

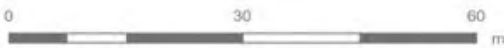
Appendix 'A' – Location Map



LOCATION MAP: 3218 BABY STREET

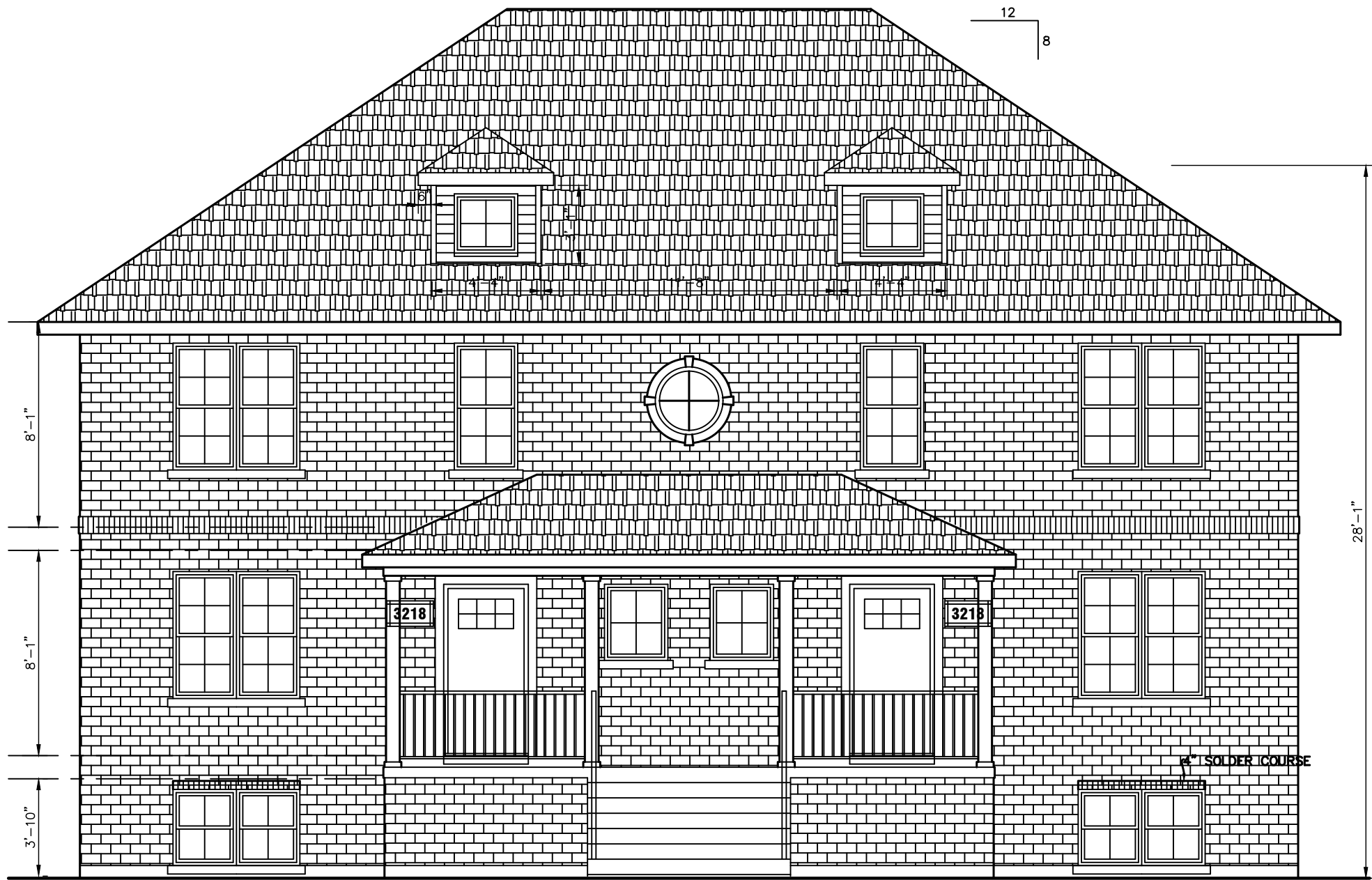


SUBJECT

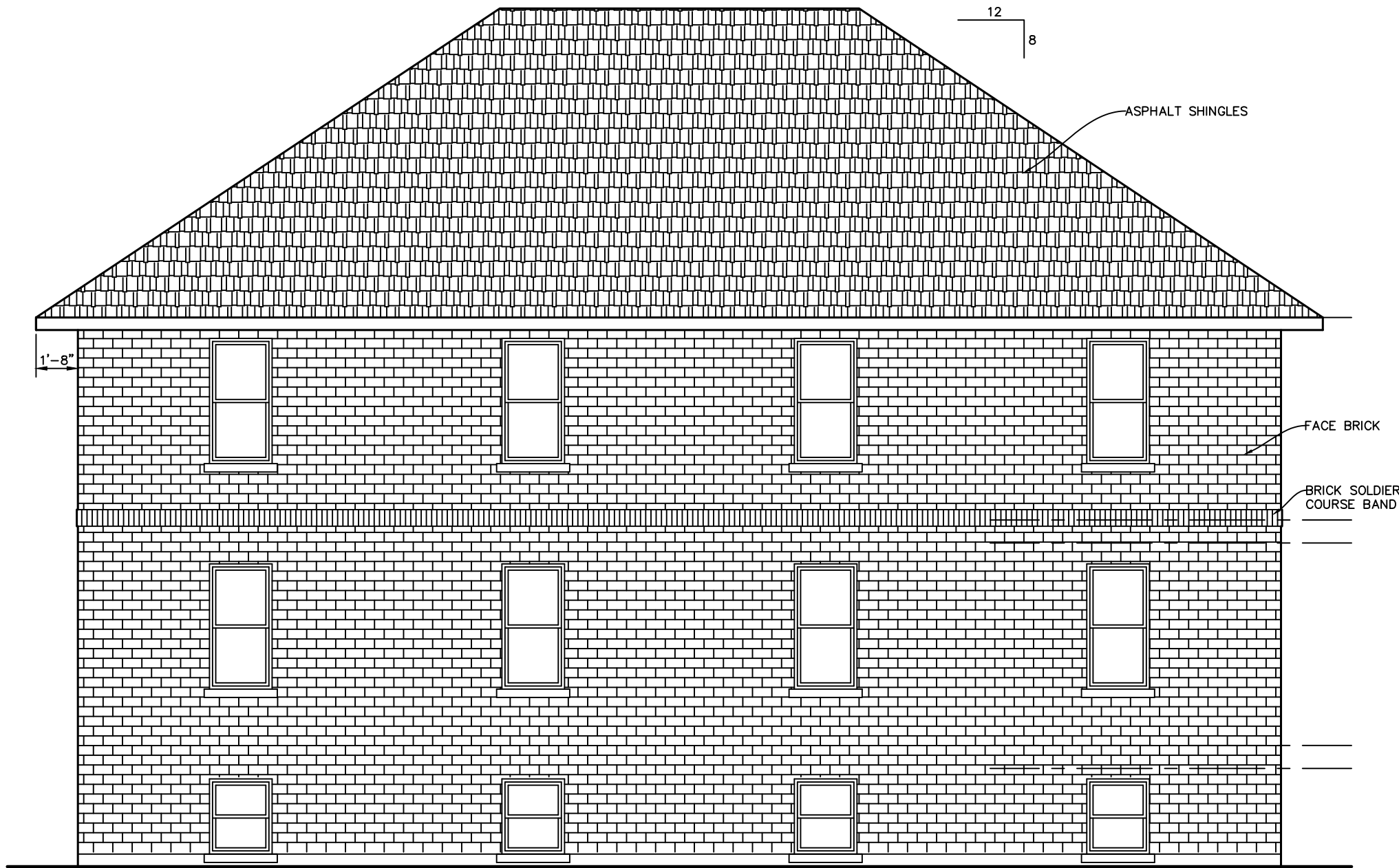


Appendix 'A' — Existing Condition (*Google Earth*)





FRONT ELEVATION



BACK ELEVATION

FRONT & BACK ELEVATIONS

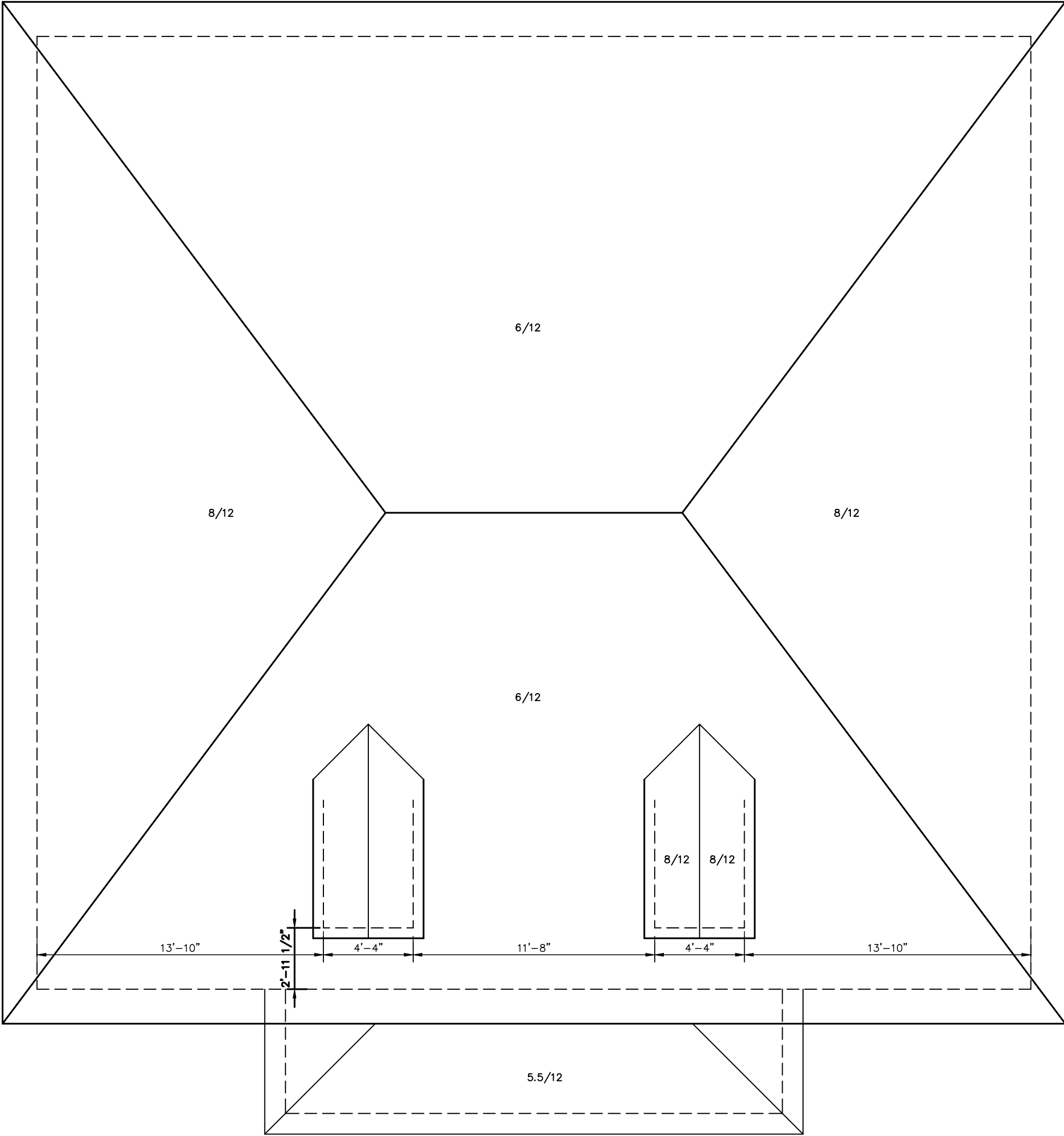
SCALE: 3/16" = 1'-0"

FEBRUARY 26, 2024

THREE UNIT APARTMENT BUILDING

3218 BABY STREET





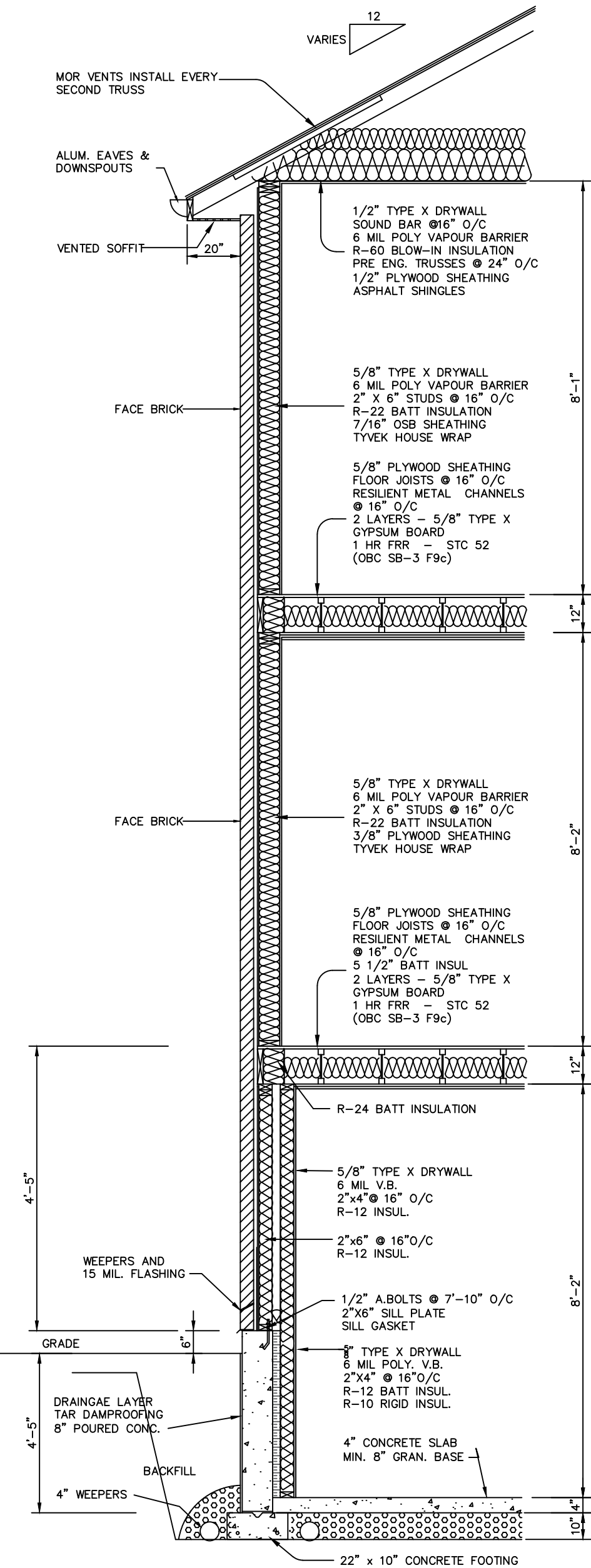
ROOF PLAN

SCALE: 3/16" = 1'-0"

3218 BABY STREET

THREE UNIT APARTMENT BUILDING

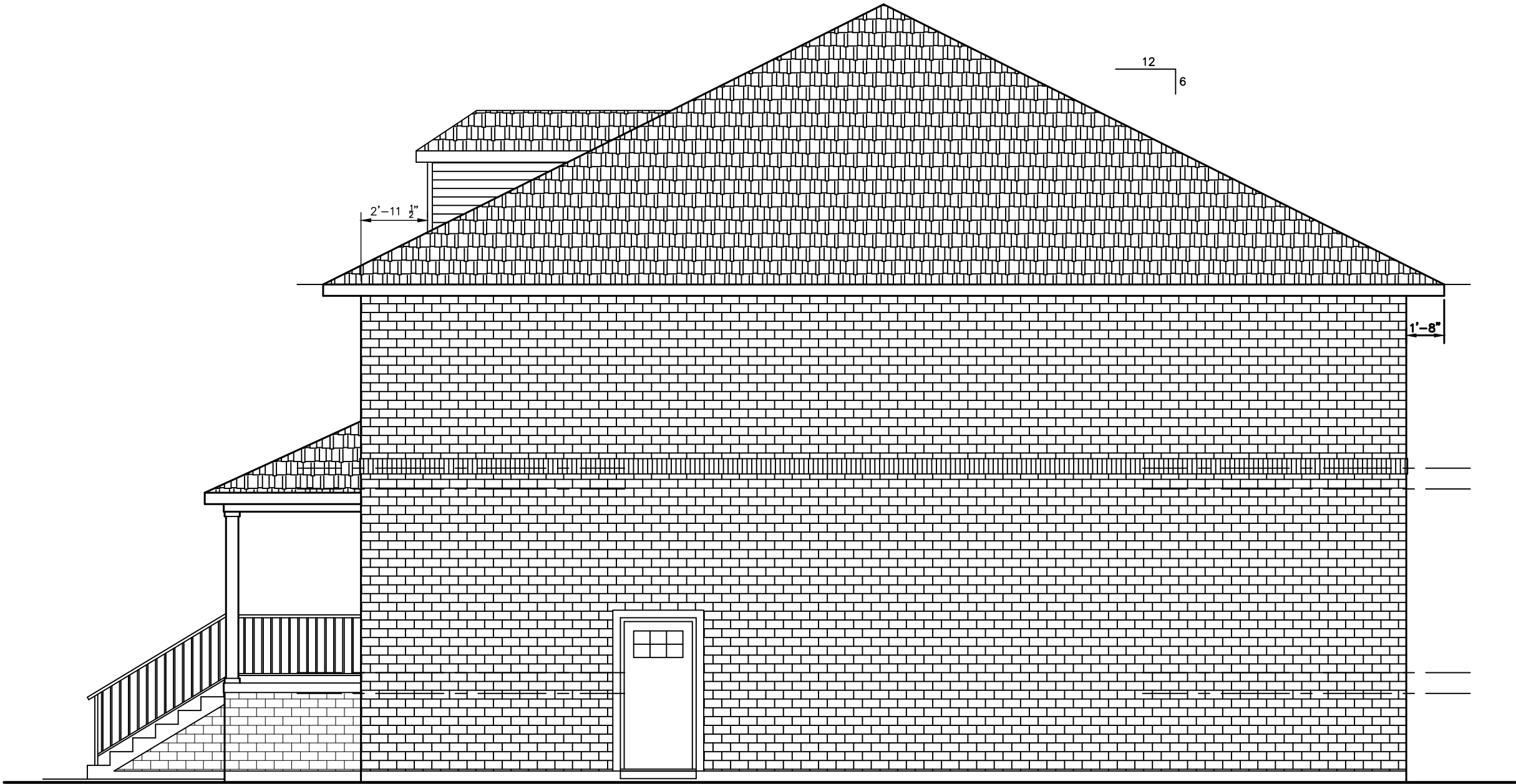
FEBRUARY 26, 2024



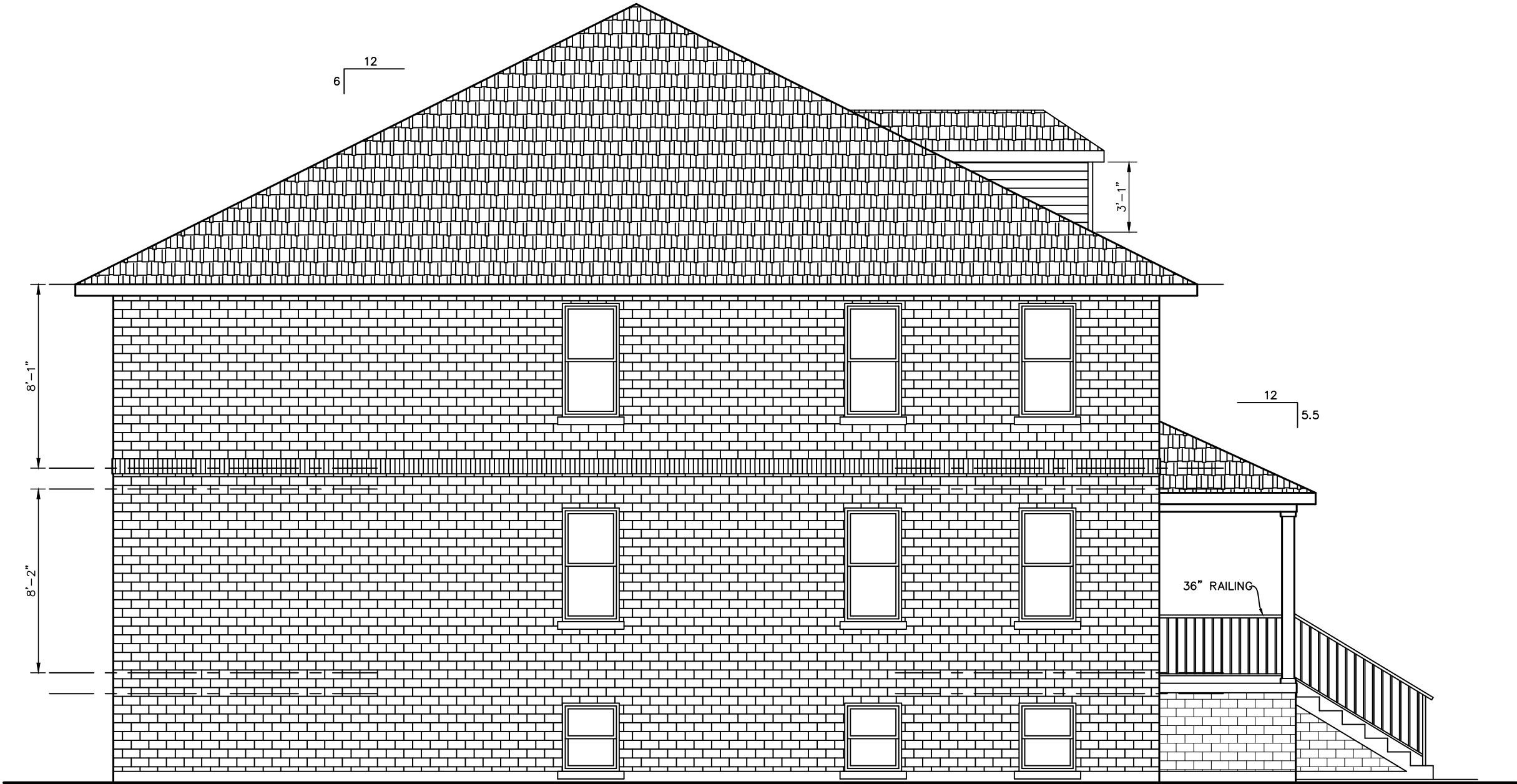
COMPLIANCE PACKAGE A-1

TYPICAL EXTERIOR WALL SECTION

SCALE: 3/8" = 1'-0"



RIGHT ELEVATION



LEFT ELEVATION

SECTION & SIDE ELEVATIONS

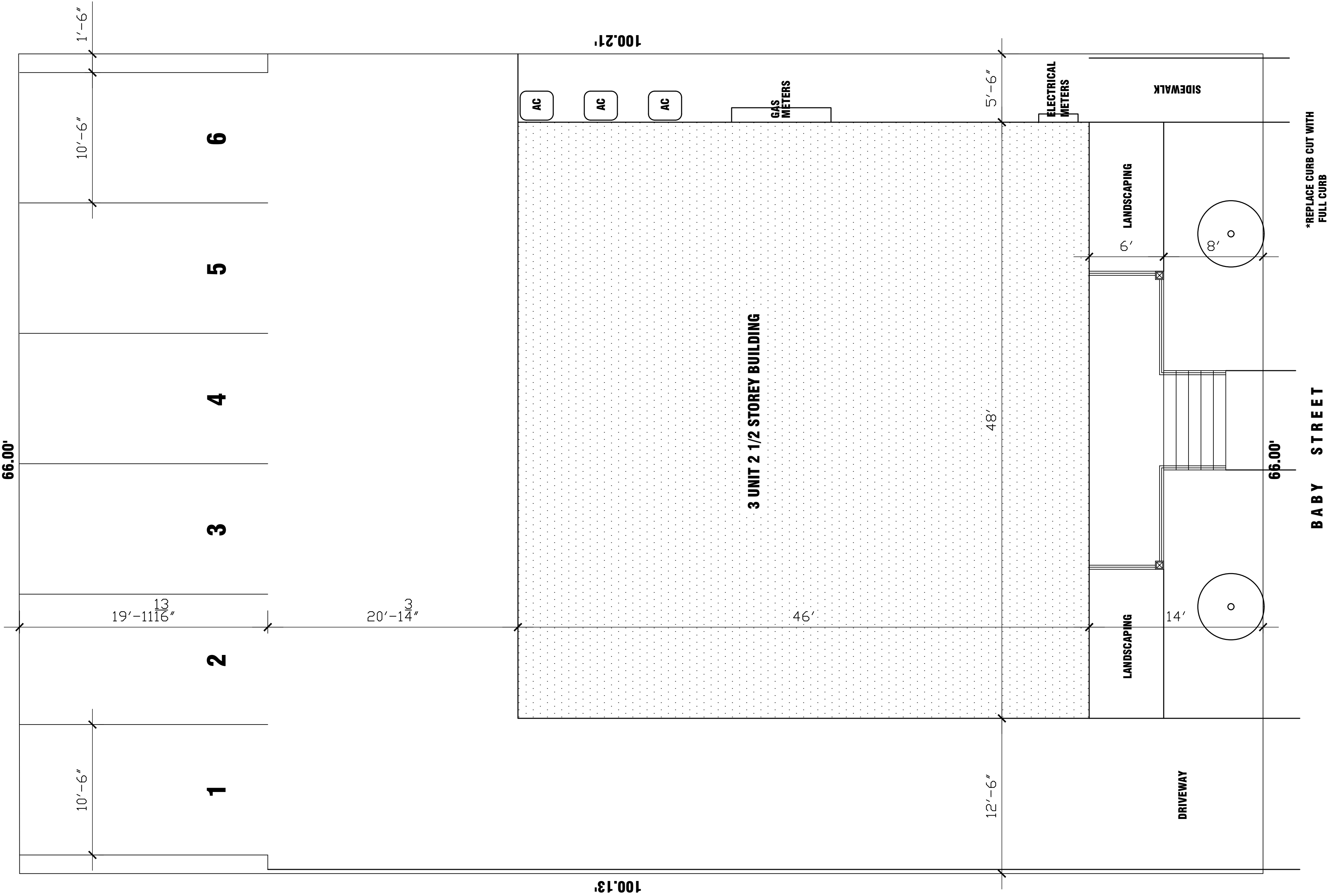
FEBRUARY 26, 2024

THREE UNIT APARTMENT BUILDING

3218 BABY STREET

SCALE: 3/16" = 1'-0"





SITE PLAN

FEBRUARY 26, 2024

THREE UNIT APARTMENT BUILDING

3218 BABY STREET

SCALE: 3/16" = 1'-0"

APPENDIX "A"
Email Correspondence with 2307 Cadillac Street

From: Nagata, Brian

Sent: Friday, February 23, 2024 3:11 PM

To: [REDACTED] <[REDACTED]>

Cc: Beckford, Shantal <sbeckford@citywindsor.ca>

Subject: RE: Closure of north/south alley located between Vimy Avenue and Ypres Avenue, Ward 5, SAA-7093

Good afternoon Brody,

This email is in response to your February 5, 2024, email to the Clerks Department regarding your opposition to the subject alley closure application.

The Development & Heritage Standing Committee (DHSC) moved the following motion deferring the subject alley closure application to a future meeting.

THAT the report of the Planner II - Development Review dated January 11, 2024 entitled "Closure of north/south alley located between Vimy Avenue and Ypres Avenue, Ward 5, SAA-7093" BE DEFERRED to a future Development & Heritage Standing Committee meeting to allow for further discussion with affected residents regarding their concerns.

In response to the aforesaid motion, I would like to arrange a virtual meeting to discuss the reasons that you have noted for opposing the subject alley closure. I intend to invite a member of the Public Works Department as well to answer any questions pertaining to the operation and maintenance of the alley.

Please provide me with some dates and times when you are available if you are open to meeting.

Regards,

BRIAN NAGATA, MCIP, RPP, B.A.A., Dipl.URPI | PLANNER II – DEVELOPMENT REVIEW



Planning & Building Services

350 City Hall Square West | Reception - 2nd Floor | Windsor, ON | N9A 6S1

(519) 255-6543 ext. 6181

From: [REDACTED] <[REDACTED]>

Sent: February 5, 2024 9:40 AM

To: clerks <clerks@citywindsor.ca>

Subject: RE: Closure of north/south alley located between Vimy Avenue and Ypres Avenue, Ward 5, SAA-7093

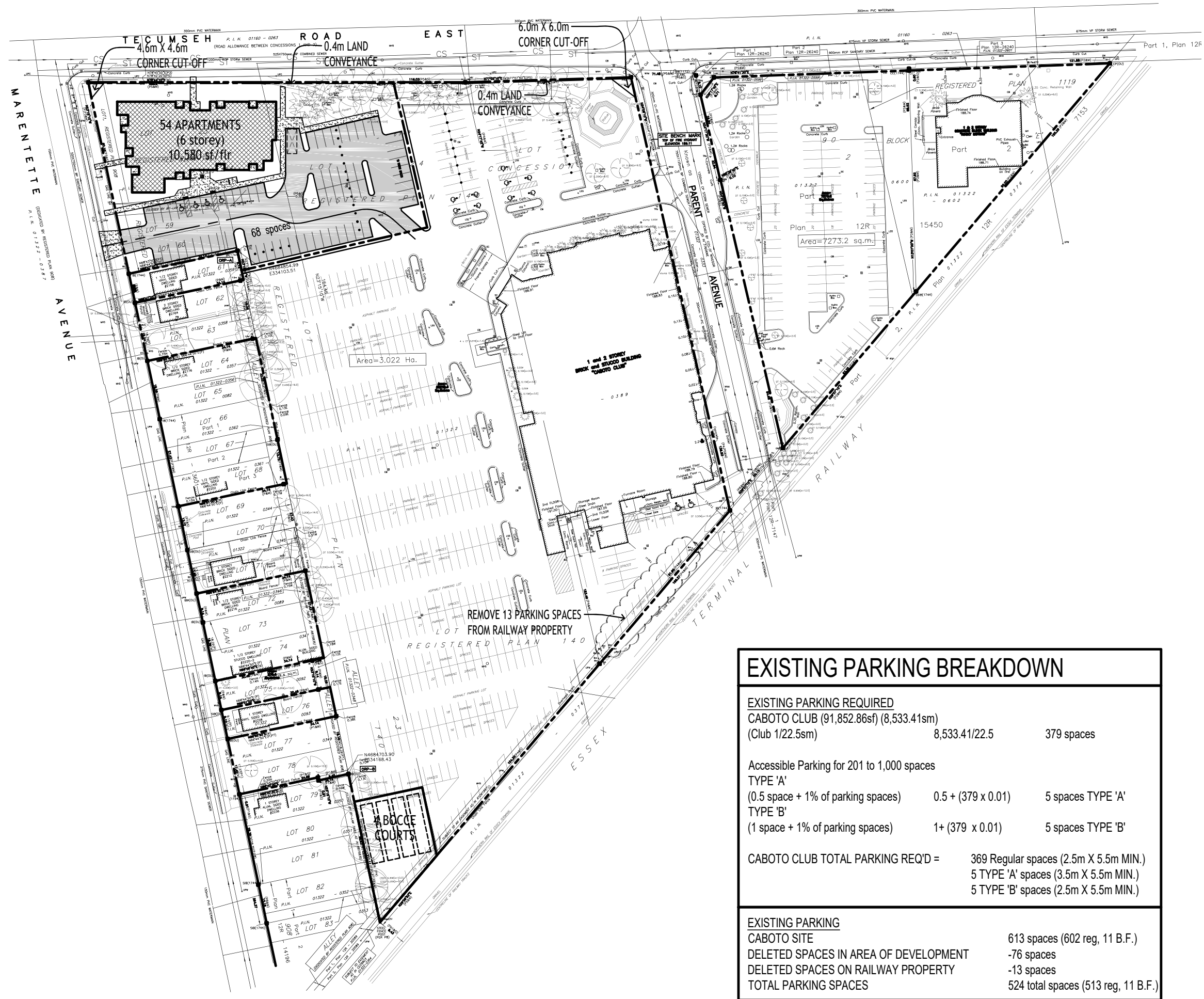
CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Requesting a deferral of the subject closure due to the fact that we are unable to attend, working afternoons and received the notice last Friday Feb 2. We are opposed to he closure as it would eliminate, the parking that is needed for 2307 Cadillac

If you have any questions, please contact myself [REDACTED] at [REDACTED]

Thank you for your consideration and time and looking forward to hearing from you

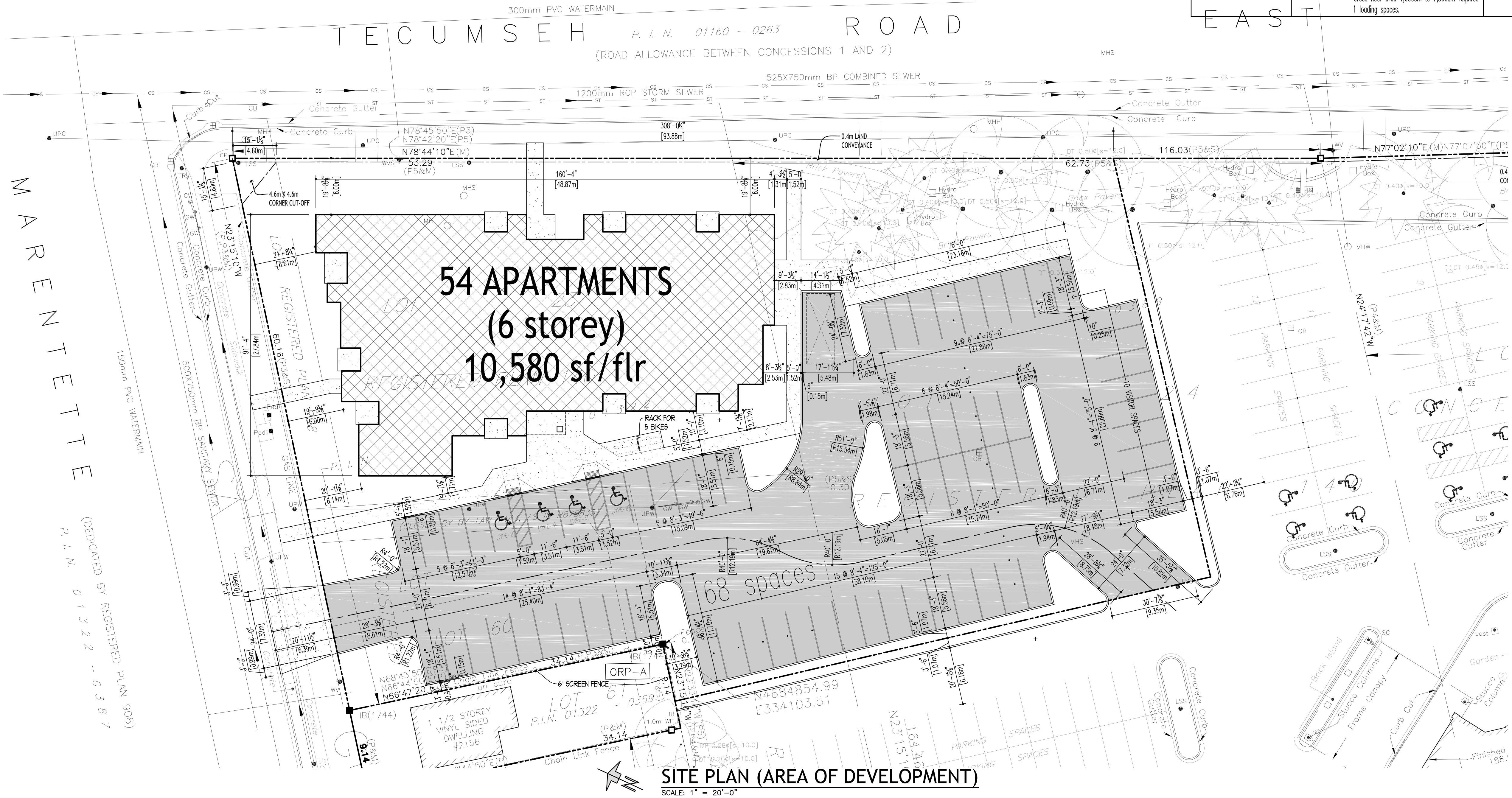
[REDACTED]



SITE KEY PLAN
SCALE: 1" = 100'-0"

EXISTING PARKING BREAKDOWN		
EXISTING PARKING REQUIRED		
CABOTO CLUB (P1,852,866) (8,533,411m)		
(Club 1/22.5m)		
8,533.41/22.5		379 spaces
Accessible Parking for 201 to 1,000 spaces		
TYPE 'A'		
(0.5 space + 1% of parking spaces)		
0.5 + (379 x 0.01)		5 spaces TYPE 'A'
TYPE 'B'		
(1 space + 1% of parking spaces)		
1 + (379 x 0.01)		5 spaces TYPE 'B'
CABOTO CLUB TOTAL PARKING REQ'D		
389 Regular spaces (2.5m X 5.5m MIN.)		
5 TYPE 'A' spaces (3.5m X 5.5m MIN.)		
5 TYPE 'B' spaces (2.5m X 5.5m MIN.)		
EXISTING PARKING		
CABOTO SITE		
613 spaces (602 reg, 11 B.F.)		
DELETED SPACES IN AREA OF DEVELOPMENT		
-76 spaces		
DELETED SPACES ON RAILWAY PROPERTY		
-13 spaces		
TOTAL PARKING SPACES		
524 total spaces (513 reg, 11 B.F.)		

DESIGN DATA TABLE		BUILDING CLASSIFICATION - O.B.C. 3.2.2.4.2	
		GROUP C, ANY HEIGHT, ANY AREA, SPRINKLERED	
		ZONING - CD 3.3, RD 1.3	
DESCRIPTION/REGULATION	REQUIRED	PROPOSED	
INTENDED USE			
LOT AREA	Minimum	For the first 19 dwelling units 1,825 s.m., for each additional unit 45 s.m. 54 units - 19 units = 35 units (35 units x 45 s.m.) + 1,825 s.m. = 3,400 s.m.	Lot Area 53,583.58 s.f. (4,978.08 s.m.) 1.23 acres (0.498 ha.)
LOT FRONTAGE	Minimum	147'-7" (45.00 m)	Tecumseh Rd. 308'-0" (93.88m)
LOT WIDTH			
BUILDING AREA			New Apartment 10,580.00 s.f. (982.94 s.m.)
LOT COVERAGE	Maximum	35%	Total Building = 19.74%
PAVED AREA			Paving 23,208.87 s.f. (2,156.23 s.m.)
LENGTH OF CURB			Curb 1,095'-0" (333.76m)
LANDSCAPE AREA	Minimum	35%	19,794.71 s.f. (1,839.03 s.m.) 36.94%
OPEN SPACE			
DWELLING UNIT DENSITY	Maximum	180 units per ha	0.498 ha x 180 = 90 units
LENGTH OF FENCE			(South) (East) 84'-8" (25.81m) 20'-11" (6.38m)
GROSS BUILDING AREA			1st Floor 10,580.00 s.f. (982.94 s.m.) 2nd Floor 10,580.00 s.f. (982.94 s.m.) 3rd Floor 10,580.00 s.f. (982.94 s.m.) 4th Floor 10,580.00 s.f. (982.94 s.m.) 5th Floor 10,580.00 s.f. (982.94 s.m.) 6th Floor 10,580.00 s.f. (982.94 s.m.) Total 63,480.00 s.f. (5,897.64 s.m.)
NUMBER OF FLOORS			6 Floors
MAX. BUILDING HEIGHT		78'-9" (24.00 m)	78'-9" (24.00 m)
SETBACKS	Front Yard Rear Yard Side Yard	19'-8" (6.00 m) 24'-7" (7.50 m) 19'-8" (6.00 m for room window) 9'-10" (3.00 m for any other)	See Site Plan
PARKING SPACES	Regular Barrier Free Type A Barrier Free Type B	(MIN. SIZE: 8'-3" X 18'-0" 2.5m X 5.5m) (MIN. SIZE: 11'-6" X 18'-0" 3.5m X 5.5m) (MIN. SIZE: 8'-3" X 18'-0" 2.5m X 5.5m)	Number Of Regular Spaces Provided = 64
	Number Of Regular Spaces Req'd 54 Apartments x 1.25 spaces per unit = 67 spaces		
	Number Of B.F. Spaces Req'd For 26 to 100 Type A = 2.0% of 67 parking spaces = 1 space Type B = 2.0% of 67 parking spaces = 1 space Total Required = 65 Regular + 2 B.F. = 67 spaces		Type A Provided = 2 spaces Type B Provided = 2 spaces Total Provided = 64 Regular + 4 B.F. = 68 spaces 15% of 68 provided spaces shall be marked as visitor spaces. = 10 spaces
BICYCLE PARKING	Regular Min. req'd bicycle spaces for 20 or more parking spaces: 2 for the first 19 spaces + 1 for each additional 20 parking spaces Total Required = 5 spaces	(MIN. SIZE: 2'-0" X 8'-3" 0.6m X 2.5m)	Total bicycle parking provided 5
LOADING AREA	Loading Area (MIN. SIZE: 9'-10" X 24'-8" 3.0m X 7.5m) Gross floor area 1,000sm to 7,500sm requires 1 loading spaces.		Loading spaces provided 1



SITE PLAN (AREA OF DEVELOPMENT)
SCALE: 1" = 20'-0"



WINDSOR ONTARIO T 519.254.2600 F 519.254.2670

All drawings and related documents are the copy-right property of the ARCHITECT and must be returned upon request. Reproduction of drawings and related documents in part or whole is forbidden without the ARCHITECT'S WRITTEN PERMISSION.

