Address	Email

Appendices:

Appendix 'A' - Neighbourhood Map

Appendix 'B' – SGN 002-24 Proposed Sign & Location

Appendix 'C' - RATIONALE



APPENDIX 'A' SGN 002-24 – NEIGHBOURHOOD MAP

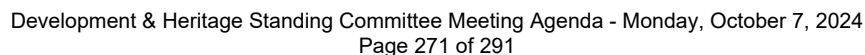


APPLICANT: PATTISON - BILLBOARD

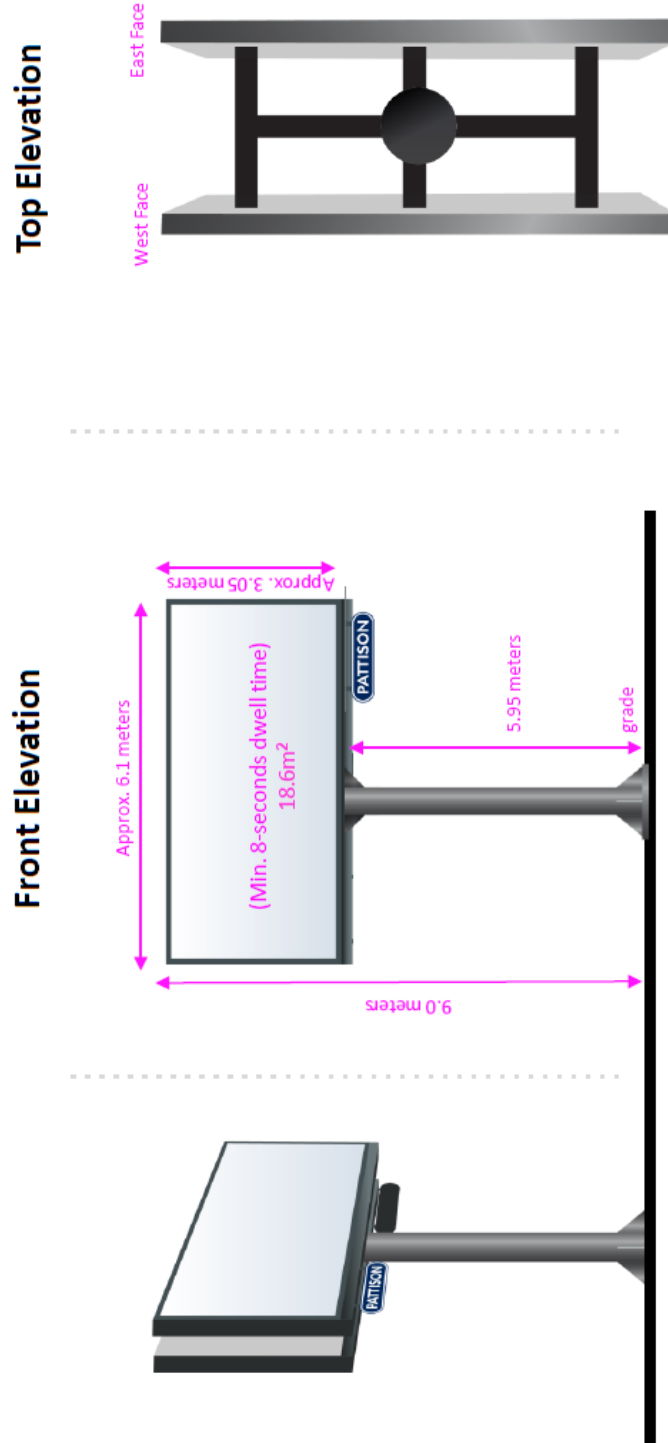


SUBJECT LANDS

SGN-002/24 Proposed Electronic Billboard Sign and Site Plan



Elevation Drawings of the Proposed Sign (9250 Tecumseh Rd East)

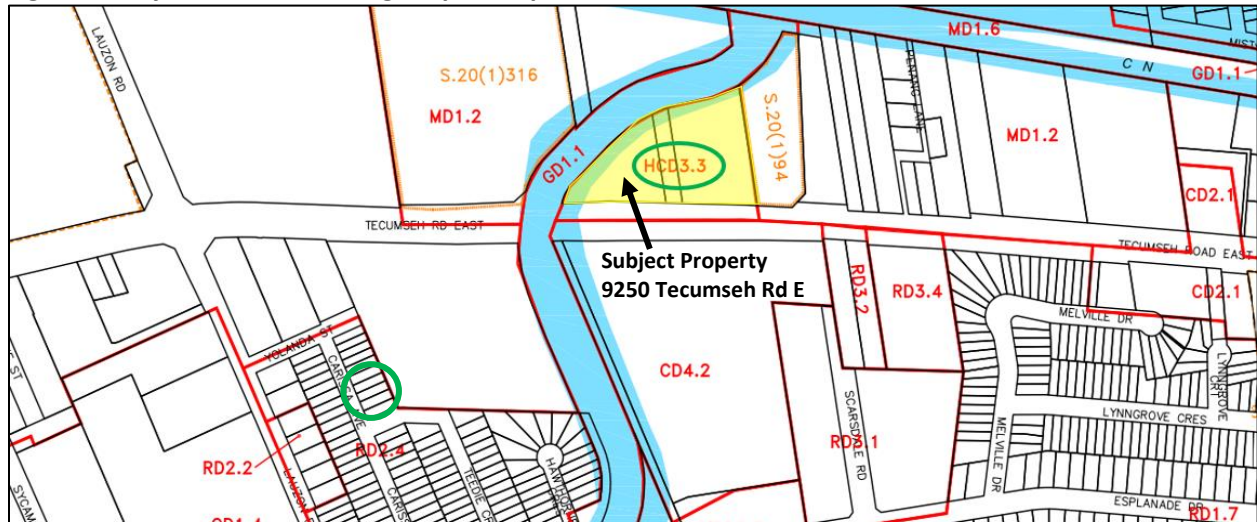


City of Windsor – 9250 Tecumseh Road East

On behalf of the Property Owner, Pattison Outdoor is seeking approval to build one new double-sided Electronic Changing Copy Billboard Ground Sign at 9250 Tecumseh Road East (the “Subject Property”).

This ground sign would have two electronic sign faces in a back-to-back configuration, each measuring 3.03 m tall by 5.91 metres wide, at an overall height of 9.0 metres from grade (the “Proposed Sign”). The Subject Property has a “**HCD 3.3**” Zoning designation, which is “**General Commercial - Warehouse**”, as shown in Figure 1 below. This commercial designation, along with the property being within a permitted area for Electronic Changing Copy Billboard Ground Signs in both the old and new Windsor Sign By-law, establishes its suitability at the Subject Property.

Figure 1: City of Windsor Zoning Map Excerpt – 9250 Tecumseh Road East



Prior to the recent amendments to the Sign By-law, Pattison Outdoor was issued a Sign Permit to build one new Electronic Changing Copy Billboard Ground Sign identical to the one being proposed at a different location on the subject property. However, due to underground infrastructure in place, it could not be installed.

Pattison Outdoor has also been issued a sign permit to re-locate an existing first party ground sign to a new placement further East at the Subject property, which will create sufficient separation between the two signs as specified in the Sign By-law.

The Subject Property is located on the North side of Tecumseh Road East, and contains an “Enterprise” auto rental facility, and associated parking lot. The surrounding area includes many other commercial, retail and employment uses on both the North and South sides of Tecumseh Road East.

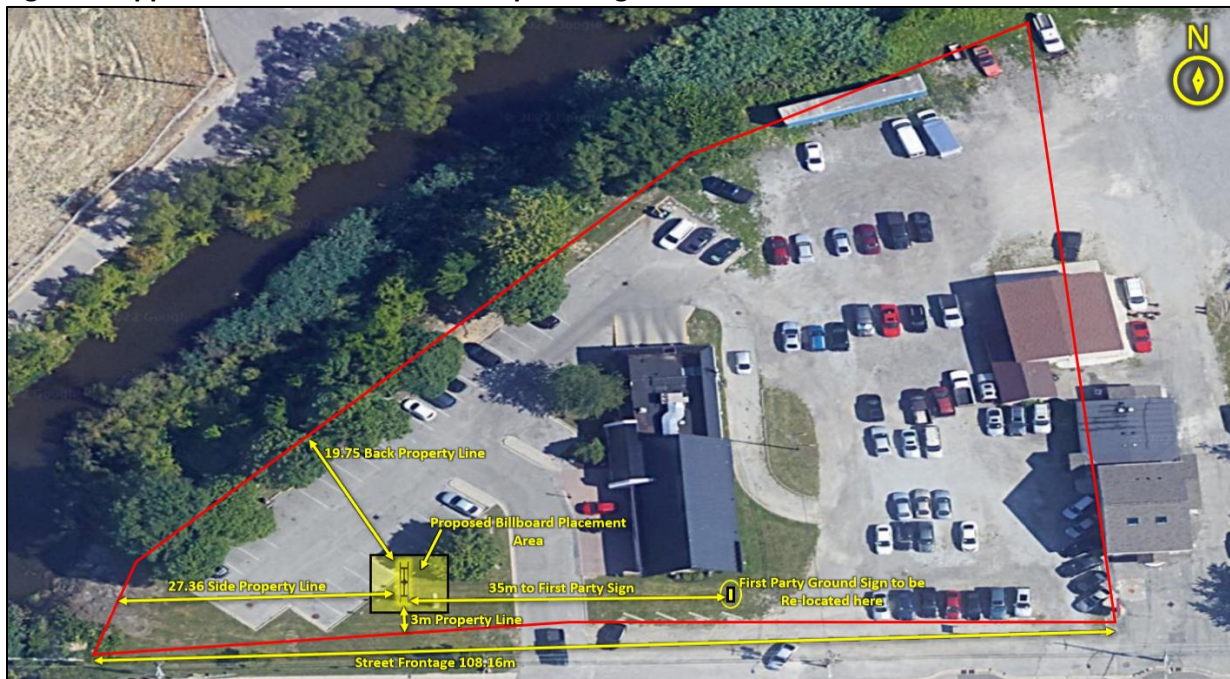
There are several commercial businesses and warehouses located to the south of the subject property, including Canadian Tire, Jaguar Windsor, Mercedes-Benz, and Rafih Auto Group. Beyond these commercial / employment uses which line the south side of Tecumseh Road East, is a dense section of natural growth, which will act as a physical barrier between the Proposed Sign and the residential areas located 200+ metres south of the Subject Property. The existing buildings and large trees will obstruct any potential visibility of the Proposed Sign from these residential areas.