A. DETAIL NO.
B. LOCATION SHEET
C. DETAILED ON

ISSUE DATE ISSUED FOR

NOV. 22, 2023 PLANNING PRE-SUB 1
FEB. 21, 2024 PLANNING PRE-SUB 2

PROJECT

APARTMENT
FOR
CABOTO CLUB

WINDSOR ONTARIO

DRAWING TITLE

SITE KEY PLAN
SITE PLAN
DESIGN DATA TABLE

DRAWING DATE:

DRAWN BY:

CHECKED BY:

SCALE: AS SHOWN

JOB FILE NO.:

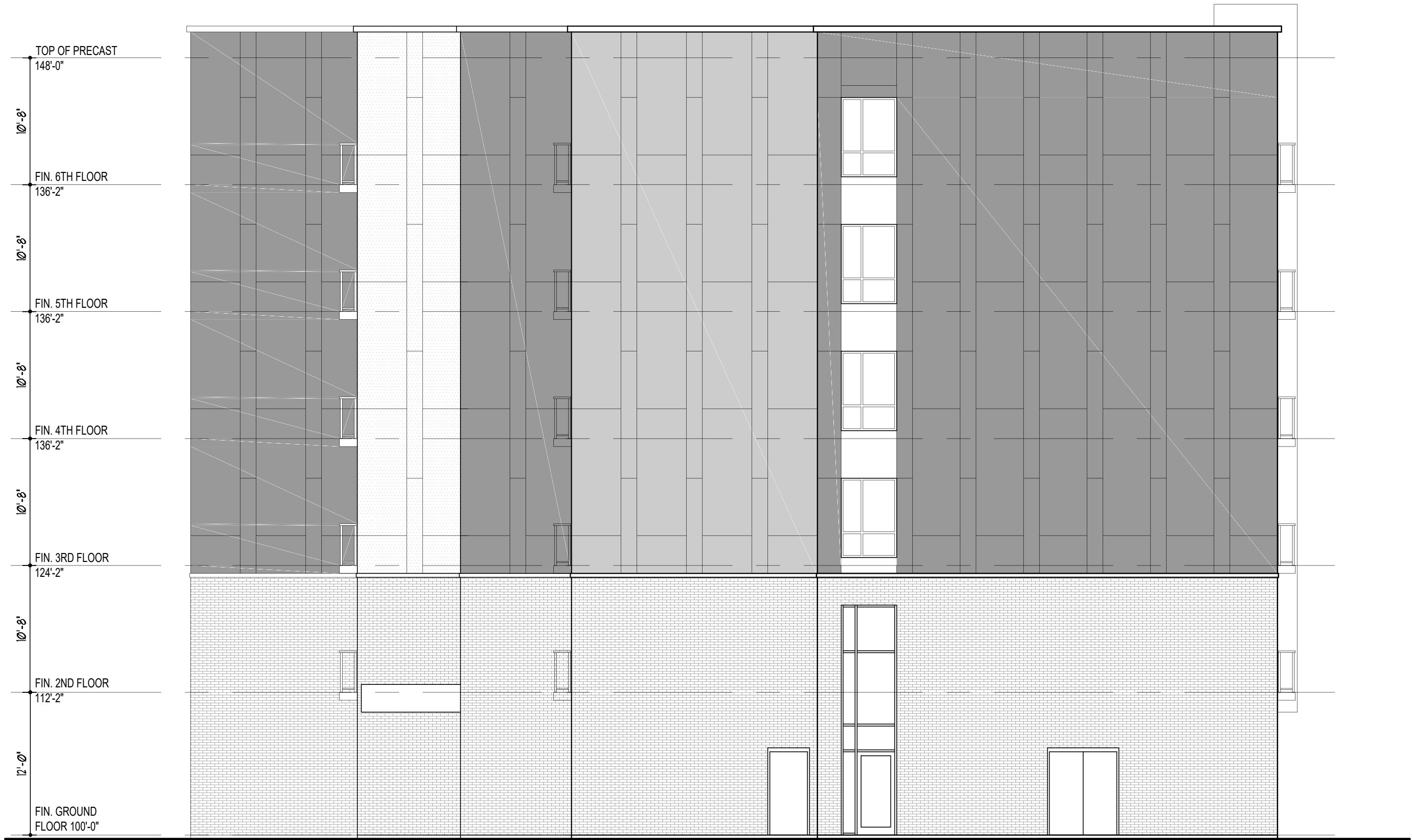
22108

DRAWING NO.:

A-100



PRELIMINARY NORTH ELEVATION
SCALE: 1/8" = 1'-0"



PRELIMINARY EAST ELEVATION
SCALE: 1/8" = 1'-0"



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NOV. 22, 2023 PLANNING PRE-SUB 1
FEB. 21, 2024 PLANNING PRE-SUB 2

PROJECT

APARTMENT
FOR
CABOTO CLUB

WINDSOR ONTARIO

DRAWING TITLE

PRELIMINARY
ELEVATIONS

DRAWING DATE:

DRAWN BY:

CHECKED BY:

SCALE: AS SHOWN DRAWING NO.:

JOB FILE NO.:

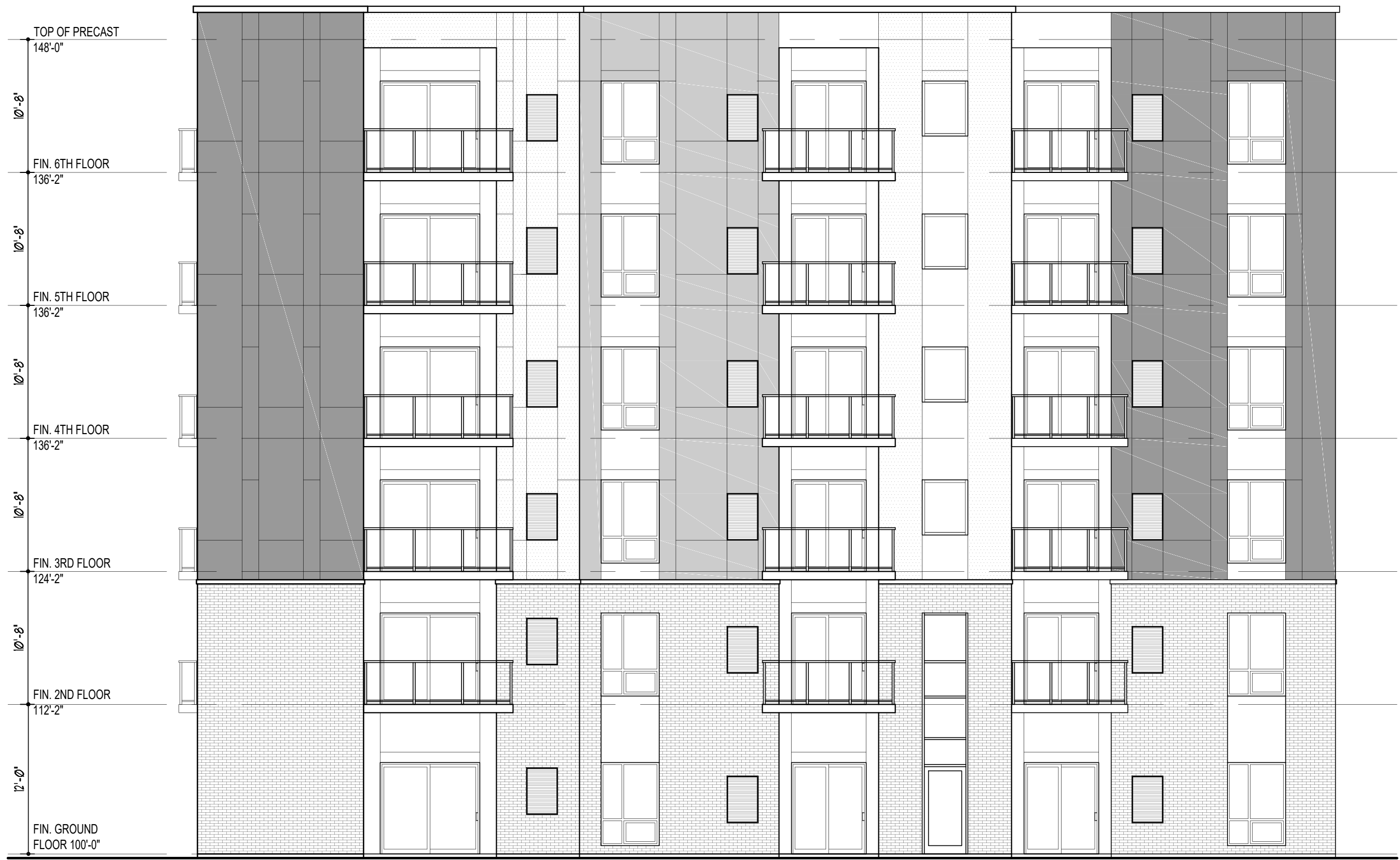
22108 A-400



WINDSOR T 519.254.2600
ONTARIO F 519.254.2670



PRELIMINARY SOUTH ELEVATION
SCALE: 1/8" = 1'-0"



PRELIMINARY WEST ELEVATION
SCALE: 1/8" = 1'-0"

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ISSUE DATE ISSUED FOR

NOV. 22, 2023 PLANNING PRE-SUB 1
FEB. 21, 2024 PLANNING PRE-SUB 2

PROJECT

APARTMENT
FOR
CABOTO CLUB

WINDSOR ONTARIO

DRAWING TITLE

PRELIMINARY
ELEVATIONS

DRAWING DATE:

DRAWN BY:

CHECKED BY:

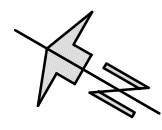
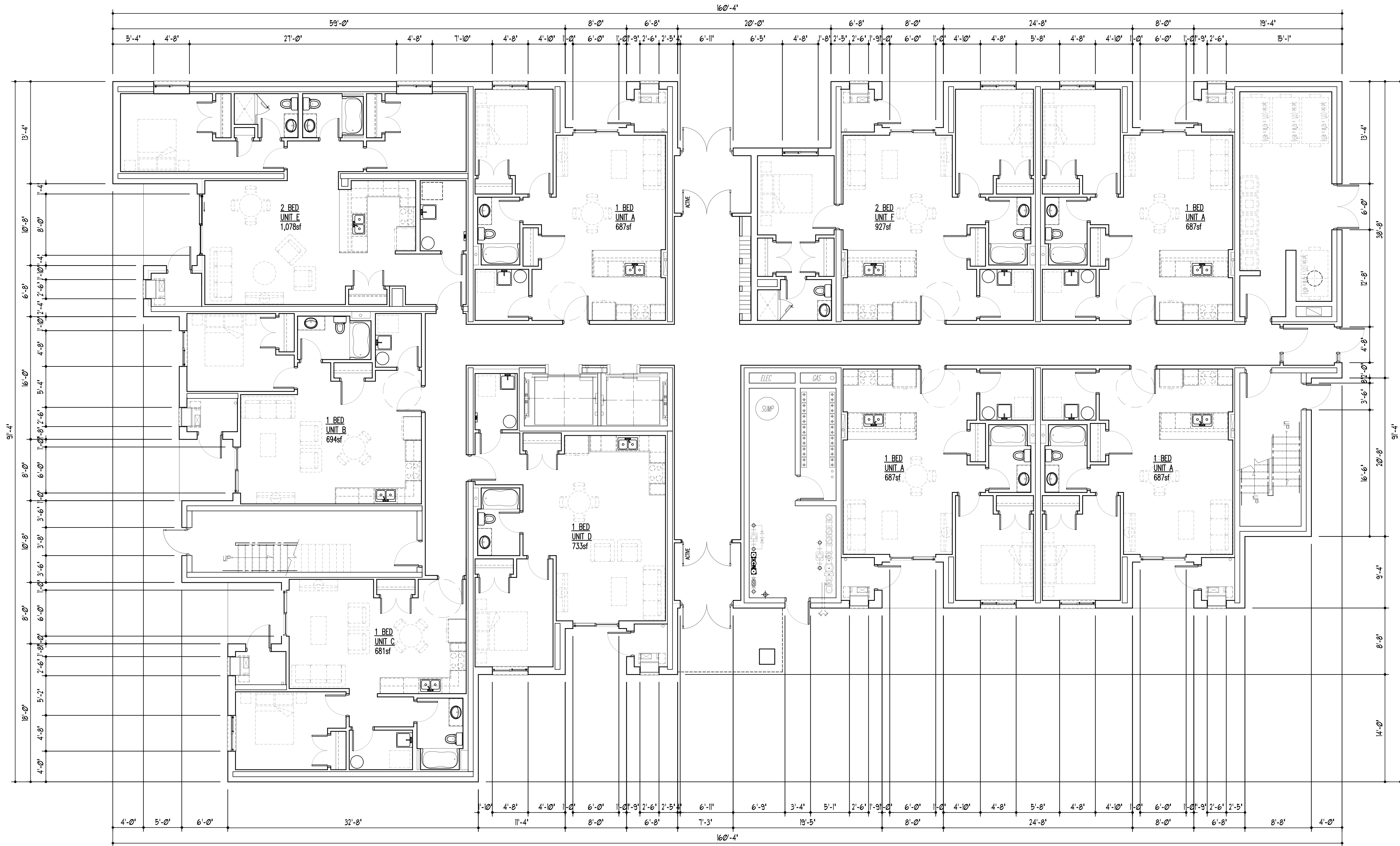
SCALE: AS SHOWN DRAWING NO.:

JOB FILE NO.: 22108 A-401



WINDSOR
ONTARIO

T 519.254.2600
F 519.254.2670



PRELIMINARY GROUND FLOOR PLAN

SCALE: 1/8" = 1'-0"

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A. DETAIL NO.
B. LOCATION SHEET
C. DETAILED ON

ISSUE DATE ISSUED FOR

NOV. 22, 2023 PLANNING PRE-SUB 1
FEB. 21, 2024 PLANNING PRE-SUB 2

PROJECT

APARTMENT
FOR
CABOTO CLUB

WINDSOR ONTARIO

DRAWING TITLE

**PRELIMINARY
GROUND FLOOR PLAN**

DRAWING DATE:

DRAWN BY:

CHECKED BY:

SCALE: AS SHOWN DRAWING NO.:

JOB FILE NO.:

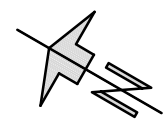
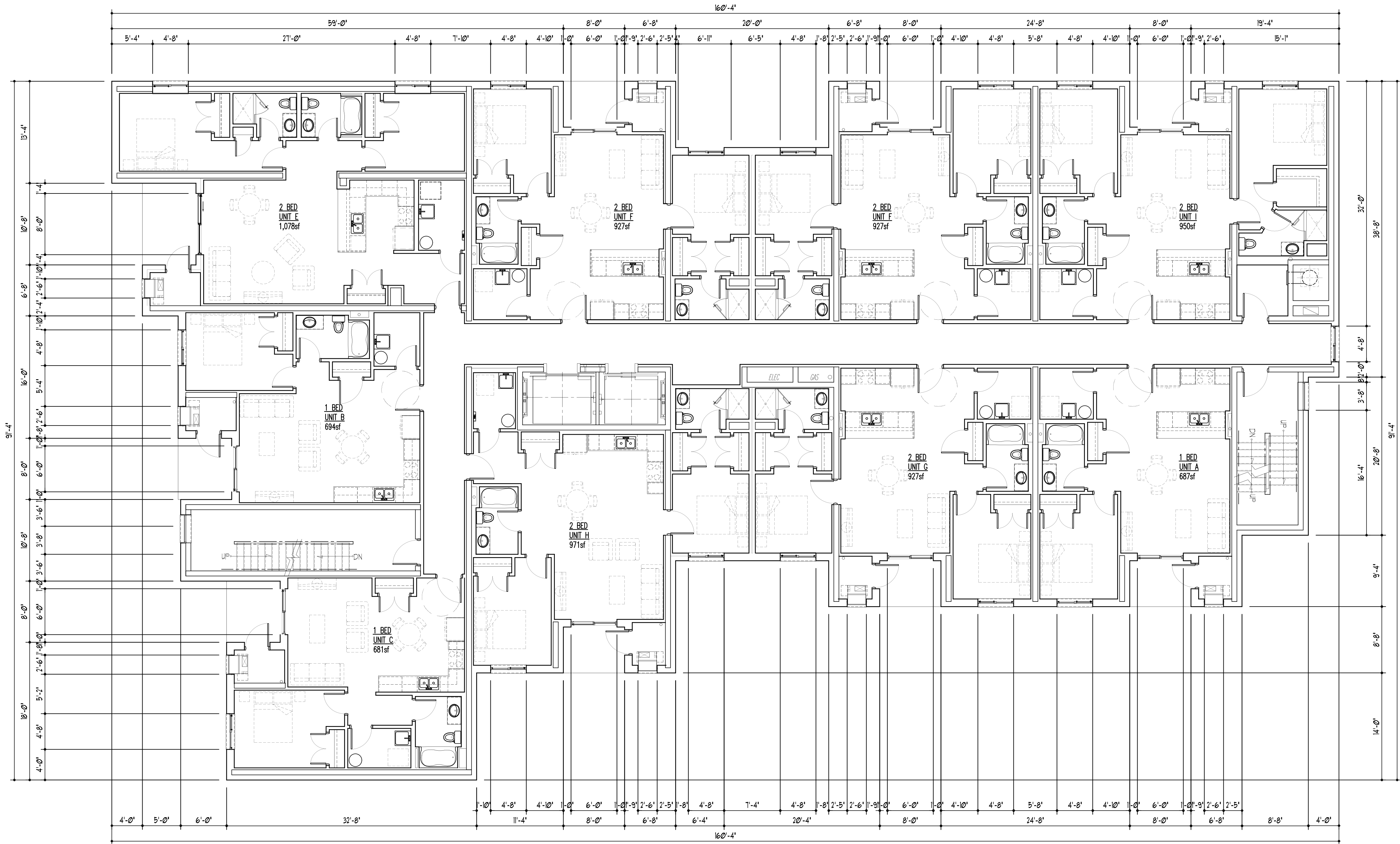
22108

A-202



WINDSOR
ONTARIO

T 519.254.2600
F 519.254.2670



PRELIMINARY 2ND - 6TH FLOOR PLAN

SCALE: 1/8" = 1'-0"

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A. DETAIL NO.
B. LOCATION SHEET
C. DETAILED ON

ISSUE DATE ISSUED FOR

NOV. 22, 2023 PLANNING PRE-SUB 1
FEB. 21, 2024 PLANNING PRE-SUB 2

PROJECT

APARTMENT
FOR
CABOTO CLUB

WINDSOR ONTARIO

DRAWING TITLE

**PRELIMINARY
2ND - 6TH FLOOR PLAN**

DRAWING DATE:

DRAWN BY:

CHECKED BY:

SCALE: AS SHOWN

JOB FILE NO.:

22108

DRAWING NO.:

A-203

APPENDIX “E” Consultations

ENGINEERING GAS

After reviewing the provided drawing at 2175 Tecumseh Rd E. and consulting our mapping system, please note that Enbridge Gas has active infrastructure in the proposed area. A PDF drawing has been attached for reference.

Please Note:

1. The shown piping locations are approximate and for information purposes only
2. The drawings are not to scale
3. This drawing does not replace field locates. Please contact Ontario One Call for onsite locates prior to excavating, digging, etc.

Enbridge Gas requires a minimum separation of 0.6 m horizontal and 0.3 m vertical from all of our plant less than NPS 16 and a minimum separation 1.0 m horizontal and 0.6 m vertical between any CER-regulated and vital pipelines. For all pipelines (including vital pipelines), when drilling parallel to the pipeline, a minimum horizontal clearance measured from the edge of the pipeline to the edge of the final bore hole of 1 m (3.3 ft) is required. Please ensure that this minimum separation requirement is maintained, and that the contractor obtains locates prior to performing any work and utilizes safe excavation practices while performing any work in the vicinity.

Also, please note the following should you find any abandoned infrastructure in the area:

- Any pipe that is excavated, please assume that it is live
- If during the course of any job, any pipe is found that is not on the locate sheet and is in conflict with your work, please call our emergency number (1-877-969-0999), and one of our Union Gas representatives will respond to determine if that plant is in fact live or dead
- Please note that our Enbridge Gas representative will respond to the live or dead call within 1-4 hours, so please plan your work accordingly

[Gord Joynson, Drafter Estimator]



ENWIN UTILITIES - HYDRO ENGINEERING

No objection provided adequate clearances are achieved and maintained. Take note of ENWIN owned hydro poles carrying overhead primary conductors at the north edge of the proposed construction area, overhead secondary conductors at the western edge of the proposed construction area, and overhead secondary conductors within the 835 Tecumseh Rd E property to service the existing building.

Be advised: The hydro poles carrying overhead secondary conductors mentioned above may require relocation/removal or accommodations in design in order to maintain adequate clearance or to clear land for construction. Removal or relocations would be at the customer's cost.

[Jeremy Allossery, Hydro Engineering Technologist]

ENWIN UTILITIES - WATER ENGINEERING

Water Engineering has no objections to the rezoning.

[Bruce Ogg, Water Project Review Officer]

ESSEX TERMINAL RAILWAY

No comments received

PARKS DEPARTMENT

Parks Development & Design has no objection to this Liaison.

[Hoda Kameli, Landscape Architect]

Natural Areas has no comments on this liaison.

[Karen Alexander, Naturalist & Outreach Coordinator]

Forestry has following comments on this property.

An onsite inspection was completed, and no city owned trees will be affected from the construction of the new apartments and parking lot.

There will be some mature privately owned trees that will be affected and likely removed.

Under the City of Windsor's Official Plan section 5.3.6.7, section 10.2.1.7 and section 10.2.14 we will be requesting a private tree inventory be done by the builder.

[Yemi Adeyeye, City Forester / Manager of Forestry & Natural Areas]

PLANNING DEPARTMENT - HERITAGE

There is no apparent built heritage concern with this property, and it is located on an area of low archaeological potential.

Nevertheless, the Applicant should be notified of the following archaeological precaution.

1. Should archaeological resources be found during grading, construction or soil removal activities, all work in the area must stop immediately and the City's Planning & Building Department, the City's Manager of Culture and Events, and the Ontario Ministry of Citizenship and Multiculturalism must be notified and confirm satisfaction of any archaeological requirements before work can recommence.
2. In the event that human remains are encountered during grading, construction or soil removal activities, all work in that area must be stopped immediately and the site secured. The local police or coroner must be contacted to determine whether or not the skeletal remains are human, and whether the remains constitute a part of a crime scene. The Local police or coroner will then notify the Ontario Ministry of Citizenship and Multiculturalism and the Registrar at the Ministry of Government and Consumer Services if needed, and notification and satisfactory confirmation be given by the Ministry of Citizenship and Multiculturalism.

Contacts:

Windsor Planning & Building Department:

519-255-6543 x6179, ktang@citywindsor.ca, planningdept@citywindsor.ca

Windsor Manager of Culture and Events (A):

Michelle Staadegaard, (O) 519-253-2300x2726, (C) 519-816-0711,
mstaadegaard@citywindsor.ca

Ontario Ministry of Citizenship and Multiculturalism

Archaeology Programs Unit, 1-416-212-8886, Archaeology@ontario.ca

Windsor Police: 911

Ontario Ministry of Government & Consumer Services

A/Registrar of Burial Sites, War Graves, Abandoned Cemeteries and Cemetery Closures, 1-416-212-7499, Crystal.Forrest@ontario.ca

[Tracy Tang, Planner III – Economic Development]

PLANNING DEPARTMENT - LANDSCAPE

The applicant has provided the required support studies as requested. Of note is the Tree inventory encompassing the proposed development. The existing landscape associated with Caboto Club appears to not be impacted by this development. The inventory identifies five trees to be removed in fair to good condition, totaling 251cm DBH. These will be required to be included in the tree requirements for Site Plan Control at an equivalent caliper rate, as required by the Landscape Manual for Development which states: One 75 mm tree for every 10 m of street frontage, or one 75 mm tree for every 250 sm of hard or soft landscaped area (whichever is greater), plus equal size diameter for any trees greater than 100 mm (4 inch) caliper removed from site.

All other requirements will be made at the time of Site Plan Control.

[Stefan Fediuk, Landscape Architect]

PLANNING DEPARTMENT - SITE PLAN CONTROL

The development proposal is subject to Site Plan Control pursuant to the Planning Act and City of Windsor By-law 1-2004. Where preceding development applications are required, inclusive of Official Plan and Zoning By-law Amendments, request for Site Plan Control Pre-Consultation Stage 1 may be made following completion of the requisite Development and Heritage Standing Committee meeting at <https://ca.cloudpermit.com/login>.

[Jacqueline Cabral, Clerk Steno]

PUBLIC WORKS DEPARTMENT

We have reviewed the subject Re-zoning and OPA application and offer the following comments:

Sewers

The site may be serviced by a 500 mm x 750 mm brick sanitary sewer and a 900 mm RCP storm sewer located within the Marentette Ave right-of-way and a 500 mm x 750 mm combined sewer and a 1200 mm RCP storm sewer located within the Tecumseh Rd E right-of-way. Any redundant sewer connections shall be abandoned in accordance with the City of Windsor Engineering Best Practice B.P 1.3.3.

A Sanitary Sewer Study dated September 1, 2023, by Aleo Associates INC, has been received. The applicant's consultant has confirmed that the existing sanitary sewers on Tecumseh Road and Marentette Avenue will effectively accommodate the site's sewer servicing needs. The study demonstrates that the municipal sanitary sewer has adequate capacity, and no adverse impacts are expected on the surrounding areas as a result of the proposed development. The Sanitary Sewer Study has been deemed acceptable, and the proposed sanitary servicing strategy is supported by the Engineering Development department.

The applicant will be required to submit, prior to the application for a building permit, a stormwater management plan in accordance with Windsor Essex Region Stormwater

Management Standards Manual, restricting stormwater runoff to predevelopment levels. This will include, at a minimum:

- Submission of stormwater management review fee
- Stormwater management report stamped by a professional engineer.
- Site servicing drawings stamped by a professional engineer.
- Stormwater management checklist (see link to website below)

For more information of SWM requirements, visit: link

- <https://essexregionconservation.ca/wp-content/uploads/2018/12/WE-Region-SWM-Standards-Manual.pdf>
- <https://www.citywindsor.ca/business/buildersanddevelopers/Pages/Stormwater-Management-Requirements.aspx>

Roads and Right-of-Way

Tecumseh Rd E is classified as a Class II Arterial Road by the Official Plan with a required right-of-way width of 27 meters according to Schedule X. The current right-of-way width is insufficient, a conveyance of approximately 0.4 m is required along Tecumseh Rd E frontage of 835 Tecumseh Rd E and 2175 Parent Ave. The new north property line of 835 Tecumseh Rd E is to be squared off to be parallel with Tecumseh Rd E.

The northwest corner of Tecumseh Rd E and Marentette Ave requires a corner cut-off in the amount of 4.6 m x 4.6 m, and the northeast corner of Tecumseh Rd E and Parent Ave requires a corner cut-off in the amount of 6.0 m x 6.0 m. It is requested that the applicant update the drawings to reflect the conveyances along the entire frontage of the property.

The latest concept site plan provided includes a new connection between the Caboto Club parking lot and the subject development, which was not included in previous submissions. The review and approval of this new parking lot connection will be addressed through Site Plan Control, subject to the submission of a Transportation Impact Study (TIS), satisfactory to Transportation Planning.

In summary we have no objection to the proposed development, subject to the following requirements:

Site Plan Control Agreement - The applicant enter into an agreement with the City of Windsor for all requirements under the General Provisions of the Site Plan Control Agreement for the Engineering Department.

Corner Cut-Off - The owner(s) agrees, prior to the issuance of a construction permit, to gratuitously convey a 4.6 m x 4.6 m (15' x 15') corner cut-off at the intersection of Marentette Ave and Tecumseh Rd E in accordance with City of Windsor Standard Drawing AS-230, and a 6 m x 6 m (20' x 20') corner cut-off at the intersection of Tecumseh Rd E and Parent Ave in accordance with City of Windsor Standard Drawing AS-230.

Land Conveyance - Prior to the issuance of a construction permit, the owner(s) shall agree to gratuitously convey to the Corporation, land sufficient to create a 27-metre-wide right-of-way on Tecumseh Rd E. This conveyance shall be approximately 0.4 metres along the entire Tecumseh Rd E frontage of both 835 Tecumseh Rd E and 2175 Parent Ave.

If you have any further questions or concerns, please contact Andrew Boroski, of this department at aboroski@citywindsor.ca

[Andrew Boroski, Technologist II]

TRANSIT WINDSOR

Transit Windsor has no objections to this development. The closest existing transit routes to this property are with the Transway 1C, Parent 14, and Route 418X. The closest existing bus stop to this property is directly adjacent to this property on Tecumseh at Marentette Southeast Corner providing direct transit access to this development. This will remain consistent with Transit Windsor's City Council Approved Transit Master Plan. There are no plans to relocate this bus stop. If the bus stop needs to temporarily be shutdown for construction, Transit Windsor requires a minimum of 2 weeks' notice.

[Jason Scott, Manager, Transportation Planning]

TRANSPORTATION PLANNING

Tecumseh Rd E is classified as a Class II Arterial Road with a required right-of-way width of 27 meters per the Schedule X. Between Marentette Ave and Parent Ave, the existing right-of-way width of Tecumseh varies between 25.5 m and 26.2 m. A varying conveyance is required to achieve the 27 m right-of-way along the (835 Tecumseh Rd E/2175 Parent Ave) property.

Parent Avenue is classified as a Local Road with a required right-of-way width of 20 meters as per the Official Plan. The existing right-of-way along the frontage of the subject property is sufficient; therefore, a conveyance is not required at this time.

Marentette Avenue is classified as a Local Road with a required right-of-way width of 20 meters per the Official Plan. The existing right-of-way along the frontage of the subject property is sufficient; therefore, a conveyance is not required at this time.

A corner cut-off of 4.6 meters is required at the intersection of Tecumseh Rd E and Marentette Ave for a non-signalized intersection.

A corner cut-off of 6.1 meters is required at the intersection of Tecumseh Rd E and Parent Ave for a signalized intersection.

All parking must comply with Zoning By-Law 8600.

All proposed bicycle parking must comply with ZBL 8600 and be clearly indicated on revised site plan

Each parking space shall have a minimum length of 5.5 metres and a minimum width of 2.5 metres, except where one side of the parking space is flanked by a wall or fence, each parking space shall have a minimum length of 5.5 metres and a minimum width of 3.5 metres.

All access aisles must be at least 6.0 metres in width for turning maneuvers and emergency vehicle access.

Parking provided for the entire site (proposed apartment and existing Caboto Club) must comply with Zoning By-Law 8600 as the proposal for the apartment removes existing parking that is normally used for the Caboto Club. A statement is required from the developer that states that the parking provided for both the apartment and Caboto Club will be compliant with the Zoning By-Law 8600.

A Transportation Impact Study is required. Please see scope attached as Appendix 1.

All accesses shall conform to the TAC Geometric Design Guide for Canadian Roads and the City of Windsor Standard Engineering Drawings.

- Driveways proposed must be 7-9 metres total at the property line (minimum 3.5m/lane, maximum 4.5m/lane)

All exterior paths of travel must meet the requirements of the Accessibility for Ontarians with Disabilities Act (AODA).

[Clare Amicarelli, Transportation Planning Coordinator]

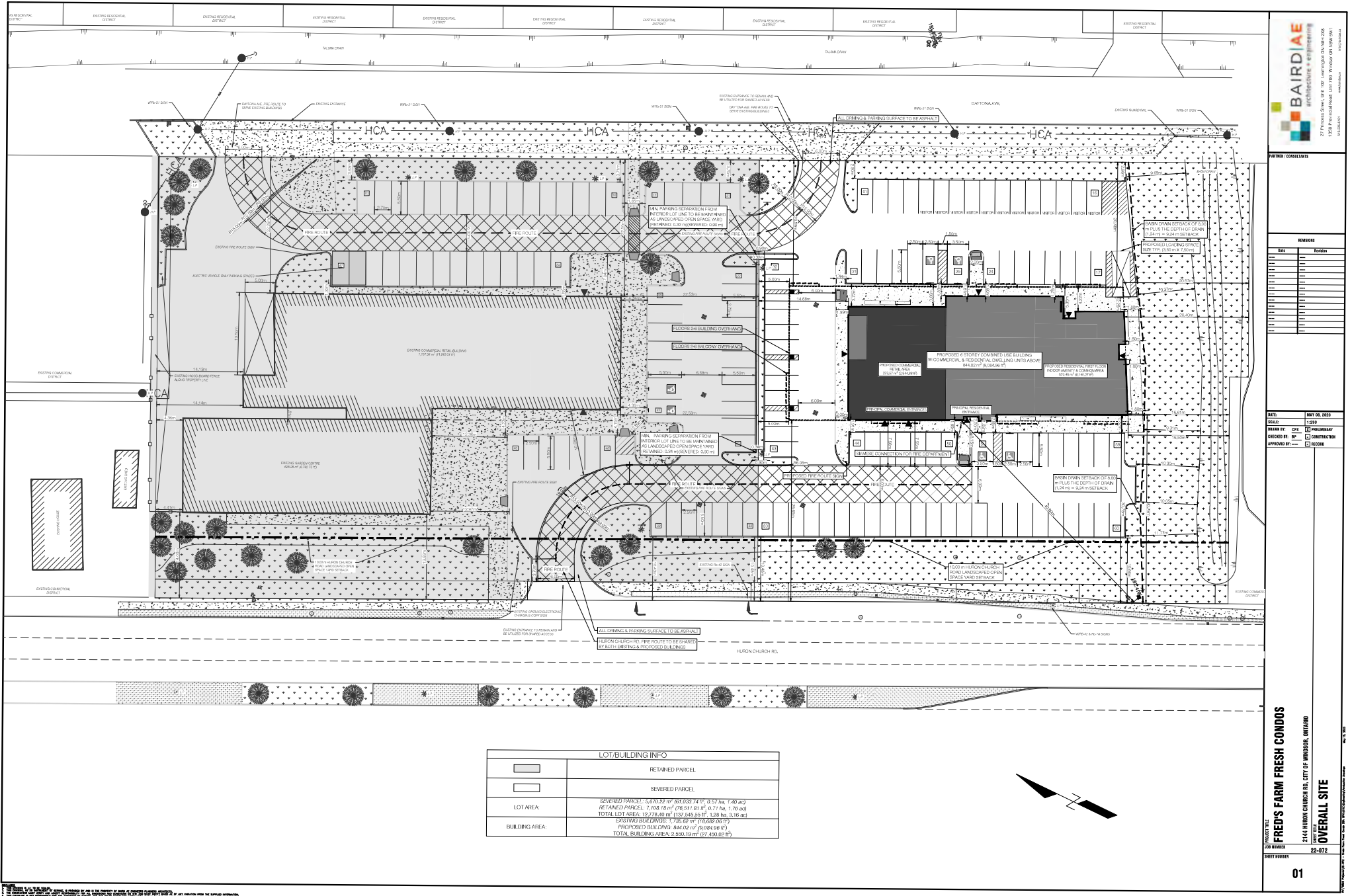
APPENDIX “1” TIS Scope

The following link contains overall information on the scope of a TIS (City of Windsor TIS Guidelines): <http://www.citywindsor.ca/residents/Traffic-And-Parking/Transportation-Planning/Documents/TIS-Guidelines.pdf>

- Traffic data collection / review at the following intersections:
 - Development Accesses
 - *The TIS must analyze the new access onto Marentette Ave as this proposal creates a new connection from the Caboto Club parking lot to Marentette Ave.*
 - Tecumseh Rd E and Parent Ave
 - Tecumseh Rd E and Marentette Ave
- Trip generation for the proposed site using ITE trip generation data.
- Traffic distribution and assignment of site generated traffic onto the road network.
 - Analysis periods: *Weekday PM peak hour, Weekday AM peak hour, and Saturday peak hour if >100 trips are expected in either peak hour.*
- Baseline traffic network modelling and development of total traffic projections for existing and future weekday/weekend conditions, accounting for possible growth of background traffic on existing and future road network.
 - Horizon years: Opening day + 5 years
- Capacity and level of service analysis
- Mode shares targets should be incorporated as assumed for this area per the Active Transportation Master Plan (pg. 35 of report); [ATMP Windsor](#)
- Identification of applicable improvements for future consideration
- Preparation of a final report (to include engineers' comments and supporting figures)

Required: Please coordinate with the involved planner to determine if there are any other known development proposals in the area. If there are proposed developments in the nearby area, the traffic generated by these developments must be accounted for in the TIS analysis.

Available City of Windsor traffic and intersection data can be acquired/purchased from traffic operations, contact: Mike Spagnuolo mspagnuolo@citywindsor.ca



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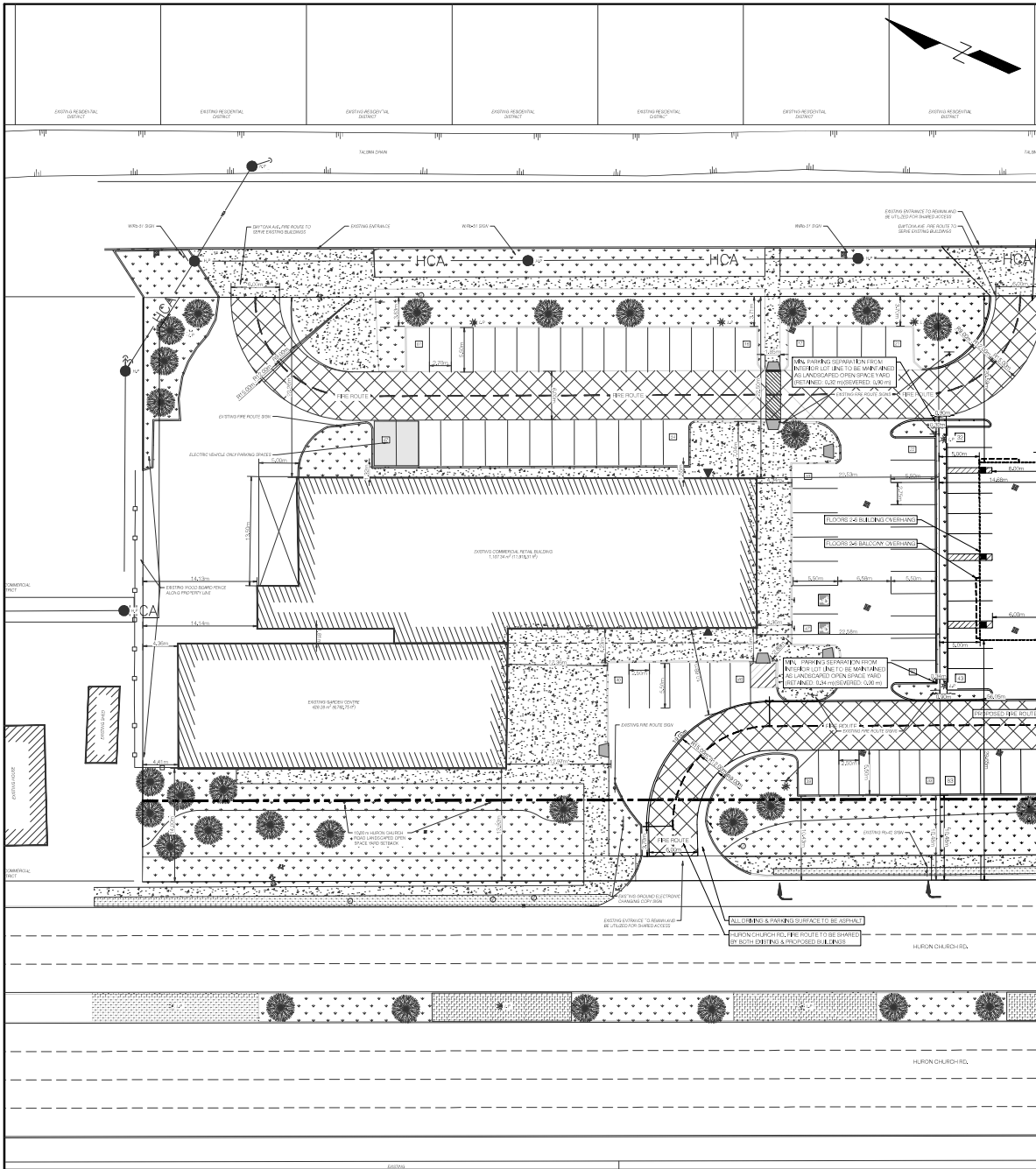
REVISIONS

DATE	REVISION

DATE: MAY 06, 2025
SCALE: 1:500
DRAWN BY: CTS
CHECKED BY: JF
APPROVED BY: JF

PROJECT TITLE: FRED'S FARM FRESH CONDOS
CLIENT: 1144 HURON CHURCH RD., CITY OF WINDSOR, ONTARIO
SHEET: OVERALL SITE
SHEET NUMBER: 22-072

01



LEGEND	
	EXISTING NUMBERED PARKING SPACES
	PROPOSED NUMBERED PARKING SPACES
	PARKING/ROAD SIGN
	ACCESSIBLE SPACE SYMBOL
	BICYCLE PARKING SPACE SIZE: 1.00m x 2.50m
	CURB CUT
	EXISTING FIRE HYDRANT
	EXISTING WATER VALVE
	PROPOSED BUILDING ENTRANCE
	PROPOSED OVERHEAD DOOR
	FLOORS 2-4 BUILDING OVERHANG
	FLOORS 2-4 BALCONY OVERHANG
	EXISTING BUILDING
	PROPOSED RESIDENTIAL GROUND FLOOR
	PROPOSED COMMERCIAL RETAIL GROUND FLOOR
	LANDSCAPED OPEN SPACE
	SIDEWALK CONCRETE
	FIRE ROUTE
	EXISTING TREE
	EXISTING LIGHT POLE (LP)
	EXISTING LIGHT POLE (HP)
	GULLY WIRE
	EXISTING CATCH BASIN
	EXISTING UTILITY MANHOLE
	EXISTING STORM MANHOLE
	EXISTING SANITARY MANHOLE
	PROPOSED SUPPORT COLUMN FOR CANTILEVERED FLOORS
	EXISTING HYDRO CABLE
	EXISTING WOODEN BOARD FENCE
	PROPERTY LINE
	DRAIN SETBACK
	10.00m HURON CHURCH ROAD LANDSCAPED OPEN SPACE YARD SETBACK

- NOTES
1. ALL FIRE DEPARTMENT SERVICE TO BE COMPLETED AFTER CONSTRUCTION IN COORDINATION WITH FIRE DEPARTMENT.
 2. FIRE ROUTE TO BE IN COMPLIANCE WITH CBC REQUIREMENTS.
 3. ALL ACCESSIBLE PARKING SPACES TO HAVE R640 SIGNS INSTALLED (TYPE 'M' TO HAVE R640 SIGN INSTALLED & TYPE 'P' TO HAVE R640 SIGN INSTALLED).
 4. PARKING AREA TO BE CLOSED IN WITH 0.150m PARALLEL CURBING.
 5. GARBAGE DISPOSAL UNIT LOCATED IN NORTHEAST REAR CORNER OF LOT FOR PICK UP.
 6. ALL LIGHTING TO BE DARK SKY COMPLIANT.

SITE DATA			
EXISTING SITE ZONING:	COMMERCIAL DISTRICT 2.1 (CD2.1) SITE SPECIFIC EXCEPTION 27B(CD2.1S-201)		
PROPOSED ZONING:	COMMERCIAL DISTRICT 2.1 SITE SPECIFIC EXCEPTION XX (CD2.1S-201XX)		
EXISTING USE:	GARDEN CENTRE & RETAIL STORE, AMONGST OTHER USES PERMITTED WITHIN THE CD2.1S-201/27B ZONE		
PROPOSED USE:	GARDEN CENTRE & RETAIL STORE, AMONGST OTHER USES PERMITTED WITHIN THE CD2.1S-201XX		
DESCRIPTION:	REQUIRED:	PROVIDED:	ZONING COMPLIANCE:
MIN. LOT AREA:	0.00 m ² (0.00 ft ²)	7,108.18 m ² (76,511.81 ft ²) 0.71 ha (1.76 ac)	COMPLIES
MIN. FRONTAGE:	0.00 m (0.00 ft)	96.62 m (316.93 ft)	COMPLIES
MAX. BUILDING HEIGHT:	14.00 m (45.93 ft)	≤ 14.00 m (45.93 ft) EXISTING	COMPLIES
MIN. FRONT YARD SETBACK (WEST):	0.00 m (0.00 ft)	13.90 m (45.61 ft) EXISTING	COMPLIES
MIN. INTERIOR YARD SETBACK (SOUTH):	0.00 m (0.00 ft)	22.53 m (73.92 ft)	COMPLIES
MIN. REAR YARD SETBACK (EAST):	0.00 m (0.00 ft)	22.26 m (73.03 ft) EXISTING	COMPLIES
MIN. INTERIOR YARD SETBACK (NORTH):	0.00 m (0.00 ft)	4.26 m (14.00 ft)	COMPLIES
MIN. HURON CHURCH ROAD LANDSCAPED OPEN SPACE (YARD):	10.00 m (32.81 ft)	10.43 m (34.22 ft) EXISTING	COMPLIES
MIN. PARKING SEPARATION FROM INTERIOR LOT LINE TO BE MAINTAINED AS LANDSCAPED OPEN SPACE (YARD):	0.90 m (2.95 ft)	0.32 m (1.05 ft)	RELIEF REQUESTED
LOT COVERAGE:	0.00% MAX.	24.42% (1,736.62 m ² , 18,682.06 ft ²)	COMPLIES
COMMERCIAL PARKING (CALCULATED BASED ON GFA (GARDEN CENTRE)):	1 SPACE PER 22.50 m ² (242.19 ft ²) OF GFA 108.28 m ² (1,162.75 ft ²) = 27 SPACES	27	COMPLIES
COMMERCIAL PARKING (CALCULATED BASED ON GFA (RETAIL STORE)):	1 SPACE PER 22.50 m ² (242.19 ft ²) OF GFA 1,107.34 m ² (11,919.31 ft ²) = 49 SPACES	40	RELIEF REQUESTED
ACCESSIBLE PARKING (INCLUDED IN TOTAL PARKING NUMBER CALCULATIONS):	BOTH TYPE 'M' & TYPE 'P' SPACES ARE TO BE 2% OF TOTAL PARKING COUNT. 60 TOTAL SPACES = 2 TYPE 'M' & 2 TYPE 'P' SPACES	1 TYPE 'M' & 1 TYPE 'P' EXISTING	COMPLIES
BICYCLE PARKING:	WHEN 20 OR MORE TOTAL PARKING SPACES THERE SHALL BE 2 BICYCLE SPACES FOR THE FIRST 19 PARKING SPACES PLUS 1 BICYCLE SPACE FOR EACH ADDITIONAL 20 PARKING SPACES. 60 PARKING SPACES = 3 BICYCLE SPACES	0 SPACES	RELIEF REQUESTED
LOADING SPACES:	FOR ANY NON-RESIDENTIAL BUILDING WITH GFA OF 2,250 m ² TO 2,500 m ² 1 LOADING SPACE. EXISTING BUILDING AREA OF 1,736.62 m ² = 1 SPACE	1 SPACE	COMPLIES
LOT BUILDING INFO:			
LOT AREA:	RETAINED PARCEL: 7,108.18 m ² (76,511.81 ft ² , 0.71 ha, 1.76 ac)		
BUILDING AREA:	EXISTING BUILDING AREA: 1,736.62 m ² (18,682.06 ft ²)		
INDOOR GROSS FLOOR AREA (GFA):	RETAIL STORE: 1,107.34 m ² (11,919.31 ft ²) GARDEN CENTRE: 608.28 m ² (6,572.75 ft ²) TOTAL BUILDING GFA: 1,736.62 m ² (18,682.06 ft ²)		
TOTAL UNITS:	1 COMMERCIAL (RETAIL & GARDEN CENTRE UNIT)		
HEIGHT OF BUILDING NUMBER OF STOREYS:	2 SINGLE STOREY BUILDINGS @ MAX 14.00m (45.93 ft)		
TOTAL PARKING SPACES:	67 TOTAL SPACES (2 ACCESSIBLE SPACES & 65 STANDARD SPACES)		
BUILDING USE, OCCUPANCY & CLASSIFICATION:	EXISTING		

KEY PLAN
1:1,750



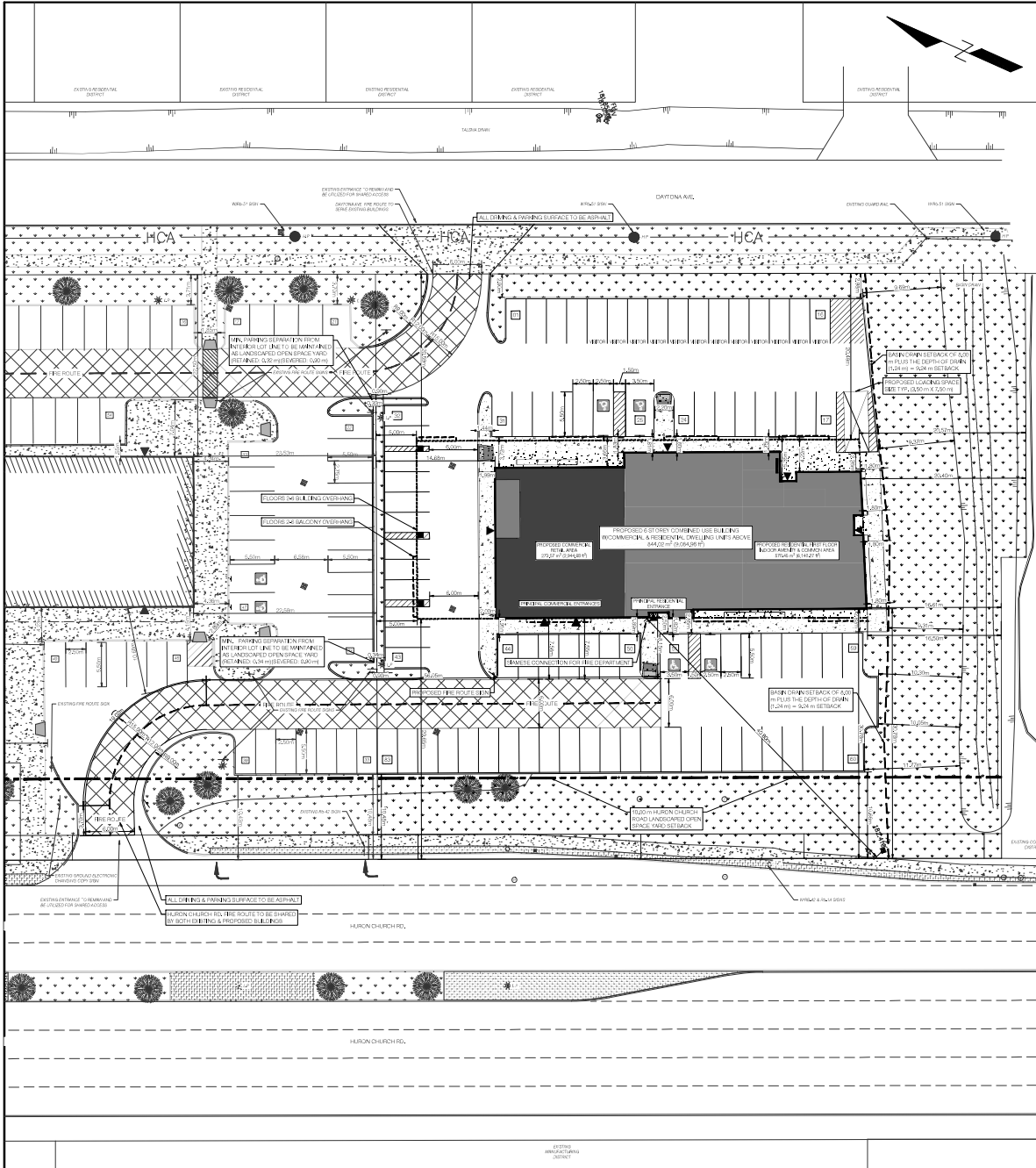
BAIRD AE
ARCHITECTURE • ENGINEERING

PREPARED BY: [Name]
CHECKED BY: [Name]
APPROVED BY: [Name]

DATE: [Date]
SCALE: [Scale]
SHEET NUMBER: 22-072

PROJECT NAME: FRED'S FARM FRESH CONDOS
2714 HURON CHURCH RD., CITY OF AMHERST, ONTARIO
CONCEPT SITE PLAN - RETAINED PARCEL

02



LEGEND	
	EXISTING NUMBERED PARKING SPACES
	PROPOSED NUMBERED PARKING SPACES
	PARKING ROAD SIGN
	ACCESSIBLE SPACE SYMBOL
	BICYCLE PARKING SPACE
	CURB CUT
	EXISTING FIRE HYDRANT
	EXISTING WATER VALVE
	PROPOSED BUILDING ENTRANCE
	PROPOSED OVERHEAD DOOR
	FLOORS 2-4 BUILDING OVERHANG
	FLOORS 2-4 BALCONY OVERHANG
	EXISTING BUILDING
	PROPOSED RESIDENTIAL GROUND FLOOR
	PROPOSED COMMERCIAL RETAIL GROUND FLOOR
	LANDSCAPE OPEN SPACE
	SIDEWALK CONCRETE
	FIRE ROUTE
	EXISTING TREE
	EXISTING LIGHT POLE (LP)
	EXISTING LIGHT POLE (HP)
	GUY WIRE
	EXISTING CATCH BASIN
	EXISTING UTILITY MANHOLE
	EXISTING STORM MANHOLE
	EXISTING SANITARY MANHOLE
	PROPOSED SUPPORT COLUMN FOR CANTILEVERED FLOORS
	EXISTING HYDRO CABLE
	EXISTING WOODEN BOARD FENCE
	PROPERTY LINE
	DRAIN SETBACK
	10.00 m HURON CHURCH ROAD LANDSCAPED OPEN SPACE YARD SETBACK

- NOTES:
1. BUILDING TO BE SPRINKLERED.
 2. ALL FIRE DEPARTMENT SERVICE TO BE COMPLETED AFTER CONSTRUCTION IN COORDINATION WITH FIRE DEPARTMENT.
 3. FIRE ROUTE TO BE IN COMPLIANCE WITH CBC REQUIREMENTS.
 4. ALL ACCESSIBLE PARKING SPACES TO HAVE REAR SIGNS INSTALLED (TYPE 'M' TO HAVE REAR SIGN INSTALLED & TYPE 'M' TO HAVE REAR SIGN INSTALLED).
 5. PARKING AREA TO BE CLOSED IN WITH 0.150 m BARRIER CURBING.
 6. GARBAGE DISPOSAL UNIT LOCATED WITHIN THE BUILDING, TO BE ROLLED OUT TO LOADING AREAS FOR PRIVATE SERVICE PICK UP.
 7. ALL LIGHTING TO BE DARK SKY COMPLIANT.