Figure 2: Aerial view of 9250 Tecumseh Road East Property Boundaries



We have strategically designed the Proposed Sign with a back-to-back orientation, in order to effectively direct the sign copy to the intended audience travelling along Tecumseh Road. The angle of the sign faces, along with the built form of existing buildings and the mature trees in place between the Subject Property and residential uses, will ensure there is no visibility of sign copy from these areas to the south, as can be seen in Figures 3 and 4 below.

Figure 3: Approximate Location of the Proposed Sign at 9250 Tecumseh Road East



Clearance of 5.95 metres from grade to the bottom of the sign face will exceed the requirements set out in Sign By-law and the Ontario Building Code, and will ensure that pedestrians and vehicles can pass safely underneath the Proposed Sign. It is also located outside the Daylight Corner, and meets the required setback from the driveway entrance to the Subject Property. As such, the proposed placement will not obstruct the visibility of any drivers or cyclists.

Figure 4: Looking South from the Proposed Sign Location

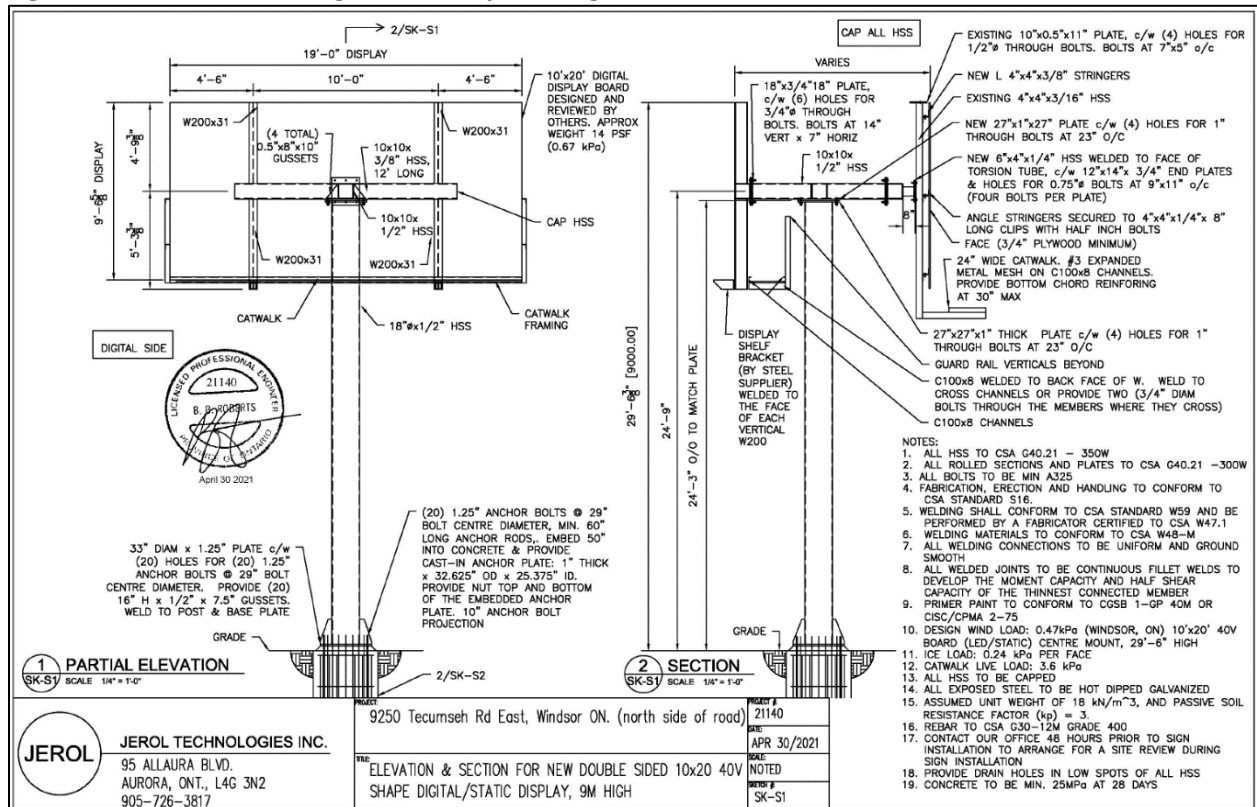


The Proposed Sign will meet nearly all of the applicable requirements in the Windsor Sign By-law:

- It is located on a property within a Commercial District / Zoning Designation (HCD 3.3);
- It is located on a property with frontage on Tecumseh Road East, between Lauzon Parkway and the Eastern City Boundary;
- It is **not** located within a Special Sign District as set out in Section 9.0;
- The maximum sign face area will not exceed 22.0 square metres (21.94 m² proposed);
- The maximum height will not exceed 9.0 metres (9.0 metres proposed);
- There are **no** other billboards within 200 metres, or digital billboards within 500 metres;
- It will **not** be erected within a Daylighting Corner or the driveway entrance to the property;
- There will be a minimum of 2.4 metres clearance from grade to the bottom of the sign face;
- The Subject Property has a minimum frontage of 30.0 metres;
- There are **no** other billboards ground signs or ground signs at the Subject Property;
- It is **not** within 300 metres of the Windsor Airport Lands;
- It is **not** within 400 metres of E.C. Row Expressway or Highway 401 (MTO-Controlled Areas);
- It will meet the setback from the adjacent lot line of 3.0 metres (13.1 m proposed), and;
- It will meet the setback from the rear lot line of 6.0 metres (31.1 m proposed)

Because the height of the Proposed Sign will exceed 7.5 metres, Pattison will undertake an engineering review and provide engineered-stamped drawings (see Figure 6 below), and an Engineer's Field Review following the installation of the Proposed Sign being completed.