SITE DATA:			
EXISTING SITE ZONING:	COMMERCIAL DISTRICT 2.1 (CD2.1) SITE SPECIFIC EXCEPTION (CD2.1-SS.01(028))		
PROPOSED SITE ZONING:	COMMERCIAL DISTRICT 2.2 (CD2.2) SITE SPECIFIC EXCEPTION (CD2.2-SS.01(028))		
PERMITTED USE:	DWELLING UNITS IN A COMMERCE USE BUILDING WITH RETAIL STORE ON THE MAIN FLOOR, AMONGST OTHER USES PERMITTED WITHIN THE CD2.2-SS.01(028) ZONE.		
PROPOSED USE:	DWELLING UNITS IN A COMMERCE USE BUILDING		
DESCRIPTION:	REQUIRED:	PROVIDED:	COMING COMPLIANCE:
MIN. LOT AREA:	5,000 m ² (6,000 ft ²)	5,670.22 m ² (61,032.74 ft ²)	COMPLIES
MIN. FRONTAGE:	0.00 m (0.00 ft)	0.57 m (1.89 ft)	COMPLIES
MAX. BUILDING HEIGHT:	14.00 m (45.93 ft)	25.12 m (82.42 ft)	RELIEF REQUESTED
MIN. FRONT YARD SETBACK (M/S):	0.00 m (0.00 ft)	29.00 m (95.28 ft)	COMPLIES
MIN. INTERIOR YARD SETBACK (SOUTH):	0.00 m (0.00 ft)	16.81 m (55.15 ft)	COMPLIES
MIN. REAR YARD SETBACK (EAST):	0.00 m (0.00 ft)	20.48 m (67.19 ft)	COMPLIES
MIN. INTERIOR YARD SETBACK (NORTH):	0.00 m (0.00 ft)	5.00 m (16.40 ft)	COMPLIES
MIN. HURON CHURCH ROAD LANDSCAPED OPEN SPACE (M/S):	10.00 m (32.81 ft)	10.48 m (34.38 ft)	COMPLIES
MIN. PARKING SEPARATION FROM INTERIOR LOT LINE TO BE MAINTAINED AS LANDSCAPED OPEN SPACE YARD:	0.90 m (2.95 ft)	0.90 m (2.95 ft)	COMPLIES
MIN. PARKING SEPARATION FROM ANY OTHER STREET:	3.00 m (9.84 ft)	2.90 m (9.51 ft)	RELIEF REQUESTED
MAX. LOT COVERAGE:	N/A	21.23%	COMPLIES
AMENITY AREA:	12.00 m ² (129.17 ft ²) PER DWELLING UNIT, 58 DWELLING UNITS = 696.00 m ² (7,491.88 ft ²)	INDOOR AMENITY AREA: 213.40 m ² (2,291.02 ft ²) OUTDOOR LANDSCAPE OPEN SPACE: 1,543.30 m ² (16,612.38 ft ²) TOTAL AMENITY AREA: 1,756.70 m ² (18,903.40 ft ²) = 30.28 m ² (322.06 ft ²) PER DWELLING UNIT	COMPLIES
RESIDENTIAL PARKING CALCULATED BASED ON GFA:	MULTIPLE DWELLING CONFORMING A MINIMUM OF 1.25 SPACES PER DWELLING UNIT, 58 DWELLING UNITS = 72 SPACES	1.25 SPACES PER DWELLING UNIT, 58 DWELLING UNITS = 71 SPACES	RELIEF REQUESTED
COMMERCIAL RETAIL PARKING CALCULATED BASED ON GFA:	1 SPACE PER 22.5 m ² (242.49 ft ²) OF COMMERCIAL RETAIL GFA, 273.57 m ² (2,944.68 ft ²) GFA = 12 SPACES	12	COMPLIES
ACCESSIBLE PARKING (INCLUDED IN TOTAL PARKING NUMBER CALCULATIONS):	BOTH TYPE 'M' & TYPE 'M' SPACES ARE TO BE 2% OF TOTAL PARKING COUNT, 77 TOTAL SPACES = 2 TYPE 'M' & 2 TYPE 'M' SPACES	2 TYPE 'M' & 2 TYPE 'M'	COMPLIES
VISITOR PARKING:	MIN OF 10% OF PARKING SPACES SHALL BE MARKED FOR VISITOR PARKING = 12 SPACES	12 SPACES	COMPLIES
BICYCLE PARKING:	WHEN 20 OR MORE TOTAL PARKING SPACES THERE SHALL BE 2 BICYCLE SPACES FOR THE FIRST 19 PARKING SPACES PLUS 1 BICYCLE SPACE FOR EACH ADDITIONAL 20 PARKING SPACES, 83 PARKING SPACES = 8 BICYCLE SPACES	8 BICYCLE SPACES	COMPLIES
LOADING SPACES:	FOR A MULTIPLE DWELLING WITH 4 OR MORE UNITS THAT HAVE A GFA OF 1,000 m ² TO 7,200 m ² = 1 SPACE	1 SPACE FOR 6,963.24 m ² (75,186.96 ft ²) RESIDENTIAL GFA	COMPLIES
LOT BUILDING INFO:			
LOT AREA:	5,670.22 m ² (61,032.74 ft ²)	0.57 ha, 1.40 ac	
BUILDING AREA:	PROPOSED BUILDING AREA: 644.02 m ² (6,944.68 ft ²)		
INDOOR GROSS FLOOR AREA (GFA):	COMMERCIAL (FLOOR 1) 273.57 m ² (2,944.68 ft ²) RESIDENTIAL DWELLING UNITS FLOORS 2-5: 4,633.45 m ² (50,244.25 ft ²) AMENITY & COMMON AREA FLOORS 1, 2, 3, 4, 5: 6,963.24 m ² (75,186.96 ft ²) TOTAL GFA: 10,870.26 m ² (117,376.89 ft ²)		
TOTAL UNITS:	58 RESIDENTIAL DWELLING UNITS & 2 COMMERCIAL RETAIL UNITS		
HEIGHT OF BUILDING NUMBER OF STOREYS:	6 STOREYS @ 20.12 m (66.00 ft)		
CONCRETE CURBING LENGTH:	421.27 m (1,383.76 ft)		
TOTAL PARKING SPACES:	83 TOTAL SPACES (4 ACCESSIBLE SPACES, 12 VISITOR SPACES, & 67 STANDARD SPACES)		
BUILDING USE, OCCUPANCY, & CLASSIFICATION:	RESIDENTIAL GROUP 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100		

27 Prince Street Unit 103, Cambridge ON N6H 2M6
416-291-0000 (toll free) 416-291-0001 (local)
www.bairdae.com

DATE: MAY 04, 2023
DRAWN BY: JLD
CHECKED BY: JLD
APPROVED BY: JLD

PROJECT NAME: FRED'S FARM FRESH CONDOS
2714 HURON CHURCH RD., CITY OF WINNIPEG, ONTARIO
CONCEPT SITE PLAN - SEVERED PARCEL
JOB NUMBER: 22-072
SHEET NUMBER: 03

APPENDIX D – CONSULTATION

CANADA POST – Bruno Desando - bruno.desando@canadapost.ca

This development, as described, falls within our centralized mail policy. I will specify the condition which I request to be added for Canada Post Corporation's purposes.

Canada Post's multi-unit policy, which requires that the owner/developer provide the centralized mail facility (front loading lockbox assembly or rear-loading mailroom [mandatory for 100 units or more]), at their own expense, will be in effect for buildings and complexes with a common lobby, common indoor or sheltered space. Should the description of the project change, I would appreciate an update to assess the impact of the change on mail service. If you have any questions or concerns regarding these conditions, please contact me. I appreciate the opportunity to comment on this project.

Lock-Box Assembly Requirements - The Canada Post Standards Manual for Builders & Developers can be downloaded at:

https://www.canadapost.ca/cpo/mc/assets/pdf/business/standardsmanual_en.pdf

Compartments Size - Horizontal lock-box models used in mailrooms must have the following minimums:

- o Residential compartments must be at least 12.5 x 13.5 cm
- o Commercial compartments at least 13.5 x 30.5 cm
- o Parcel compartments at least 30.5 x 30.5 cm

Vertical lock-box models must have min comp size of 25 x 12.5 cm. (Most models are 40 x 12.7 cm)

Heights - All lock-box assemblies must be installed in a manner that will not require the delivery employee to reach higher than 170cm or lower than 45cm when delivering to the equipment. With respect to horizontal lock-boxes, the limits above will likely mean that maximum number of compartments that can be included in each column of residential compartments would be eight

Rear-loading Lock-boxes - Projects with more than 100 units are required to be serviced via a rear-loading lock-box assembly. There must be a width of at least 100cm of working space from the back of the boxes to the wall. A ledge under the bottom row of boxes is also recommended in rear-loading designs. This ledge is to be directly under the bottom row of boxes (no space between ledge and bottom of boxes) and must stick out at least 20cm from the back of the boxes. Mailroom door is required to provide a minimum 81cm opening. Lighting should be at least 100 lux (measured 75 cm from floor)

Access - All buildings where the lock-boxes are required to be serviced from inside the building are required to install a Canada Post Crown lock in the building intercom. The intercom is prefabricated with an internal housing for the lock. The lock can be obtained from the local deliver supervisor. If the building has more than 100 units, a rear-loading lock-box assembly will be installed. The door to the Canada Post delivery area must be fitted with a specific model of deadbolt. This is because Canada Post will supply a key cylinder made specifically for the Canada Post key that will fit inside the deadbolt purchased by the developer.

Numbering - Compartments should be numbered vertically and left to right on the delivery side of the boxes.

APPENDIX D – CONSULTATION

Grade-level Components - If the development includes grade level retail or residential units, please take note that door-to-door delivery will not be provided to these units. Canada Post is happy to install a Community Mailbox to provide service to these units. Please coordinate a location with the Canada Post Delivery Planner for the area. If there is no room on the property for the Community Mailbox, service can be provided via another Community Mailbox in the area. Options to service the units from the tower (lobby) lock-boxes or via a front-loading lock box erected on the outside of the building can also be discussed with the Delivery Planner.

CITY OF WINDSOR – ENGINEERING DEVELOPMENT & ROW – Juan Paramo - Revised 2024 Mar 25

The Engineering Development & ROW departments have conducted a preliminary review of the supporting documents pertaining to the subject application and have the following comments:

Sanitary and Storm Sewers - A Functional Servicing Study, dated May 2023 and revised December 2023 by Baird AE, has been received and reviewed. The applicant's consultant has confirmed that the existing 250mm PVC sanitary sewer on Daytona Avenue and the existing 675mm RCP storm sewer on Huron Church Road will effectively accommodate the site's sewer servicing needs. The study demonstrates that both the municipal storm and sanitary sewers have adequate capacity, and no adverse impacts are expected on the surrounding areas because of the proposed development.

The Functional Servicing Study has been deemed acceptable, and the proposed servicing strategy is supported by the Engineering Development department.

The proponent will be required to use existing connections to municipal sewer where feasible. Any redundant connections shall be abandoned in accordance with the City of Windsor Engineering Best Practice B.P 1.3.3. All private storm and sanitary sewers must be located within private property. If a sanitary sampling manhole is not already in place, one may be required for any sanitary connection at the property line, to the satisfaction of the City Engineer.

The applicant will be required to submit, prior to the issuance of building permits, a stormwater management plan in accordance with Windsor Essex Region Stormwater Management Standards Manual, restricting stormwater runoff to pre development levels.

The submission for a stormwater management plan will include, at a minimum:

- a. Submission of stormwater management review fee,
- b. Stormwater management report stamped by a professional engineer.
- c. Stormwater management check list (see link below)
- d. Site servicing drawings stamped by a professional engineer.

Submission of a stormwater management report alone will be deemed incomplete, unless accompanied by the additional requirements specified above. Please visit the [City of](#)

APPENDIX D – CONSULTATION

[Windsor Website](#) and the [ERCA Website](#) for additional information on stormwater management requirements.

Municipal Drains - The Janisse Drain is a municipal drain with by-laws and governed under the Drainage Act. The municipal drain currently runs through the subject property, along the southern limit. The site is within the regulated area under the jurisdiction of the Essex Region Conservation Authority (ERCA). Any impact or alterations to the Janisse Drain would be subject to the provisions of the Drainage Act administered through the City of Windsor.

Right-of-Way - The Official Plan classifies Huron Church Road as a Class 1 Arterial Road with a required right-of-way width of 46 meters. While the existing right-of-way is deficient, it has been determined that no conveyance is required at this time.

In summary we have no objection to the proposed development, subject to the following requirements:

Site Plan Control Agreement – The applicant enters into an agreement with the City of Windsor for all requirements under the General Provisions of the Site Plan Control Agreement for the Engineering Department.

Reciprocal Access & Services – The owner agrees to enter into a reciprocal agreement with the abutting property owners for access and services.

ERCA Requirements – The owner(s) further agrees to follow all drainage and flood proofing recommendations of the Essex Region Conservation Authority (ERCA) may have with respect to the subject land, based on final approval by the City Engineer. If applicable, the Owner will obtain all necessary permits from ERCA with respect to the drainage works on the subject lands.

Drain Easement – Prior to the issuance of a construction permit, the owner shall gratuitously grant a 6m easement along the south property line of 2144 Huron Church Rd for the maintenance and improvement of the Janisse Drain.

Utility Easement – Prior to the issuance of a construction permit, the owner shall gratuitously grant a 1m utility easement along the west property line from the northern boundary of the property to the existing driveway on Huron Church Rd and a 2m utility easement along the west property line from the existing driveway on Huron Church Rd to the south property line in order to have sufficient space around the existing fire hydrants for maintenance.

If you have any further questions or concerns, please contact Andrew Boroski, Technologist II, at aboroski@citywindsor.ca

CITY OF WINDSOR - FORESTRY

City Forester - Yemi Adeyeye - There are 4 city owned trees in fair health observed on the Daytona side of this development. Please request a root zone protection plan be developed by the contractor and implemented for these trees prior to construction. The developer needs to provide a private tree inventory for this site to identify species, size, and health of existing trees.

APPENDIX D – CONSULTATION

Natural Areas - Karen Alexander - This area is Regulated Eastern Foxsnake habitat and the drain feature along the south edge of the development proposal would be considered preferred habitat with potential for use. Provided the ~9 m setback from the drain stays as planned, along with no mowing in this setback, all should be fine. I recommend requesting on-site workers receive SAR Training.

Parks - Hoda Kameli - Parks D&D has no objection.

CITY OF WINDSOR – PLANNING & DEVELOPMENT SERVICES - HERITAGE PLANNER – Kristina Tang

There is no apparent built heritage concern with this property, and it is located on an area of low archaeological potential. Nevertheless, the Applicant should be notified of the following archaeological precaution.

1. Should archaeological resources be found during grading, construction or soil removal activities, all work in the area must stop immediately and the City's Planning & Building Department, the City's Manager of Culture and Events, and the Ontario Ministry of Citizenship and Multiculturalism must be notified and confirm satisfaction of any archaeological requirements before work can recommence.
2. If human remains are encountered during grading, construction or soil removal activities, all work in that area must be stopped immediately and the site secured. The local police or coroner must be contacted to determine whether the skeletal remains are human, and whether the remains constitute a part of a crime scene. The Local police or coroner will then notify the Ontario Ministry of Citizenship and Multiculturalism and the Registrar at the Ministry of Government and Consumer Services if needed, and notification and satisfactory confirmation be given by the Ministry of Citizenship and Multiculturalism.

Contacts:

Windsor Police: 911

Windsor Planning & Building Department, 519-255-6543 x6179, ktang@citywindsor.ca, planningdept@citywindsor.ca

Windsor Manager of Culture and Events, Michelle Staaedegaard, (O) 519-253-2300x2726,
(C) 519-816-0711, mstaadegaard@citywindsor.ca

Ontario Ministry of Citizenship and Multiculturalism, Archaeology Programs Unit,
1-416-212-8886, Archaeology@ontario.ca

Ontario Ministry of Government & Consumer Services, A/Registrar of Burial Sites, War Graves, Abandoned Cemeteries and Cemetery Closures, 1-416-212-7499,
Crystal.Forrest@ontario.ca

CITY OF WINDSOR – PLANNING & DEVELOPMENT SERVICES - SITE PLAN CONTROL

APPENDIX D – CONSULTATION

The development proposal is subject to Site Plan Control pursuant to the Planning Act and City of Windsor By-law 1-2004. Where preceding development applications are required, inclusive of Official Plan and Zoning By-law Amendments, request for Site Plan Control Pre-Consultation Stage 1 may be made following completion of the Development and Heritage Standing Committee meeting at <https://ca.cloudpermit.com/login>.

CITY OF WINDSOR – PLANNING & DEVELOPMENT SERVICES - ZONING COORDINATOR– Ana Lucas

- **Current Zoning Designation:** CD2.1, S.20(1)278
- **Proposed Zoning Designation:**
 - Retained lot (north): CD2.1, S.20(1)278, with site specific requested relief.
 - Severed lot (south): CD2.2, S.20(1)278, with site specific requested relief.

Comments:

Provide lot dimensions on site plan.

Possible expansion of greenhouse as it's larger than shown on SPC-032/10 drawing.

Revise drawings to show elevation of crown of road at Huron Church Road which is defined as Grade and is used to determine building height.

Existing Outdoor Storage Yard present in the northeast corner of retained lot, blocking access area into parking area, and blocking parking spaces. No screening fence present. The items stored are not goods displayed as an accessory to retail store, but a transport trailer, shipping containers, crates, shelving etc.

A minimum of 15 percent of parking spaces provided for dwelling units shall be marked as visitor parking. Parking spaces for commercial use in a Combined Use Building don't need to be included in calculation of required visitor parking spaces.

Bicycle parking spaces not labelled on submitted drawing.

Shared access agreement required for access areas into parking area.

Indicate on drawings which parking spaces are existing and which are new.

Existing refuse bins not shown on submitted drawings.

The proposed development doesn't meet the Zoning By-law 8600 as follows:

Severed Lot:

- Maximum Building Height: (15.2.5.4)
 - 14.0 m (Required)
 - 20.12 m (Provided) – Requested in PRR
- Required Number of Parking Spaces: (24.20.5.1)
 - 84 (Required)
 - 83 (Provided) – Requested in PRR (12 Commercial, 71 Residential)
- Minimum Parking Rate for Multiple Dwelling containing a minimum of 5 Dwelling units: (24.20.5.1) (Severed lot)

APPENDIX D – CONSULTATION

- 1.25 for each dwelling unit (Required)
- 1.23 for each dwelling unit (Provided) – See above
- Minimum Parking Area Separation from a street: (25.5.20.1.2)
 - 3.00 m (Required)
 - 2.90 m (Provided) – Requested in PRR
- Minimum Parking Area Separation from a building wall in which is located a main pedestrian entrance facing the parking area: (25.5.20.1.5)
 - 2.00 m (Required)
 - 1.90 m (Provided at Retail Store entrances)
 - 1.80 m (Provided at east dwelling entrance)
- Minimum Parking Area Separation from a building wall containing a habitable room window or containing both a main pedestrian entrance and a habitable room window facing the parking area where the building is located on the same lot as the parking area: (25.5.20.1.6)
 - 4.50 m (Required)
 - 0 m (Provided)
 - Parking area separations are measured to any building wall containing a habitable room window, including windows on the 2nd floor and higher.

Retained Lot:

- Required Number of Parking Spaces: (24.20.5.1)
 - 76 (Required)
 - 67 (Provided) – Requested in PRR
- Required Number of Bicycle Parking Spaces: (24.30.1.1)
 - 5 (Required)
 - 0 (Provided)
 - 6 bicycle parking spaces were initially provided as per SPC-032/10, and visible in Google Streetview in 2012, but removed from site afterwards.
- Minimum Parking Area Separation from an interior lot line: (25.5.20.1.3)
 - 0.90 m (Required)
 - 0.32 m (Provided at south interior lot line)

CITY OF WINDSOR - TRANSIT WINDSOR – Jason Scott

Transit Windsor has no objections to this development. The closest existing transit route to this property is with the Central 3. The closest existing bus stop to this property is located on Industrial at Ambassador Southwest Corner. This bus stop is approximately 330 m away from this property falling within Transit Windsor's 400 m walking distance guidelines to a bus stop.

Transit service will be improved in this area in 2024 with the implementation of a new local route as part of Transit Windsor's City Council approved 2023 Service Plan. This route will provide direct transit service in both directions along Daytona Avenue between

APPENDIX D – CONSULTATION

Northwood Street and Totten Street. This remains consistent with Transit Windsor's City Council Approved Transit Master Plan.

CITY OF WINDSOR – TRANSPORTATION PLANNING – Clare Amicarelli

Huron Church Road is a Class I Arterial Road with a required right-of-way width of 46 meters per the Official Plan. The existing right-of-way along the frontage of the subject property is not sufficient. A 1 metre utility easement from the northern boundary of the property to the northern edge of the existing driveway is required to have sufficient space around the existing fire hydrant for maintenance. Similarly, a 2-metre utility easement from the southern boundary of the property to the southern edge of the existing driveway is required to have sufficient space around the other existing fire hydrant for maintenance.

Daytona Avenue is classified as a Local Road with a required right-of-way width of 20 meters as per the Official Plan. The current right-of-way width is sufficient; therefore, a conveyance is not required.

Transportation Planning has reviewed the Parking Justification Report submitted for the above-noted application, titled, "Fred's Farm Fresh Condos Mix-Use Development 2144 Huron Church Road Windsor, Ontario" dated May 8, 2023 with project number "22-072" reviewed by Shurjeel Tunio, P.Eng. Senior Project Manager and prepared by Nii Nartey Nartey, M.Eng. E.I.T. Junior Engineer of Baird AE and has reviewed the Traffic Impact Study submitted for the above-noted application, titled, "Fred's Farm Fresh Condos Mix-Use Development 2144 Huron Church Road Windsor, Ontario" dated May 8, 2023 with project number "22-072" by Shurjeel Tunio, P.Eng. Senior Project Manager. Below are Transportation Planning's comments on the two submissions;

- The studies submitted are the same studies submitted at Pre-Consultation PC 034-23 (STAGE 2), therefore, the comments made at Pre-Consultation are still valid and still must be addressed (please refer to Transportation Planning's comments made through PC 034-23 (STAGE 2))
- In addition, the consultant must ensure the same land use codes are used in the revised TIS and revised Parking Study.
- Extension of phase timing as a mitigation approach is proposed to improve traffic operations at the intersection of Huron Church Road and Northwood Street. The consultant is to simulate and evaluate the proposed mitigation. Also, the consultant is to consider advanced green phase for EB as another mitigation and evaluate the intersection operation under this situation.
- The consultant must submit the traffic simulation files.

All accesses shall conform to the TAC Geometric Design Guide for Canadian Roads and the City of Windsor Standard Engineering Drawings.

Reciprocal access agreement required between the owners of the retained and severed parcel to access both parking lots as proposed on the site plan.

All exterior paths of travel must meet the requirements of the Accessibility for Ontarians with Disabilities Act (AODA).

APPENDIX D – CONSULTATION

ENWIN - HYDRO ENGINEERING - Zachary Mancini

No Objection provided adequate clearances are achieved and maintained.

ENWIN has existing overhead pole lines with 27,600 volt primary, 347/600 volt secondary, and 120/240 volt secondary hydro distribution East of the development property.

Prior to working in these areas, we would suggest notifying your contractor and referring to the Occupational Health and Safety Act and Regulations for Construction Projects to confirm clearance requirements during construction.

Also, we suggest referring to the Ontario Building Code for permanent required clearances for New Building Construction.

ENWIN - WATER ENGINEERING - Bruce Ogg

Water Engineering has no objections.

ESSEX REGION CONSERVATION AUTHORITY (ERCA)

Natural Hazards and Regulatory Responsibilities Under the Conservation Authorities Act, O. Reg 686/21, PPS

The following comments reflect ERCA's role in protecting people and property from the threats of natural hazards and regulating development hazards lands under Section 28 of the *Conservation Authorities Act*.

The above noted lands are subject to our Development, Interference with Wetlands and Alteration to Shorelines and Watercourses Regulation under the *Conservation Authorities Act* (Ontario Regulation No. 158/06). The parcel falls within the regulated area of the Basin Drain. The property owner will be required to obtain a Permit from the Essex Region Conservation Authority prior to any construction or site alteration, or other activities affected by Section 28 of the *Conservation Authorities Act*.

ERCA has concerns with the potential impact to the quantity and quality of runoff in the downstream watercourse due to the proposed development on this site. ERCA recommends that both the quantity and quality of excess runoff be adequately controlled avoid any adverse impacts to the downstream watercourse. We therefore request inclusion of the following conditions in the Development Agreement:

1. That the developer undertakes an engineering analysis and implement the necessary measures to control any increases in flows to the downstream watercourse, to the satisfaction of the Municipality and the Essex Region Conservation Authority.
2. That the developer obtains the necessary approval / authorization from the Essex Region Conservation Authority prior to undertaking site alteration and/or construction activities.

APPENDIX D – CONSULTATION

We note that the proposed development will be subject to setbacks from the natural hazard on the site. Additionally, the site is within the South Cameron Secondary Planning Area and should be designed accordingly.

Final Recommendation

Our office has **no objection** to OPA 180 and ZBA 002-24. As noted above, the property owner will be required to obtain a Permit from the Essex Region Conservation Authority prior to any construction or site alteration, or other activities affected by Section 28 of the *Conservation Authorities Act*.



Planning Department
Suite 320-350 City Hall Sq W
Windsor ON N9A 6S1

HERITAGE PERMIT APPLICATION

Revised 11/2023

APPLICATION TYPE

☐ **Minor Heritage Permit**
(Delegated Authority Approval)

☒ **Major Heritage Permit**
(City Council Approval)

☐ **Demolition**

1. APPLICANT, REGISTERED OWNER, AND AGENT INFORMATION

Provide in full the name of the applicant, registered owner and agent, the name of the contact person, and address, postal code, phone number, and email address. If the applicant or registered owner is a numbered company, provide the name of the principals of the company. If there is more than one applicant or registered owner, copy this page, complete in full and submit with this application.

APPLICANT

Contact Name(s) Anthony Prail; Justin Kelley (Alternate)
Company or Organization BNA Constructors Canada GP
Mailing Address 35 Prospect Avenue
City, Province Windsor, Ontario Postal Code N9C 3G3
Email apraill@bnacagp.com; jkelley@bnacagp.com Phone(s) (226)787-6522; (519)984-6679

REGISTERED OWNER IF NOT APPLICANT

Contact Name(s) Stacey McGuire
Company or Organization Corporation of the City of Windsor
Mailing Address 350 City Hall Square West
City, Province Windsor, Ontario Postal Code N9A 6S1
Email smcguire@citywindsor.ca Phone(s) 311; (226) 350-4740

AGENT AUTHORIZED BY REGISTERED OWNER TO FILE THE APPLICATION

Contact Name(s) _____
Company or Organization _____
Mailing Address _____
City, Province _____ Postal Code _____
Email _____ Phone(s) _____

Who is the primary contact?

☒ Applicant

☐ Registered Owner

☐ Agent

2. SUBJECT PROPERTY

Municipal Address: Sandwich Street right-of-way within Sandwich Heritage Conservation District boundary

Legal Description (if known): _____

Building/Structure Type:

- ☐ Residential ☐ Commercial ☐ Industrial ☐ Institutional

Heritage Designation:

- ☐ Part IV (Individual) ☒ Part V (Heritage Conservation District)

By-law #: _____ District: Sandwich Heritage Conservation District

Is the property subject to a Heritage Easement or Agreement?

- ☐ Yes ☒ No

3. TYPE OF APPLICATION

Check all that apply:

- ☐ Demolition/Removal of heritage attributes ☐ Addition ☐ Erection ☒ Alteration*
☐ Demolition/Removal of building or structure ☐ Signage ☐ Lighting

*The *Ontario Heritage Act's* definition of "alter" means to change in any manner and includes to restore, renovate, repair or disturb.

4. HERITAGE DESCRIPTION OF BUILDING

Describe the existing design or appearance of buildings, structures, and heritage attributes where work is requested. Include site layout, history, architectural description, number of storeys, style, features, etc..

Public right-of-way surface features (sidewalk, boulevard, paved roadway) within the
Sandwich Heritage Conservation District boundary.

5. PROPOSED WORK

Provide a detailed written description of work to be done, including any conservation methods you plan to use. Provide details, drawings, and written specifications such as building materials, measurements, window sizes and configurations, decorative details, etc.. Attach site plans, elevations, product spec sheets, etc. to illustrate, if necessary.

Civil design includes reconstruction of the roadway with asphalt pavement; construct on-road cycling facilities; replace sidewalks and driveway approaches within ROW; restore landscaped areas to City of Windsor standards; remove and relocate existing streetscaping (benches, trash receptacles, bike racks).

Landscaping design includes construction of the boulevards outside of the roadway and sidewalks, including brick pavers, sodded grassed areas, concrete planters, plantings, soil cells, trees with tree grates, and upgraded cast iron tactile plates.

6. HERITAGE PERMIT RATIONALE

Explain the reasons for undertaking the proposed work and why it is necessary.
Reconstruction of municipal infrastructure.

Describe the potential impacts to the heritage attributes of the property.
Updated and enhanced municipal infrastructure within the right-of-way.

7. CHECKLIST OF MATERIALS SUBMITTED

 Check all that apply:

Required:

- ☒ Photographs (showing the current condition and context of existing buildings, structures, and heritage attributes that are affected by the application)
- ☒ Site plan/ Sketch (showing buildings on the property and location of proposed work(s))
- ☐ Architectural drawings of proposed work(s) (e.g. existing and proposed elevations, floor plans, roof plans, etc., as determined by Heritage Planning staff)
- ☒ Specifications of proposed work(s) (e.g. construction specification details)

Potentially required (to be determined by Heritage Planning staff):

- ☐ Registered survey
- ☐ Material samples, brochures, product data sheets etc.
- ☐ Cultural Heritage Evaluation Report
- ☐ Heritage Impact Assessment (HIA)
- ☐ Heritage Conservation Plan
- ☐ Building Condition Assessment


8. NOTES FOR DECLARATION

The applicant hereby declares that the statements made herein and information provided are, to the best of their belief and knowledge, a true and complete representation of the purpose and intent of this application.

The applicant agrees that the proposed work shall be done in accordance with this application, including attachments, and understands that the issuance of the Heritage Alteration Permit under the Ontario Heritage Act shall not be a waiver of any of the provisions of any By-Law of the Corporation of the City of Windsor, or the requirements of the Building Code Act, RSO 1980, c51.

The applicant acknowledges that in the event a permit is issued, any departure from the conditions imposed by the Council of the Corporation of the City of Windsor, or plans and specifications approved is prohibited and could result in the permit being revoked. The applicant further agrees that if the Heritage Alteration Permit is revoked for any cause of irregularity, in the relation to non-conformance with the said agreements, By-Laws, acts or regulations that, in consideration of the issuance of the permit, all claims against the City for any resultant loss or damage are hereby expressly waived.

Anthony Joseph Prail

 Digitally signed by Anthony Prail
Date: 2024.01.30 13:59:25-05'00'

Signature of Applicant(s)

Date

Signature of Applicant(s)

Date

SCHEDULE A

A. Authorization of Registered Owner for Agent to Make the Application

If the applicant is not the registered owner of the land that is the subject of this application, the written authorization of the registered owner that the applicant is authorized to make the application must be included with this application form or the authorization below must be completed.

I, _____, am the registered owner of the land that is
name of registered owner

subject of this application for a Heritage Alteration Permit and I authorize

_____ to make this application on my behalf.
name of agent

Signature of Registered Owner

Date

If Corporation – I have authority to bind the corporation.

B. Consent to Enter Upon the Subject Lands and Premises

I, _____, hereby authorize the members of the Windsor Heritage Committee and City Council and staff of the Corporation of the City of Windsor to enter upon the subject lands and premises described in Section 3 of the application form for the purpose of evaluating the merits of this application and subsequently to conduct any inspections on the subject lands that may be required as condition of approval. This is their authority for doing so.

Signature of Registered Owner

Date


If Corporation – I have authority to bind the corporation.

C. Acknowledgement of Applicant

I understand that receipt of this application by the City of Windsor Planning Department does not guarantee it to be a complete application. Further review of the application will occur and I may be contacted to provide additional information and/or resolve any discrepancies or issues with the application as submitted.

I further understand that pursuant to the provisions of the *Ontario Heritage Act* and the *Municipal Freedom of Information and Protection of Privacy Act*, this application and all material and information provided with this application are made available to the public.

Anthony Joseph Prail

 Digitally signed by Anthony Prail
Date: 2024.01.31 12:29:59-05'00'

Signature of Applicant

Date

GORDIE HOWE INTERNATIONAL BRIDGE

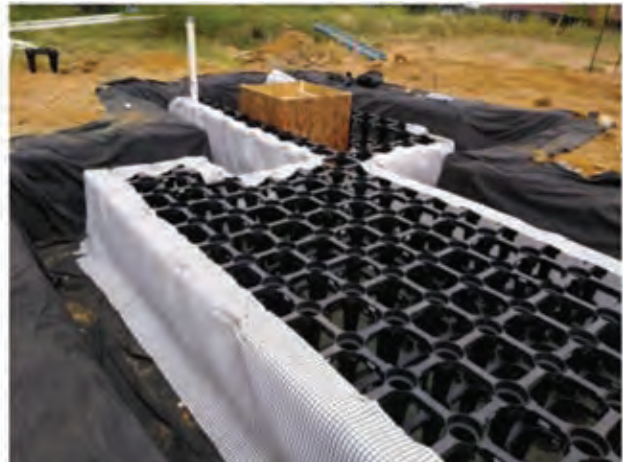
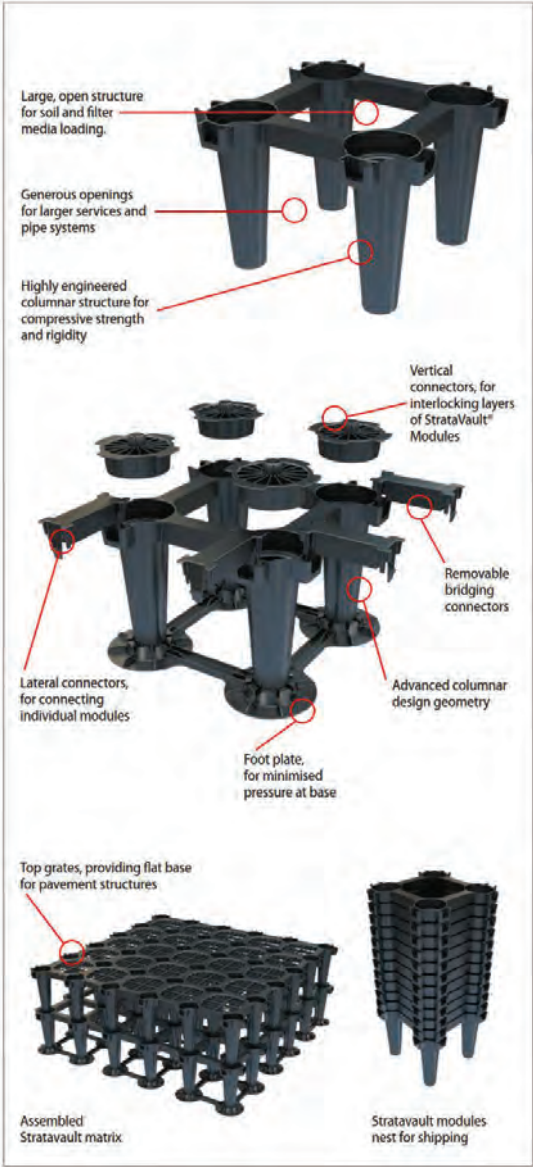
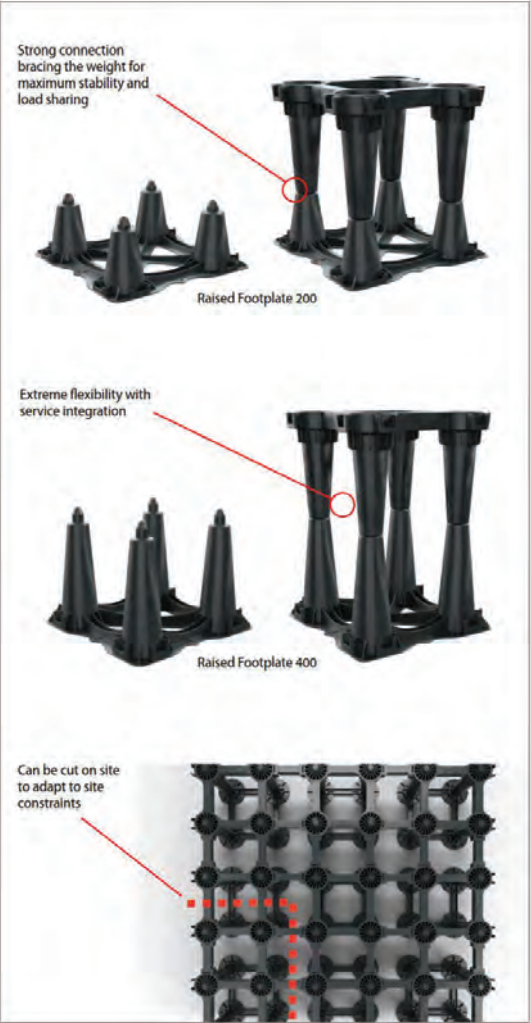
SANDWICH STREET STREETScape



March 8, 2021

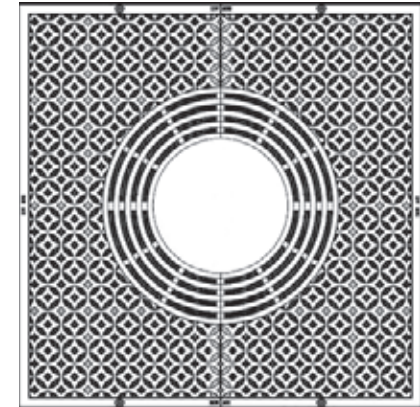
PROPOSED SOIL CELLS

Citygreen® StrataVault™



TREE GATES AND GUARDS

BLACK INTERLAKEN as discussed 2024-01-18

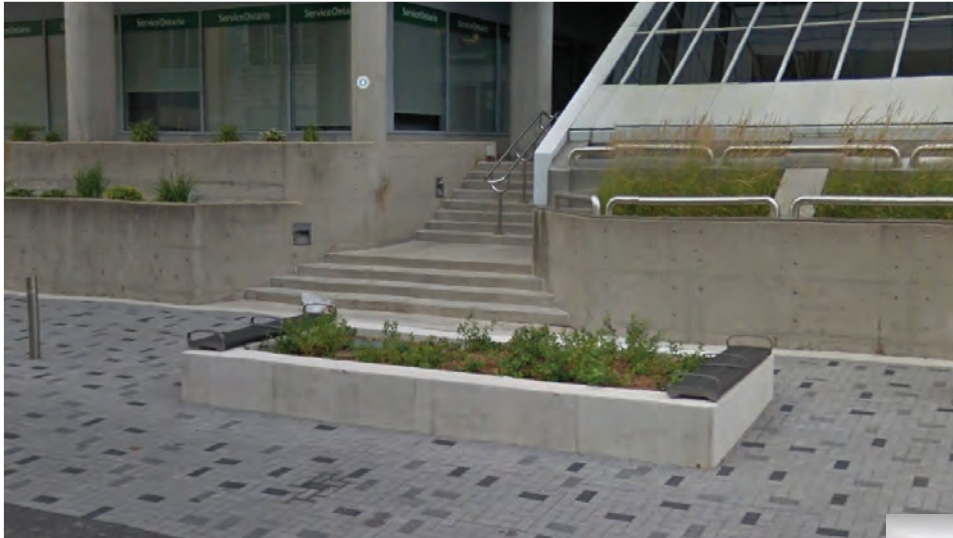


PROPOSED PAVER OPTION

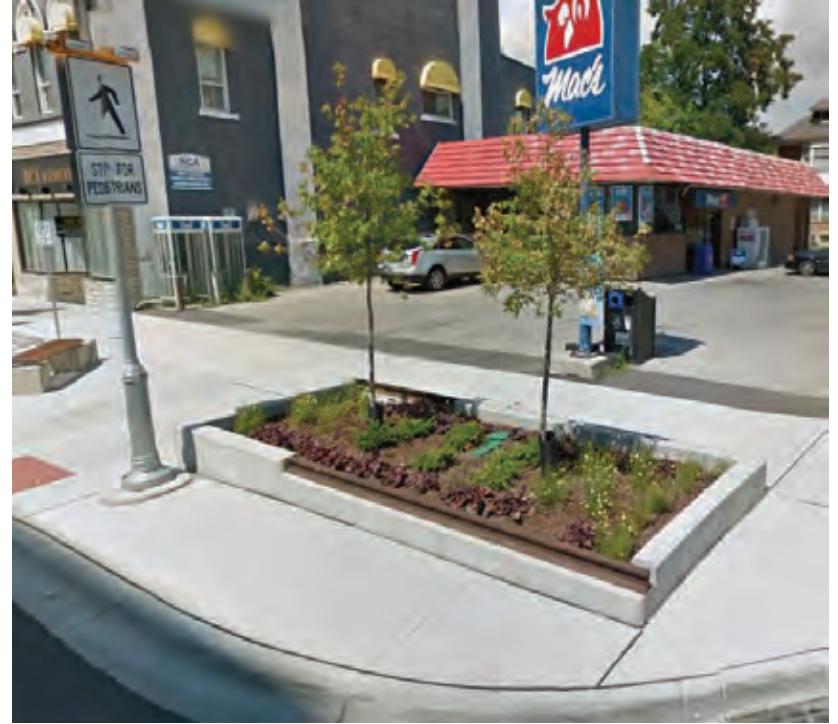
Brussels Block from Unilock



PRECEDENT PHOTOS – Dundas St. London Ontario



PRECEDENT PHOTOS – Talbot St., St. Thomas



PRECEDENT PHOTOS – Prince of Wales Hotel, Niagara on the lake





Gordie Howe International Bridge Project

Bridging North America Consultation Report - Sandwich Street Enhancements

Prepared by:	Nicole Flippance	Communications Manager	
Reviewed by:	Anthony Prail	Senior Project Coordinator	
Approved by:	Nicole Flippance	Communications Manager	

Name

Title

Signature

Document No. GHIB-SAND-COM-GN-PLN-0012

Rev: 1

Rev Date: 02/16/2024

Document Owner: BNA Communications

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REVISION HISTORY

Rev	Date	Description	Prepared by	Reviewed by	Approved by
0	02/16/2024	Rev 0	Nicole Flippance	Anthony Prail	Nicole Flippance
1	04/15/2024	Rev 1	Nicole Flippance	Anthony Prail	Nicole Flippance

TABLE OF CONTENTS

1. Objectives 4

2. Context 4

3. Public survey results 7

4. Incorporation of consultation feedback 8

5. Appendices 10

Document No. GHIB-SAND-COM-GN-PLN-0012	Consultation Report - Sandwich Street Enhancements	
Rev: 1	FOUO – FOR OFFICIAL USE ONLY	Page: 4 of 26

1. Objectives

Bridging North America (BNA) is the private-sector partner contracted by Windsor-Detroit Bridge Authority (WDBA) to deliver the Gordie Howe International Bridge project between Windsor, ON, and Detroit, MI, through a design-build-finance-operate-maintain P3 model.

A component of the Gordie Howe International Bridge Community Benefits Plan is a \$1 million investment to select, design and install streetscape enhancements along Sandwich Street in the Sandwich Towne Business Improvement Area (BIA). BNA delivered several tactics including virtual and in-person meetings with the public and key stakeholders, a public survey and public opportunity for comment to communicate details about the Sandwich Street reconstruction work and to seek feedback about priority investment areas for the Sandwich Street Enhancements initiative. This report summarizes the communications and consultation efforts and how feedback received informed the proposed Sandwich Street Enhancements design, submitted to the City of Windsor in February 2024, as part of the Heritage Alteration Permit.

2. Context

Windsor-Detroit Bridge Authority (WDBA) is working closely with the Michigan Department of Transportation (MDOT), Federal Highway Administration (FHWA), as well as Infrastructure Canada (INFC) on the delivery of a new bridge between Windsor, Ontario and Detroit, Michigan and associated Ports of Entry (POE) and an interchange to Interstate-75.

In 2022, BNA undertook consultation relating to the Sandwich Street Enhancements. The purpose was to provide the community and key stakeholders information about the Sandwich Street reconstruction project and to request feedback on priority investment areas for Sandwich Street Enhancements.

Tactics implemented to communicate information about the Sandwich Street reconstruction project and promote Sandwich Street Enhancement consultation included:

- news release (Appendix A)
- media advisory (Appendix B)
- email blast and email notifications
- social media posts (Appendix C)
- flyers/mail outs (Appendix D)
- web copy

Consultation tactics included:

- a public survey that was promoted online, with key stakeholders and delivered to over 1,800 residential and commercial buildings along the Sandwich Street reconstruction route (Appendix E)
- two public meetings (presentation in Appendix F)
- several meetings with stakeholders, key audiences and the City of Windsor.

Through this process, consultation feedback was requested from:

- City of Windsor staff
- Sandwich Towne Business Improvement Association (BIA)
- Elected officials serving Sandwich
- Sandwich residents and property owners
- Industrial businesses and other businesses near the Canadian Port of Entry
- Windsor Essex Community Benefits Coalition
- Bike Windsor Essex
- Local heritage groups including Friends of the Court – Mackenzie Hall and Windsor Historical Society
- St. John's Anglican and Bedford United Churches

Below is a summary of the consultation meetings where the Sandwich Street reconstruction project and Sandwich Street Enhancement initiative were discussed with the public, community and/or key stakeholders.

Date	Topic	Audience	Number of Attendees (excluding project staff)	City of Windsor Department (as appropriate)
August 10, 2021	Sandwich Street Reconstruction and Enhancements	City of Windsor staff	8	<ul style="list-style-type: none"> • Planning • Engineering
November 18, 2021	Sandwich Street Reconstruction Design	City of Windsor staff	7	<ul style="list-style-type: none"> • Planning • Engineering • Transit
November 25, 2021	Sandwich Street Reconstruction Design	Sandwich Towne BIA	7 (5 BIA members, 2 City staff)	<ul style="list-style-type: none"> • Planning • Engineering
December 16, 2021	Streetscape Enhancement Consultation Plans	City of Windsor staff	9	<ul style="list-style-type: none"> • Planning • Engineering • Operations • Traffic operations and parking
February 9, 2022	Sandwich Street Reconstruction and Enhancements Survey	Businesses near project construction site	9	
February 16, 2022	Sandwich Street Reconstruction and Enhancements Survey	Industrial businesses	11 (8 businesses, three City staff)	<ul style="list-style-type: none"> • Engineering • Policing
February 23, 2022	Sandwich Street Reconstruction and	Public meeting with residents	31	<ul style="list-style-type: none"> • Engineering

Date	Topic	Audience	Number of Attendees (excluding project staff)	City of Windsor Department (as appropriate)
	Enhancements Survey	and stakeholders	(29 community members, 2 City staff)	<ul style="list-style-type: none"> Culture
March 3, 2022	Sandwich Street Reconstruction	Coco Paving	2	
February 21, 2023	Sandwich Pergola and Enhancements	Sandwich BIA	5 (4 BIA members, 1 City staff)	<ul style="list-style-type: none"> Planning
March 21, 2023	Sandwich Pergola and Enhancements	City of Windsor staff	2	<ul style="list-style-type: none"> Planning Engineering
May 1, 2023	Meeting with Sandwich BIA Coordinator	Sandwich BIA	1	
November 29, 2024	Meeting with Sandwich BIA	Sandwich BIA	2	
January 18, 2024	Sandwich Street Enhancements Design	City of Windsor staff, AECOM, BNA, WDBA, Parsons	7	
January 21, 2024	Sandwich BIA January 2024 Monthly Meeting	Sandwich BIA, WDBA	7	
April 3, 2024	Design Reveal Meeting with BIA	Sandwich BIA, BNA, WDBA, City of Windsor	5	Planning Department
April 5, 2024	Design Reveal Meeting with BIA	Sandwich BIA and WDBA	2	
		TOTAL	115* * Note: not a unique count as individuals may be duplicated across meetings	

3. Public survey results

The public survey required participants to indicate their preference to place greater project investment and design focus on one of four categories (hardscaping, landscaping, gathering spaces and active transportation) using a scale of one to four with one being most favorable and four being least favorable. The survey also allowed respondents to submit comments.

Sixty-eight eligible surveys were received. Four surveys were considered ineligible and not included in the analysis due to not being completed correctly or being submitted from outside of Canada. One submitted survey did not indicate investment preferences, however, did include comments.

Survey results (unweighted)

Category	Most Important	Somewhat Important	Less Important	Least Important
Hardscaping	8	19	21	20
Landscaping	21	18	20	9
Gathering Spaces	14	20	17	17
Active Transportation	25	11	10	22

Survey results (weighted)

The data analysis process included modifying the survey result data to give more weight to each vote for the most important investment areas and less weight to the least important investment areas. Specifically, each single vote that ranked a category as:

- 'most important' was multiplied by four
- 'somewhat important' was multiplied by three
- 'less important' was multiplied by two, and
- 'least important' was multiplied by one.

	4	3	2	1	
Category	Most Important	Somewhat Important	Less Important	Least Important	Total
Landscaping	84	54	40	9	187
Active Transportation	100	33	20	22	175
Gathering Spaces	56	60	34	17	167
Hardscaping	32	57	42	20	151

This weighted analysis led to the following prioritization of the categories as per the survey results:

Document No. GHIB-SAND-COM-GN-PLN-0012	Consultation Report - Sandwich Street Enhancements	
Rev: 1	FOUO – FOR OFFICIAL USE ONLY	Page: 8 of 26

1. landscaping (most important)
2. active transportation
3. gathering spaces
4. hardscaping (least important).

4. Incorporation of consultation feedback

Feedback from the public, community and key stakeholders, including the City of Windsor, was sought and has informed the proposed design for the Sandwich Street Enhancements initiative. BNA considered comments collected during meetings and noted in survey submissions, submitted letters and emails and data the survey results. All feedback was reviewed and feedback that was in-scope of the Sandwich Street Enhancements initiative has informed the proposed design.

Below is a summary of elements that have been incorporated into the Sandwich Street Enhancements Design in consideration of consultation feedback. The proposed enhancements have been determined and refined collaboratively with the City of Windsor (the asset owner) to ensure that the proposed designs align with the Sandwich Heritage Conservation District Plan, City standards and other relevant requirements.

As landscaping was the most highly prioritized investment area and key stakeholders articulated support for landscaping investments, the proposed design includes several enhanced landscaping features, such as:

- Soil cells (underground infrastructure that provide favorable conditions to support health tree growth and expanded canopy in urban environments)
- Decorative tree grates and supports that accommodate trunk growth and align with Sandwich Heritage Conservation District Plan guidelines
- Construction of planters, with set-backs from the curb and in compliance with AODA-requirements
- New plantings of vegetation and trees that align with City of Windsor Forester requirements.
- Active transportation was also ranked as an important category of investment, however, the survey results were divided, with a high number of votes recommending deprioritizing investments into active transportation. The Sandwich Street reconstruction project scope includes the addition of on-road cycling lanes and multi-purpose paths in the reconstruction footprint, in addition to other significant Community Benefits Plan investments into active transportation infrastructure in west Windsor. In consideration of the planned infrastructure, physical limitations that did not easily accommodate additions and consultation with City staff, BNA did not incorporate additional investment into active transportation assets in the proposed Sandwich Street Enhancements design.

Gathering spaces received some support for investment, however, was not as highly prioritized as landscaping. In recognition of this, space was maintained in the proposed design at the northwest corner of Sandwich Street and Mill Street for installation of an enhanced infrastructure feature that promotes that area as a gathering space; this feature will be installed by the Sandwich BIA and has been funded by a separate initiative of the Community Benefits Plan.

Document No. GHIB-SAND-COM-GN-PLN-0012	Consultation Report - Sandwich Street Enhancements	
Rev: 1	FOUO – FOR OFFICIAL USE ONLY	Page: 9 of 26

Hardscaping was not as highly prioritized through the survey as other categories, leading to limited hardscaping enhancements being proposed in the design. However, supporting the creation of an aesthetically distinct district that aligns with the Sandwich Heritage Conservation Plan was noted as important by key stakeholders. In recognition of this, the proposed design includes:

- Existing benches and other street furniture being reinstated without enhancements to aesthetics or quantity to allow the enhancements budget to be directed to more highly prioritized investment areas.
- A quantity of pre-cast paver features incorporated in select areas to create a feature area with enhanced aesthetics.
- Decorative tree grates and supports that accommodate trunk growth and align with Sandwich Heritage Conservation District Plan guidelines.

Please note the Sandwich Street Enhancements design submitted to the City of Windsor in February 2024 is not finalized and remains subject to change. Some items noted above may be modified or eliminated and other features may be added.

BNA will continue to provide the local community, key stakeholders and general public with ongoing communication about the Sandwich Street reconstruction project, including the Sandwich Street Enhancements and consider feedback received as the project moves forward.

5. Appendices

Appendix A – News Release



MEDIA RELEASE

Gordie Howe International Bridge team seeks
feedback on Sandwich Street enhancements
February 7, 2022

Windsor, Ontario – As part of the Gordie Howe International Bridge project, three kilometres of Sandwich Street will be reconstructed. On top of this significant infrastructure improvement, the project team is investing \$1 million into streetscape enhancements to create a welcoming and vibrant space for residents and visitors to the area. To ensure the enhancements reflect community priorities, the project team is requesting assistance in prioritizing the investment areas of most interest.

The \$1 million investment is part of the project Community Benefits Plan under the Neighbourhood Infrastructure Strategy, which is a \$20 million community infrastructure investment focused on priorities identified through stakeholder and community consultation.

While Sandwich Street will be reconstructed from south of the Rosedale Avenue roundabout to Ojibway Parkway, the \$1 million enhancement focuses on the Business Improvement District, between Detroit Street through Brock Street.

Community members are asked to complete an online or paper survey that will help the project team prioritize how the \$1 million investment will be spent and asks participants to rank four categories of streetscape enhancement options in order of importance:

- hardscaping: benches, additional garbage bins and sidewalks made of brick paving stones instead of poured concrete
- landscaping: potted plants and planter boxes, additional trees, perennials, and tall grasses.
- gathering spaces: seating and social areas, tables, and benches.
- active transportation features: additional bike racks, directional signage, or bike repair stations.

The project team will review survey results and identify the areas of highest priority for incorporation into the streetscaping designs for Sandwich Street. Designs will be shared later this year.

The survey and additional information are available online at GordieHoweInternationalBridge.com. An information package including the survey is also being distributed to approximately 1800 households in Sandwich/west Windsor. Paper copies can be requested by email to info@wdbridge.com or by calling 1-844-322-1773. Paper surveys can be

Document No. GHIB-SAND-COM-GN-PLN-0012	Consultation Report - Sandwich Street Enhancements	
Rev: 1	FOUO – FOR OFFICIAL USE ONLY	Page: 11 of 26

scanned or photographed and submitted by email to info@wdbridge.com or by dropping it off at the Windsor Public Library John Muir Branch at 363 Mill Street, Windsor, Ontario.

The survey closes March 7, 2022.

QUOTES:

“We know a vital part of making the Gordie Howe International Bridge project a success is investing and engaging with the host communities. Through the survey, the Sandwich community can share their unique perspectives and influence how the \$1 million Community Benefits investment is spent. This helps ensure we’re making community-driven investments that we all can be proud of.”

- Bryce Phillips, CEO, Windsor-Detroit Bridge Authority (WDBA)

“Bridging North America is committed to ongoing collaboration with the Sandwich community and being good neighbours and community partners throughout the Sandwich Street reconstruction project. We look forward to learning directly from the Sandwich community what’s important to them, and how we can incorporate their vision into the Sandwich Street reconstruction design.”

- Michael Hatchell, CEO of Bridging North America (BNA)

QUICK FACTS:

- The project team will also host two online community meetings about the Sandwich Street improvements and upcoming construction work on Wednesday, February 23, 2022, from 3:00 - 4:00 p.m. and again from 6:30 - 7:30 p.m. Registration information is available at [link].
- Once complete the Sandwich Street reconstruction is complete, the City of Windsor will assume responsibility of maintaining the street.
- Sandwich Street reconstruction designs and plans will meet the City of Windsor’s standards and follow the Sandwich Heritage Conservation District, the Community Improvement Plan and Heritage Alteration Permit requirements and processes.

RELATED PRODUCTS

- Fact sheet: [Sandwich Street reconstruction <insert link>](#)
- Fact sheet: [\\$1 million enhancements <insert link>](#)

ABOUT THE GORDIE HOWE INTERNATIONAL BRIDGE

The Gordie Howe International Bridge project is a once-in-a-generation undertaking. Delivered by Windsor-Detroit Bridge Authority with its private-sector partner, Bridging North America, it will lead to much-needed transportation improvements for international travellers and provide jobs and opportunities for growth to the Windsor-Detroit region. Its four components include the Canadian Port of Entry, the bridge, the US Port of Entry and the Michigan Interchange to Interstate-75 (I-75).

Document No. GHIB-SAND-COM-GN-PLN-0012	Consultation Report - Sandwich Street Enhancements	
Rev: 1	FOUO – FOR OFFICIAL USE ONLY	Page: 12 of 26

ABOUT WINDSOR-DETROIT BRIDGE AUTHORITY

Windsor-Detroit Bridge Authority (WDBA) is a not-for-profit Canadian Crown corporation created to deliver the Gordie Howe International Bridge project between Windsor, ON and Detroit, MI through a public-private partnership (P3). WDBA is responsible for overseeing and managing the construction and operation of the new crossing. For more information visit www.GordieHoweInternationalBridge.com and follow the project on Twitter at www.twitter.com/GordieHoweBrg.

ABOUT BRIDGING NORTH AMERICA

Bridging North America (BNA) became the private-sector partner to design, build, finance, operate and maintain the Gordie Howe International Bridge project in September 2018. BNA is comprised of some of the most recognized leaders in the construction and infrastructure industry which includes North American and international companies. BNA partners have significant experience carrying out major infrastructure projects such as the Rt. Hon. Herb Gray Parkway in Windsor, Ontario; New Champlain Bridge Corridor in Montreal, Quebec; Autoroute 30 in Montreal, Quebec; Eglinton Crosstown LRT in Toronto, Ontario; Réseau Express Métropolitain (REM) in Montreal, Quebec; Automated People Mover at LAX Airport, California; Harbor Bridge, Corpus Christi, Texas; Tappan Zee Bridge in New York and San Francisco-Oakland Bay Bridge, California

For information please contact:

Gordie Howe International Bridge
Media Relations
226-757-1716

Document No. GHIB-SAND-COM-GN-PLN-0012	Consultation Report - Sandwich Street Enhancements	
Rev: 1	FOUO – FOR OFFICIAL USE ONLY	Page: 13 of 26

Appendix B – Media Advisory



MEDIA ADVISORY

Upcoming Sandwich Street reconstruction and
enhancements community meeting
February 21, 2022

Windsor-Detroit Bridge Authority (WDBA) and Bridging North America (BNA) will hold online Community Meetings on February 23, 2022, to inform the public about the upcoming Sandwich Street reconstruction and \$1 million enhancements projects.

MEETING DETAILS

February 23, 2022

Two online community meetings will be held on Microsoft Teams:

3:00 – 4:00 p.m.

6:30 – 7:30 p.m.

The online community meeting will include a presentation followed by a question and answer period. The presentation will provide an overview of the Sandwich Street reconstruction project, including information about the \$1 million dollar streetscape enhancement investment made available through the project Community Benefits Plan.

Community members can register to attend by sending an email to info@wdbridge.com or phoning 1-884-322-1773. The meeting link will be shared upon registration.

Registration closes on February 22, 2022, at 12:00 p.m.

Media interested in attending the event are asked to email info@wdbridge.com for the meeting link.

A video of the meeting, project fact sheets and additional materials will be posted to the project website www.GordieHoweInternationalBridge.com following the event.

For information please contact:

Gordie Howe International Bridge
Media Relations
226-757-1716

Document No. GHIB-SAND-COM-GN-PLN-0012	Consultation Report - Sandwich Street Enhancements	
Rev: 1	FOUO – FOR OFFICIAL USE ONLY	Page: 14 of 26

Appendix C - Social media posts (posted in February and March 2022)

We want your feedback. The #GordieHoweBridge project team is seeking feedback from the community about Sandwich Street reconstruction and enhancements. Take the online survey and prioritize the investment areas: [link to online survey]

The #GordieHoweBridge project team is holding Online Community Meetings on Feb 23 at 3:00pm and 6:30pm to inform the public about the upcoming Sandwich Street reconstruction and enhancements project. The Online Community Meetings will include a presentation followed by a question & answer period. Register to attend by email at info@wdbridge.com or phoning 1-884-322-1773. The meeting link will be shared upon registration.

Have you registered for a #GordieHoweBridge Online Community Meeting on Feb 23 regarding Sandwich Street reconstruction and enhancements? Register for the 3:00pm or 6:30pm meeting by emailing info@wdbridge.com or calling 1-844-322-1773.

Did you miss the #GordieHoweBridge Online Community Meetings regarding Sandwich Street reconstruction and enhancements? Meeting materials are now available on our website. Take the online survey and provide your feedback to help inform the design and investment: [link to online survey]

You can help inform design. The #GordieHoweBridge project team is seeking feedback from the community about Sandwich Street reconstruction and enhancements. Take the online survey and prioritize the investment areas: [link to online survey]

Today is the last day to submit your feedback. The #GordieHoweBridge project team is seeking feedback from the community about Sandwich Street reconstruction and enhancements. Take the online survey and prioritize the investment areas: [link to online survey]

Document No. GHIB-SAND-COM-GN-PLN-0012	Consultation Report - Sandwich Street Enhancements	
Rev: 1	FOUO – FOR OFFICIAL USE ONLY	Page: 15 of 26

Appendix D – Flyer / Public Notice / Maildrop Package

GORDIE HOWE INTERNATIONAL BRIDGE SANDWICH STREET \$1 MILLION ENHANCEMENTS SURVEY AND ONLINE COMMUNITY MEETING

As part of the Gordie Howe International Bridge project, just over three kilometres of Sandwich Street in Windsor, Ontario will be reconstructed. Sandwich Street reconstruction will stretch from south of the Rosedale Avenue roundabout past Ojibway Parkway to McKee Avenue.

In addition to this work, the project team is investing \$1 million to further enhance the Sandwich Business Improvement Area through the Community Benefits Plan.

Learn more about the Sandwich Street Improvements and Upcoming Construction Work

Join the project team for an online meeting to learn more about the reconstruction project, information about the \$1 million enhancements, as well as a question-and-answer period.

Wednesday, February 23, 2022

3:00 - 4:00 p.m.

and/or

6:30 - 7:30 p.m.

Register to attend by emailing info@wdbridge.com or phoning 1-884-322-1773. The meeting link will be shared upon registration. The presentation, meeting materials and a video of the meeting will be posted to the project website following the event.

\$1 Million Enhancements Survey

Through, the Gordie Howe International Bridge Community Benefits Plan, the project team is investing \$1 million to further enhance the Sandwich Business Improvement Area, in addition to the Sandwich Street reconstruction project. The project team is collecting community feedback through a survey that will help determine how the \$1 million should be prioritized. Survey respondents are asked to rank these four categories of investment areas:

- hardscaping: Benches, planter fences, and waste receptacles.
- landscaping: Potted plants or trees, flowering shrubs, planter boxes, tall grasses or other vegetation.
- gathering spaces: Seating areas of tables and benches, alone or in clusters to allow for community interaction.
- active transportation: Bike racks, wayfinding signage, or bicycle repair stations.

The project team will review survey results and identify the areas of highest priority for incorporation into the streetscaping designs for Sandwich Street.

How to participate in the survey:

Document No. GHIB-SAND-COM-GN-PLN-0012	Consultation Report - Sandwich Street Enhancements	
Rev: 1	FOUO – FOR OFFICIAL USE ONLY	Page: 16 of 26

The survey is available online and/or through the enclosed paper hardcopy.

Responses to the online survey and paper hardcopies will be accepted until March 7, 2022.

To take the survey online visit gordiehoweinternationalbridge.com. Select the “Sandwich Street” tab under the “Construction” section of the website. This will direct you to the Sandwich Street Reconstruction page where the survey link is located.

You can return a scan or photo of your completed survey by email to info@wdbbridge.com or by dropping off the paper copy at the Windsor Public Library John Muir Branch at 363 Mill Street, Windsor, Ontario.

Date: _____

(Optional) Name – Please print: _____

(Optional) Email: _____

(Required) Postal Code: _____

All comments received are appreciated, however, only comments from Canadian residents can be considered in accordance with *Canadian Environmental Assessment Act* requirements.

Please rank the following four categories by most important to least important:

One (1) = most important.

Four (4) = least important.

___ Hardscaping

___ Landscaping

___ Gathering Spaces

___ Active Transportation

Find examples of the four categories by referencing your enclosed information package, or by visiting gordiehoweinternationalbridge.com and selecting the “Sandwich Street” tab under the “Construction” section of the website.

The personal information on this form is collected for the purpose of communicating with individuals external to the institution with respect to general inquiries and response, distribution of public awareness and communications material, dissemination of reports, publications, and other documentation on matters relating to the Gordie Howe International Bridge project.

The personal information is collected pursuant to and is protected under the provisions of the Privacy Act with respect to its use, disclosure, retention or disposal.

SANDWICH STREET RECONSTRUCTION

Sandwich Street Reconstruction will include a new street and sidewalks, bicycle and multipurpose lanes and a new stormwater sewer. Once complete, the City of Windsor is responsible for maintaining the street.

RECONSTRUCTION PHASES AND TIMELINES

The Sandwich Street reconstruction activities will occur in four phases and take place during the hours of 6 a.m. to 8 p.m. daily.

PHASE ONE: SUMMER 2022: McKee Road to the Ojibway Parkway.

PHASE TWO: FALL 2022 – WINTER 2022: Ojibway Parkway meets Sandwich Street to Chappell Street.

PHASE THREE: SPRING 2023: Brock Street to just south of the Rosedale Avenue roundabout, including the Sandwich Business Improvement Area.

PHASE FOUR: 2024: Chappell Street to Brock Street. Sidewalks and bike lanes will also be constructed along this stretch.

TRAFFIC AND CONSTRUCTION MITIGATION

Through traffic will be maintained along Sandwich Street. Traffic may be reduced to one lane at times. Residents and businesses along the reconstruction stretch will have driveway and parking lot access maintained and receive advance notice of upcoming traffic impacts.

Construction mitigation measures include:
vibration monitoring program
dust control
noise control.

ABOUT THE GORDIE HOWE INTERNATIONAL PROJECT

The Gordie Howe International Bridge project is a once-in-a-generation undertaking. Delivered by Windsor-Detroit Bridge Authority with its private-sector partner, Bridging North America, it will lead to much-needed transportation improvements for international travellers and provide jobs and opportunities for growth to the Windsor-Detroit region. Its four components include the Canadian Port of Entry, the bridge, the US Port of Entry and the Michigan Interchange to Interstate-75 (I-75).

For more information about the Gordie Howe International Bridge project visit www.GordieHoweInternationalBridge.com or call 1-844-322-1773. Follow us on Twitter at www.twitter.com/GordieHoweBrg, like us on Facebook at www.facebook.com/GordieHoweBridge and connect with us on LinkedIn at www.linkedin.com/company/wdba-apwd

Appendix E – Survey (online and paper)

Date: _____

(Optional) Name – Please print: _____

(Optional) Email: _____

(Required) Postal Code: _____

All comments received are appreciated, however, only comments from Canadian residents can be considered in accordance with *Canadian Environmental Assessment Act* requirements.

Please rank the following four categories by most important to least important:

One (1) = most important.

Four (4) = least important.

___ Hardscaping

___ Landscaping

___ Gathering Spaces

___ Active Transportation

Find examples of the four categories by referencing your enclosed information package, or by visiting gordiehoweinternationalbridge.com and selecting the “Sandwich Street” tab under the “Construction” section of the website.

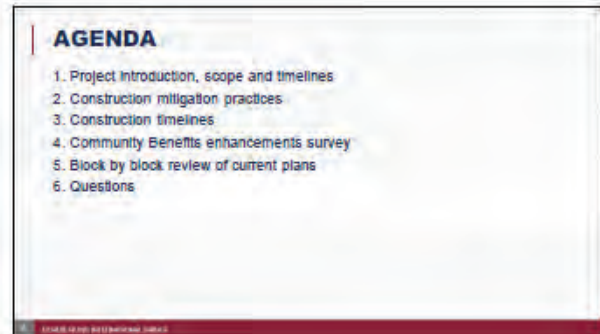
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The personal information is collected pursuant to and is protected under the provisions of the Privacy Act with respect to its use, disclosure, retention or disposal.

Appendix F – Community/public meeting presentation (February 23)



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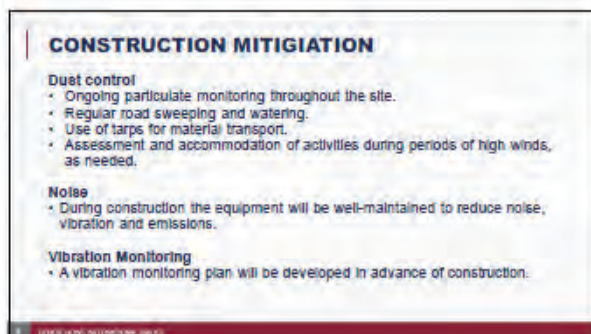
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SANDWICH RECONSTRUCTION: WHAT TO EXPECT

- Construction will be conducted between the hours of 6 a.m. and 8 p.m. daily.
- Sandwich Street will remain open to traffic during construction although lane closures may be required.
- BNA will remove the roadway and public sidewalks, up to the property line.
- Access to businesses will be maintained, including driveway access.
- BNA will provide construction updates so businesses and people can plan in advance.



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7

CONSTRUCTION AND TRAFFIC NOTIFICATIONS

Regular communication with Sandwich Street residents and businesses will occur through:

- Updates at monthly Sandwich BIA meetings
- Monthly drop-in days at the Sandwich Community Office*
- Sandwich Construction section on website including weekly construction updates
- Social media
- Flyers to homes and businesses
- Updates at quarterly public meetings and in quarterly community newsletters
- On-road signage for upcoming traffic situations

* Pending Covid-19 guidelines

Inquiries and concerns can be shared anytime to info@wdbridge.com or 1-844-322-1773

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8

SANDWICH STREET ENHANCEMENTS AND OTHER COMMUNITY BENEFITS

- The public realm could improve within Sandwich BIA area through an additional \$1 million in Community Benefits funding from the Gordie Howe International Bridge project.
- A proposed design will embrace place-making principles to make it a more enjoyable gathering space.
- The replacement of existing furniture and other features including planters, benches, garbage receptacles and bike racks will be enhanced through an additional \$1 million in Community Benefits funding towards streetscape enhancements in the BIA.
- Other upcoming Community Benefit Initiatives in the Sandwich Towne area:
 - Improvements to McKee Park
 - Windsor-Detroit Cross-River Tour – a series of historic, interpretive signs along Sandwich Street to the Canadian Port of Entry
 - Bike rack competition
 - Connection from Canadian Port of Entry to Maiden Park to connect to the Rt. Hon. Herb Gray Parkway trail.

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9


SANDWICH STREET ENHANCEMENTS COMMUNITY BENEFITS - SURVEY

- Through the Community Benefits Plan, the project team is investing \$1 million to further enhance the Sandwich Business Improvement Area.

\$1 million enhancements survey

The project team is asking the community to help prioritize how the \$1 million should be prioritized. A survey asks respondents to rank four enhancement categories: landscaping, landscaping, gathering spaces and active transportation.

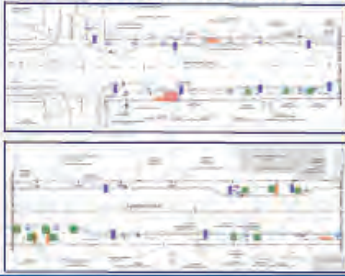
- Online survey is available under the Sandwich Street tab of project website.
- Request a paper hard copy of the survey by emailing info@wdbridge.com or by calling 1-844-322-1773.
- Paper surveys can be scanned/photographed and emailed to info@wdbridge.com or dropped off at the Windsor Public Library John C. Muir Branch at 363 Muir Street, Windsor, Ontario.
- The survey will close on March 7, 2022.



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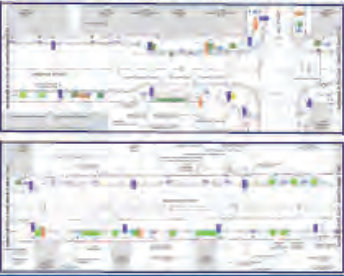
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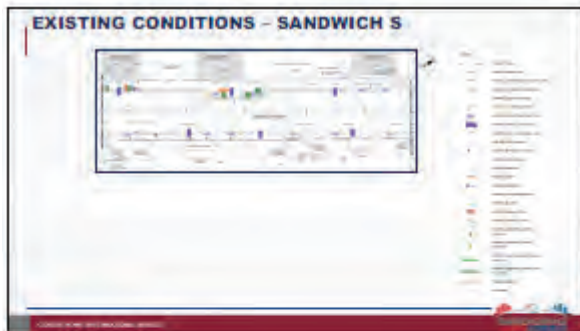
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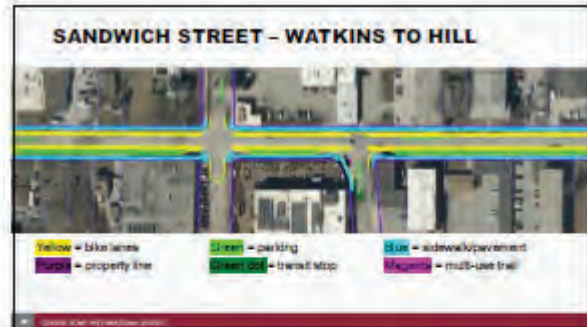
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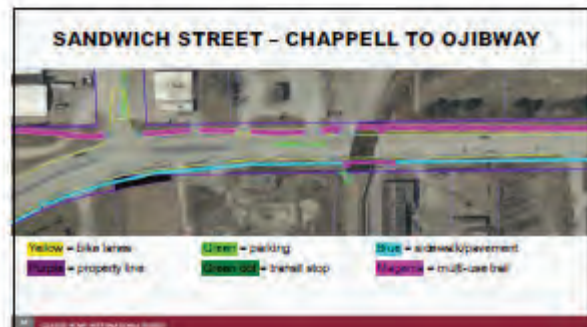
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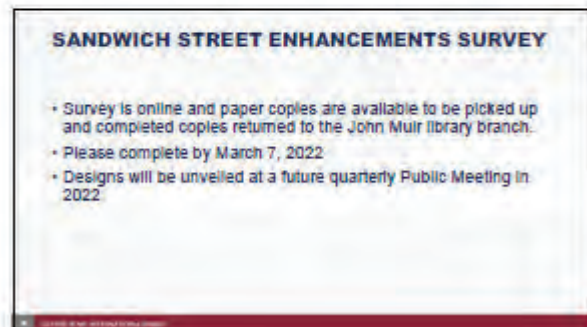
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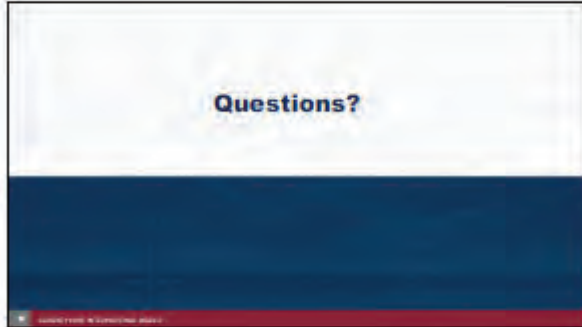
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Appendix G – Presentation for BIA Briefing April 3 2024



Appendix H – Comments from BIA meeting April 3, 2024

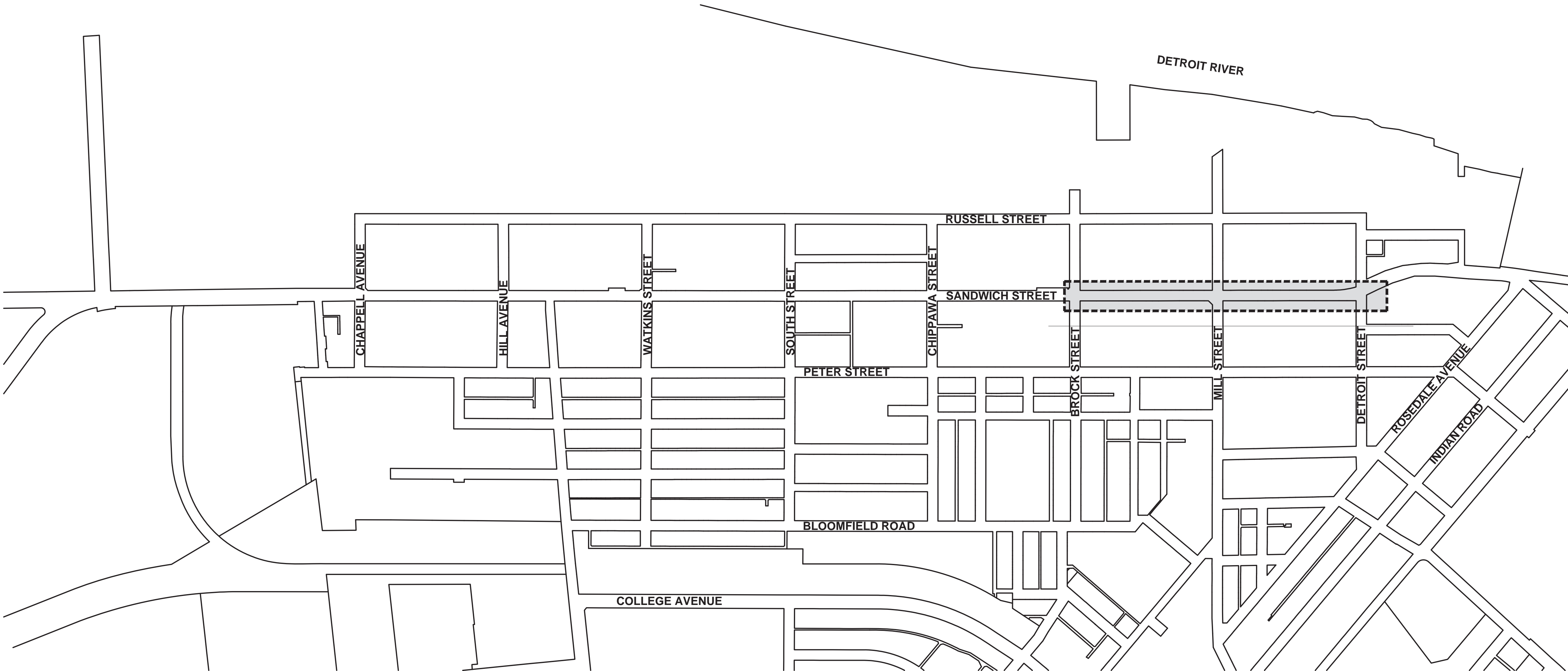
Comment Designation	Meeting Reference	Comments	Commenter	Project Team (BNA/WDBA) Replies	Actions	Action By:	Comment Responses
Streetscape comments	Presentation to BIA - April 3, 2024	Will lamps will be powder coated when refurbished?	BIA member	N/A	Project team and City to review if lampposts will be powder coated when refurbished.	CoW	
Streetscape comments	Presentation to BIA - April 3, 2024	Concerns with current low planters as these are often hit by car doors and collect garbage.	BIA member	The project team noted the proposed design includes less planters placed in key areas. These planters are set-back further from the road to address concerns about damage to/by car doors. Tree grates have been incorporated in key locations with trees, instead of planters, to create an area that is of unified height.	N/A	N/A	
Streetscape comments	Presentation to BIA - April 3, 2024	Who is responsible to water the new sod and noted challenges with sod maintenance due to frequency of cutting and damage by salt.	BIA member	The project team noted the sod will be planted in the Right-of-Way, therefore adjacent business and homeowners are responsible to maintain the sod.	Project team to review use of sod in proposed design.	BNA	
Streetscape comments	Presentation to BIA - April 3, 2024	The proposed low planters may be used by skateboarders and asked if features could be installed to deter this.	BIA member	No features currently included to deter skateboarders.	Project team to review if features can be incorporated into the low planters to deter use by skateboarders.	BNA	
Streetscape comments	Presentation to BIA - April 3, 2024	Do the enhancements include the fountain and surrounding area at the corner of Sandwich and Mill streets?	BIA member	The project team noted the enhancements end at the Right-of-Way in that area. Additional enhancements cannot be incorporated to the fountain as it is on private property.	N/A	N/A	
Streetscape comments	Presentation to BIA - April 3, 2024	Will streetcar tracks, or other treatments, will be incorporated into the crosswalks at key intersections?	BIA member	No additional enhancements to crosswalks have been included in the design.	N/A	N/A	
Streetscape comments	Presentation to BIA - April 3, 2024	Some of the existing lock-ups are missing or damaged and require replacement, adding that a meeting occurred previously with the City of Windsor to identify where additional bicycle lock ups could be added in the BIA.	BIA member	The bicycle lock stations have been replaced one-for-one with the existing count in the proposed designs.	City of Windsor to review locations where opportunities to add bike lock-ups were previously identified.	COW	
Streetscape comments	Presentation to BIA - April 3, 2024	Concerns with the safety at the intersection of Sandwich and Mill streets, noting the current corner has low visibility, challenges with the traffic signal and a tight turning radius that is used by trucks. This has resulted in accidents. Members asked if bollards could be incorporated into the intersection design at this intersection.	BIA member	The project team noted that signalization and enforcement of use of truck routes is within the City of Windsor's jurisdiction. The turning radius in the new design will be compliant with current standards.	Stephen Hargraves to email Kevin Alexander noting safety concerns relating to the intersection of Sandwich and Mill streets. Project team to review incorporation of bollards at intersection of Sandwich and Mill streets.	BNA and COW	

10	Streetscape comments	Presentation to BIA members - April 5, 2024	Will the existing trolley tracks that are embedded at the intersection of Mill and Sandwich Streets be reincorporated?	BIA member	Any trolley tracks found during the reconstruction project will be salvaged, as appropriate but may not be reinstalled.	Comment provided to the design team for consideration.	BNA	
11	Streetscape comments	Presentation to BIA members - April 5, 2024	Proposed sod may not be regularly cut by those responsible to maintain the Right-of-Way.	BIA member	Noted.	N/A	BNA	
12	Streetscape comments	Presentation to BIA members - April 5, 2024	Will the street poles be consistent with what current exists as the BIA uses this infrastructure to hang holiday decorations	BIA member	The project team noted that the existing poles will be refurbished and replaced.	Project team to confirm existing decorative street poles will be replaced.	BNA	
13	Streetscape comments	Presentation to BIA members - April 5, 2024	Can a heritage style bus shelter be installed instead of the City standard as this would contribute to the heritage look and feel in the area.	BIA member	Noted.	Comment will be provided to the design team for consideration.	BNA	
14	Streetscape comments	Presentation to BIA members - April 5, 2024	Is there is water access along the street or in bump outs to water the new vegetation?	BIA member	Will review and advise.	Project team to review if water access is available to water vegetation in planters.	BNA and COW	
15	Streetscape comments	Presentation to BIA members - April 5, 2024	Will parking be maintained in front of Mackenzie Hall and where will the bus stop in that area?	BIA member	Will review and advise.	Project team and City of Windsor to review if parking will be maintained in front of Mackenzie Hall and where the City bus will stop.	BNA and COW	
16	Streetscape comments	Presentation to BIA members - April 5, 2024	How long will the parking lot to the Sandwich Towne Family Dentistry be inaccessible	Dr. Hanaka	Will review and advise.	Project team to review and advise how long will the parking lot to the Sandwich Towne Family Dentistry will be inaccessible.	BNA	
17	Streetscape comments	Presentation to BIA members - April 5, 2024	Does the scope of the reconstruction work included a 'shave and pave' or full reconstruction	BIA member	Will review and advise.	Project team to review and advise whether the scope of the reconstruction work includes a 'shave and pave' or full reconstruction.	BNA and COW	
18	Streetscape comments	Presentation to BIA members - April 5, 2024	There is interest in having wayfinding signage on Ojibway Parkway to direct bridge users to Sandwich Town.	BIA member	Out of project scope.	N/A	COW	
19	Streetscape comments	Presentation to BIA members - April 5, 2024	The existing historic Sandwich signs at the corners of Brock and Detroit streets require cleaning if the area will serve as a gateway to the community.	BIA member	Out of project scope.	N/A	COW	

GORDIE HOWE INTERNATIONAL BRIDGE

SANDWICH STREET NORTH IMPROVEMENTS

LANDSCAPE



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CLIENT

WDBA

APWD

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PROJECT Co.

BRIDGING

NORTHAMERICA

CONSULTANT

AECOM

PROJECT

GORDIE HOWE

INTERNATIONAL BRIDGE

SANDWICH STREET CORRIDOR

WINDSOR, ONTARIO

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ISSUE/REVISION		
A	MAR-19-24	ISSUED FOR RFC SNS
I/R	DATE	DESCRIPTION

DESIGN UNIT
LANDSCAPE

SHEET TITLE
COVER SHEET

SHEET No.
L-000

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SHEET LIST	
SHEET	DESCRIPTION
L-000	Cover Sheet
L-001	List of Sheets & Landscape Notes
LR101	Sandwich Street North Existing Conditions & Removals 29+50 to 31+70
LR102	Sandwich Street North Existing Conditions & Removals 31+70 to 33+90
LR103	Sandwich Street North Existing Conditions & Removals 33+90 to 35+75
L-101	Sandwich Street Landscape Plan 29+70 to 30+37
L-102	Sandwich Street Landscape Plan 30+37 to 31+02
L-103	Sandwich Street Landscape Plan 31+02 to 31+67
L-104	Sandwich Street Landscape Plan 31+67 to 32+32
L-105	Sandwich Street Landscape Plan 32+32 to 32+97
L-106	Sandwich Street Landscape Plan 32+97 to 33+67
L-107	Sandwich Street Landscape Plan 33+67 to 34+27
L-108	Sandwich Street Landscape Plan 34+27 to 34+93
L-109	Sandwich Street Landscape Plan 34+93 to 35+20
L-500	Landscape Details - Planting Sandwich Street
L-501	Landscape Details - Hardscape Sandwich Street
L-502	Landscape Details - Hardscape Sandwich Street
L-503	Landscape Details - Tree Grates Sandwich Street
L-504	Landscape Details - Planters Sandwich Street
L-505	Landscape Renderings - Sandwich Street & Brock Street
L-506	Landscape Renderings - Sandwich Street & Mill Street
L-507	Landscape Renderings - Sandwich Street & Detroit Street
L-508	Landscape Renderings - Mackenzie Hall & Parkette
SC01	Site Key Plan & Soil Cell Layouts
SC02	Soil Cell Cross Sections

LANDSCAPE NOTES

1.

GENERAL NOTES:
- 1.1.

THESE DRAWINGS SHALL NOT BE USED FOR CONSTRUCTION UNLESS STAMPED AND SIGNED BY THE LANDSCAPE ARCHITECT.
- 1.2.

ANY AMBIGUITY IN THE DRAWINGS, SPECIFICATIONS OR DETAILS IS TO BE REPORTED TO THE CONTRACT ADMINISTRATOR FOR DIRECTION. DO NOT PROCEED IN UNCERTAINTY.
- 1.3.

DRAWINGS MAY BE SCALED FOR APPROPRIATE LAYOUT ONLY. ALL MEASUREMENTS IN METRIC. ALL LAYOUT, INCLUDING PLANTING, TO BE STAKED BY THE CONTRACTOR AND APPROVED BY THE CONTRACT ADMINISTRATOR.
- 1.4.

THE CONTRACTOR IS TO TAKE NECESSARY PRECAUTION TO PROTECT ALL EXISTING SITE FEATURES UNLESS SPECIFIED FOR DEMOLITION. THIS INCLUDES ALL SURVEY BARS, STAKES AND MONUMENTS. MAKE GOOD ANY DAMAGE.
- 1.5.

UPON COMPLETION OF WORK EACH DAY, REMOVE ALL DEBRIS, GARBAGE AND SURPLUS MATERIALS FROM THE SITE. KEEP THE SITE CLEAN AND USABLE AT ALL TIMES.
- 1.6.

ANY CHANGES TO THIS ACCEPTED PLAN MUST BE REVIEWED AND APPROVED BY THE CONTRACT ADMINISTRATOR.
- 1.7.

FOR EXISTING TREE AND SITE FURNISHING INFORMATION REFER TO AECOM TREE AND SITE FURNISHING INVENTORY MEMORANDUM - TREE AND SITE FURNISHING INVENTORY ON SANDWICH STREET BETWEEN BROCK STREET AND DETROIT STREET IN WINDSOR, ONTARIO, DATED DECEMBER 9, 2022.
- 1.8.

REFER TO ACL-GHIB-SAND-SPEC0001 AS WELL AS ALL RELEVANT CITY OF WINDSOR STANDARDS.
2.

PLANTING NOTES
- 2.1.

ALL PLANTING SHALL BE UNDERTAKEN BY THE CITY OF WINDSOR STAFF.
- 2.2.

ALL AREAS OF DISTURBED SOIL MUST BE STABILIZED WITH EROSION CONTROL MEASURES AND/OR SODDED IMMEDIATELY UPON COMPLETION OF WORK.
- 2.3.

THE CONTRACTOR IS TO REVIEW WITH CONTRACT ADMINISTRATOR ANY MAINTENANCE REQUIREMENTS NECESSARY FOR WARRANTY PURPOSES.
3.

SODDING NOTES
- 3.1.

THE CONTRACTOR IS TO ENSURE COVERAGE AND SOD GROWTH.
- 3.2.

SOD APPLICATION TO BE INSTALLED IN A CONSISTENT AND THOROUGH FASHION COVERING ENTIRE SITE.
- 3.3.

SOD ALL DISTURBED AREAS WITHIN THE CONSTRUCTION/ACCESS EASEMENTS UNLESS SHOWN OTHERWISE.
- 3.4.

TOPSOIL IN SODDED AREAS TO BE 150mm MINIMUM.
- 3.5.

SOD WITH TURF GRASS NURSERY SOD TYPES:
- 3.5.1.

NUMBER ONE KENTUCKY BLUE GRASS SOD AND/OR FESCUE SOD. NURSERY SOD GROWN SOLELY FROM SEED MIXTURE OF CULTIVARS OF KENTUCKY BLUEGRASS AND/OR CHEWING FESCUE OR CREEPING RED FESCUE, CONTAINING NOT LESS THAN 40% KENTUCKY BLUEGRASS CULTIVARS AND 30% CHEWING FESCUE OR CREEPING RED FESCUE CULTIVARS.
- 3.6.

CONTRACT ADMINSTRATOR TO BE CONTACTED PRIOR TO SODDING TO COORDINATE OPERATIONS IN ORDER TO ENSURE FINAL ACCEPTANCE.
- 3.7.

ALL SODDING OF DISTURBED AREAS MUST OCCUR IMMEDIATELY UPON COMPLETION OF THE GRADING WORK, WEATHER PERMITTING. SODDING OPERATIONS SHOULD TAKE PLACE BETWEEN APRIL 15 AND JUNE 15 OR BETWEEN AUGUST 15 AND OCTOBER 30 AT THE LATEST, PROVIDING THE GROUND IS IN A FROST-FREE CONDITION. IN THE EVENT THAT IMMEDIATE COMPLETION OF THE SODDING IS NOT FEASIBLE OTHER EROSION CONTROL METHODS WILL BE REQUIRED. DISCUSS WITH CONTRACT ADMINISTRATOR PRIOR TO PROCEEDING.
- 3.8.

ALL AREAS DISTURBED DURING REMOVAL OF SEDIMENT AND EROSION CONTROL MEASURES ARE TO BE SODDED.
- 3.9.

ALL TOPSOIL COMPACTED DURING CONSTRUCTION ACTIVITIES IS TO BE SCARIFIED TO THE SATISFACTION OF THE CONTRACT ADMINISTRATOR PRIOR TO SOD APPLICATION.
- 3.10.