Figure 6: Structural Drawings of the Proposed Sign at 9250 Tecumseh Road East



The sign copy displayed will comply with all of the Sign By-law provisions for electronic changing copy signs, including: only displaying a static (fixed) message for no less than 8.0 seconds, with an instantaneous change between messages (approximately 0.01-second interval before the next static message is displayed). At no time will the sign display any distracting visual effects such as scrolling, flashing, blinking, full-motion video, or any similar effects.

The Proposed Sign will also comply with the illumination requirements of the Sign By-law, and be programmed to not exceed the maximum brightness during daytime hours of 5,000 NITS, or the maximum brightness between dusk and dawn of 300 NITS. It will also be turned off and display no content between 11 pm and 7 am.

The Proposed Sign will meet all the requirements set out in Section 3.3.1(c) of the Sign By-law, specifically that it will be programmed to have an intensity of illumination and luminance not to exceed 3.0 lux (or 0.3 foot-candles) above ambient light conditions during its operating hours. We have provided a Letter of Certification from the Sign Manufacturer to confirm that the Proposed Sign will achieve these requirements.

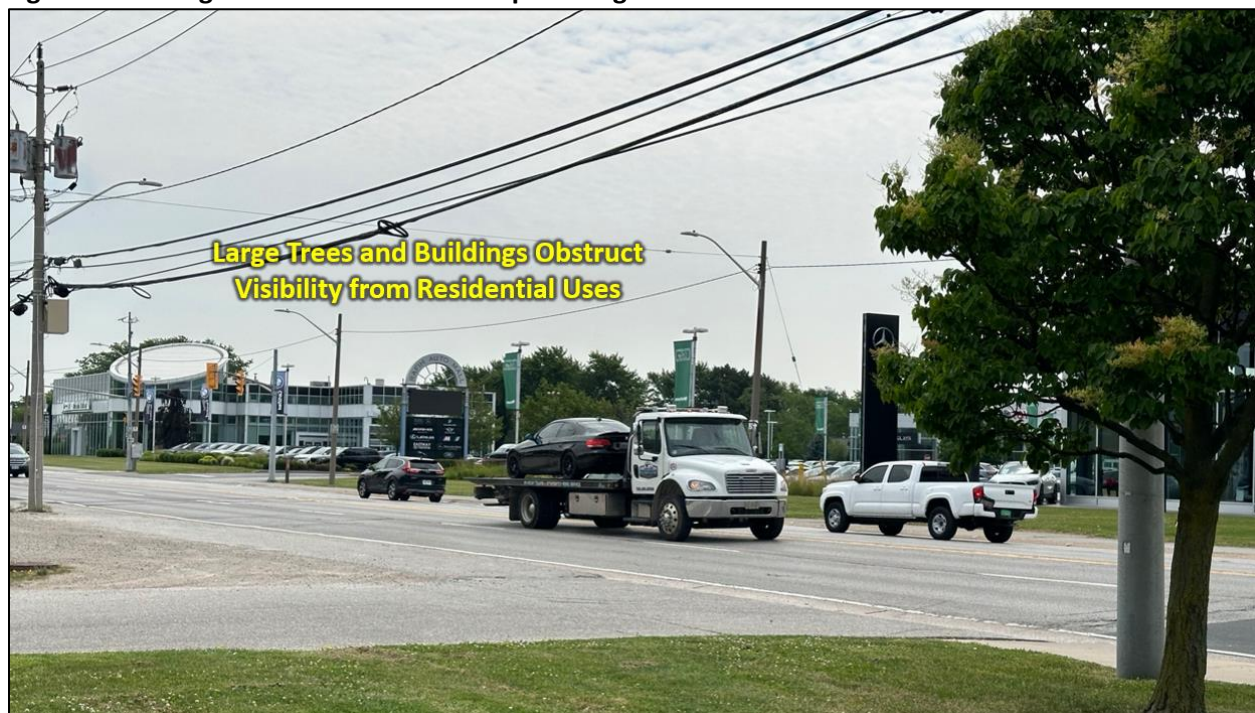
The Sign By-law was recently amended following a thorough review by Staff to refine and update the requirements for Electronic Changing Copy Ground Signs. Following these updates, the Subject Property remains in an area of the City where this type of sign would be compatible. The Subject Property was – and continues to be – within one of the permitted areas for Electronic Changing Copy Ground Signs, which includes properties along Tecumseh Road East, between Lauzon Parkway and the Eastern City Boundary.

Figure 7: Proximity to Residential Uses to the South and South-East



While the Proposed Sign would be located approximately 215m metres from residential uses to the South-East on Scarsdale Road, and 218m to the South on Teedie Crescent, we have taken a strategic approach to planning the sign's orientation, placement, and attributes in order to minimize or eliminate any potential impacts on these properties.