IF DISTURBED AREA EXTENDS BEYOND CONSTRUCTION LIMIT, APPLY 150mm THICK TOPSOIL AND SOD.

CLIENT

WDBA

APWD

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PROJECT Co.

BRIDGING

NORTHAMERICA

CONSULTANT

AECOM

PROJECT

GORDIE HOWE

INTERNATIONAL BRIDGE

SANDWICH STREET CORRIDOR

WINDSOR, ONTARIO

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ASSOCIATION OF LANDSCAPE ARCHITECTS OF CANADA

ALAC

MEMBER SINCE 2014

REGISTERED PROFESSIONAL

ISSUE/REVISION

A	MAR-19-24	ISSUED FOR RFC SNS
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DESIGN UNIT

LANDSCAPE

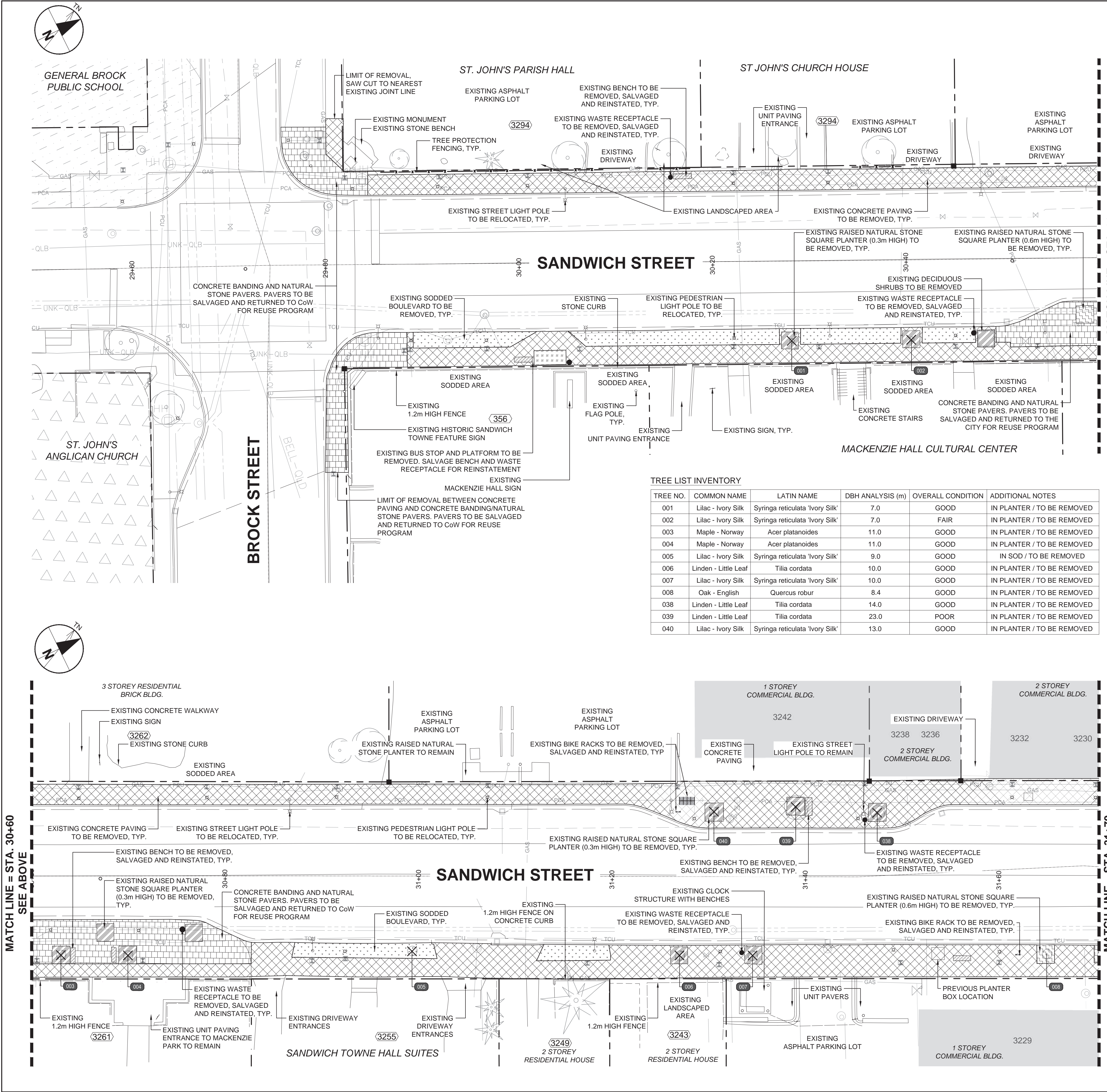
SHEET TITLE

LIST OF SHEETS & LANDSCAPE NOTES

SHEET No.

L-001

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LEGEND:

TCU

PCU

PCA

GAS

PROPERTY LINE

EXISTING PHONE LINE

EXISTING UNDERGROUND POWER LINE

EXISTING OVERHEAD POWER LINE

EXISTING GAS LINE

EXISTING DECIDUOUS TREE

EXISTING CONIFEROUS TREE

EXISTING PEDESTRIAN LIGHT POLE TO BE RELOCATED

EXISTING STREET LIGHT POLE TO BE RELOCATED

EXISTING ELECTRICAL HAND HOLE

EXISTING CATCH BASIN

EXISTING MAINTENANCE HOLE

EXISTING FIRE HYDRANT AND WATER VALVE

EXISTING BENCH TO BE REMOVED AND SALVAGED FOR REINSTATEMENT

EXISTING BIKE RACK TO BE REMOVED AND SALVAGED FOR REINSTATEMENT

EXISTING WASTE RECEPTACLE TO BE REMOVED AND SALVAGED FOR REINSTATEMENT

EXISTING BUS STOP AND PLATFORM TO BE REMOVED

EXISTING RAISED NATURAL STONE SQUARE PLANTER (0.3m HIGH) TO BE REMOVED

EXISTING RAISED NATURAL STONE SQUARE PLANTER (0.6m HIGH) TO BE REMOVED

EXISTING SIGN

EXISTING CONCRETE PAVING TO BE REMOVED

EXISTING SODDED BOULEVARD TO BE REMOVED

PART IV HERITAGE ACT AREA

ARCHAEOLOGY NO-GO AREA

PROPOSED TREE PROTECTION FENCING

CLIENT

WDBA APWD

WINDSOR-DETROIT BRIDGE AUTHORITY | AUTORITÉ DU PONT WINDSOR-DETROIT

PROJECT Co.

BRIDGING NORTH AMERICA

CONSULTANT

AECOM

PROJECT

GORDIE HOWE

INTERNATIONAL BRIDGE

SANDWICH STREET CORRIDOR

WINDSOR, ONTARIO

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STAMP

KEY PLAN

0 5 10 m

1:200

ISSUE/REVISION

NO.	DATE	DESCRIPTION
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DESIGN UNIT

LANDSCAPE

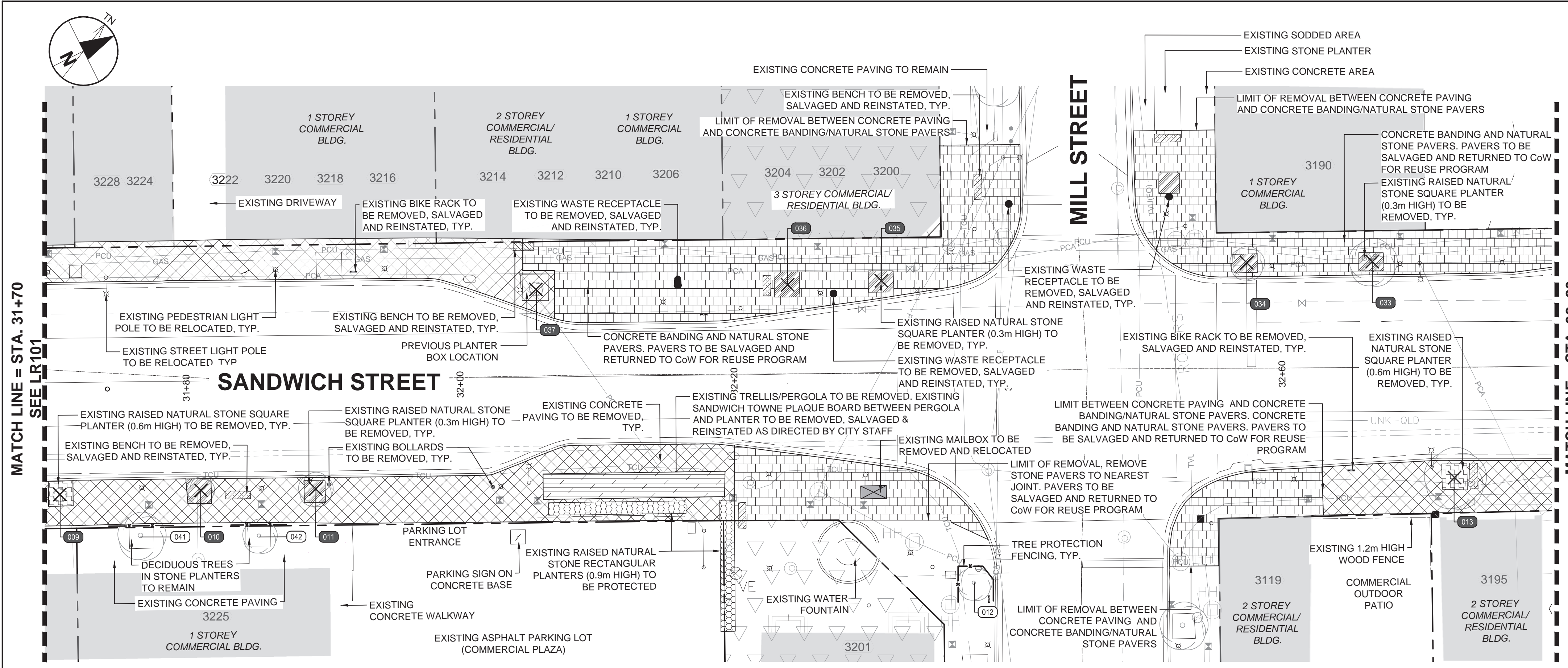
SHEET TITLE

SANDWICH STREET NORTH EXISTING CONDITIONS 29+50 TO 31+70

SHEET No.

LR101

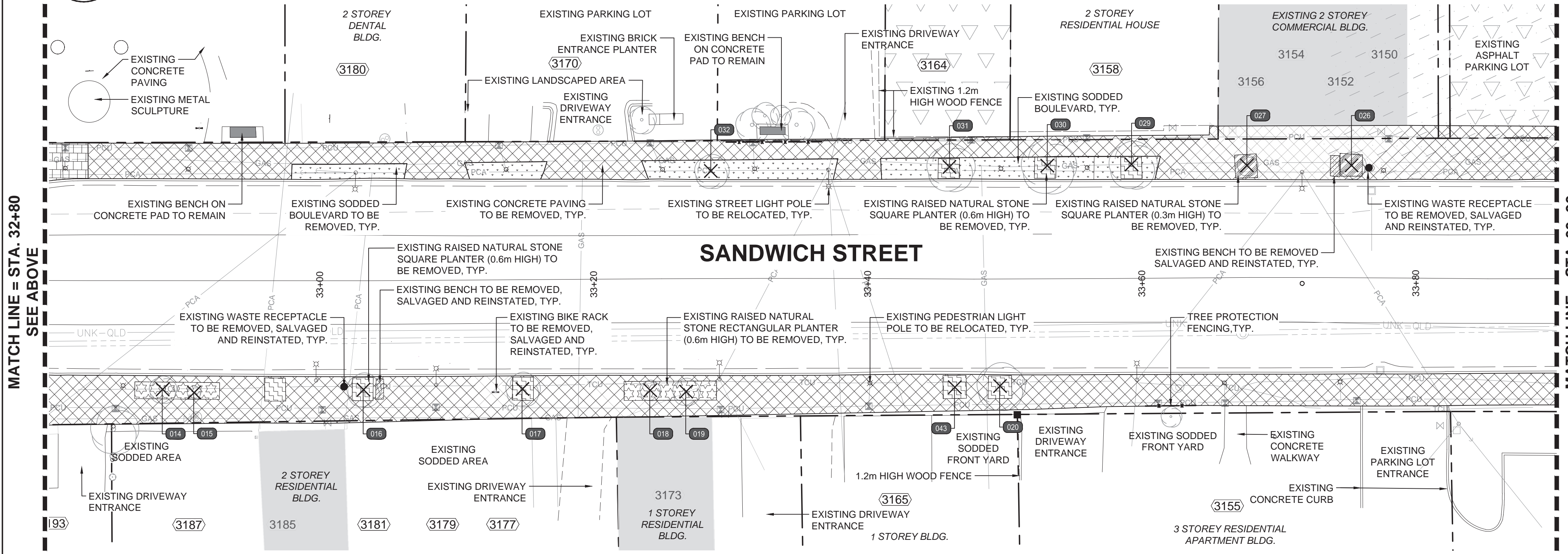
TREE LIST INVENTORY						
TREE NO.	COMMON NAME	LATIN NAME	DBH ANALYSIS (m)	OVERALL CONDITION	ADDITIONAL NOTES	
001	Lilac - Ivory Silk	Syringa reticulata 'Ivory Silk'	7.0	GOOD	IN PLANTER / TO BE REMOVED	
002	Lilac - Ivory Silk	Syringa reticulata 'Ivory Silk'	7.0	FAIR	IN PLANTER / TO BE REMOVED	
003	Maple - Norway	Acer platanoides	11.0	GOOD	IN PLANTER / TO BE REMOVED	
004	Maple - Norway	Acer platanoides	11.0	GOOD	IN PLANTER / TO BE REMOVED	
005	Lilac - Ivory Silk	Syringa reticulata 'Ivory Silk'	9.0	GOOD	IN SOD / TO BE REMOVED	
006	Linden - Little Leaf	Tilia cordata	10.0	GOOD	IN PLANTER / TO BE REMOVED	
007	Lilac - Ivory Silk	Syringa reticulata 'Ivory Silk'	10.0	GOOD	IN PLANTER / TO BE REMOVED	
008	Oak - English	Quercus robur	8.4	GOOD	IN PLANTER / TO BE REMOVED	
038	Linden - Little Leaf	Tilia cordata	14.0	GOOD	IN PLANTER / TO BE REMOVED	
039	Linden - Little Leaf	Tilia cordata	23.0	POOR	IN PLANTER / TO BE REMOVED	
040	Lilac - Ivory Silk	Syringa reticulata 'Ivory Silk'	13.0	GOOD	IN PLANTER / TO BE REMOVED	



TREE LIST INVENTORY

TREE NO.	COMMON NAME	LATIN NAME	DBH ANALYSIS (m)	OVERALL CONDITION	ADDITIONAL NOTES
009	Oak - English	Quercus robur	8.4	GOOD	IN PLANTER / TO BE REMOVED
010	Pear - Bradford	Pyrus calleryana 'Bradford'	22.0	FAIR	IN PLANTER / TO BE REMOVED
011	Pear - Bradford	Pyrus calleryana 'Bradford'	23.0	POOR	IN PLANTER, DEAD LIMB OVER ROAD
012	Honey-Locust	Gleditsia triacanthos	28.0	GOOD	OUTSIDE PROJECT LIMIT
013	Linden - Little Leaf	Tilia cordata	41.0	FAIR	IN PLANTER, DEAD BRANCH OVER SIDEWALK, VERTICAL CRACK ON TRUNK
014	Maple - Norway	Acer platanoides	20.0	FAIR	IN PLANTER / TO BE REMOVED
015	Maple - Norway	Acer platanoides	16.0	FAIR	IN PLANTER / TO BE REMOVED
016	Maple - Norway	Acer platanoides	25.0	FAIR	IN PLANTER / TO BE REMOVED
017	Maple - Norway	Acer platanoides	24.0	FAIR	N/A / TO BE REMOVED
018	Maple - Norway	Acer platanoides	15.0	GOOD	IN PLANTER / TO BE REMOVED
019	Maple - Norway	Acer platanoides	20.0	GOOD	IN PLANTER / TO BE REMOVED
020	Maple - Norway	Acer platanoides	27.0	GOOD	IN PLANTER, BROKEN / TO BE REMOVED
026	Pear - Bradford	Pyrus calleryana 'Bradford'	20.0	GOOD	IN PLANTER / TO BE REMOVED

TREE NO.	COMMON NAME	LATIN NAME	DBH ANALYSIS (m)	OVERALL CONDITION	ADDITIONAL NOTES
027	Pear - Bradford	Pyrus calleryana 'Bradford'	18.0	GOOD	IN PLANTER / TO BE REMOVED
029	Maple - Norway	Acer platanoides	25.0	GOOD	IN PLANTER / TO BE REMOVED
030	Maple - Norway	Acer platanoides	30.0	GOOD	IN PLANTER / TO BE REMOVED
031	Maple - Norway	Acer platanoides	30.0	FAIR	IN PLANTER, LARGE DEADWOOD OVER SIDEWALK
032	Pear - Bradford	Pyrus calleryana 'Bradford'	22.0	GOOD	N/A / TO BE REMOVED
033	Honey-Locust	Gleditsia triacanthos	31.0	GOOD	IN PLANTER / TO BE REMOVED
034	Honey-Locust	Gleditsia triacanthos	29.0	GOOD	IN PLANTER / TO BE REMOVED
035	Maple - Norway	Acer platanoides	0.0	DEAD - STUMP	N/A / TO BE REMOVED
036	Maple - Norway	Acer platanoides	0.0	DEAD - STUMP	N/A / TO BE REMOVED
037	Lilac - Ivory Silk	Syringa reticulata 'Ivory Silk'	10.0	GOOD	IN PLANTER / TO BE REMOVED
041	Pear - Bradford	Pyrus calleryana 'Bradford'	30.0	FAIR	OUTSIDE PROJECT LIMIT
042	Pear - Bradford	Pyrus calleryana 'Bradford'	28.0	FAIR	OUTSIDE PROJECT LIMIT
043	Maple - Norway	Acer platanoides	0.0	DEAD - STUMP	IN PLANTER / TO BE REMOVED



LEGEND:

- PROPERTY LINE
- EXISTING PHONE LINE
- EXISTING UNDERGROUND POWER LINE
- EXISTING OVERHEAD POWER LINE
- EXISTING GAS LINE
- EXISTING DECIDUOUS TREE
- EXISTING PEDESTRIAN LIGHT POLE TO BE RELOCATED
- EXISTING STREET LIGHT POLE TO BE RELOCATED
- EXISTING ELECTRICAL HAND HOLE
- EXISTING CATCH BASIN
- EXISTING MAINTENANCE HOLE
- EXISTING FIRE HYDRANT AND WATER VALVE
- EXISTING BOLLARD
- EXISTING BENCH TO BE RETAINED
- EXISTING BENCH TO BE REMOVED AND SALVAGED FOR REINSTATEMENT
- EXISTING BIKE RACK TO BE REMOVED AND SALVAGED FOR REINSTATEMENT
- EXISTING WASTE RECEPTACLE TO BE REMOVED AND SALVAGED FOR REINSTATEMENT
- EXISTING MAILBOX TO BE REMOVED AND RELOCATED
- EXISTING TRELLIS/PERGOLA TO BE REMOVED
- EXISTING RAISED NATURAL STONE SQUARE PLANTER (0.3m HIGH) TO BE REMOVED
- EXISTING RAISED NATURAL STONE RAISED SQUARE PLANTER (0.6m HIGH) TO BE REMOVED
- EXISTING RAISED NATURAL STONE RECTANGULAR PLANTER (0.6m HIGH) TO BE REMOVED
- EXISTING RAISED NATURAL STONE RECTANGULAR PLANTER (0.9m HIGH) TO BE PROTECTED
- EXISTING SIGN
- EXISTING CONCRETE PAVING TO BE REMOVED
- EXISTING SODDED BOULEVARD TO BE REMOVED
- PART IV HERITAGE ACT AREA
- ARCHEOLOGY NO-GO AREA
- PROPOSED TREE PROTECTION FENCING

CLIENT

WDBA APWD
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PROJECT Co.

BRIDGING NORTHAMERICA

CONSULTANT

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PROJECT

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SANDWICH STREET CORRIDOR
WINDSOR, ONTARIO

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ASSOCIATION OF LANDSCAPE ARCHITECTS
AALA
WINDSOR - 1985

KEY PLAN

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1:200
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ISSUE/REVISION

A	MAR-19-24	ISSUED FOR RFC SNS
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DESIGN UNIT

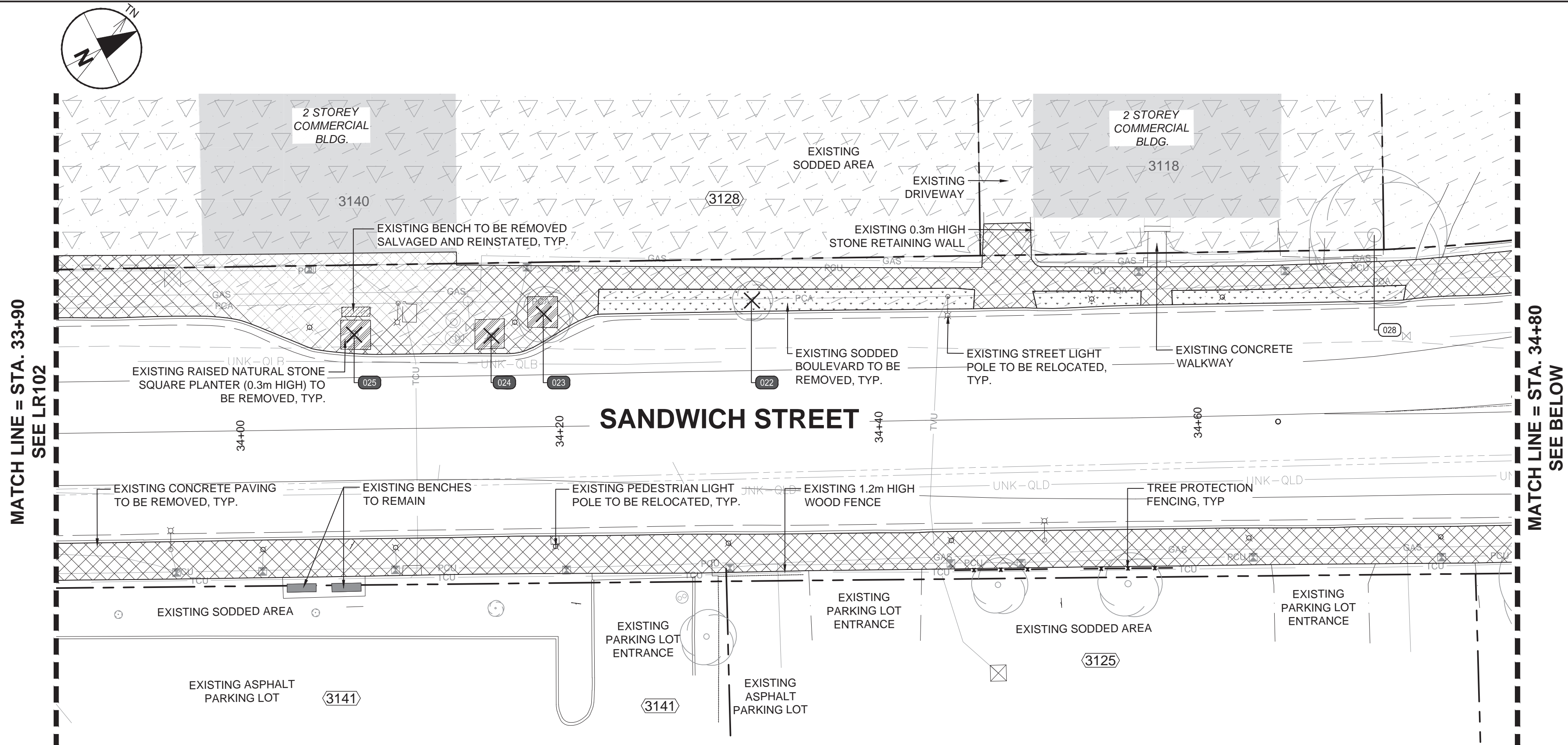
LANDSCAPE

SHEET TITLE

SANDWICH STREET NORTH EXISTING CONDITIONS 31+70 TO 33+90

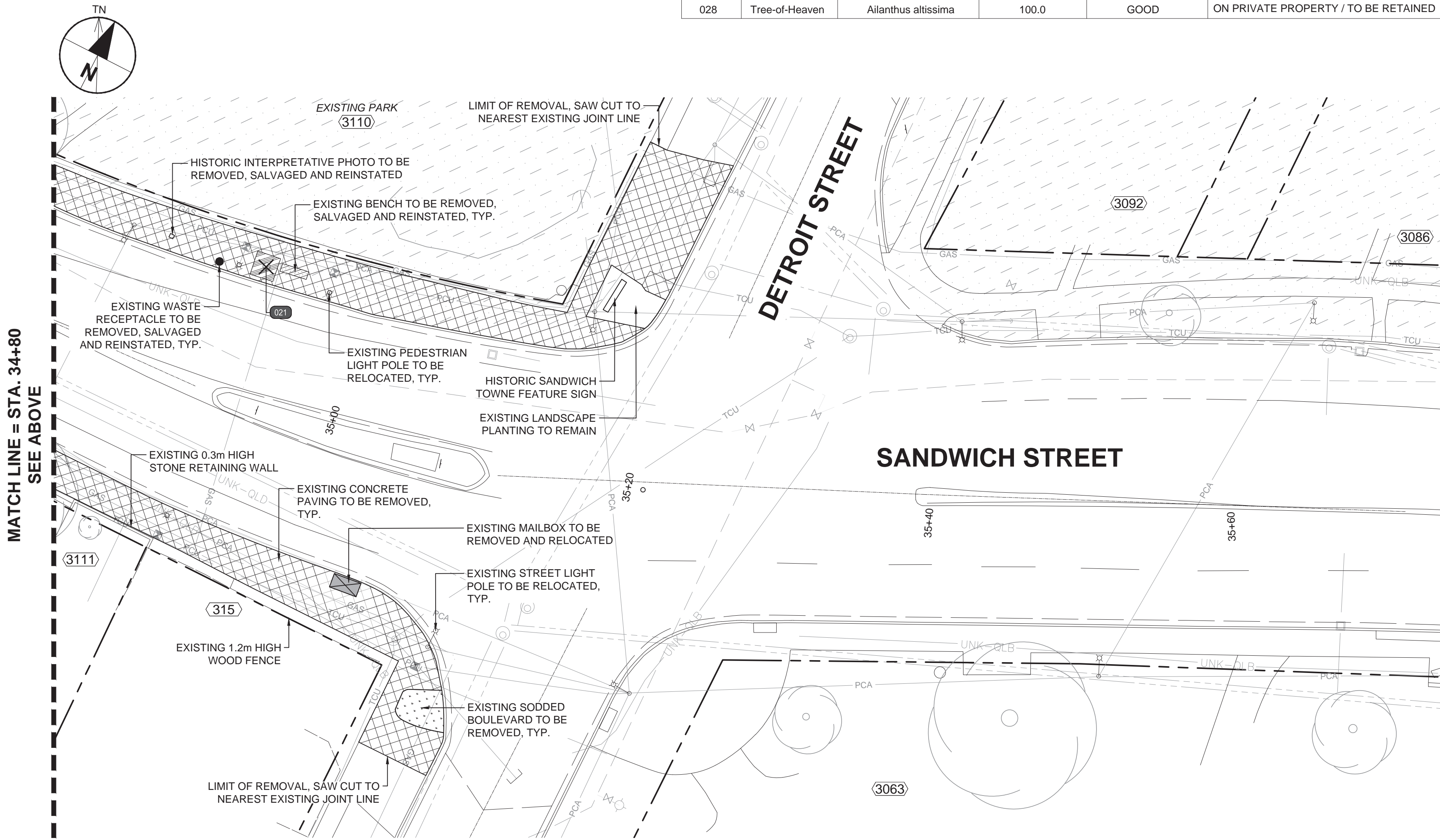
SHEET No.

LR102



TREE LIST INVENTORY

TREE NO.	COMMON NAME	LATIN NAME	DBH ANALYSIS (m)	OVERALL CONDITION	ADDITIONAL NOTES
021	Lilac - Ivory Silk	Syringa reticulata 'Ivory Silk'	8.0	GOOD	IN PLANTER / TO BE REMOVED
022	Maple - Norway	Acer platanoides	29.0	GOOD	IN BOULEVARD / TO BE REMOVED
023	Maple - Norway	Acer platanoides	0.0	DEAD - STUMP	IN PLANTER / TO BE REMOVED
024	Lilac - Ivory Silk	Syringa reticulata 'Ivory Silk'	9.0	GOOD	IN PLANTER / TO BE REMOVED
025	Lilac - Ivory Silk	Syringa reticulata 'Ivory Silk'	7.0	GOOD	IN PLANTER / TO BE REMOVED
028	Tree-of-Heaven	Ailanthus altissima	100.0	GOOD	ON PRIVATE PROPERTY / TO BE RETAINED



LEGEND:

- PROPERTY LINE
- EXISTING PHONE LINE
- EXISTING UNDERGROUND POWER LINE
- EXISTING OVERHEAD POWER LINE
- EXISTING GAS LINE
- EXISTING DECIDUOUS TREE
- EXISTING PEDESTRIAN LIGHT POLE TO BE RELOCATED
- EXISTING STREET LIGHT POLE TO BE RELOCATED
- EXISTING ELECTRICAL HAND HOLE
- EXISTING CATCH BASIN
- EXISTING MAINTENANCE HOLE
- EXISTING FIRE HYDRANT
- EXISTING BENCH TO BE RETAINED
- EXISTING BENCH TO BE REMOVED AND SALVAGED FOR REINSTATEMENT
- EXISTING WASTE RECEPTACLE TO BE REMOVED AND SALVAGED FOR REINSTATEMENT
- EXISTING MAILBOX TO BE REMOVED AND RELOCATED
- EXISTING RAISED NATURAL STONE SQUARE PLANTER (0.3m HIGH) TO BE REMOVED
- EXISTING SIGN
- EXISTING CONCRETE PAVING TO BE REMOVED
- EXISTING SODDED BOULEVARD
- PART IV HERITAGE ACT AREA
- ARCHAEOLOGY NO-GO AREA
- PROPOSED TREE PROTECTION FENCING

CLIENT



PROJECT Co.



CONSULTANT



PROJECT

GORDIE HOWE
INTERNATIONAL BRIDGE
SANDWICH STREET CORRIDOR
WINDSOR, ONTARIO

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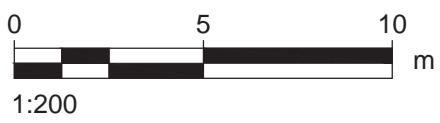
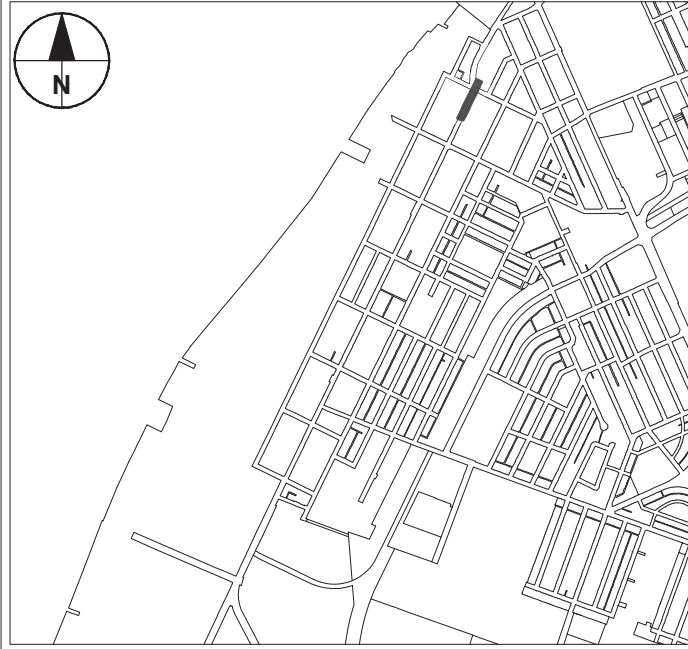
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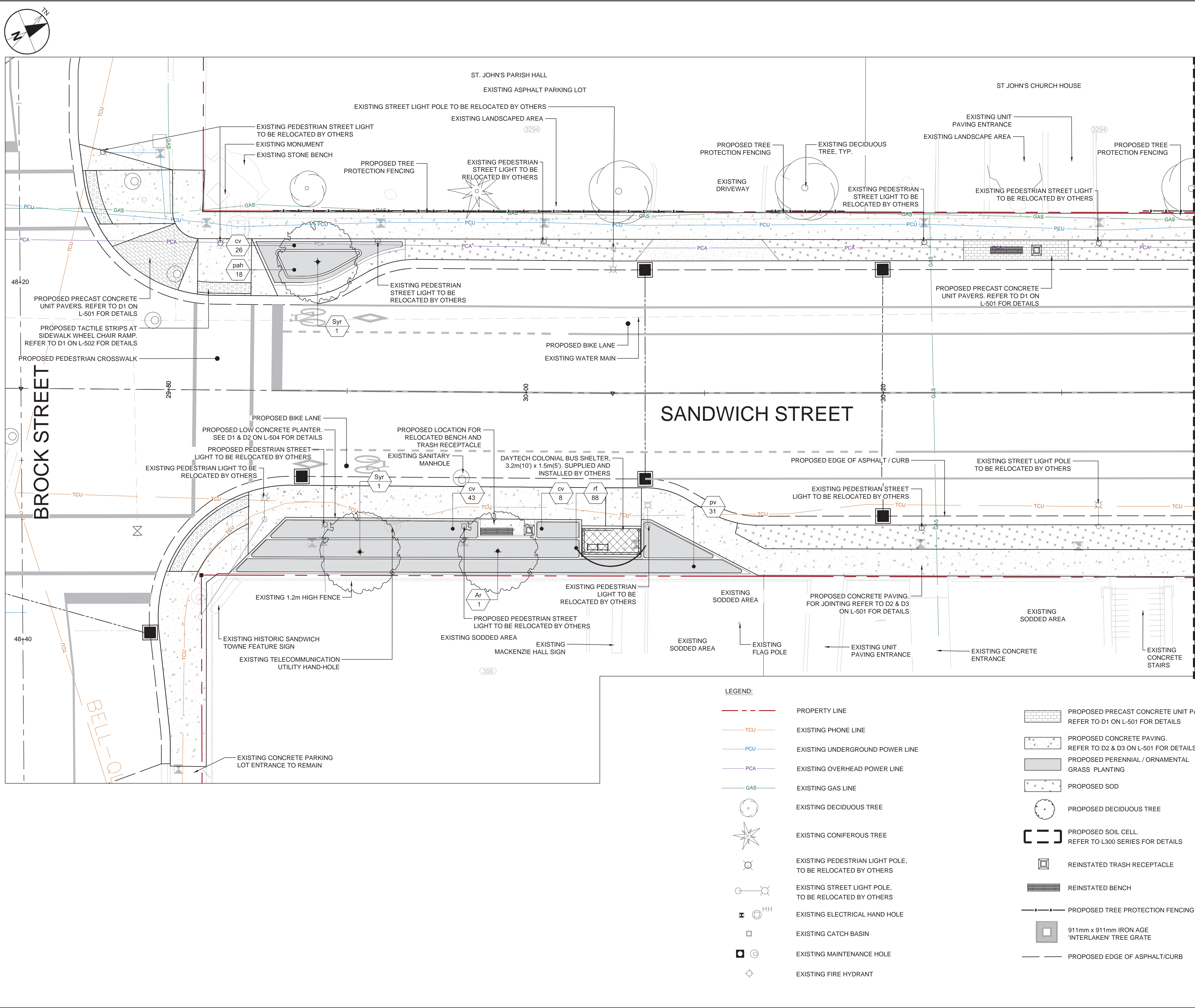
SHEET TITLE

**SANDWICH STREET NORTH
EXISTING CONDITIONS
33+90 TO 35+75**

SHEET No.

LR103

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PROJECT Co.

CONSULTANT

PROJECT

GORDIE HOWE
INTERNATIONAL BRIDGE
SANDWICH STREET CORRIDOR
WINDSOR, ONTARIO

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LANDSCAPE

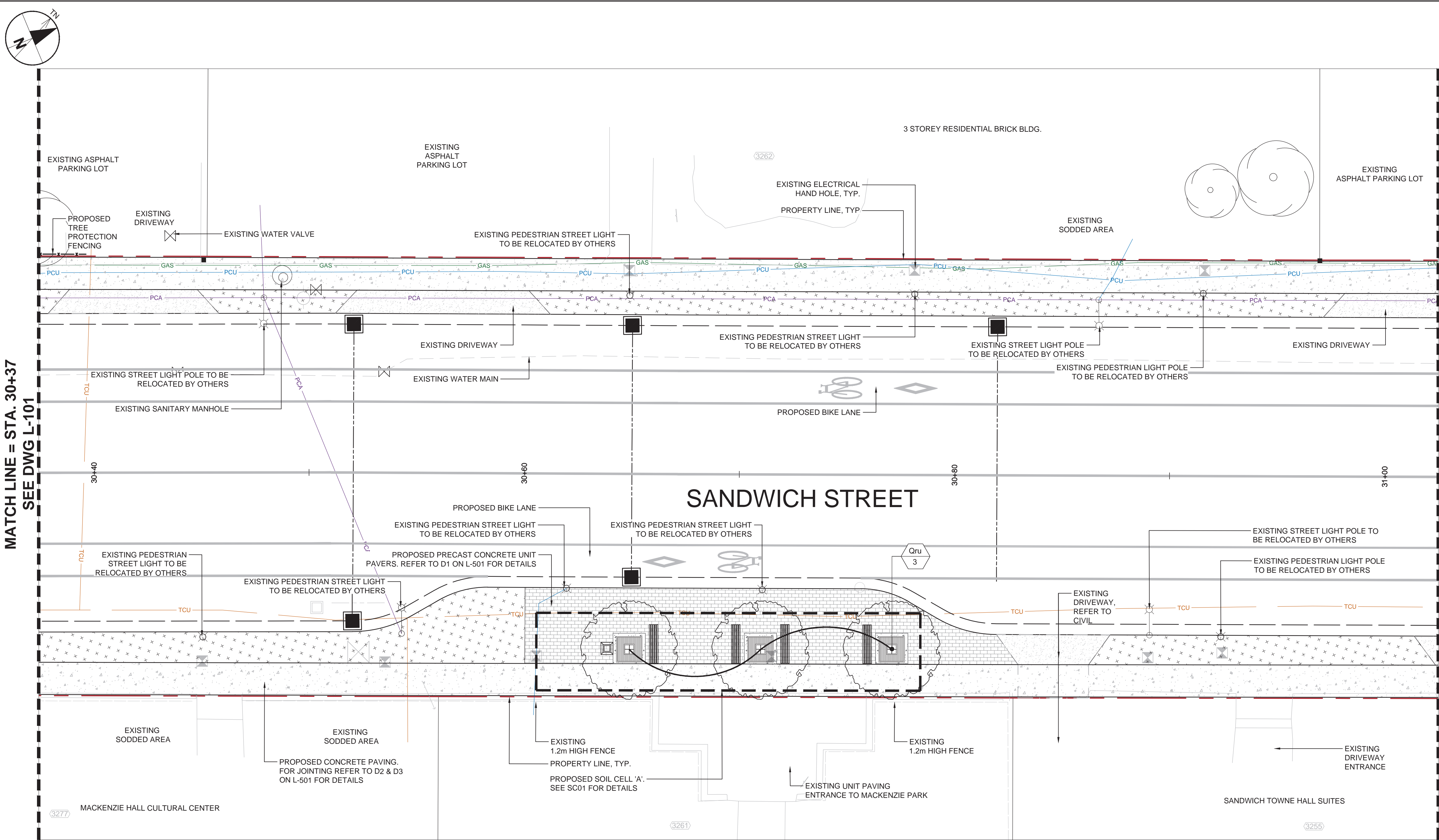
SHEET TITLE

**SANDWICH STREET
LANDSCAPE PLAN
29+70 TO 30+37**

SHEET No.

L-101

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LEGEND:

- PROPERTY LINE
- EXISTING PHONE LINE
- EXISTING UNDERGROUND POWER LINE
- EXISTING OVERHEAD POWER LINE
- EXISTING GAS LINE
- EXISTING DECIDUOUS TREE
- EXISTING CONIFEROUS TREE
- EXISTING PEDESTRIAN LIGHT POLE, TO BE RELOCATED BY OTHERS
- EXISTING STREET LIGHT POLE, TO BE RELOCATED BY OTHERS
- EXISTING ELECTRICAL HAND HOLE
- EXISTING CATCH BASIN
- EXISTING MAINTENANCE HOLE
- EXISTING FIRE HYDRANT

PROPOSED PRECAST CONCRETE UNIT PAVERS. REFER TO D1 ON L-501 FOR DETAILSPROPOSED CONCRETE PAVING. REFER TO D2 & D3 ON L-501 FOR DETAILSPROPOSED PERENNIAL / ORNAMENTAL GRASS PLANTINGPROPOSED SODPROPOSED DECIDUOUS TREEPROPOSED SOIL CELL. REFER TO L300 SERIES FOR DETAILSREINSTATED TRASH RECEPTACLEREINSTATED BENCHPROPOSED TREE PROTECTION FENCING911mm x 911mm IRON AGE 'INTERLAKEN' TREE GRATEPROPOSED EDGE OF ASPHALT/CURB

CLIENT

WINDSOR-DETROIT BRIDGE AUTHORITY | AUTORITÉ DU PORT WINDSOR-DETROIT

PROJECT Co.

CONSULTANT

PROJECT

GORDIE HOWE
INTERNATIONAL BRIDGE
SANDWICH STREET CORRIDOR
WINDSOR, ONTARIO

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ISSUE/REVISION

#	#	#
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I/R	DATE	DESCRIPTION

DESIGN UNIT

LANDSCAPE

SHEET TITLE

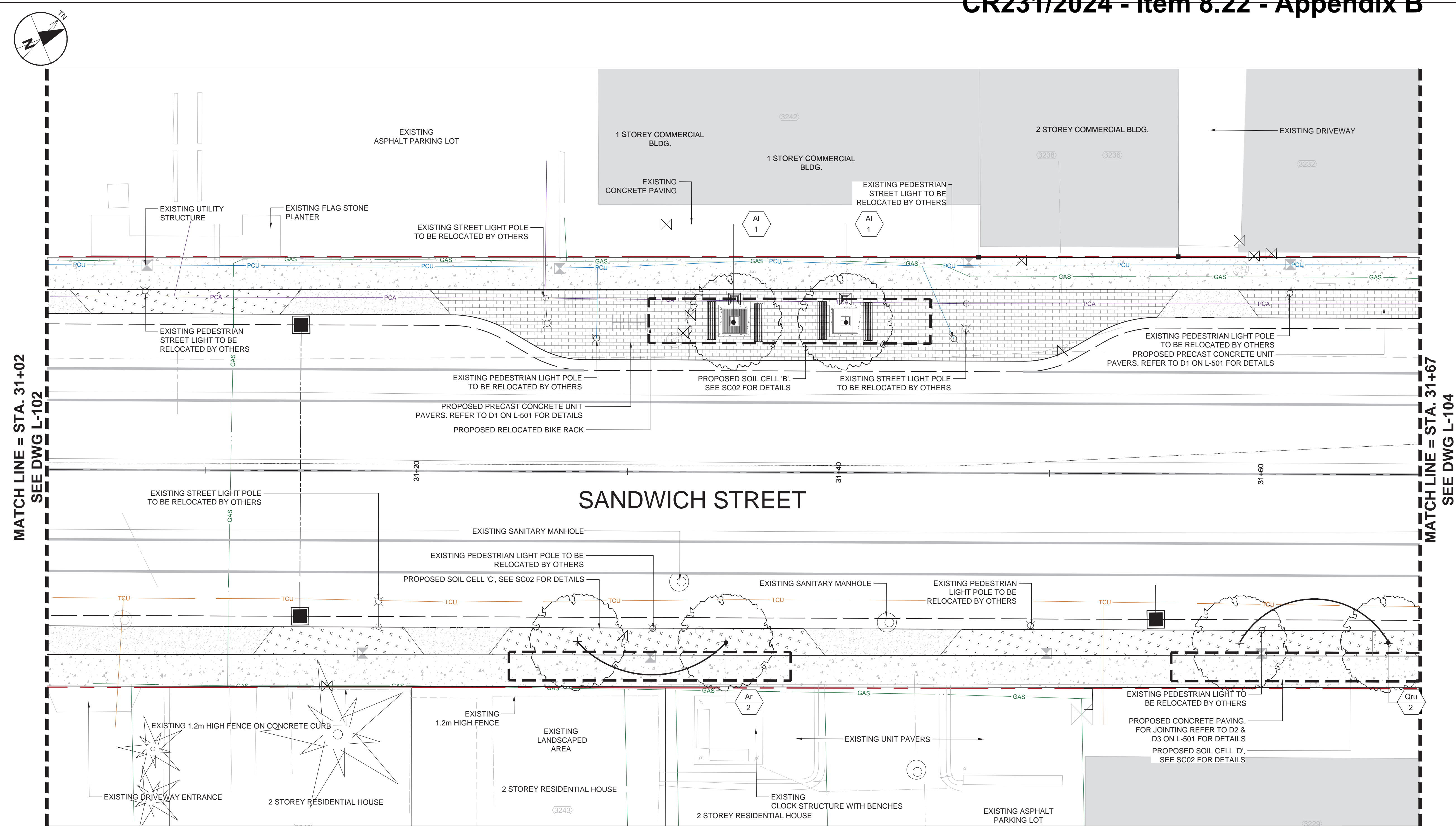
SANDWICH STREET LANDSCAPE PLAN
30+37 TO 31+02

SHEET No.

L-102

MATCH LINE = STA. 30+37
SEE DWG L-101

MATCH LINE = STA. 31+02
SEE DWG L-103



SANDWICH STREET

LEGEND:

- PROPERTY LINE
- EXISTING PHONE LINE
- EXISTING UNDERGROUND POWER LINE
- EXISTING OVERHEAD POWER LINE
- EXISTING GAS LINE
- EXISTING DECIDUOUS TREE
- EXISTING CONIFEROUS TREE
- EXISTING PEDESTRIAN LIGHT POLE, TO BE RELOCATED BY OTHERS
- EXISTING STREET LIGHT POLE, TO BE RELOCATED BY OTHERS
- EXISTING ELECTRICAL HAND HOLE
- EXISTING CATCH BASIN
- EXISTING MAINTENANCE HOLE
- EXISTING FIRE HYDRANT

- PROPOSED PRECAST CONCRETE UNIT PAVERS. REFER TO D1 ON L-501 FOR DETAILS
- PROPOSED CONCRETE PAVING. REFER TO D2 & D3 ON L-501 FOR DETAILS
- PROPOSED PERENNIAL / ORNAMENTAL GRASS PLANTING
- PROPOSED SOD
- PROPOSED DECIDUOUS TREE
- PROPOSED SOIL CELL. REFER TO L300 SERIES FOR DETAILS
- REINSTATED TRASH RECEPTACLE
- REINSTATED BENCH
- 911mm x 911mm IRON AGE 'INTERLAKEN' TREE GRATE
- PROPOSED EDGE OF ASPHALT/CURB

CLIENT



PROJECT Co.