Figure 8: Looking South-East from the Proposed Sign Location



The back-to-back configuration of the sign faces will direct the sign copy to the West and East, toward traffic travelling along Tecumseh Road East. There is also a mature tree line which forms a natural barrier and further obstructs visibility of the Proposed Sign from the residential uses on Teedie Crescent and Scarsdale Road, as shown in Figures 8, 9 and 10.

Figure 9: Looking South towards Teedie Crescent from the Proposed Sign Location



Figure 10: Looking North Towards the Subject Property from Teedie Crescent



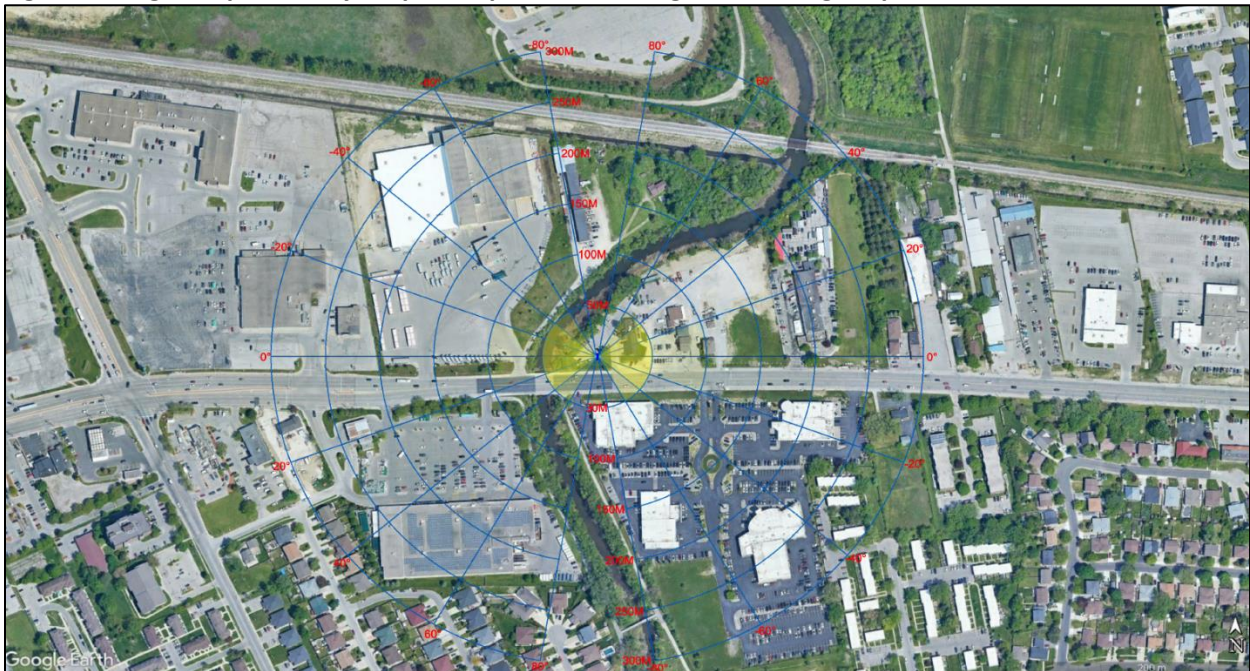
Based on the site-specific conditions and the built form and natural obstructions in the surrounding area, we believe the variances required for the Proposed Sign are minor in nature and can be granted with confidence that the Proposed Sign will have no negative impacts on the Subject Property or any of the uses found in the surrounding area.

Figure 11: Condition of Approval – Existing Billboard Ground Sign to be Removed



Pattison Outdoor is committed to helping achieve higher-level objectives of the Sign By-law. As such, we will remove an existing billboard ground sign as a Condition of Approval, to reduce overall sign clutter in the City of Windsor. This existing sign (Shown in Figure 11 above) is located on Jefferson Ave., only 55 metres from a residential use. This will result in a reduction of 37.2 square metres of signage. We will request that the associated permits be revoked, upon issuance of a Sign Permit for the Proposed Sign.

Figure 12: Light Impact Study Map – Responsible Management of Light Spill Confirmed



To ensure that any lighting impacts created by the Proposed Sign do not affect any of the adjacent properties, we had qualified experts conduct a Light Impact Study based on the planned placement of the Proposed Sign. As can be seen in Figure 12 above, any areas identified beyond 50 metres of the Proposed Sign will only see an increase of 0.3 lux or less – a negligible amount considered to be equivalent to the light cast by a full moon, and in line with the Sign By-law requirements. The full report of this Light Impact Study has been included with our application materials.

The natural and built-form obstructions, along with the back-to-back configuration and turning off the sign completely off between the 11 p.m. and 7 a.m., will mitigate any impacts on the nearby residential uses and natural sensitive areas, such as Little River. The location previously approved for a Sign Permit was closer to the Little River, so the additional separation will further minimize any potential impacts on this waterway.

We have also confirmed that the Proposed Sign location is outside of the MTO Controlled Area (it is further than 400 metres away from Highway 401 and from E.C. Row Expressway), and further than 300 metres from the property boundaries of the Windsor Airport.

If you have any questions or concerns about our application or the Proposed Sign, we are available any time to discuss as required. Thank you for your consideration of our application, and take care.