CONSULTANT

AECOM

PROJECT

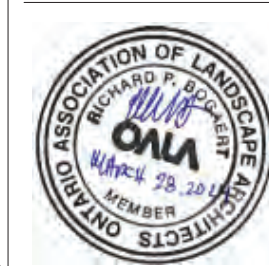
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INTERNATIONAL BRIDGE
SANDWICH STREET CORRIDOR
WINDSOR, ONTARIO**

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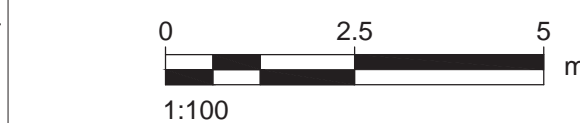
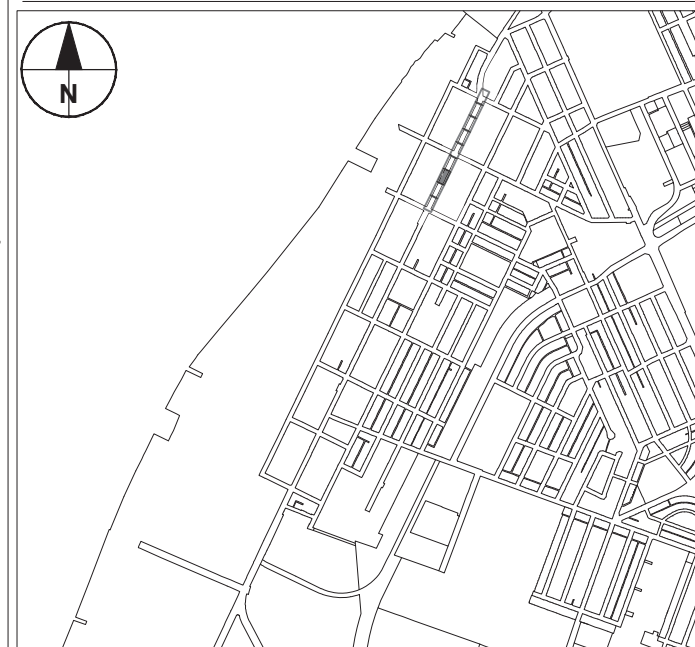
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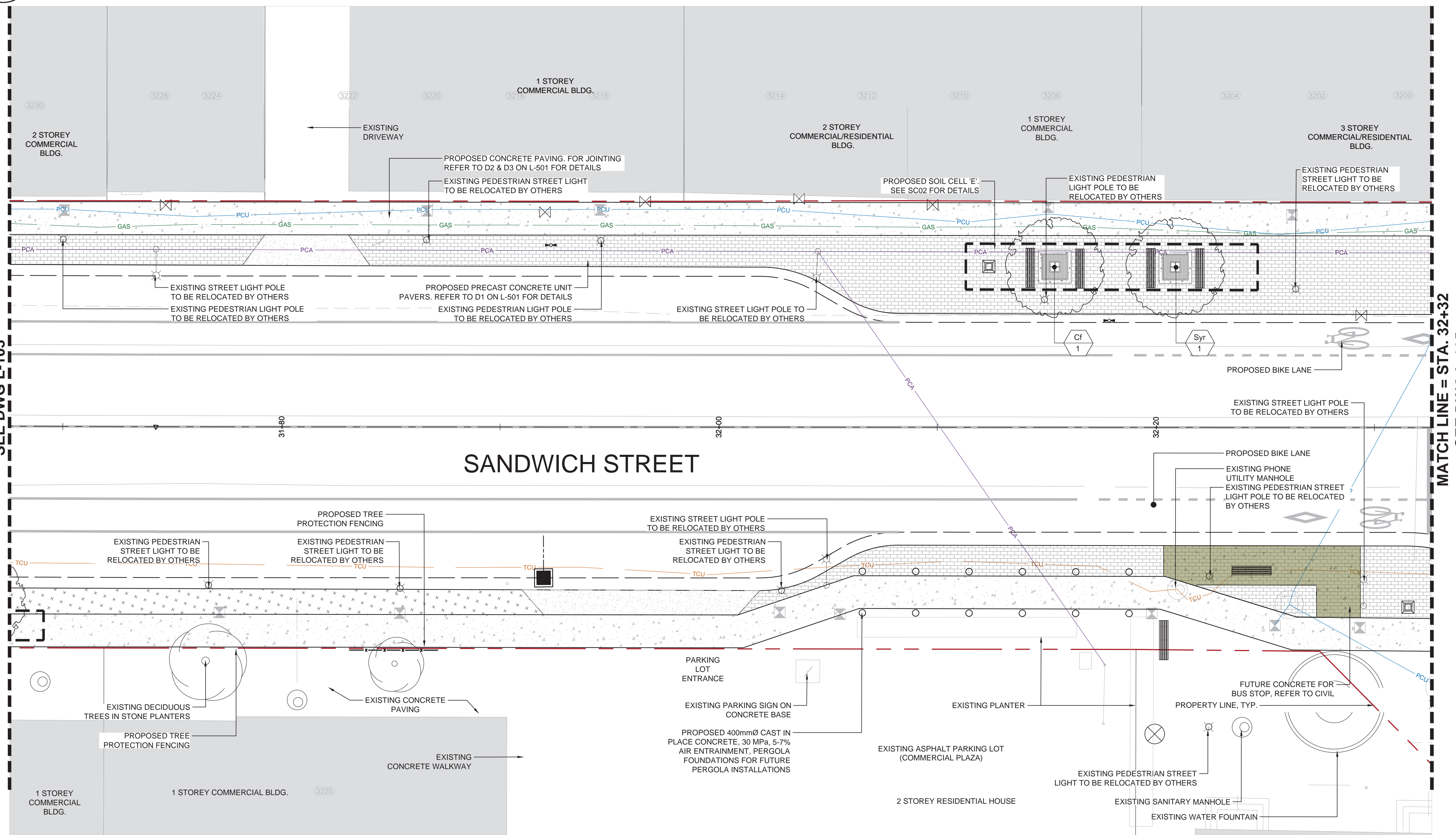
LANDSCAPE

SHEET TITLE










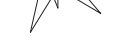

**SANDWICH STREET
LANDSCAPE PLAN
31+02 TO 31+67**








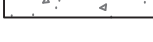
SHEET No.






L-103



LEGEND:

- | | |
|---|--|
|  | PROPERTY LINE |
|  | EXISTING PHONE LINE |
|  | EXISTING UNDERGROUND POWER LINE |
|  | EXISTING OVERHEAD POWER LINE |
|  | EXISTING GAS LINE |
|  | EXISTING DECIDUOUS TREE |
|  | EXISTING CONIFEROUS TREE |
|  | EXISTING PEDESTRIAN LIGHT POLE,
TO BE RELOCATED BY OTHERS |
|  | EXISTING STREET LIGHT POLE,
TO BE RELOCATED BY OTHERS |
|  | EXISTING ELECTRICAL HAND HOLE |
|  | 911mm x 911mm IRON AGE
'INTERLAKEN' TREE GRATE |

- | | |
|---|--|
|  | PROPOSED EDGE OF ASPHALT/CURB |
|  | EXISTING CATCH BASIN |
|  | EXISTING MAINTENANCE HOLE |
|  | EXISTING FIRE HYDRANT |
|  | PROPOSED PRECAST CONCRETE UNIT PAVERS.
REFER TO D1 ON L-501 FOR DETAILS |
|  | PROPOSED CONCRETE PAVING.
REFER TO D2 & D3 ON L-501 FOR DETAILS |
|  | PROPOSED PERENNIAL / ORNAMENTAL
GRASS PLANTING |
|  | PROPOSED SOD |

-  PROPOSED DECIDUOUS TREE
-  PROPOSED SOIL CELL.
REFER TO L300 SERIES FOR DETAILS
-  REINSTATED TRASH RECEPTACLE
-  REINSTATED BENCH
-  PROPOSED TREE PROTECTION FENCING

CLIENT



CONSULTANT

AECOM

PROJECT

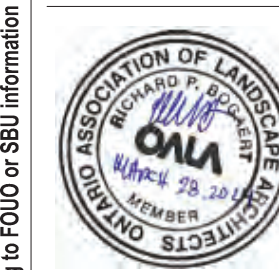
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INTERNATIONAL BRIDGE
SANDWICH STREET CORRIDOR
WINDSOR, ONTARIO

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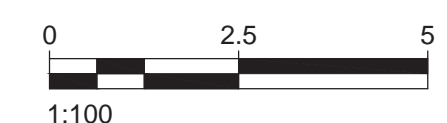
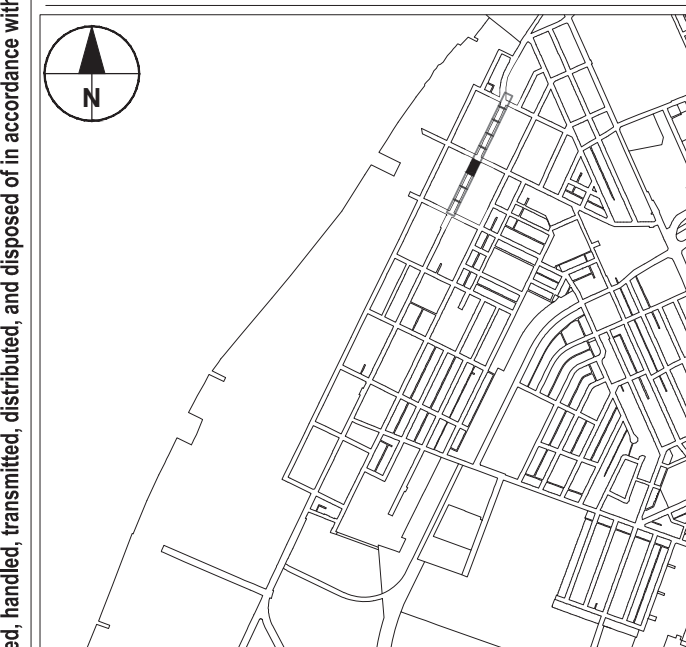
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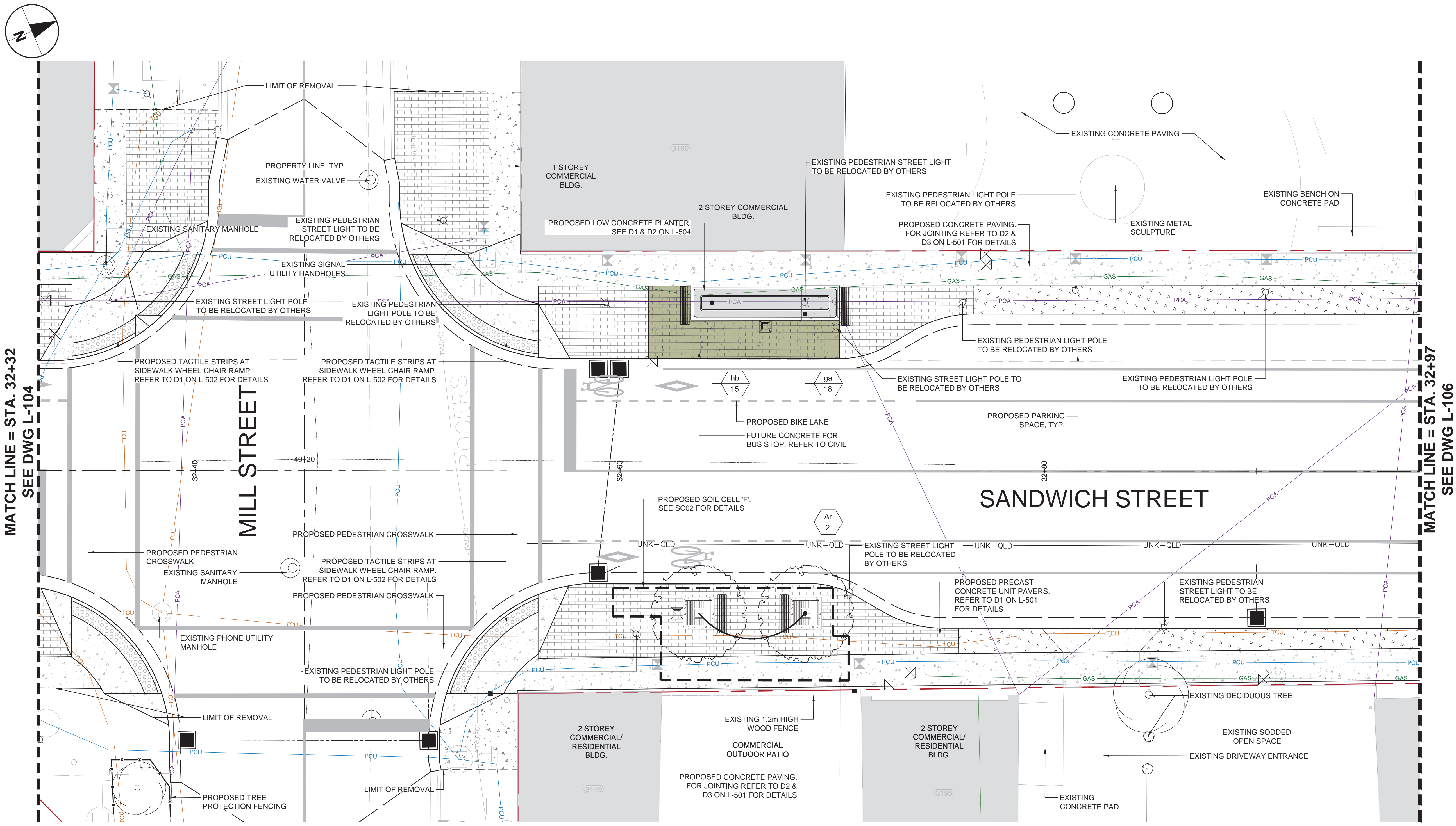
LANDSCAPE

SHEET TITLE

SANDWICH STREET LANDSCAPE PLAN 31+67 TO 32+32

SHEET No.

L-104



- LEGEND:
- | | | | |
|--|---|--|---|
| | PROPERTY LINE | | PROPOSED PRECAST CONCRETE UNIT PAVERS. REFER TO D1 ON L-501 FOR DETAILS |
| | EXISTING PHONE LINE | | PROPOSED CONCRETE PAVING. REFER TO D2 & D3 ON L-501 FOR DETAILS |
| | EXISTING UNDERGROUND POWER LINE | | PROPOSED PERENNIAL / ORNAMENTAL GRASS PLANTING |
| | EXISTING OVERHEAD POWER LINE | | PROPOSED SOD |
| | EXISTING GAS LINE | | PROPOSED DECIDUOUS TREE |
| | EXISTING DECIDUOUS TREE | | PROPOSED SOIL CELL. REFER TO L300 SERIES FOR DETAILS |
| | EXISTING CONIFEROUS TREE | | REINSTATED TRASH RECEPTACLE |
| | EXISTING PEDESTRIAN LIGHT POLE, TO BE RELOCATED BY OTHERS | | REINSTATED BENCH |
| | EXISTING STREET LIGHT POLE, TO BE RELOCATED BY OTHERS | | PROPOSED TREE PROTECTION FENCING |
| | EXISTING ELECTRICAL HAND HOLE | | 911mm x 911mm IRON AGE 'INTERLAKEN' TREE GRATE |
| | EXISTING CATCH BASIN | | PROPOSED EDGE OF ASPHALT/CURB |
| | EXISTING MAINTENANCE HOLE | | |
| | EXISTING FIRE HYDRANT | | |

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WINDSOR-DETROIT BRIDGE AUTHORITY | AUTORITÉ DU PONT WINDSOR-DETROIT

PROJECT Co.

CONSULTANT

PROJECT

GORDIE HOWE INTERNATIONAL BRIDGE
SANDWICH STREET CORRIDOR
WINDSOR, ONTARIO

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KEY PLAN

1:100

ISSUE/REVISION

A	MAR-19-24	ISSUED FOR RFC SNS
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DESIGN UNIT

LANDSCAPE

SHEET TITLE

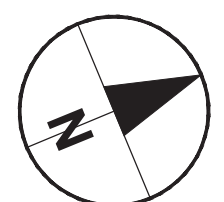
SANDWICH STREET LANDSCAPE PLAN
32+32 TO 32+97

SHEET No.

L-105

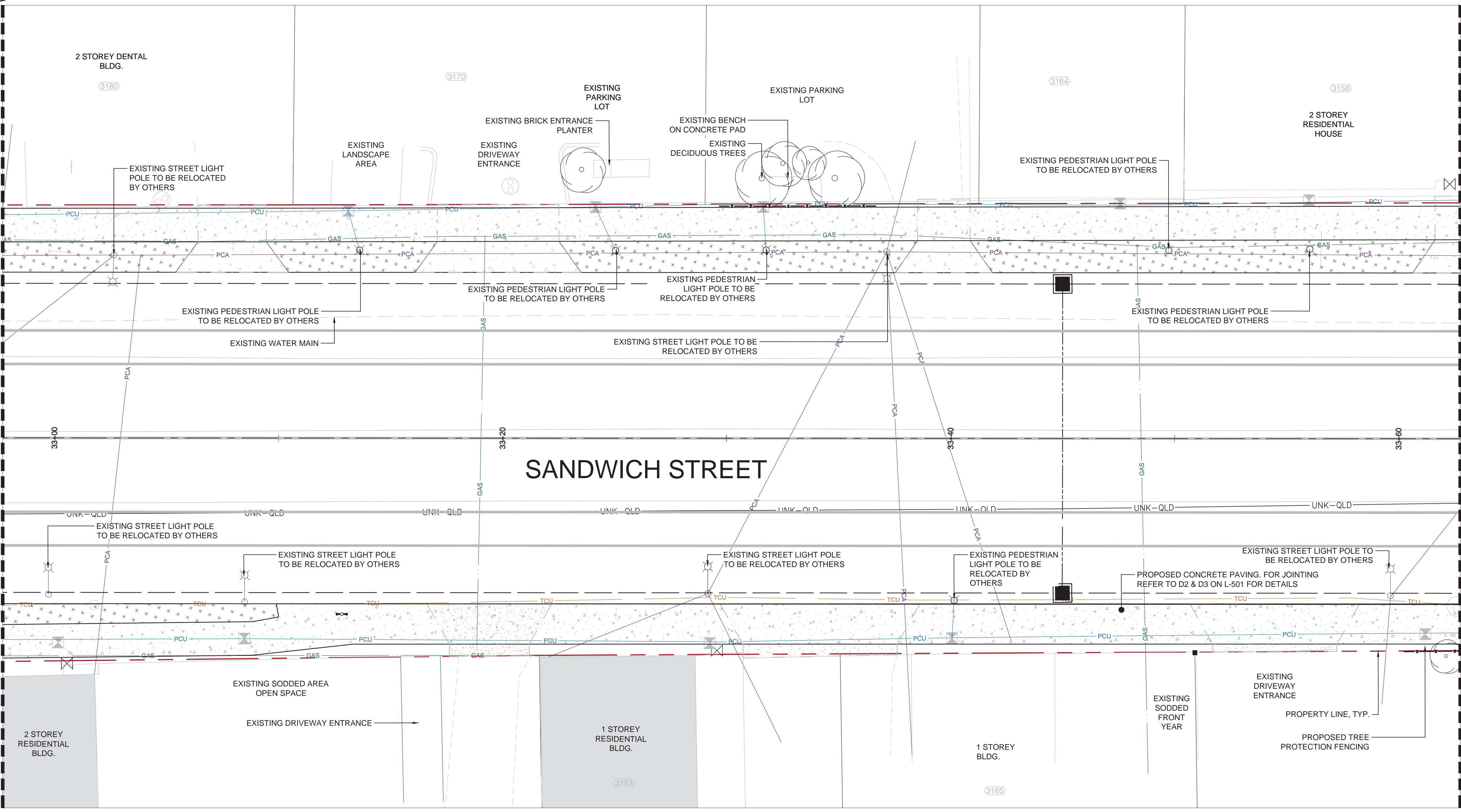
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MATCH LINE = STA. 32+97
SEE DWG L-105

MATCH LINE = STA. 33+67
SEE DWG L-107



LEGEND:

- PROPERTY LINE
- TCU EXISTING PHONE LINE
- PCU EXISTING UNDERGROUND POWER LINE
- PCA EXISTING OVERHEAD POWER LINE
- GAS EXISTING GAS LINE
- EXISTING DECIDUOUS TREE
- ★ EXISTING CONIFEROUS TREE
- EXISTING PEDESTRIAN LIGHT POLE, TO BE RELOCATED BY OTHERS
- EXISTING STREET LIGHT POLE, TO BE RELOCATED BY OTHERS
- ⊗ EXISTING ELECTRICAL HAND HOLE
- EXISTING CATCH BASIN
- ⊗ EXISTING MAINTENANCE HOLE
- ⊗ EXISTING FIRE HYDRANT
- PROPOSED PRECAST CONCRETE UNIT PAVERS. REFER TO D1 ON L-501 FOR DETAILS
- PROPOSED CONCRETE PAVING. REFER TO D2 & D3 ON L-501 FOR DETAILS
- PROPOSED PERENNIAL / ORNAMENTAL GRASS PLANTING
- PROPOSED SOD
- PROPOSED TREE PROTECTION FENCING
- PROPOSED EDGE OF ASPHALT/CURB

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WDBA APWD
WINDSOR-DETROIT BRIDGE AUTHORITY / AUTORITÉ DU PONT WINDSOR-DETROIT

PROJECT Co.

BRIDGING NORTH AMERICA

CONSULTANT

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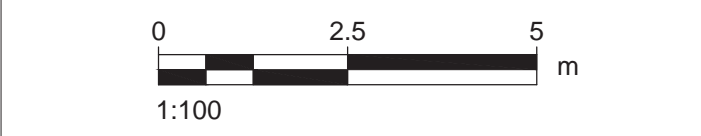
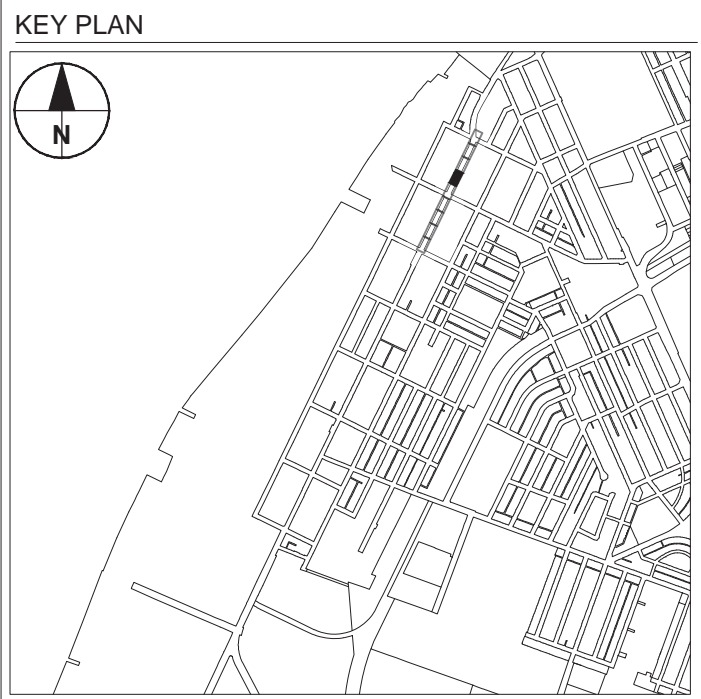
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WINDSOR, ONTARIO

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NO.	DATE	DESCRIPTION

DESIGN UNIT

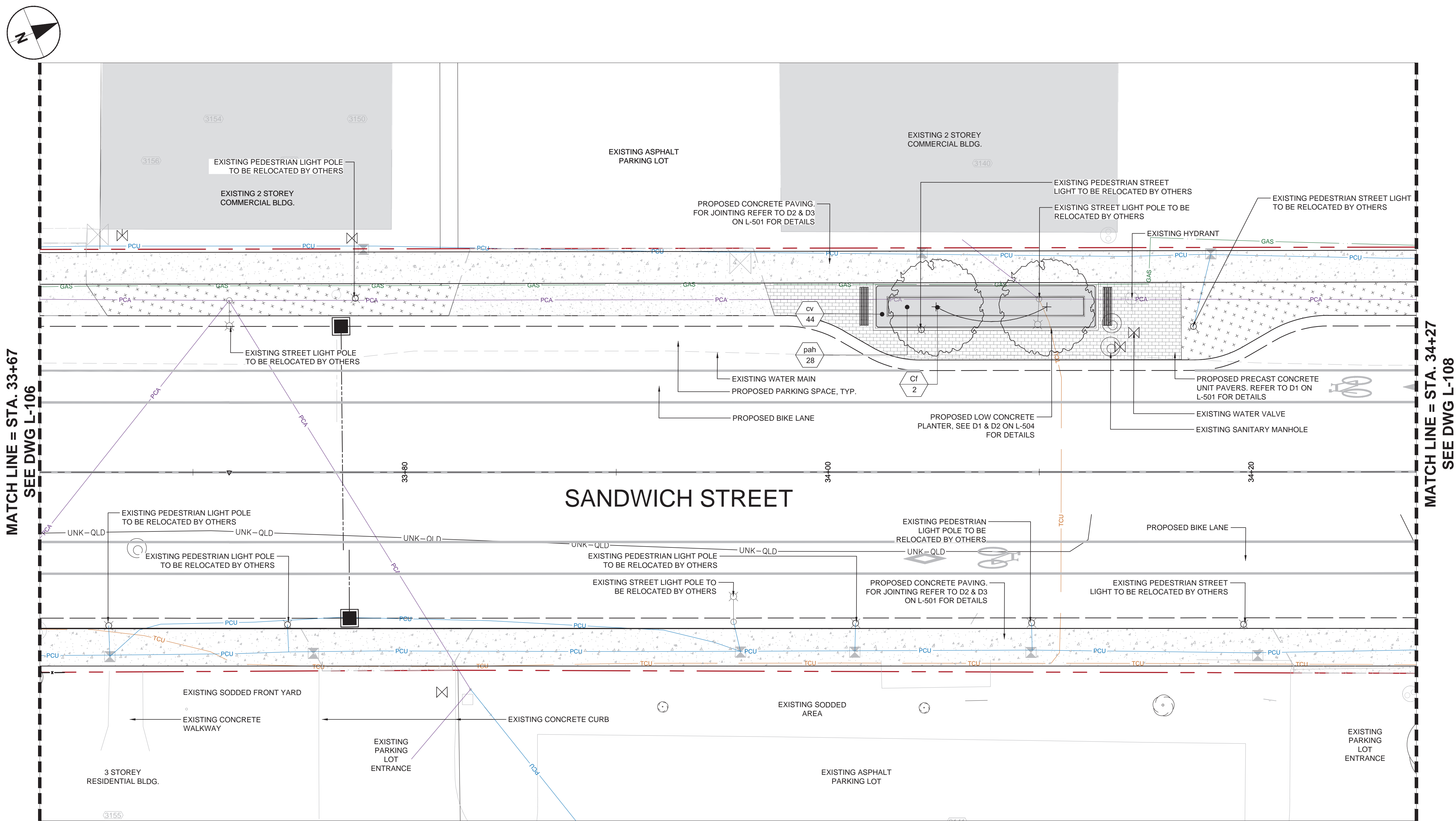
LANDSCAPE

SHEET TITLE


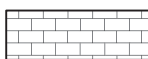

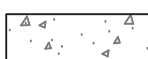



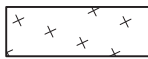














SANDWICH STREET LANDSCAPE PLAN 32+97 TO 33+67

SHEET No.

L-106



LEGEND:

- | | | | |
|---|--|---|--|
|  | PROPERTY LINE |  | PROPOSED PRECAST CONCRETE UNIT PAVERS.
REFER TO D1 ON L-501 FOR DETAILS |
|  | EXISTING PHONE LINE |  | PROPOSED CONCRETE PAVING.
REFER TO D2 & D3 ON L-501 FOR DETAILS |
|  | EXISTING UNDERGROUND POWER LINE |  | PROPOSED PERENNIAL / ORNAMENTAL
GRASS PLANTING |
|  | EXISTING OVERHEAD POWER LINE |  | PROPOSED SOD |
|  | EXISTING GAS LINE |  | PROPOSED DECIDUOUS TREE |
|  | EXISTING DECIDUOUS TREE |  | PROPOSED SOIL CELL.
REFER TO L300 SERIES FOR DETAILS |
|  | EXISTING CONIFEROUS TREE |  | REINSTATED TRASH RECEPTACLE |
|  | EXISTING PEDESTRIAN LIGHT POLE,
TO BE RELOCATED BY OTHERS |  | REINSTATED BENCH |
|  | EXISTING STREET LIGHT POLE,
TO BE RELOCATED BY OTHERS |  | PROPOSED EDGE OF ASPHALT/CURB |
|  | EXISTING ELECTRICAL HAND HOLE | | |
|  | EXISTING CATCH BASIN | | |
|  | EXISTING MAINTENANCE HOLE | | |
|  | EXISTING FIRE HYDRANT | | |

CLIENT



PROJECT Co.



CONSULTANT



PROJECT

**GORDIE HOWE
INTERNATIONAL BRIDGE**
SANDWICH STREET CORRIDOR
WINDSOR, ONTARIO

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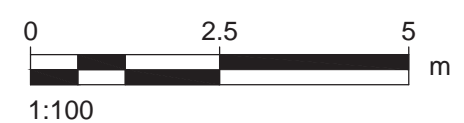
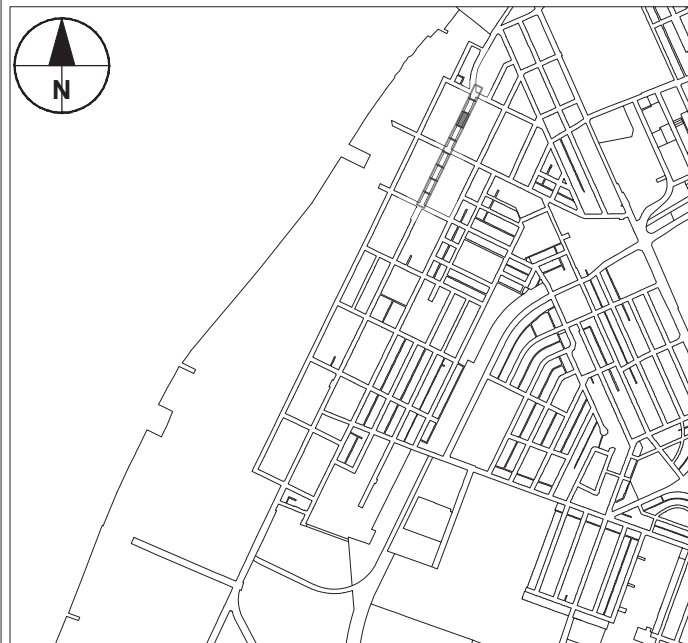
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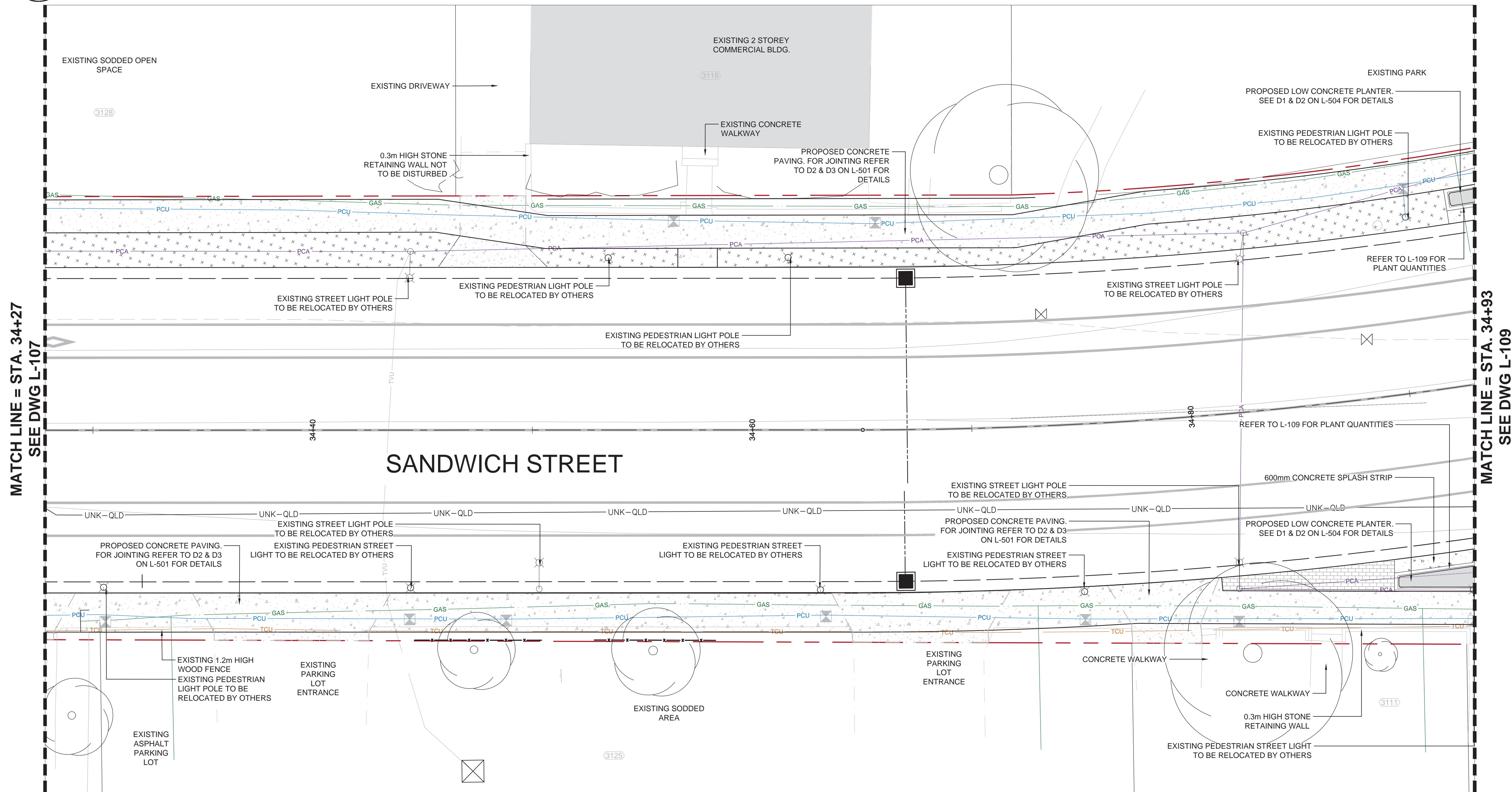
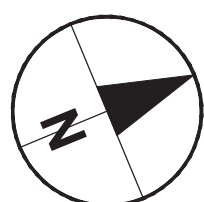
LANDSCAPE

SHEET TITLE


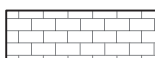





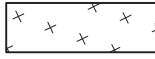




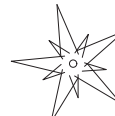






SANDWICH STREET LANDSCAPE PLAN 33+67 TO 34+27

SHEET No.

L-107



LEGEND:

- | | | | |
|---|--|---|--|
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REFER TO D1 ON L-501 FOR DETAILS |
|  | EXISTING PHONE LINE |  | PROPOSED CONCRETE PAVING.
REFER TO D2 & D3 ON L-501 FOR DETAILS |
|  | EXISTING UNDERGROUND POWER LINE |  | PROPOSED PERENNIAL / ORNAMENTAL
GRASS PLANTING |
|  | EXISTING OVERHEAD POWER LINE |  | PROPOSED SOD |
|  | EXISTING GAS LINE |  | PROPOSED TREE PROTECTION FENCING |
|  | EXISTING DECIDUOUS TREE |  | PROPOSED EDGE OF ASPHALT/CURB |
|  | EXISTING CONIFEROUS TREE | | |
|  | EXISTING PEDESTRIAN LIGHT POLE,
TO BE RELOCATED BY OTHERS | | |
|  | EXISTING STREET LIGHT POLE,
TO BE RELOCATED BY OTHERS | | |
|  | EXISTING ELECTRICAL HAND HOLE | | |
|  | EXISTING CATCH BASIN | | |
|  | EXISTING MAINTENANCE HOLE | | |
|  | EXISTING FIRE HYDRANT | | |

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PROJECT Co



CONSULTANT



PROJECT

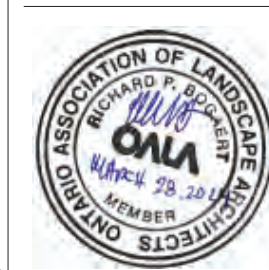
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INTERNATIONAL BRIDGE
SANDWICH STREET CORRIDOR
WINDSOR, ONTARIO

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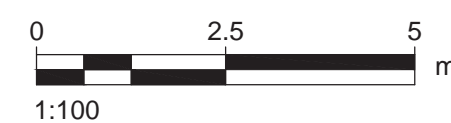
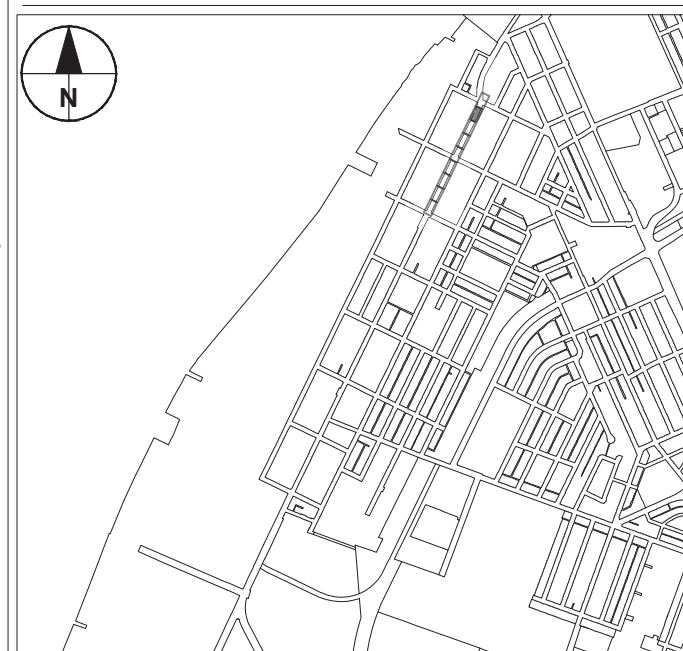
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DESIGN UNIT

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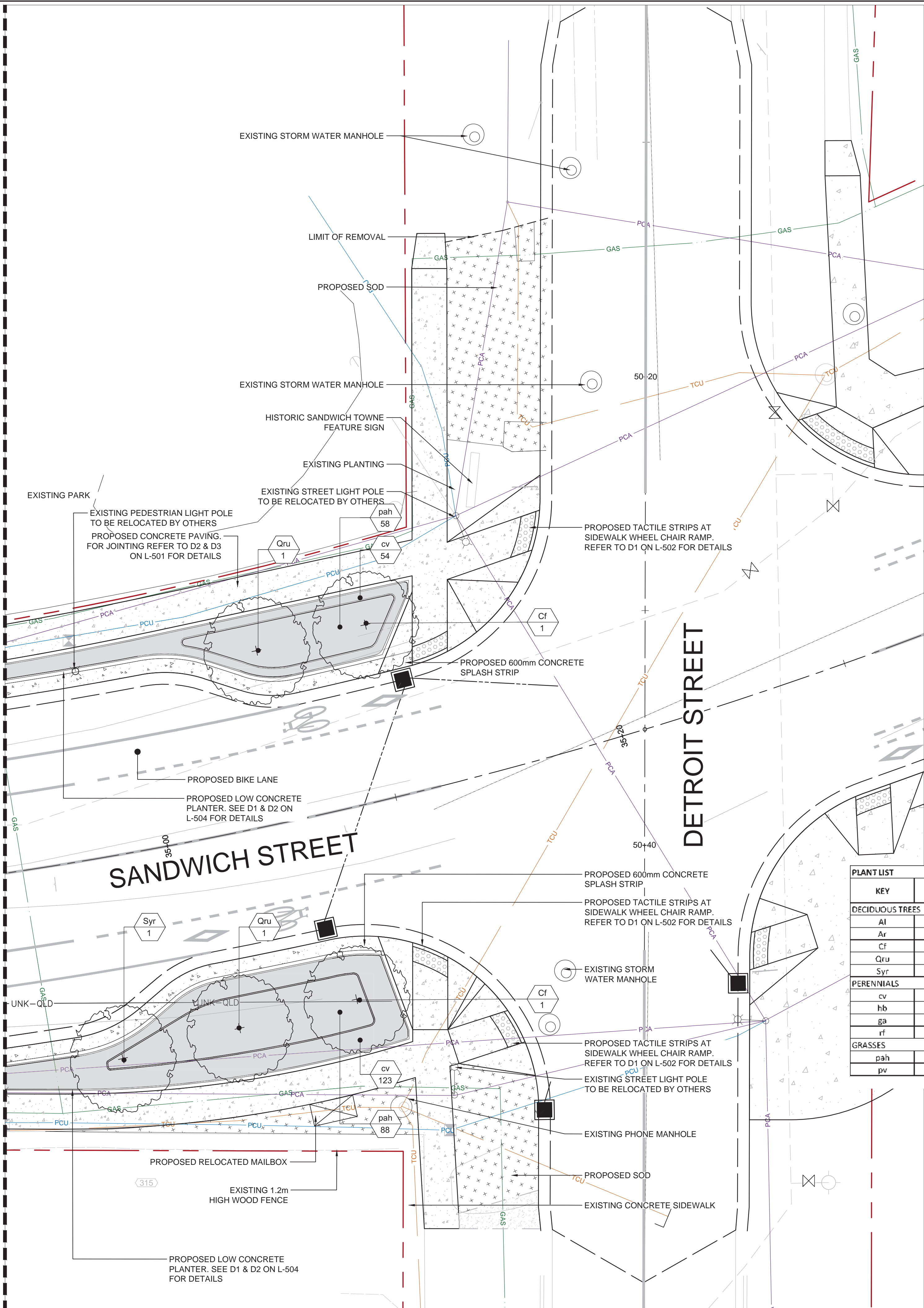
SHEET TITLE

SANDWICH STREET LANDSCAPE PLAN 34+27 TO 34+93

SHEET No.

L-108

MATCH LINE = STA. 34+93
SEE DWG L-108



LEGEND:

- PROPERTY LINE
- EXISTING PHONE LINE
- EXISTING UNDERGROUND POWER LINE
- EXISTING OVERHEAD POWER LINE
- EXISTING GAS LINE
- EXISTING DECIDUOUS TREE
- EXISTING CONIFEROUS TREE
- EXISTING PEDESTRIAN LIGHT POLE, TO BE RELOCATED BY OTHERS
- EXISTING STREET LIGHT POLE, TO BE RELOCATED BY OTHERS
- EXISTING ELECTRICAL HAND HOLE
- EXISTING CATCH BASIN
- EXISTING MAINTENANCE HOLE
- EXISTING FIRE HYDRANT
- PROPOSED CONCRETE PAVING. REFER TO D2 & D3 ON L-501 FOR DETAILS
- PROPOSED PERENNIAL / ORNAMENTAL GRASS PLANTING
- PROPOSED SOD
- PROPOSED DECIDUOUS TREE
- PROPOSED SOD
- PROPOSED RELOCATED MAILBOX
- PROPOSED EDGE OF ASPHALT/CURB

PLANT LIST							
KEY	QTY.	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION	SPACING	CANOPY HT. AT MATURITY
DECIDUOUS TREES							
Al	2	Amelanchier laevis	Allegheny Serviceberry	30mm cal	WB	As shown on drawings.	5.0m
Ar	5	Acer rubrum	Red Maple	70mm cal	WB	As shown on drawings.	15.0m
Cf	5	Cornus florida	Flowering Dogwood	70mm cal	WB	As shown on drawings.	8.0m
Qru	7	Quercus rubra	Red oak	70mm cal	WB	As shown on drawings.	22.0m
Syr	4	Syringa reticulata	Japanese Tree Lilac	60mm cal	WB	As shown on drawings.	10.0m
PERENNIALS							
cv	298	Coreopsis verticillata 'Moon Beam'	Threadleaf tickseed	1 gal.	Potted	50cm O.C.	60cm
hb	15	Hemerocallis 'Bonanza'	Bonanza Daylily	1 gal.	Potted	50cm O.C.	80cm
ga	18	Gaillardia aristata	Blanket Flower	1 gal.	Potted	50cm O.C.	60cm
rf	88	Rudbeckia fulgida 'Goldsturm'	Black-Eyed Susan	1 gal.	Potted	50cm O.C.	60cm
GRASSES							
pah	192	Pennisetum alopecuroides 'Hameln'	Dwarf Fountain Grass	1 gal.	Potted	50cm O.C.	90cm
pv	31	Panicum virgatum 'Heavy Metal'	Heavy Metal Switch Grass	1 gal.	Potted	80cm O.C.	1.20m

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SANDWICH STREET CORRIDOR
WINDSOR, ONTARIO

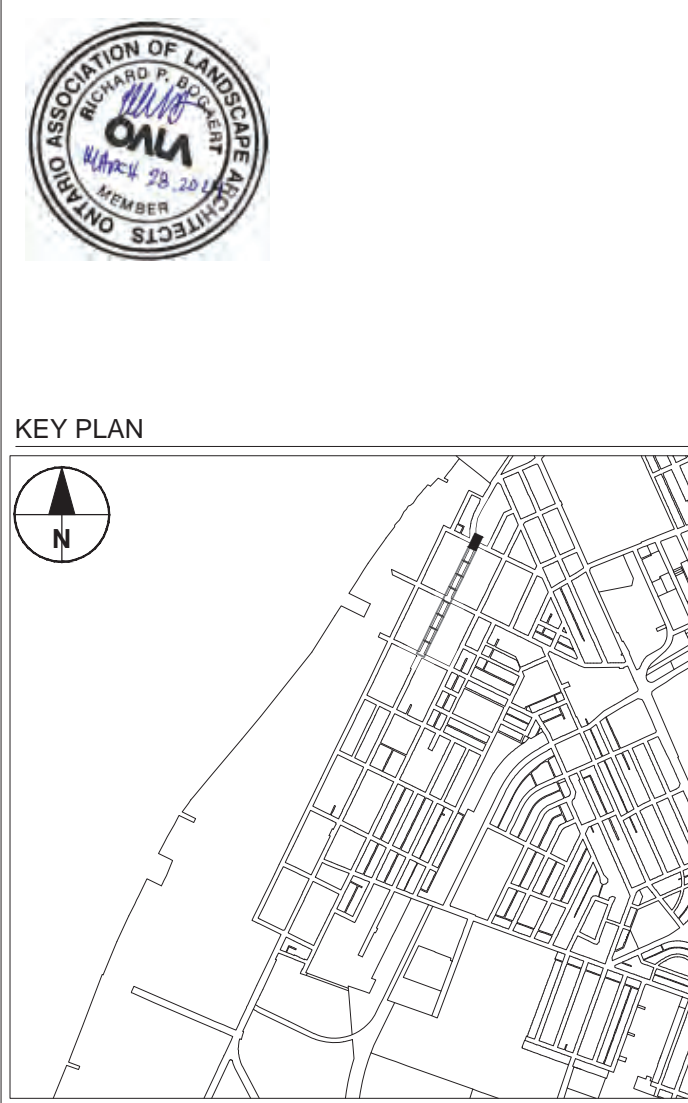
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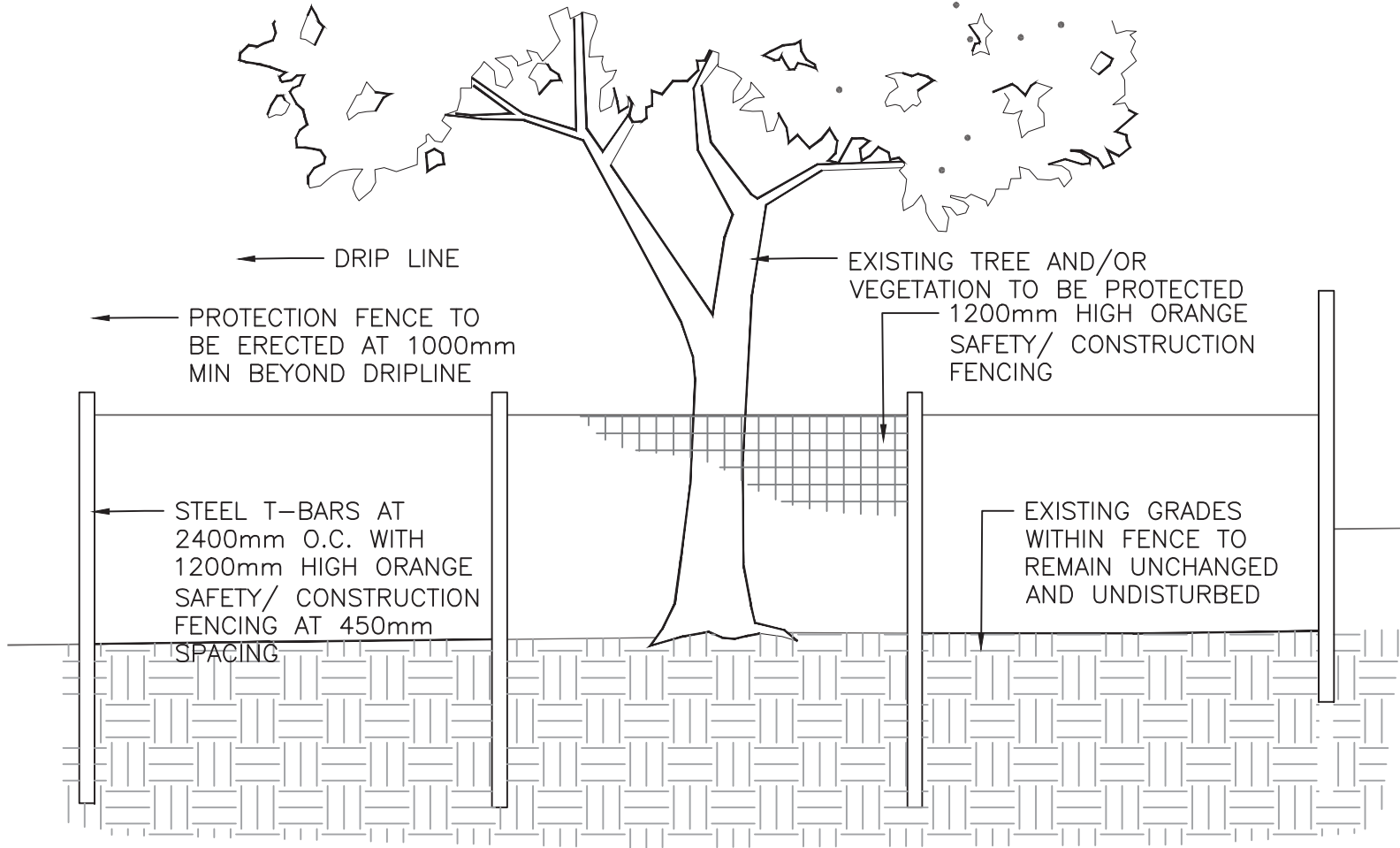
LANDSCAPE

SHEET TITLE

SANDWICH STREET LANDSCAPE PLAN
34+93 TO 35+20

SHEET No.

L-109

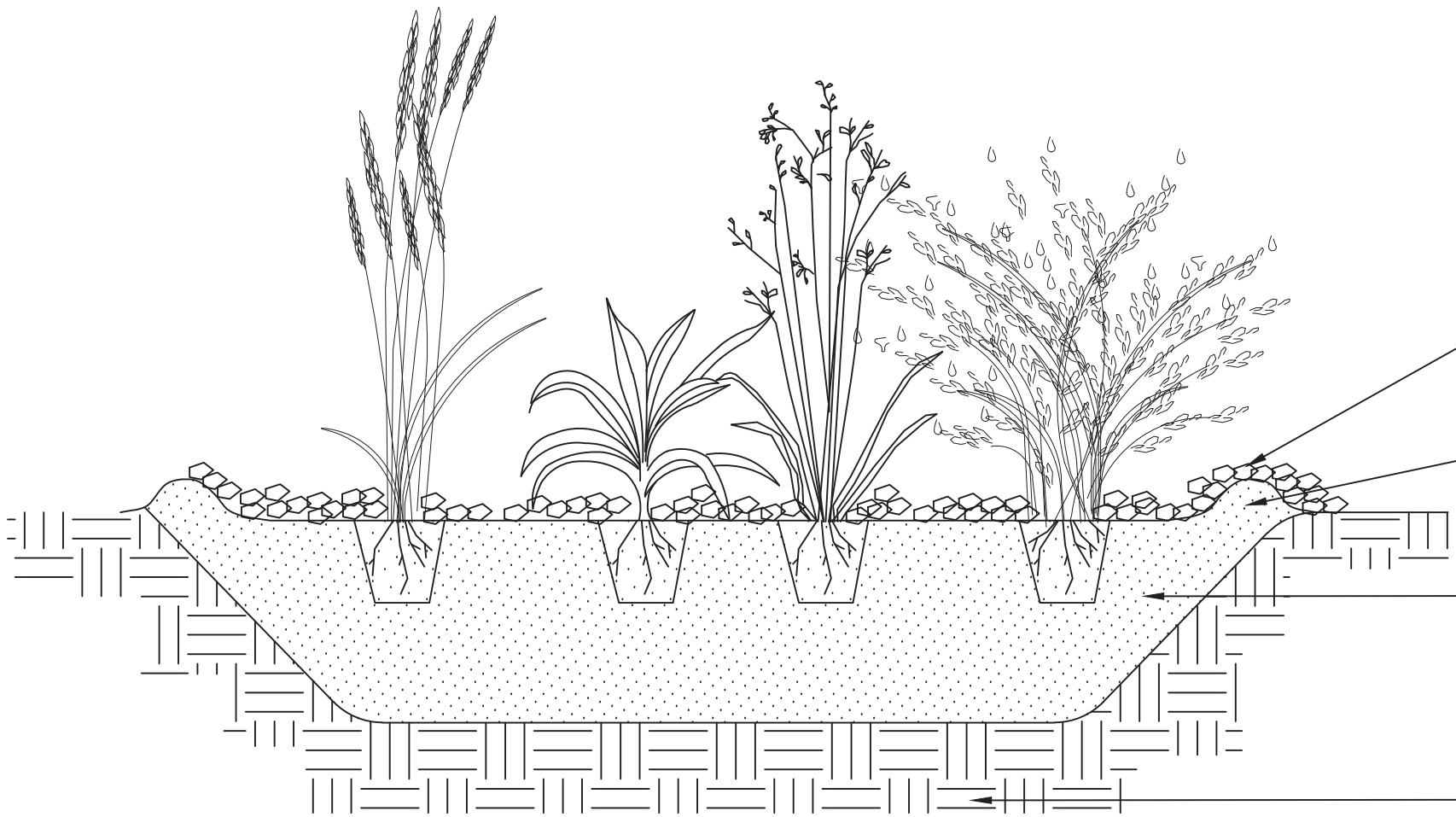


- NOTES:
1. ATTACHMENT OF FENCE TO TREES WILL NOT BE PERMITTED.
 2. ANY EXPOSED ROOTS ARE TO BE HAND PRUNED USING PROPER ARBORICULTURAL PRACTICES.
 3. UNDER NO CIRCUMSTANCES SHALL ANY CONSTRUCTION MATERIALS, EQUIPMENT OR VEHICLES BE PLACED WITHIN THE TREE PROTECTION ZONE.
 4. ALL TREE PROTECTION TO BE ERECTED PRIOR TO ANY CONSTRUCTION ACTIVITY AND IS TO REMAIN IN PLACE UNTIL ALL CONSTRUCTION HAS BEEN COMPLETED. OBTAIN WRITTEN APPROVAL FROM CONTRACT ADMINISTRATOR PRIOR TO REMOVAL OF FENCING.

D1 TREE PROTECTION FENCING
L-500 SCALE N.T.S.

GENERAL NOTES:

1. THE CANADIAN STANDARDS FOR NURSERY STOCK, 8TH EDITION (CANADIAN NURSERY LANDSCAPE ASSOCIATION, 2006) SHALL BE USED FOR ALL SPECIFICATIONS NOMENCLATURE, INCLUDING MINIMUM STANDARDS FOR PLANTING MATERIAL, AND DESCRIPTIVE TERMS USED IN THE PLANTING PLANS.
2. ALL EXPOSED SUB-GRADE SOIL AND FILL MATERIAL TO BE COVERED WITH TOPSOIL TO FINAL ELEVATIONS, PRIOR TO RECEIVING SEED OR PLANTING TREATMENT, REFER TO SURFACE GRADING DRAWINGS FOR FINISH GRADES. REFER TO LANDSCAPE DETAILS FOR TOPSOIL/SUB-SOIL DEPTH.
3. EXCAVATION AND PREPARATION OF PLANTING BEDS AND INDIVIDUAL TREE PLANTING PITS; SEEDING; AND MULCHING SHALL BE UNDERTAKEN IN ACCORDANCE WITH THE PLANTING DETAILS ON THIS SHEET. PLANTING PITS FOR TREES SHALL NOT BE DUG WITH AN AUGER OF ANY TYPE.



- NOTES:
1. PERENNIALS AND ORNAMENTAL GRASSES TO BE PLANTED AT FINISHED GRADE.
 2. REMOVE ALL NURSERY TAGS AFTER SITE INSPECTION BY LANDSCAPE ARCHITECTS.
 3. PRUNE ONLY DEAD OR INJURED BRANCHES, RETAIN NATURAL SHAPE, WATER THOROUGHLY AFTER PLANTING.

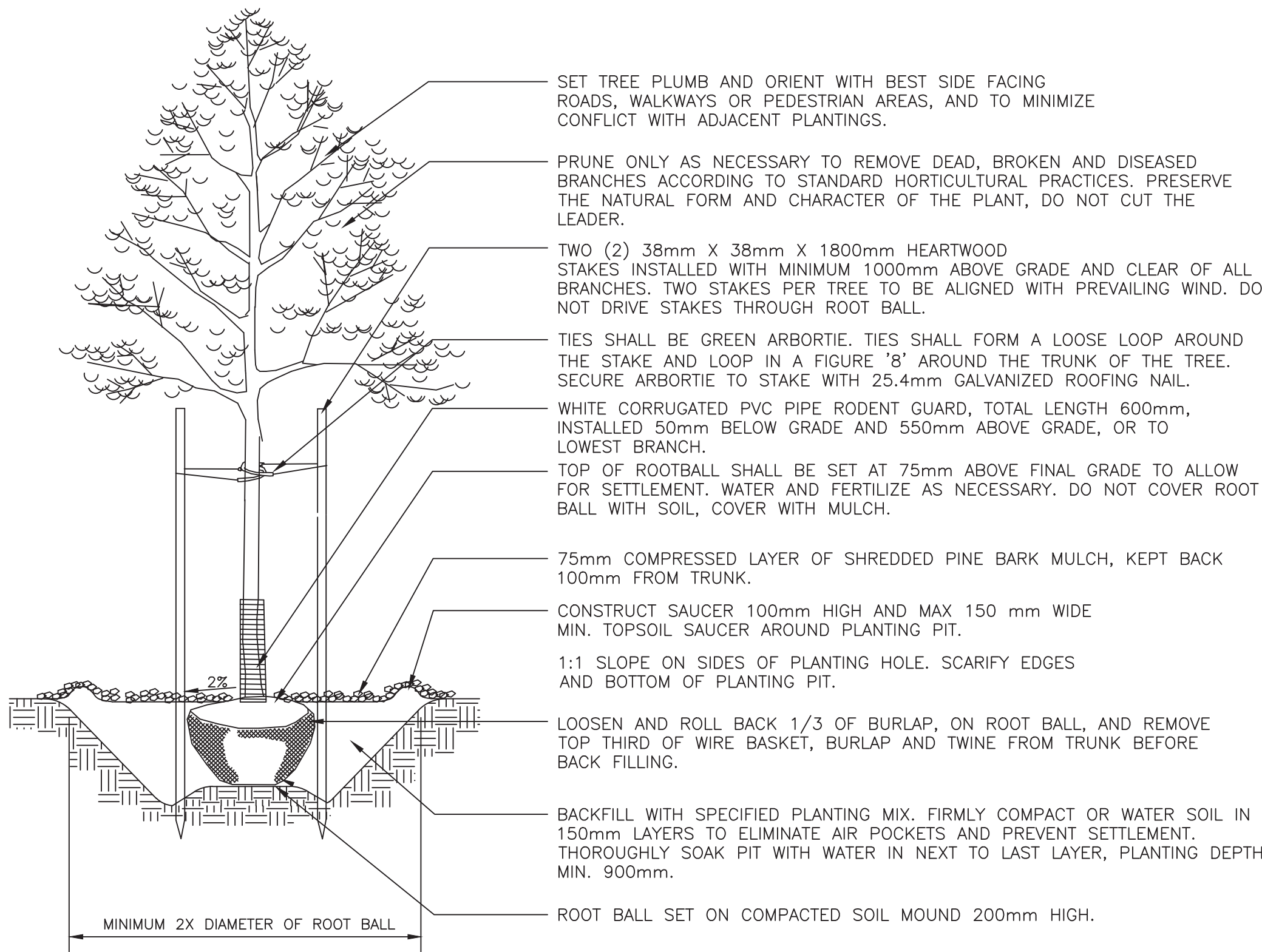
75mm DEPTH COMPRESSED LAYER OF SHREDDED PINE BARK MULCH, CONTINUOUS THROUGH PLANTING BED.

100mm HIGH SAUCER AT PLANTING BED EDGES.

THOROUGHLY SOAK PIT WITH WATER IN NEXT TO LAST 150mm LAYER OF COMPACTED TOPSOIL. REMOVE ALL FOREIGN MATERIAL FROM EXISTING SOIL. BACKFILL WITH SPECIFIED PLANTING MIX. PIT DEPTH, MINIMUM 450mm.

UNDISTURBED SOIL

D3 PERENNIAL AND ORNAMENTAL GRASS PLANTING DETAIL
L-500 SCALE 1:20



NOTE: CONTRACTOR SHALL REMOVE TREE STAKES AND TIES ONE YEAR AFTER PLANTING, OR AT THE END OF THE WARRANTY PERIOD IF STAKES ARE STILL REQUIRED.

WATER THOROUGHLY AFTER PLANTING. DO NOT CUT OR DAMAGE LEADER. PRUNE ONLY INJURED OR DEAD BRANCHES. RETAIN NATURAL SHAPE.

CONTRACTOR SHALL CHECK RODENT GUARD IN COMPARISON TO TREE DIAMETER. IF DIAMETER IS GREATER THAN 50mm, CHANGE RODENT GUARD TO 100mm DIAMETER.

D2 DECIDUOUS TREE PLANTING DETAIL
L-500 SCALE 1:20

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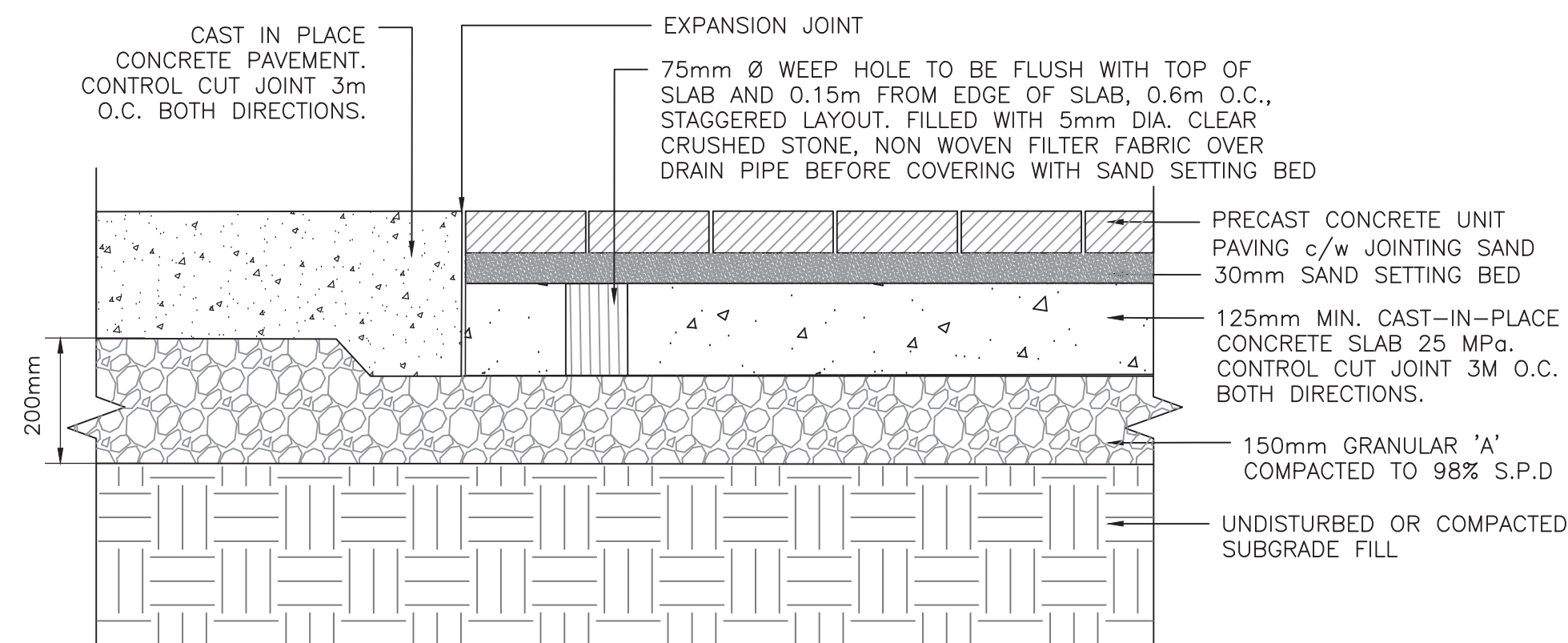
DESIGN UNIT
CIVIL

SHEET TITLE
LANDSCAPE DETAILS -
PLANTING
SANDWICH STREET

SHEET No.

L-500

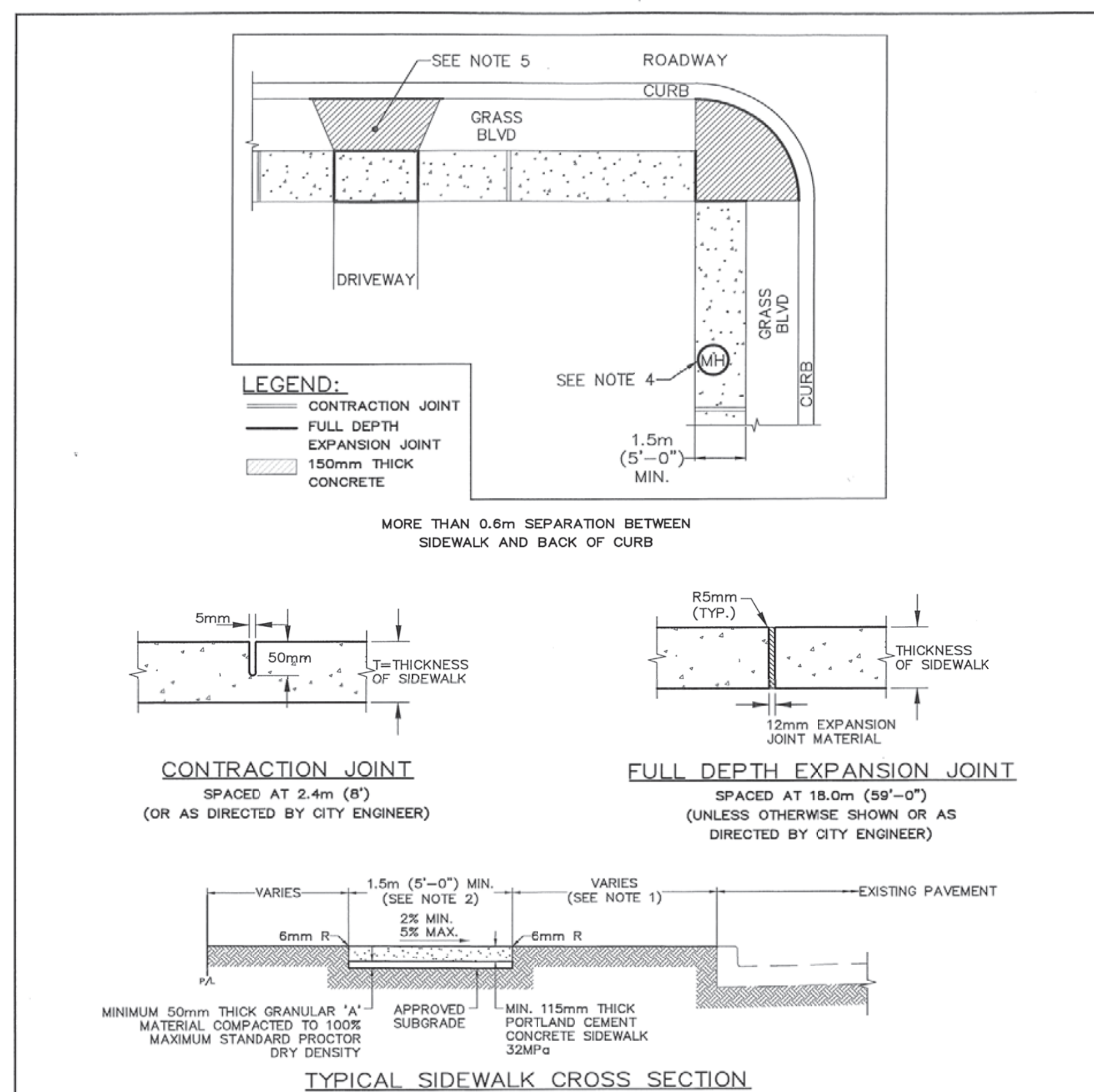
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PAVERS:


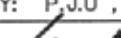
- BRUSSELS BLOCK BY UNILOCK OR APPROVED EQUIVALENT
- COLOUR: SANDSTONE
- BLOCK SIZES:
 - HALF: 104mm x 173mm x 70mm
 - XL: 208mm x 348mm x 70mm
 - STANDARD: 173mm x 208mm X 70mm

D1 PRECAST CONCRETE UNIT PAVER ON CONCRETE SLAB
L-501 SCALE 1:10

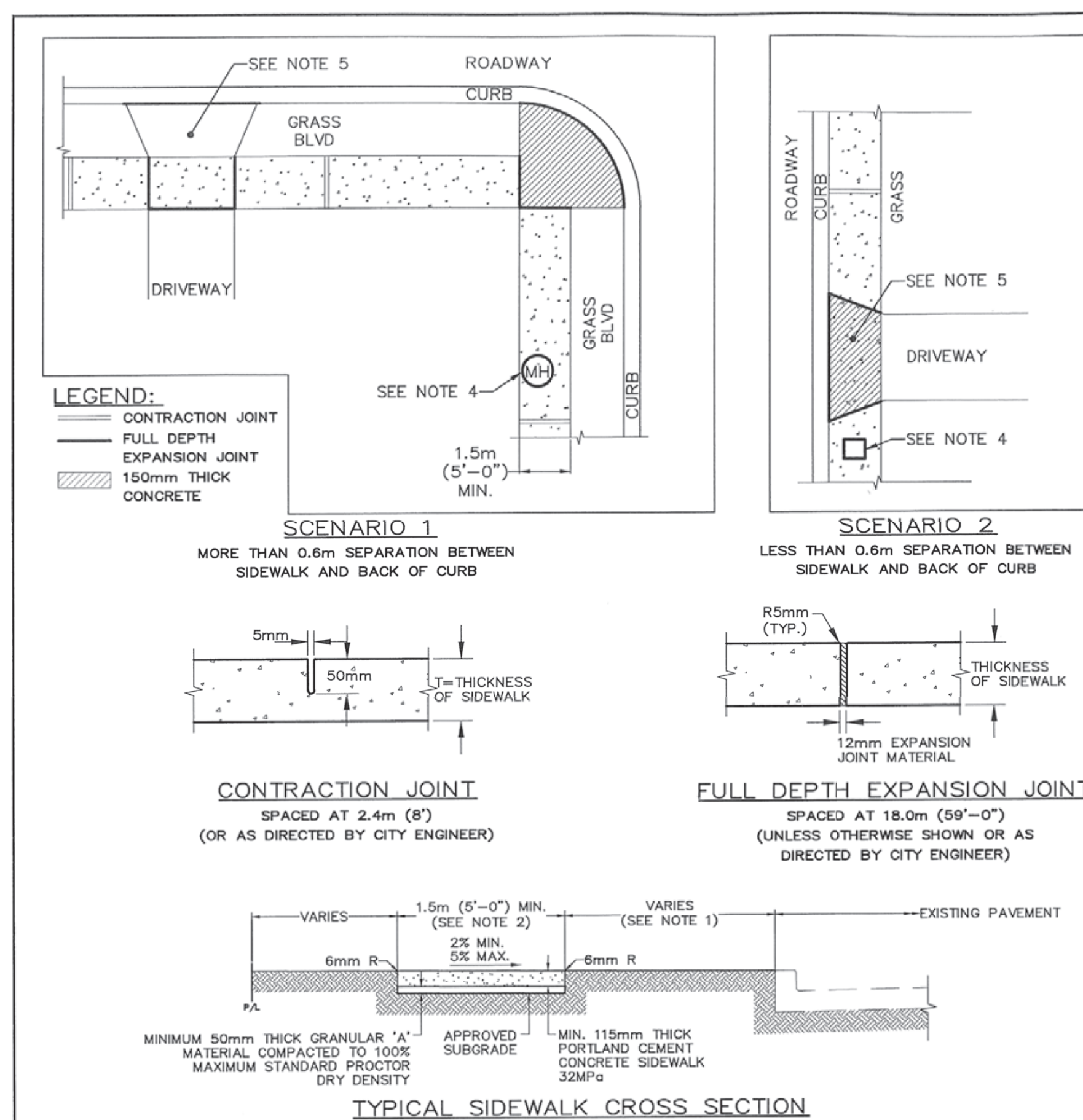


GENERAL NOTES:

1. Unless sidewalk abuts the curb, the distance between sidewalk and back of curb must be 500mm minimum.
2. Any sidewalk with exceeding 3m must have a centered longitudinal contraction joint. Maximum spacing between longitudinal contraction joints to be 1.5m.
3. Expansion joints are required at all fixed objects (poles, hydrants, manholes, structures, etc.)
4. All manholes, boxes, valves, etc. to match finished grade of sidewalk.
5. To be read in conjunction with AS-222, AS-402 and AS-404.
6. To be read in conjunction with AS-405 and AS-406.



		<h1 style="text-align: center;">CITY OF WINDSOR</h1> <p style="text-align: center;">ENGINEERING DEPARTMENT</p>	
<h2>RESIDENTIAL CONCRETE SIDEWALK</h2>			
DR'N BY: B.C., A.C		DATE: APRIL, 2004	
REVISION: APR 2015/DEC 2021		CH'KD BY: W.B., JH	
CH'KD BY: P.J.U., M.C.		PASSED BY: T.M.	
 CITY ENGINEER		AS-401	

D2 CITY OF WINDSOR – RESIDENTIAL CONCRETE SIDEWALK
L-501 SCALE N.T.S.



GENERAL NOTES:

1. Unless sidewalk abuts the curb, the distance between sidewalk and back of curb must be 800mm minimum.
2. Any sidewalk with exceeding 3m must have a centered longitudinal expansion joint. Maximum spacing between longitudinal contraction joints to be 1.5m.
3. Expansion joints are required at all fixed objects (poles, hydrants, manholes, structures, etc.) and at sidewalk direction changes.
4. Manholes, boxes, valves, etc. to match finished grade of sidewalk.
5. To be read in conjunction with AS-204, AS-402, AS-404 and S-6.
6. All work within public right of way.
7. Work shall be done to the satisfaction of the City Engineer.

 <h1 style="text-align: center;">CITY OF WINDSOR</h1> <p style="text-align: center;">ENGINEERING DEPARTMENT</p>	
<h2>COMMERCIAL CONCRETE SIDEWALK</h2>	
DR'N BY: B.C., N.B., A.C	DATE: APRIL, 2004
REVISION: MAR 2017/DEC 2021	CH'KD BY: W.B., A.C
CH'KD BY: P.J.U., M.C.	PASSED BY: T.M.
 _____ CITY ENGINEER	AS-403

D3 CITY OF WINDSOR - COMMERCIAL CONCRETE SIDEWALK
-501 SCALE N.T.S.



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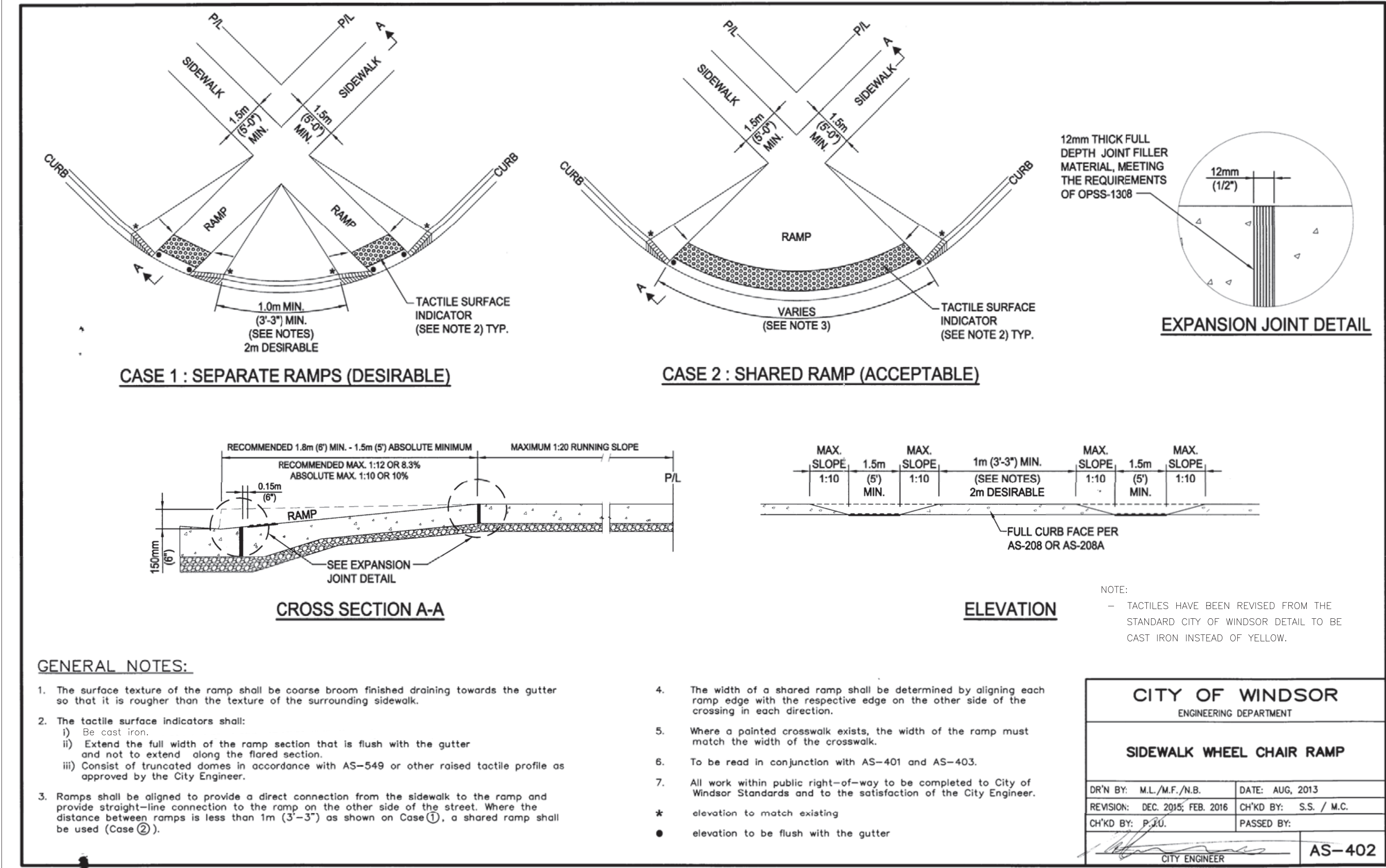
CIVIL

SHEET TITLE

LANDSCAPE DETAILS - HARDSCAPE SANDWICH STREET

SHEET No.

L-501



D1 CITY OF WINDSOR – SIDEWALK WHEEL CHAIR RAMP
L-502 SCALE N.T.S.

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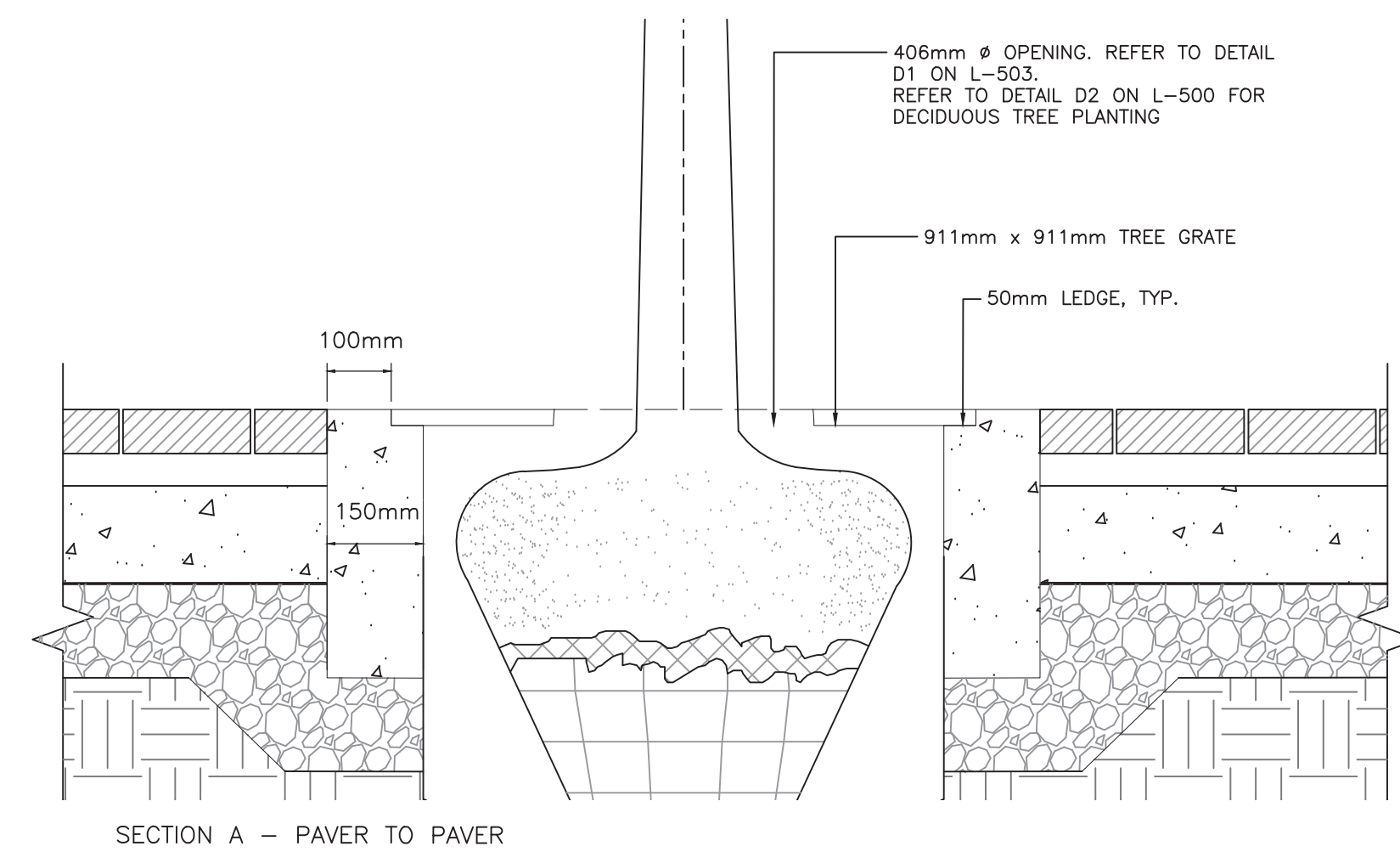
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CIVIL	

SHEET TITLE	
LANDSCAPE DETAILS - HARDSCAPE SANDWICH STREET	

SHEET No.	
L-502	



8mm 25mm EDGE



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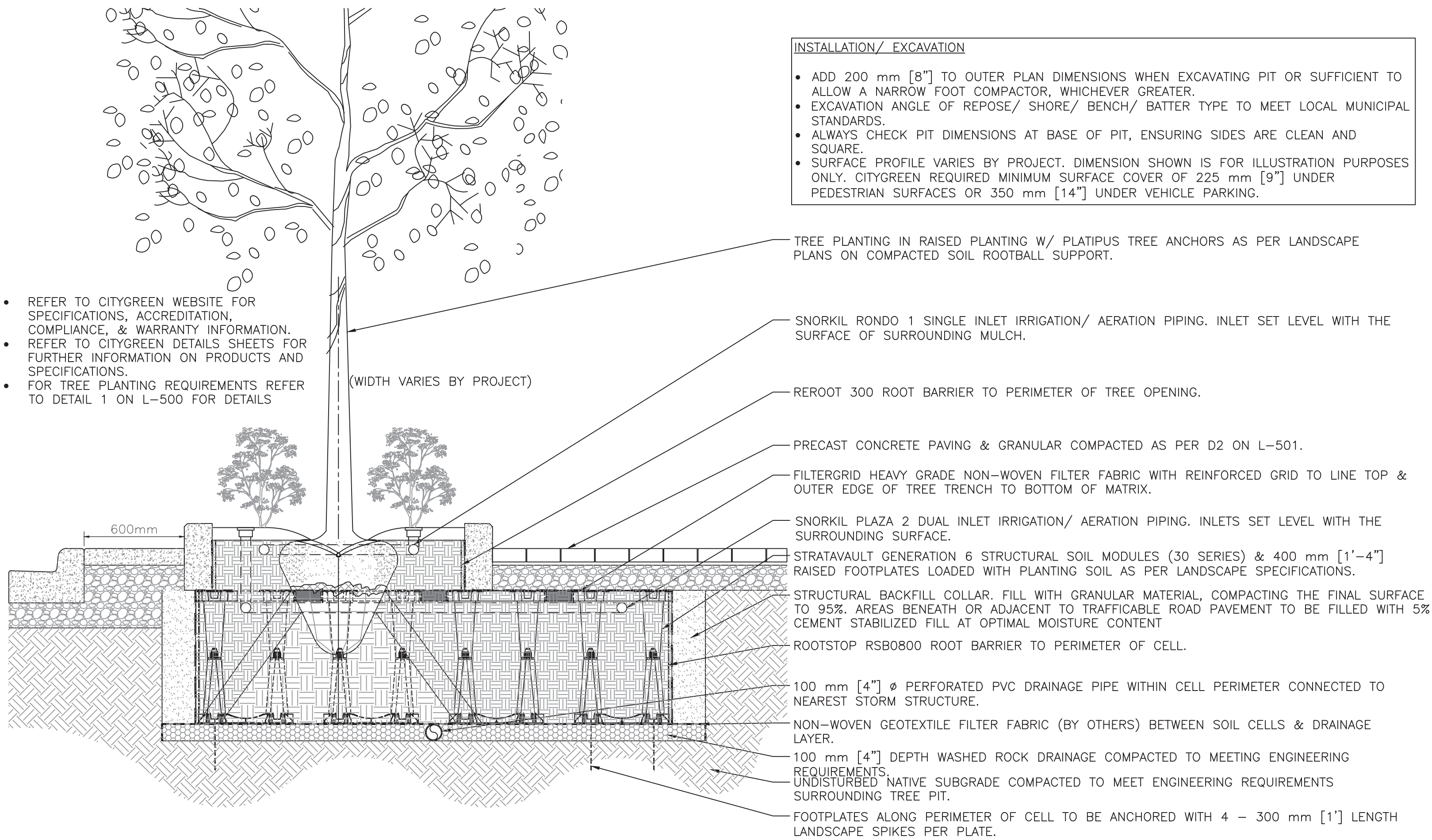
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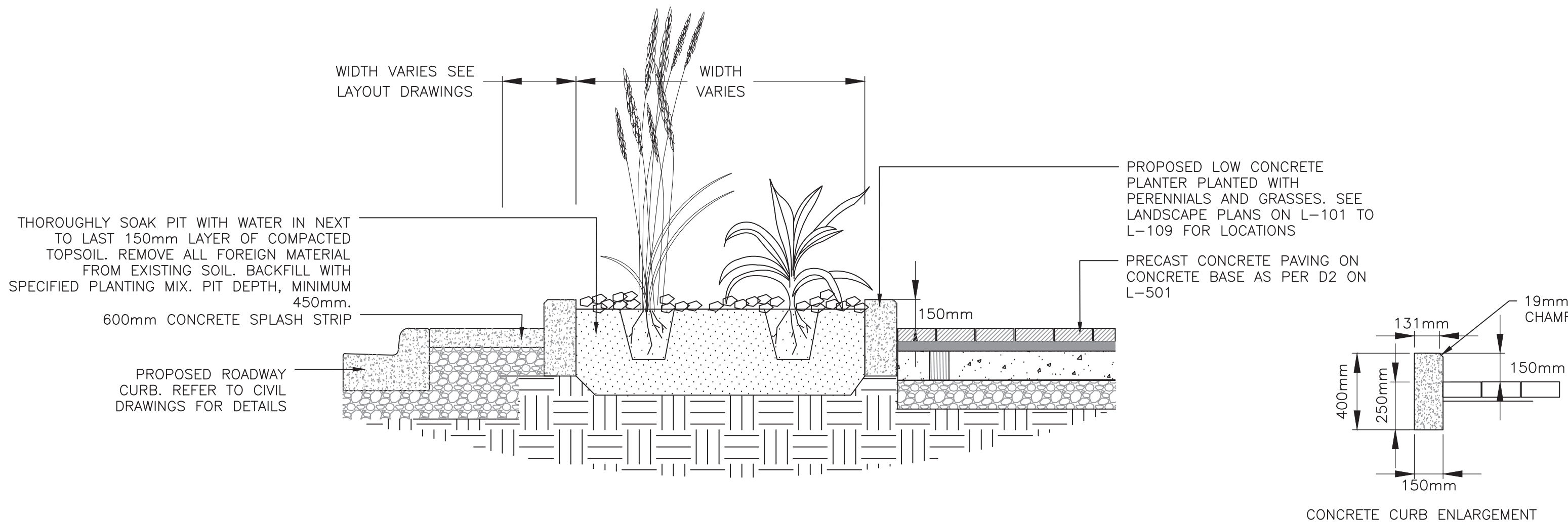
LANDSCAPE DETAILS - TREE GRATES SANDWICH STREET

SHEET No.

L-503



D1 LOW CURB CONCRETE PLANTER WITH TREE PLANTING / SOIL CELLS
L-504 SCALE 1:20



D2 LOW CURB CONCRETE PLANTER WITH PERENNIAL AND ORNAMENTAL GRASS PLANTING
L-504 SCALE 1:20

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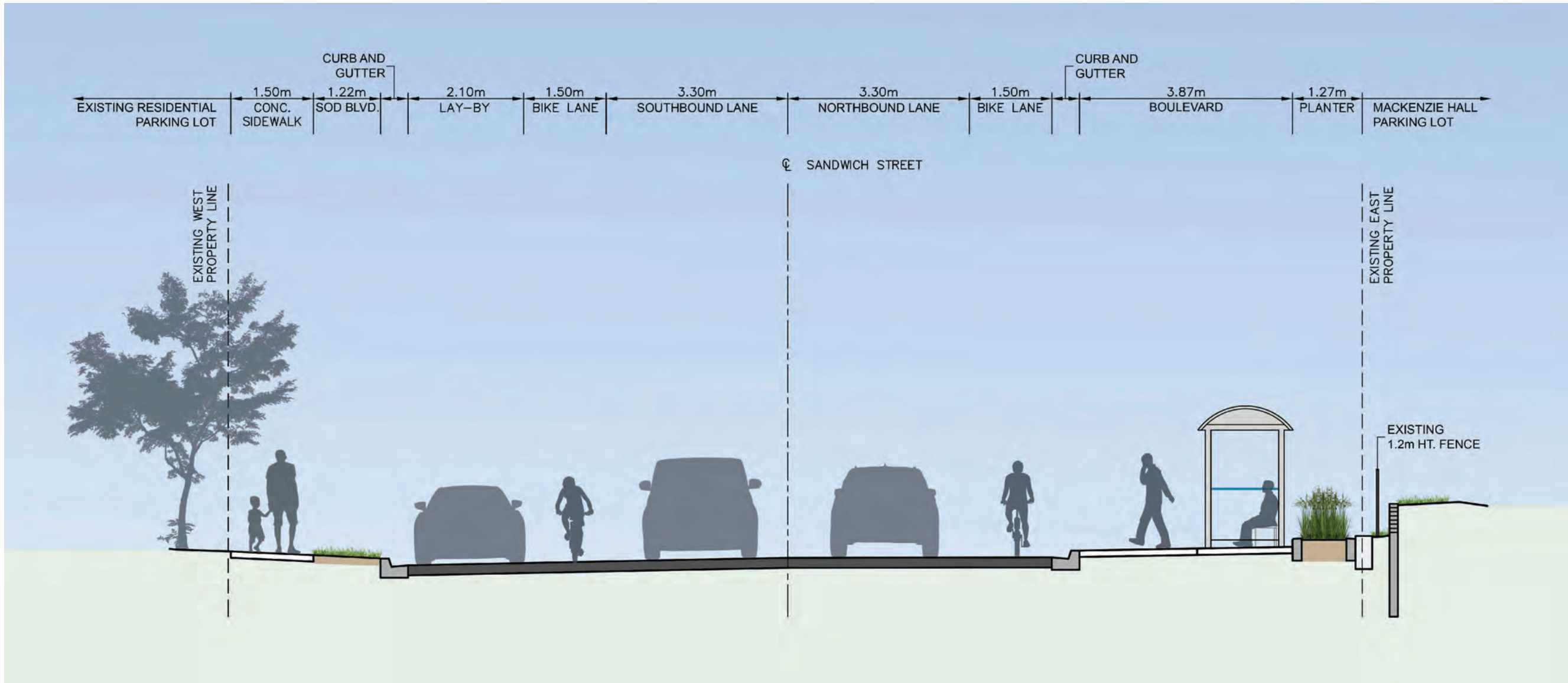
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CIVIL

SHEET TITLE
LANDSCAPE DETAILS - PLANTERS SANDWICH STREET

SHEET No.
L-504



D1 CROSS SECTION: STA 30+05, NORTHBOUND VIEW (SANDWICH STREET AND BROCK STREET)
L-505 N.T.S.