Sincerely,

Nathan Jankowski
Manager, Permits & Legislation
Pattison Outdoor

Scott Stover
Leasing Representative
Pattison Outdoor



Council Report: S 129/2024

Subject: Brownfield Redevelopment Community Improvement Plan (CIP) application submitted by Rykka Care Centres LP for 0 Hanna St (Ward 3)

Reference:

Date to Council: October 7, 2024
Author: Simona Simion, MCIP, RPP
Planner III – Economic Development Planner (Acting)
ssimion@citywindsor.ca
519-255-6543 x 6397

Planning & Building Services
Report Date: September 16, 2024
Clerk's File #: SPL2024

To: Mayor and Members of City Council

Recommendation:

- I. THAT the request made by Rykka Care Centres LP to participate in the Environmental Site Assessment Grant Program **BE APPROVED** for the completion of a proposed Phase II Environmental Site Assessment Study and Remedial Work Plan for the property located at 0 Hanna St E (north of 600 Tecumseh Rd E) pursuant to the City of Windsor Brownfield Redevelopment Community Improvement Plan.
- II. THAT the City Treasurer **BE AUTHORIZED** to issue payment up to a maximum of \$25,000 based upon the completion and submission of a Phase II Environmental Site Assessment Study and Remedial Work Plan completed in a form acceptable to the City Planner and City Solicitor.
- III. THAT the grant funds in the amount of \$25,000 under the Environmental Site Assessment Grant Program **BE TRANSFERRED** from the CIP Reserve Fund 226 to Brownfield Strategy Remediation (project 7069003) when the eligible work is completed to the satisfaction of the City Planner.
- IV. THAT should the proposed Phase II Environmental Site Assessment Study and Remedial Work Plan not be completed within two (2) years of Council approval, the approval **BE RESCINDED** and the funds be uncommitted and made available for other applications.

Executive Summary:

N/A

Background:

Brownfield Redevelopment Community Improvement Plan (CIP)

Brownfield sites are properties that may be contaminated due to previous industrial or commercial uses such as a manufacturing facility or gas station. City Council approved a Brownfield Redevelopment CIP at its April 19, 2010 meeting for the purpose of encouraging the study, clean-up, and redevelopment of contaminated properties. The approval of the CIP was the result of nearly five years of study and consultation, which began in October 2005.

Importance of Brownfield Redevelopment

Historically, there has been little interest in redeveloping brownfield sites due to the uncertainty surrounding the extent of contamination and the potential cost of clean-up. The Brownfield Redevelopment CIP was adopted in 2010 and provides financial incentives to undertake the necessary studies and remedial work necessary to redevelop brownfield sites and reduce the potential negative impacts to the City's environment and neighbourhoods.

The benefits associated with brownfield redevelopment go far beyond the boundaries of the property. For example, they are often strategically located within existing built-up areas of the City where services and other infrastructure, such as roads, schools, community facilities and public transit are already available, therefore additional infrastructure costs are not incurred to service these areas. The redevelopment of these sites also removes the negative stigma often associated with brownfield properties, which increases the value of the subject property and adjacent properties.

Site Background

The subject site consists of one property parcel located on the south side of Hanna St E Avenue in the block between Howard Ave and Marentette Ave. The property is 1.17 hectares (or 2.89 acres) in size and irregular shaped. It is currently consisting of a parking lot for a commercial medical plaza and landscaping. The site is designated 'Mixed Use Corridor' on Official Plan Schedule D: Land Use, and is zoned Commercial District CD3.3, which permits a range of commercial, and office uses.

The parking lot was severed in 2023 through consent application B-037/23 approved by the Committee of Adjustment on August 16, 2023. A concurrent minor variance application for parking reduction was approved by the Committee of Adjustment (A-054/23 approved on August 16, 2023). The proposed development will be subject to approval of a Zoning By-Law Amendment and Site Plan Control, in addition to approval of a Building Permit application.

Historically, the property was occupied by an industrial facility (iron foundry) and included two rail yards, tracks and spurs. Also noted were the presence of unknown fill material and various hazardous wastes were present on site.

The principal owner is Rykka Care Centres LP. The owner intends to redevelop the property to construct a long-term care facility (196 beds). Since the property is proposed to be redeveloped from commercial use to a more sensitive use, a Record of Site Condition (RSC) is required under Ontario Regulation 153/04.

The owner has now submitted the grant application and will be incurring the eligible Phase II ESA plus Remedial Work Plan costs and, should the application be approved, would receive the grant payment.

Discussion:

Environmental Site Assessment Grant Program

The ESA Grant Program offers a matching grant to property owners of brownfield sites to conduct environmental studies that provide information on the type and extent of contamination and potential remediation costs. The program offers 50% of the cost of an eligible study up to a maximum of \$15,000. If two studies are required, an additional \$10,000 is available for a maximum total grant value of \$25,000.

The applicant proposes to redevelop 0 Hanna St for residential use, and requires a Phase II ESA study as part of their application for a RSC. The applicant has completed a Phase I ESA, which identifies areas of potential environmental concern, and recommends that a Phase II ESA study be completed to assess the existing soil and groundwater conditions at the site, plus Remedial Work Plan to determine the extent of any contamination (if required). Upon completion, the City would retain a copy of the final Phase II ESA study report and Remedial Work Plan.

CIP Goals

City staff is supportive of the application as it meets all of the eligibility requirements specified within the Brownfield Redevelopment CIP. The proposed study of the subject site also supports the following CIP goals:

- To promote the remediation, rehabilitation, adaptive re-use and redevelopment of

brownfield sites throughout the City of Windsor in a fiscally responsible and sustainable manner over the long term;

- Improve the physical and visual quality of brownfield sites;
- Improve environmental health and public safety;
- Provide opportunities for new housing, employment uses, and commercial uses;
- Increase tax assessment and property tax revenues;
- Promote Smart Growth, including the reduction of urban sprawl and its related costs;
- Increase community awareness of the economic, environmental and social benefits of brownfield redevelopment; and
- Utilize public sector investment to leverage significant private sector investment in brownfield remediation, rehabilitation, adaptive re-use, and redevelopment.

Policy Support

The study of brownfield sites to support clean up and redevelopment is supported by policies within the 2020 Provincial Policy Statement and the 2024 Provincial Planning Statement (which takes effect October 20, 2024), the City's Official Plan and the City's Environmental Master Plan.

Risk Analysis:

As with all brownfield sites, there is a degree of risk associated with the potential presence of contamination. The proposed Phase II ESA study and Remedial Work Plan will assist in mitigating the above noted risk by confirming the presence and extent of any contamination. It may also provide an estimated cost for remediation and establish next steps in the remediation process, if required.

Climate Change Risks

Climate Change Mitigation:

The proposed residential redevelopment is supported by the Environmental Master Plan action item, which encourages use of the Brownfields Redevelopment Strategy.

Climate Change Adaptation:

The redevelopment of the existing commercial property may be affected by climate change, in particular with respect to extreme precipitation and an increase in days above 30 degrees. While not the subject of this report, any new construction would be

required to meet the current provisions of the Building Code, which would be implemented through the building permit process.

Financial Matters:

The cost estimate (excluding HST) for completing the proposed Phase II ESA study is \$66,400. The cost estimate (excluding HST) for the Remedial Work Plan is \$20,000. If approved, the maximum grant would total \$25,000 (\$15,000 for the first study and \$10,000 for the second study). Should the actual costs of the study be less than what has been estimated, the grant payments would be based on the lower amount.

If approved, the grant would be paid from the Brownfield Strategy Remediation Fund (Project #7069003). The funds would be transferred from CIP reserve fund 226 for payment when the eligible study is complete. The current uncommitted balance of the CIP reserve fund is \$299,916; however, this balance does not account for other CIP grant requests that are currently being considered by the standing committee or have been endorsed by the standing committee and are not yet approved by City Council.

Consultations:

The development and approval of the Brownfield Redevelopment CIP was subject to extensive stakeholder and public consultation, which sought input from a wide range of stakeholders and internal City departments.

Planning staff have consulted with the applicant's agent from Dillon Consulting prior to accepting the application for the Environmental Study Grant program. Greg Atkinson, Manager of Planning Development; Josie Gualtieri, Financial Planning Administrator, Finance Department; and Kate Tracey, Senior Legal Counsel, Legal Department were consulted in the preparation of this report.

Conclusion:

City Staff recommend Council approve the request from Rykka Care Centres LP to participate in the Environmental Site Assessment Grant Program. In the opinion of planning staff, the proposed study conforms to the Brownfield Redevelopment CIP and assists the City in the achievement of a number of the CIP goals.

Planning Act Matters:

N/A

Approvals:

Name	Title
Simona Simion	Planner III – Economic Development Planner (Acting)
Emilie Dunnigan	Manager Development Revenue
Jason Campigotto	Acting Deputy City Planner - Growth
Neil Robertson	City Planner / Executive Director, Planning & Development Services
Kate Tracey	Senior Legal Counsel, Legal Services & Real Estate
Janice Guthrie	Commissioner, Finance & City Treasurer
Jelena Payne	Commissioner, Economic Development
Joe Mancina	Chief Administration Officer