D2 RENDERING: NORTHBOUND VIEW (SANDWICH STREET AND BROCK STREET)
L-505 N.T.S.

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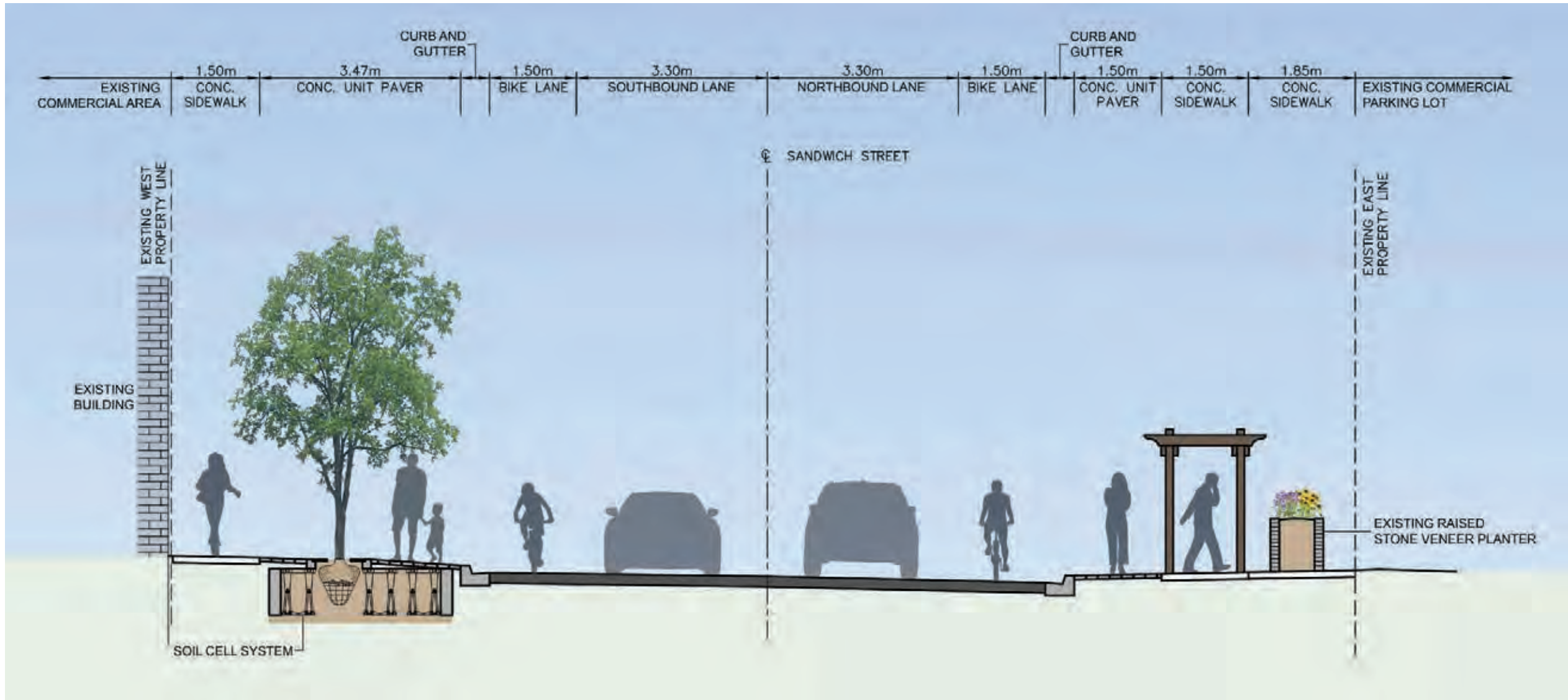
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SHEET TITLE
LANDSCAPE RENDERINGS - SANDWICH STREET & BROCK STREET

SHEET No.
L-505



D1 CROSS SECTION: STA 32+15, NORTHBOUND VIEW (SANDWICH STREET AND MILL STREET)
L-506 N.T.S.



D2 RENDERING: NORTHBOUND VIEW (SANDWICH STREET AND MILL STREET)
L-506 N.T.S.

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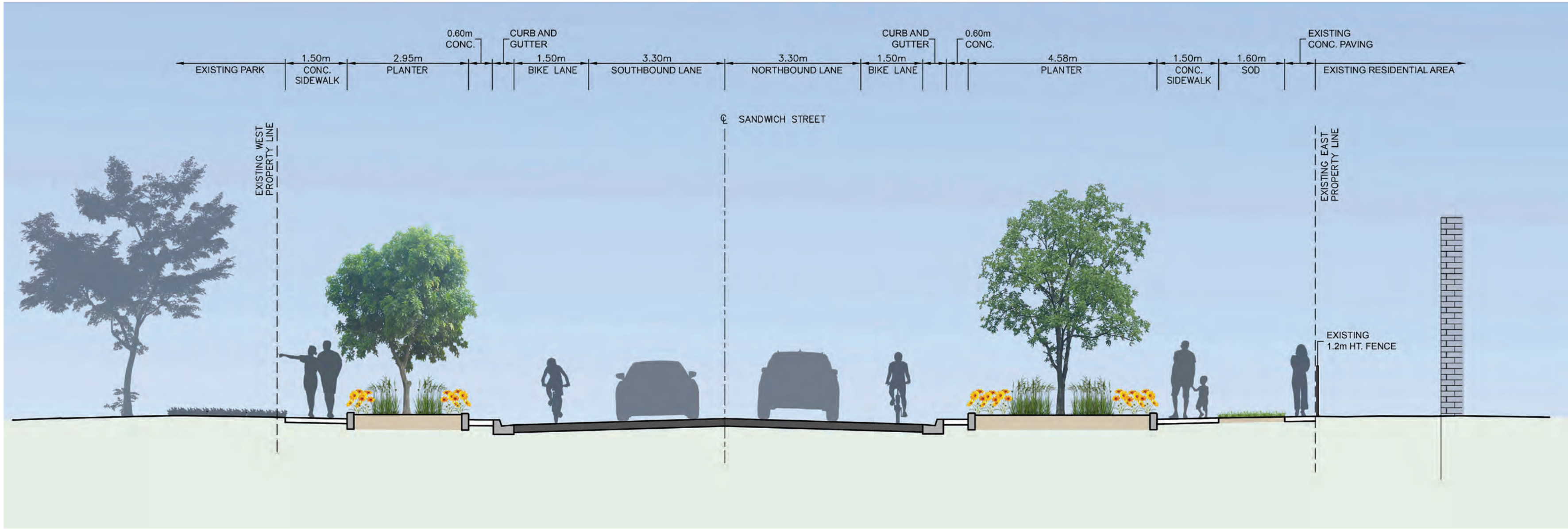
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LANDSCAPE RENDERINGS - SANDWICH STREET & MILL STREET	

SHEET No.	
L-506	

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D1 CROSS SECTION: STA 35+05, NORTHBOUND VIEW (SANDWICH STREET AND DETROIT STREET)
L-507 N.T.S.



D2 RENDERING: SOUTHBOUND VIEW (SANDWICH STREET AND DETROIT STREET)
L-507 N.T.S.

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SHEET TITLE
LANDSCAPE RENDERINGS - SANDWICH STREET & DETROIT STREET

SHEET No.
L-507



D1
L-508
RENDERING: MACKENZIE HALL & PARKETTE
N.T.S.

CR231/2024 - Item 8.22 - Appendix D

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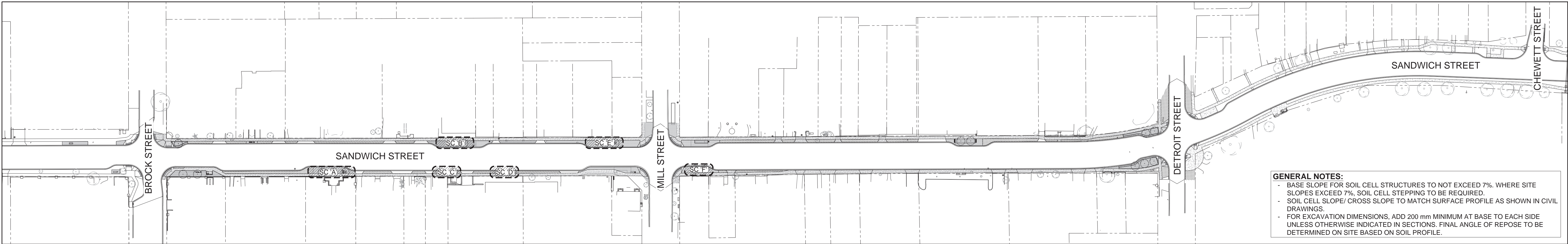
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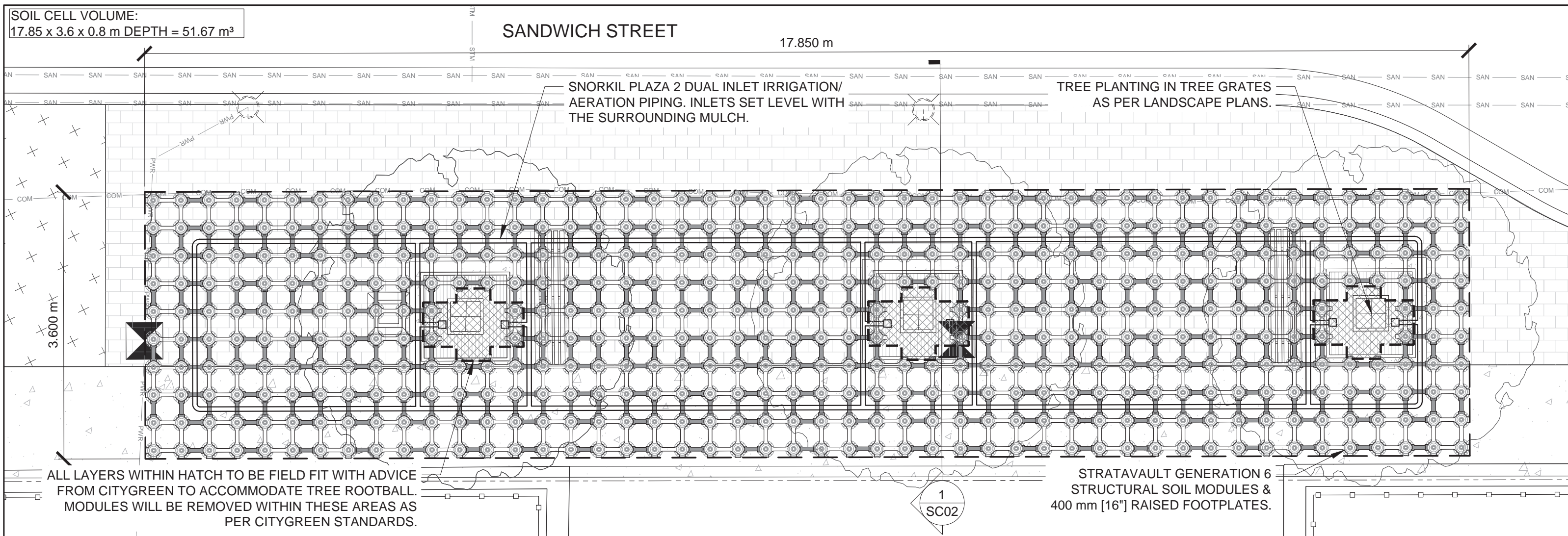
SHEET TITLE	
LANDSCAPE RENDERINGS - MACKENZIE HALL & PARKETTE	

SHEET No.	
L-508	

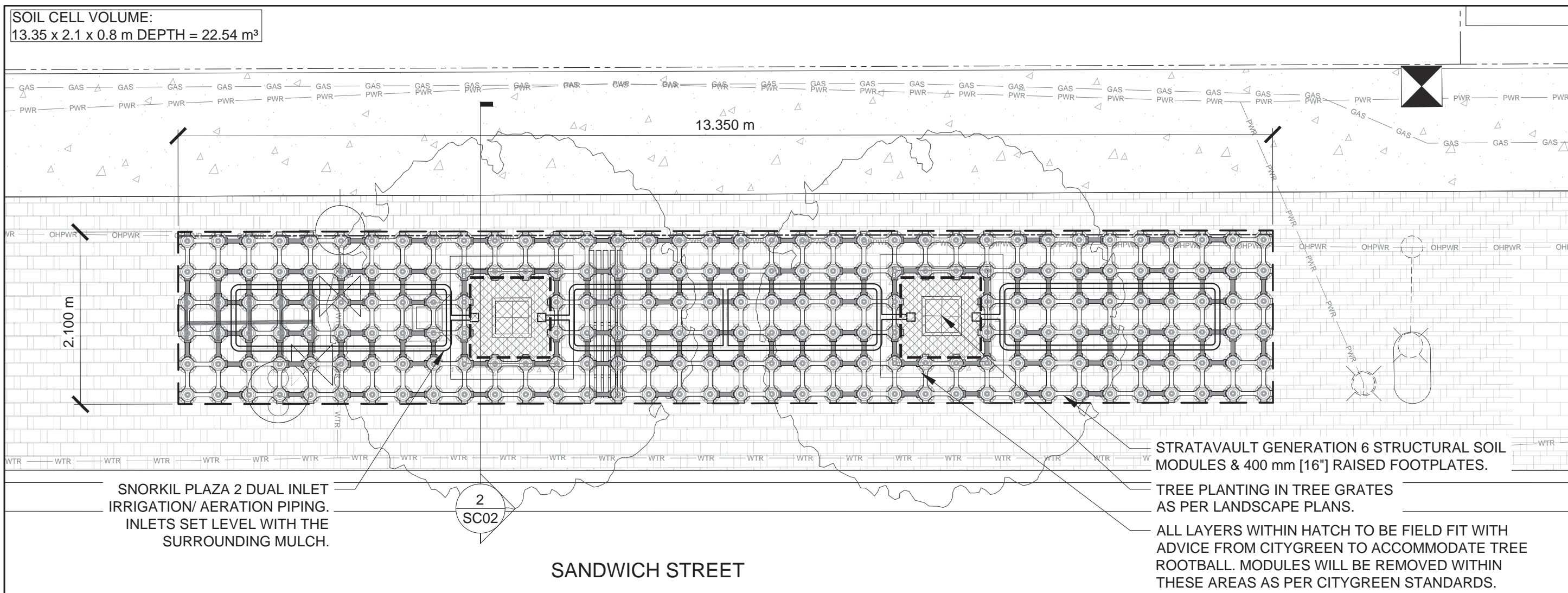


- GENERAL NOTES:**
- BASE SLOPE FOR SOIL CELL STRUCTURES TO NOT EXCEED 7%. WHERE SITE SLOPES EXCEED 7%, SOIL CELL STEPPING TO BE REQUIRED.
 - SOIL CELL SLOPE/ CROSS SLOPE TO MATCH SURFACE PROFILE AS SHOWN IN CIVIL DRAWINGS.
 - FOR EXCAVATION DIMENSIONS, ADD 200 mm MINIMUM AT BASE TO EACH SIDE UNLESS OTHERWISE INDICATED IN SECTIONS. FINAL ANGLE OF REPOSE TO BE DETERMINED ON SITE BASED ON SOIL PROFILE.

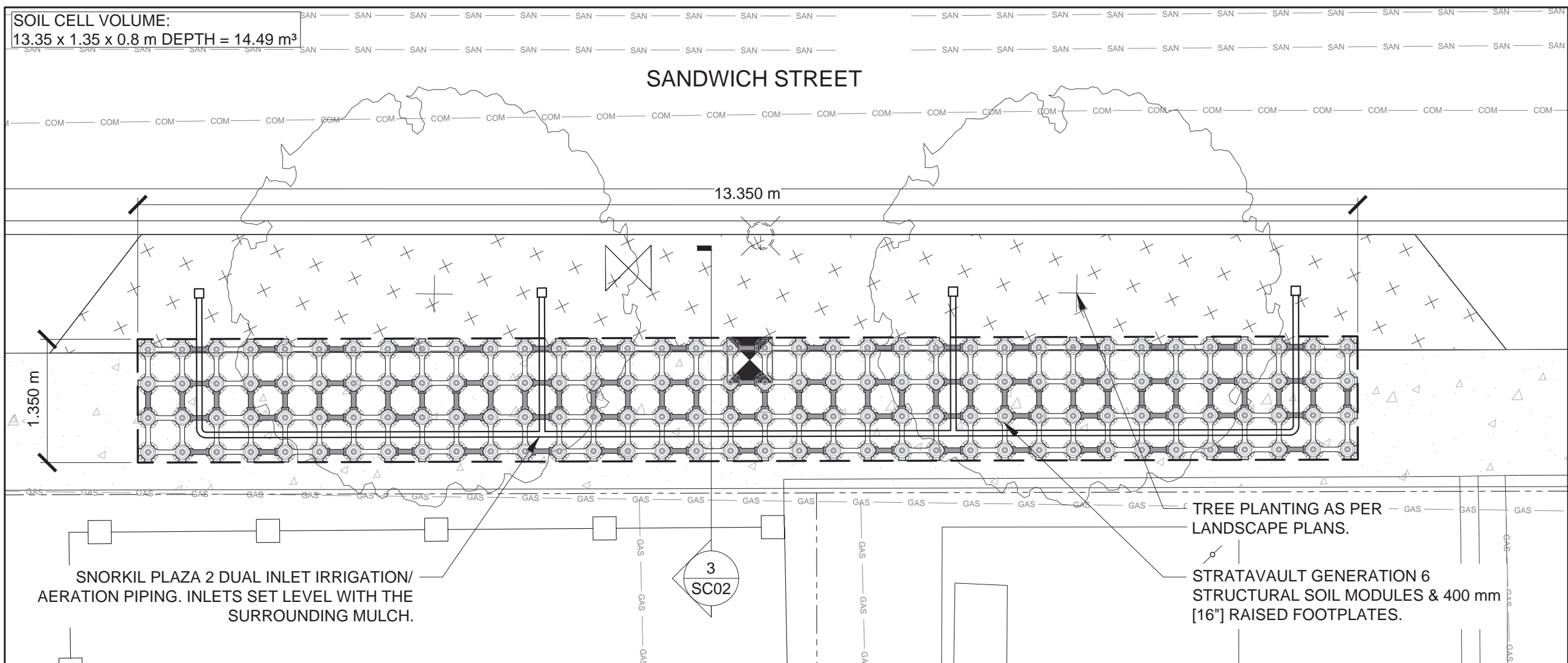
1 SITE KEYPLAN
Scale: 1:2000



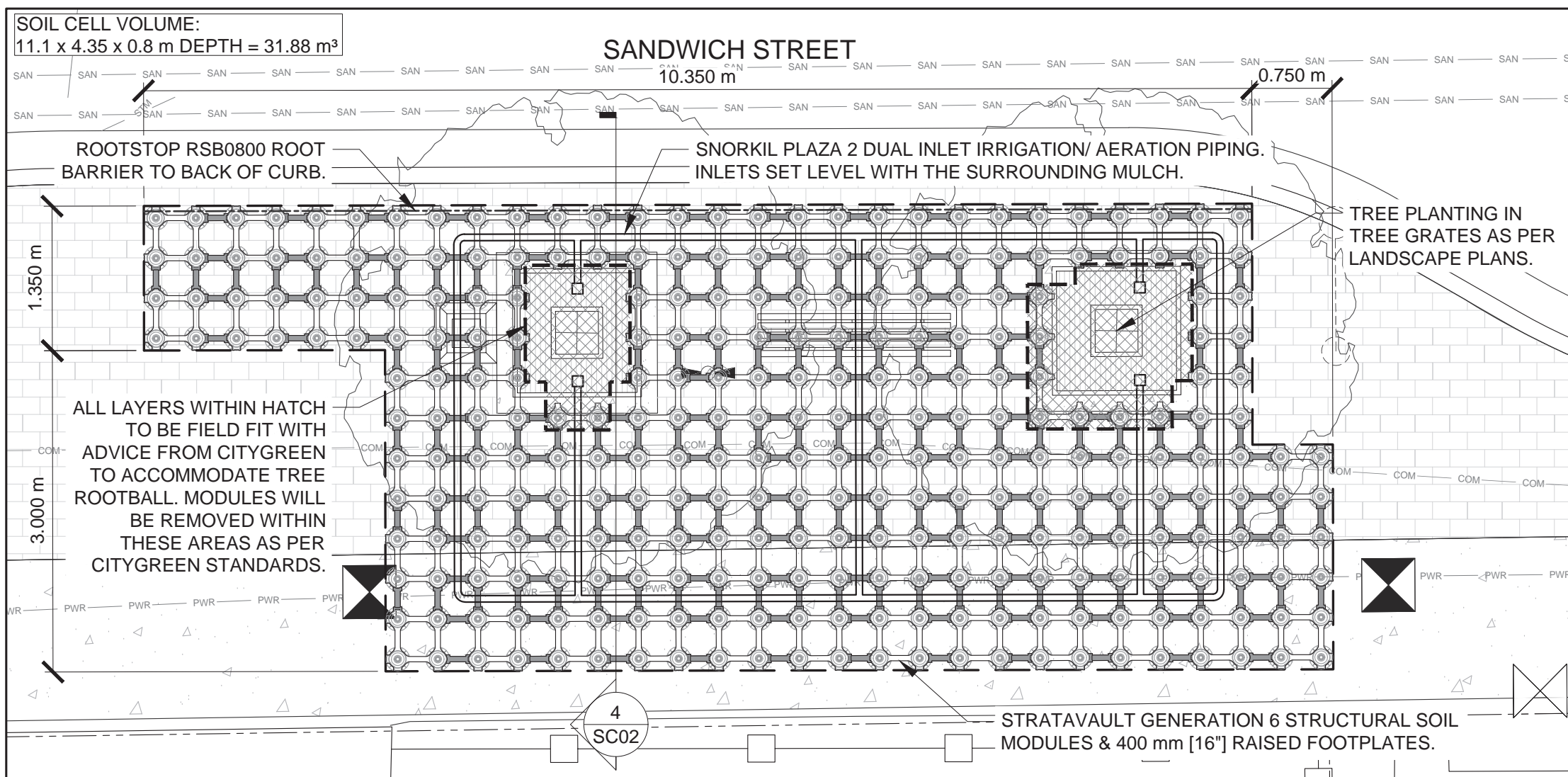
2 SOIL CELL LAYOUT 'A'
Scale: 1:50



3 SOIL CELL LAYOUTS 'B' & 'E'
Scale: 1:50



4 SOIL CELL LAYOUTS 'C' & 'D'
Scale: 1:50



5 SOIL CELL LAYOUT 'F'
Scale: 1:50



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- NOTES & DISCLAIMERS:**
1. Installation to be completed in accordance with manufacturer's specifications.
 2. Do not scale drawings.
 3. All foundation details to be verified by qualified project engineer. The manufacturer disclaims liability resulting from any loss or damage incurred from their products, or their installation method, either direct or consequential.
 4. Consultant drawings are being relied upon by Citygreen as being accurate in depicting site conditions, that they satisfy any local building code requirements, and that they accurately depict subsurface conditions.
 5. Citygreen takes no responsibility for the dimensions or the efficacy of design drawings upon which it has relied and has not performed a review of the design drawings.
 6. Citygreen has supplied the products only and takes no responsibility for installation and has no role in the installation of the products or the integration of the products into the as-constructed design.
 7. Submittal drawings and one (1) set of revisions are provided by Citygreen. Costs for additional revisions are the responsibility of others.

No.	Date	Issue Details	Initial
4	2024.03.13	REVISED AS PER COMMENTS	TB
3	2023.05.29	REVISED AS PER COMMENTS	TB
2	2023.05.26	REVISED AS PER COMMENTS	TB
1	2023.05.17	ISSUED FOR PRELIMINARY REVIEW	TB

Drawn: T.B. Checked: T.B.
Approved: T.B. Date: 2023.05.15
Scale: 1:2000 & 1:50 AS SHOWN

Client:
AECOM

Job Name:
**SANDWICH STREET CORRIDOR - WINDSOR, ON
W/ CITYGREEN STRATAVAULT GENERATION 6**
Drawing Title:
SITE KEYPLAN & SOIL CELL LAYOUTS

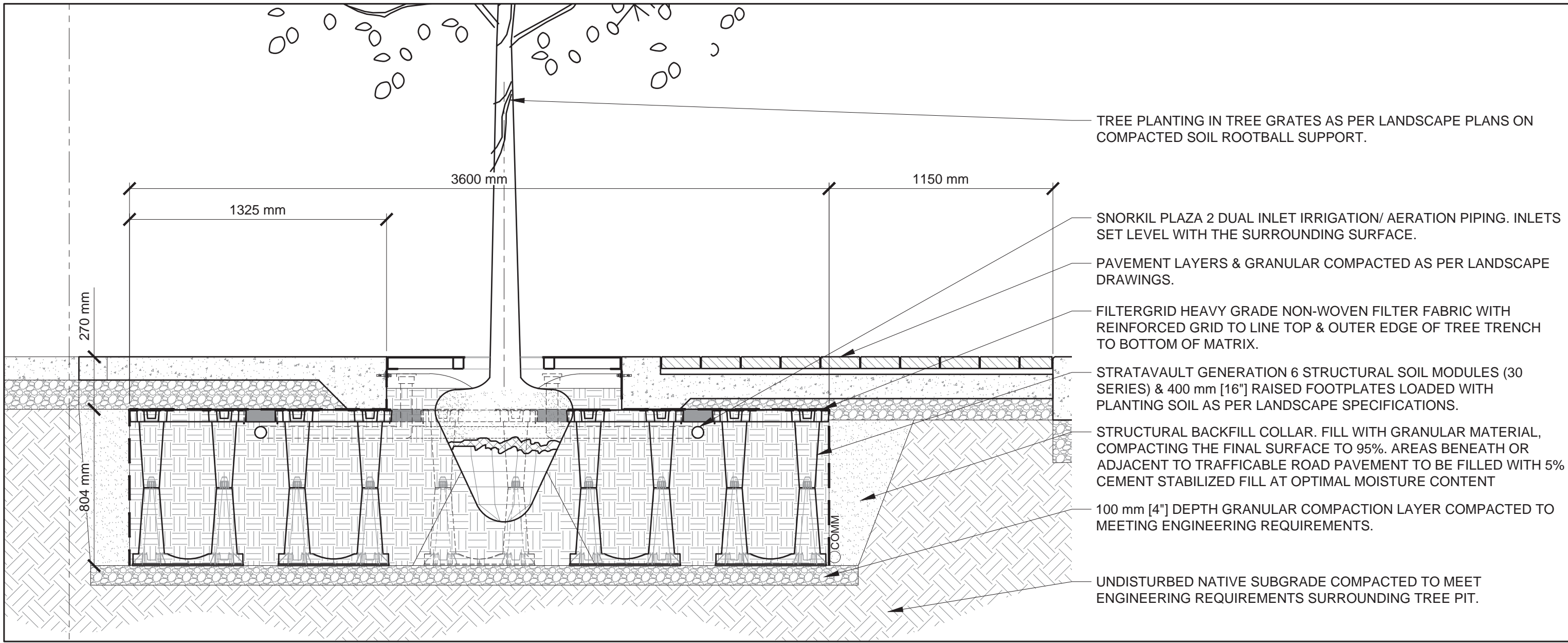
Consultant Drawings:
ACAD-GHIB_C19_CAPE_SAND_L_Base Drawing-Model
2023.05.12

Job No:
MP4052

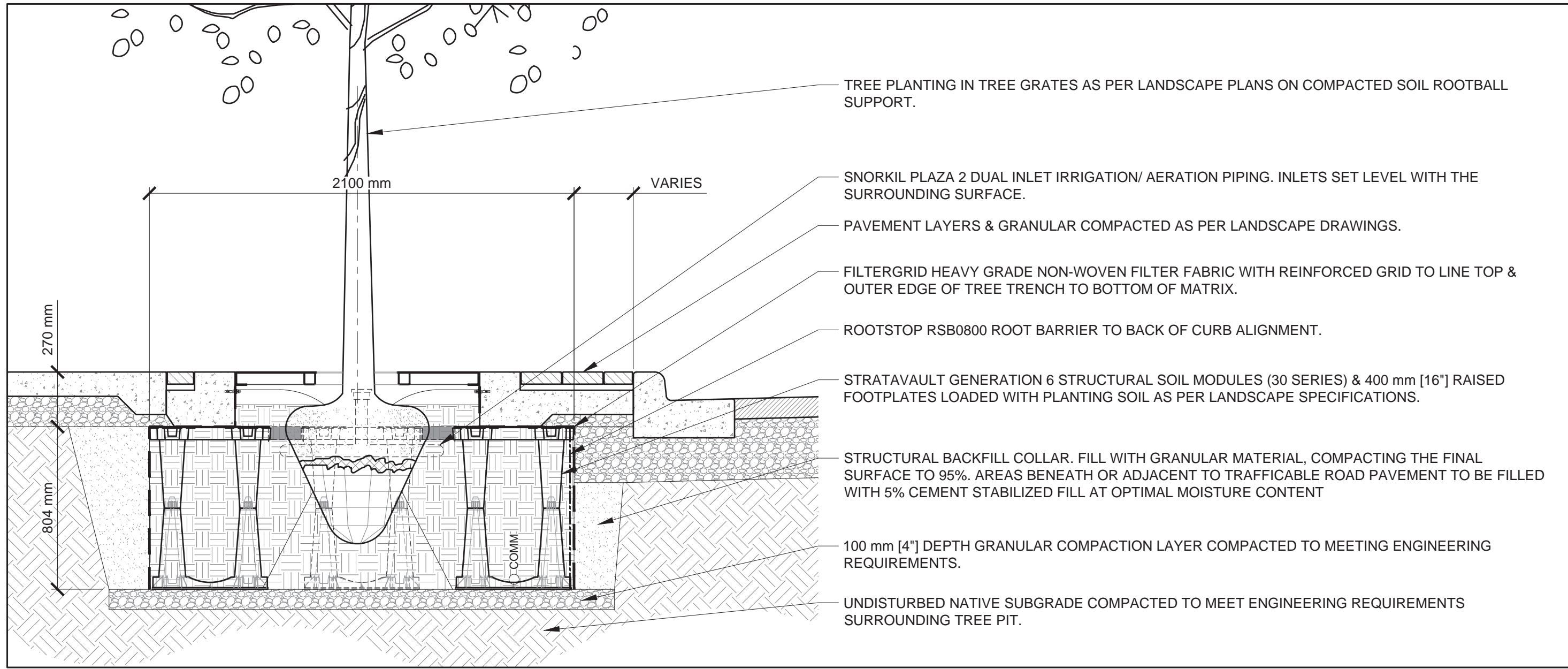
Reference No:

Drawing No:
SC01

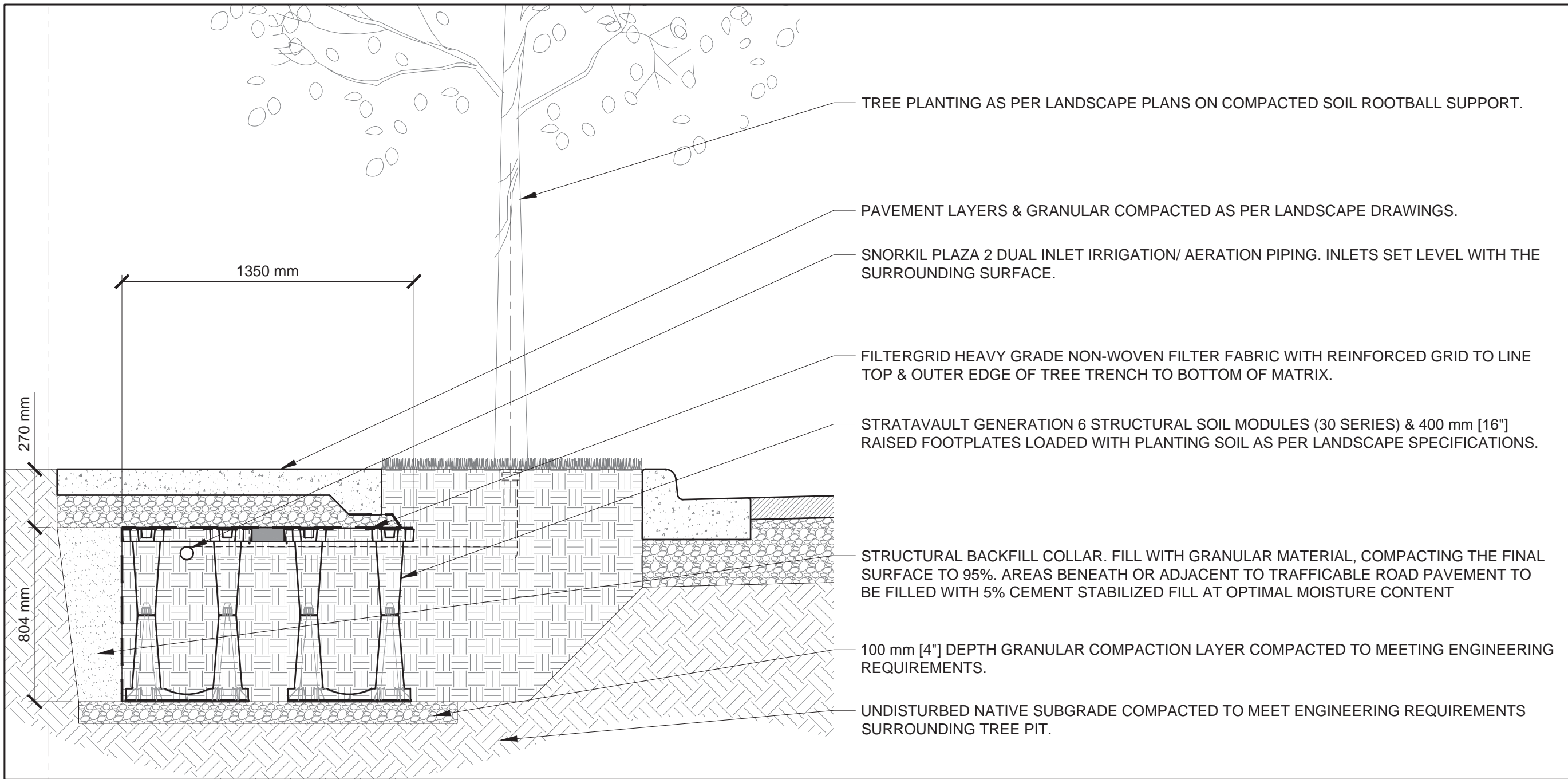
Sheet No. 1 of 2 Issue # 4



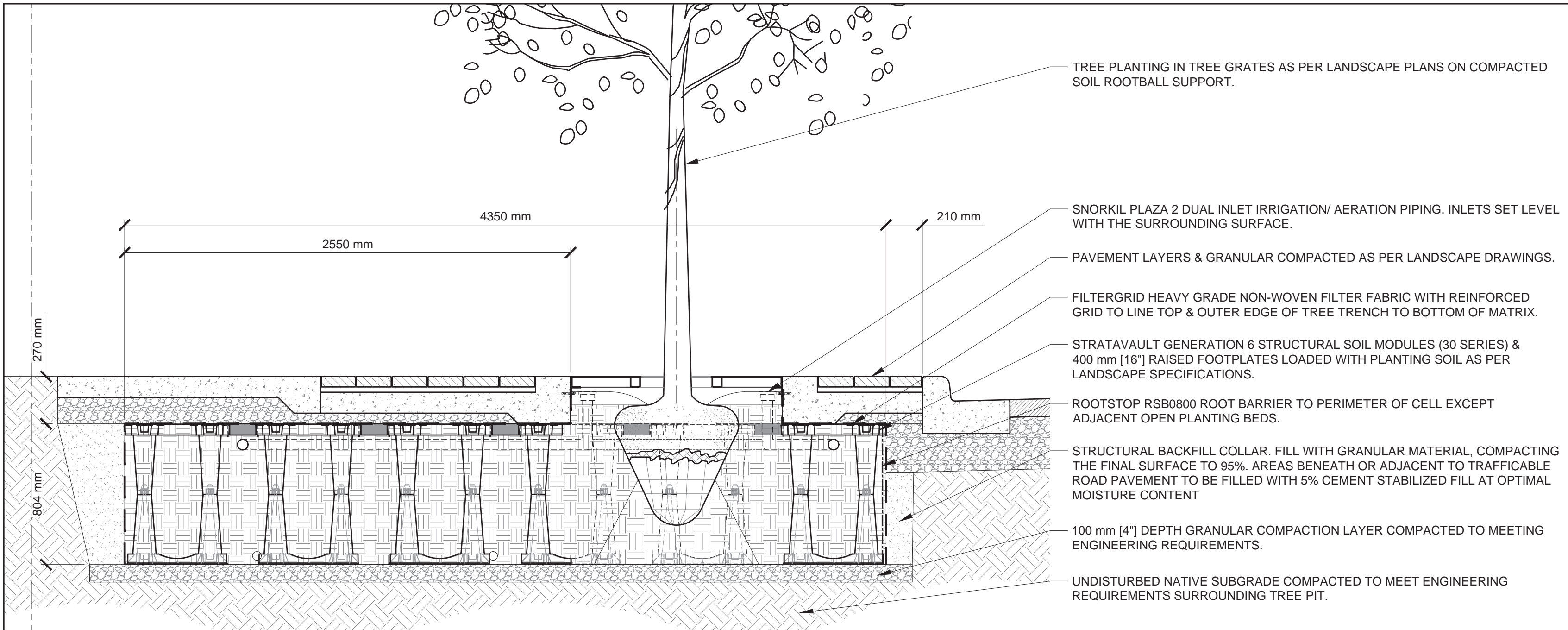
1 SOIL CELL CROSS SECTION 'AA'
SC02 Scale: 1:20



2 SOIL CELL CROSS SECTION 'BE'
SC02 Scale: 1:20



3 SOIL CELL CROSS SECTION 'CD'
SC02 Scale: 1:20



4 SOIL CELL CROSS SECTION 'FF'
SC02 Scale: 1:20

Appendix ‘A’: Proposed Statement of Cultural Heritage Value or Interest

**Strathcona Building
1958-1998 Wyandotte Street East**

Description of Historic Place

The Strathcona Building is a two storey commercial building located at 1958-1998 Wyandotte Street East, on the prominent corner of Devonshire Road and Wyandotte Street East. It was constructed in 1907 for Hiram Walker & Sons, and was designed by Detroit architect Albert Kahn during the Edwardian Period. The Strathcona Building features red brick facades with purple undertones, an articulated parapet, ornate cornices, pronounced window projections, decorative stone surrounds, and storefronts.

Cultural Heritage Value or Interest

Design or Physical Value:

The Strathcona Building is a two storey commercial building designed during the Edwardian Period. The building features a balanced design, with an articulated stone parapet raised at corners. The pediment at Wyandotte Street includes a stone plate etched with the word “Strathcona”, while the arched parapet portions features shield-shaped stone accents. A single row of rowlock brick beneath the stone capping reinforces the parapet design. The second floor ornate cornice stretches across the street-facing elevations. Four bay windows with brackets, interspaced with double and triple sets of sash windows with pronounced projections, provide texture and variety to the facade. The cornice pattern repeats above the storefront system, and together with soldier brick course on the sides, delineates between floors.

Two entrances with arched cut stone voussoirs, keystone, and surrounds were constructed and retained on the Wyandotte elevation. The original 1906 drawings shows the storefront system wrapping around the corner to Devonshire Road, truncating below the first bay window of the corner, some recessed ground floor entrances with ornate doors, and decorative brick pattern on the east facade. These features were either not constructed per drawing or later modified.

One of the greatest impacts from the redevelopment is the removal of paint from the brick facade, which revealed the stretcher red brick facade with purple undertones. The parapet were flashed over. Bay windows are reintroduced as aluminum glazing systems without the vertical muntins. As with main-street appropriate storefront systems, transoms were returned. The new black insulated aluminum panel storefront system is compatible to the original wood glass storefront (long-gone). The openings on the ground floor west elevations still feature stone sills. Awnings and new compatible signage have been proposed on the property. The rear north elevation had a variety of openings of irregular rhythm, and are replaced with some new glazing or painted black metal pan. Due to the porous nature of the brick on the rear elevation on the west side, the Owners commissioned a wall mural limited to that location.

Historical or Associative Value:

The subject property is located in the former Town of Walkerville, founded by Hiram Walker (1816-1899), the industrial entrepreneur from the United States. What is now Southwestern Ontario was inhabited by First Nations peoples, including the Ottawas. Following unsuccessful attempts to resist the French newcomers (who had settled earlier than the British), the Ottawa leader Pontiac granted lands to settlers in the vicinity of what became Walkerville. By the mid-1850s, Hiram Walker had bought several farm parcels. Walkerville was a planned community, almost entirely owned by the Walker family until the early 20th century.

As Hiram Walker's distillery business prospered, he developed the first five north-south streets of Walkerville starting from First Street (now Kildare Road) and east-west streets starting from Sandwich Street (now Riverside Drive). Development generally occurred systematically on north-south streets from Walker Road on the east to streets further west, and southward. Ferry services, rail lines, and street railways introduced by Walker spurred the growth of the distillery village further into a town with diversified businesses.

By 1890, Walkerville had obtained town status and was a self-sufficient model town. The Walker family separated the distillery and other industrial uses from commercial and residential uses, and engaged distinguished architects to design the early development in the community.

The subject property is located within Registered Plan 211, one of the earliest subdivision plans registered in 1879, creating lots from First (Kildare Road) to Fifth Streets (Walker Road) and from the Great Western Railway to Cataraqui Street. The subject building is located on Lot 10 of Block F, and like many of the vacant lots during the early 1900s, the property was originally owned by Hiram Walker and was transferred to the Walkerville Land & Building Co. (W. L. & B. Co.) with Edward Chandler Walker, Hiram Walker's oldest son, as president. After Hiram Walker's death in 1899, the remaining Walkers & associated companies led the second phase of development of the former Town of Walkerville. Devonshire Road was the main north-south street in Walkerville, while Wyandotte Street was the commercial "main street" lined with a mixture of residential dwellings, commercial block buildings, and prominent banks.

Early Tenants and Uses

The Strathcona Building was designed in 1906 and constructed in 1907 as "Stores for Walker Sons" (Strathcona Original Drawings). In a newspaper clipping from the Windsor Evening Record dated March 20, 1907, the building was referred to as the "new Walker block" and was anticipated to be "one of the handsomest business blocks in the town" when finished. The original architectural drawings show four commercial storefront units on the main floor and offices and a library on the second floor. The building finished construction c. April 1907. Upon completion, two established businesses moved in - F.J. Miller (druggist) and R.A. Holland (dry goods). Both businesses had moved from the Crown Inn on Devonshire Road. In July 1907, it was publicly announced in the Windsor Evening Record that the new building will be called "Strathcona Block".

Early tenants of the Strathcona Building include Walkerville Hardware Co., Nairn & Co. Grocers, and F.J. White (jeweler and dry goods) on the main floor and the Walkerville Public Library on the second floor. The Walkerville Public Library occupied the second floor of the Strathcona Building from its completion in 1907 until 1922, when the library moved to the main floor of

Willistead Manor which had then become the property of the Town. After the library vacated the second floor space of the Strathcona Building, it was commonly used as a community gathering and event space for various local organizations' chapters, such as the Sons of England, the Women of Mooseheart, and the Walkerville Football Club.

Another notable tenant during the building's early years was longtime tenant Lanspeary's Ltd. (druggist), and the offices of architects Stahl and Kinsey (construction supervisors of the Bank of Montreal building, 1799 Wyandotte).

Later Tenants and Uses

Through the decades after, the primary uses of the building largely remained a mix of commercial and offices. Some of the uses included: communications/telegraph offices, real estate offices, and printing and stationary services. Post mid-century, the uses changed to become more manufacturing in nature, including a cutting tools machine specialist and an industrial safety supply store. The Strathcona Building remains a valued commercial office building within Walkerville.

Architect Albert Kahn

The design of the Strathcona Building is attributed to the internationally recognized architect Albert Kahn. Although he was most famous for being a leading industrial architect, Kahn's portfolio of works includes a variety of office, commercial, public, and residential buildings. His elaborate designs and details are based in the use of historical period architectural styles, and he was best remembered for bringing natural light into industrial buildings during the auto manufacturing boom.

Kahn was a significant architect in the design of the former Town of Walkerville. While employed with Detroit firm Mason & Rice Architects, Kahn designed the beautiful interior of the Hiram Walker & Sons Office Building, which was instrumental in showcasing his talent and elevating his career. He started up his own firm "Kahn and Associates" in 1895 and was based in Detroit. The Walker family and company were major patrons of his services, and he was their choice architect commissioned to design notable properties for them. Properties designed by Kahn and Associates for the Walkers include the Willistead Manor (1899 Niagara Street), the Harrington Walker Manor (1948 St Mary's Gate), Walkerville Town Hall, and the Canadian Bank of Commerce. Kahn was responsible for the design of several listed or designated properties on Windsor's Municipal Heritage Register.

Throughout his career, he was credited with over 1000 buildings. Some of his most celebrated works in the United States are the Fisher Building, the Conservatory on Belle Isle, the Edsel and Eleanor Ford House, and the Detroit Athletic Club.

Contextual Value:

The Strathcona Building is a long-standing landmark and is highly visible from its location on the corner of Devonshire Road and Wyandotte Street East. It is evocative of the early "main street" streetscape of the former Town of Walkerville, and supports the heritage main street character of Wyandotte Street East. The Strathcona Building was strategically located a short distance away from the historic Detroit-Walkerville Ferry Terminal docks and the Pere Marquette train station.

It also reflects the supremacy of Wyandotte Street East as the commercial area for the Town since the early twentieth century, where the Strathcona Building was located along with several other banks and heritage buildings. The subject property is a notable example of early commercial buildings constructed within the core areas of Walkerville, and reflects the early economic prosperity of the former Town of Walkerville and the growing commercial needs of residents.

Heritage Attributes:

Attributes that contribute to the design or physical value of the Strathcona Building:

- Built in 1907
- Two storey building of stretcher red brick with purple undertones
- Edwardian period commercial building, featuring:
 - Articulated stone parapet raised at corners, with single course rowlock brick beneath reinforcing parapet design
 - Pediment at Wyandotte Street includes stone plate with “Strathcona” etching
 - Arched parapet features shield-shaped stone accents; two on Wyandotte St. E. elevation and one on Devonshire Rd. elevation
 - Second floor ornate cornice which stretches across the street facing elevations above openings
 - Four bay windows with brackets, interspaced with double and triple sets of sash windows with pronounced projections
 - Originally wood sash windows with vertical muntins
 - Ornate cornice above the storefront system
 - Soldier brick course delineating between floors
 - Two of the entrances with arched cut stone voussoirs, keystone and surround
 - Storefront glazing with transom glass
 - Stone sills at west elevation ground floor openings and north elevation

Attributes that contribute to the historical or associative value of the Strathcona Building:

- Developed during a time of early economic prosperity and rapid growth for the former Town of Walkerville to accommodate the growing commercial and community needs of residents
- Owned by the Walkerville