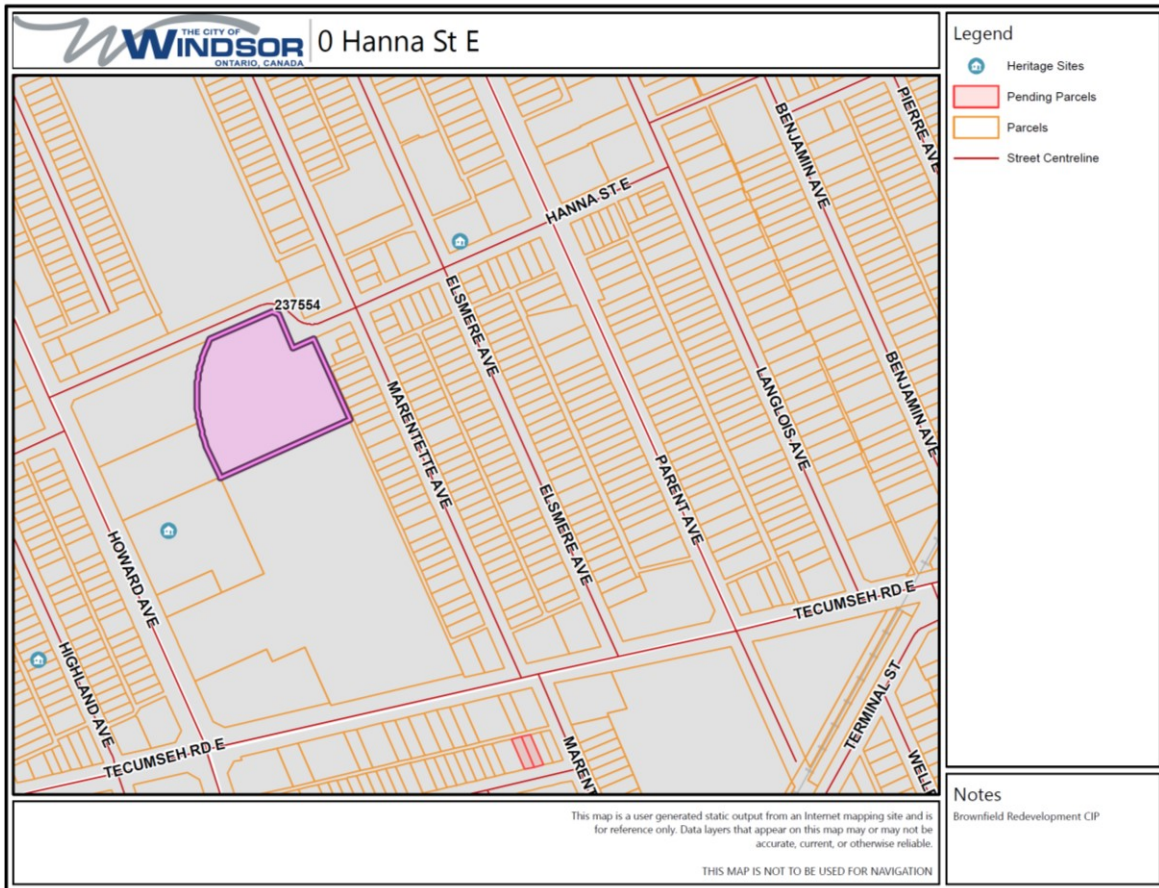
Notifications:

Name	Address	Email
Rykka Care Centres	3760 14 th Avenue, Suite 402, Markham, Ontario L3R 3R7	greg.bowman@reponsivegroup.ca
Matthew Antaya		mantaya@dillon.ca

Appendices:

- 1 Appendix A - Map

Appendix A – Location Map





Council Report: S 132/2024

Subject: Amendment to CR372/2023 for Closure of north/south alley located between Algonquin Street and 1429 Randolph Avenue, Ward 10, SAA-7025

Reference:

Date to Council: October 7, 2024
 Author: Brian Nagata, MCIP, RPP
 Planner II - Development Review
 (519) 255-6543 ext. 6181

Planning & Building Services
 Report Date: September 19, 2024
 Clerk's File #: SAA2024

To: Mayor and Members of City Council

Recommendation:

- I. THAT CR372/2023, adopted on September 5, 2023, **BE AMENDED** by **DELETING** subsection b) of Resolution II in its entirety.

Executive Summary:

N/A



Background:

CR372/2023 was adopted by Council on September 5, 2023, directing administration to undertake the necessary steps to assume, close and convey the following alley:

- 4.27-metre-wide north/south alley located between Algonquin Street and the property known municipally as 1429 Randolph Avenue (the alley).

CR372/2023 included a condition requiring an easement to be granted in favour of 1429 Randolph Avenue for access to repair and maintain the south face of the existing building bordering the alley. Administration has since concluded that easements in favour of private property owners should not be included as a condition of closure and conveyance of an alley.

The City should not be concerned about private property within an alley unless there is an encroachment agreement in place. Further, the City should not be concerned about buildings or structures bordering an alley unless they meet one of the criteria under the City's *Classification of Alleys and Suitability for Closure* guideline document and were established legally by a Building Permit.

A property owner can submit a [Right-of-Entry Permit Application](#) to the Building Department to request access to an abutting property to maintain or repair a building or structure when denied such access by the adjoining property owner.

Discussion:

CR372/2023 must be amended to remove the said easement to allow for the closure and conveyance of the alley.

Risk Analysis:

The recommended amendment to CR372/2023 poses no known risk to the City.

Climate Change Risks**Climate Change Mitigation:**

N/A

Climate Change Adaptation:

N/A

Financial Matters:

The recommended amendment to CR372/2023 does not impact the conveyance cost.

Consultations:

N/A

Conclusion:

The Planning Department recommends that CR372/2023 be amended to remove the

said easement as in Recommendation I of this report.

Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Greg Atkinson, MCIP, RPP
Manager of Development

Neil Robertson, MCIP, RPP
City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP

JM

Approvals:

Name	Title
Brian Nagata	Planner II - Development Review
Greg Atkinson	Manager of Development/Deputy City Planner
Neil Robertson	City Planner / Executive Director, Planning & Development Services
Kate Tracey	Senior Legal Counsel, Legal Services & Real Estate
Jelena Payne	Commissioner, Economic Development
Joe Mancina	Chief Administration Officer

Notifications:

Name	Address	Email
Ward 2 Councillor Fabio Costante	350 City Hall Square West, Suite 220 Windsor, Ontario N9A 6S1	fcostante@citywindsor.ca
List of mailing labels for property owners abutting the alley issued to Clerks office		

Appendices: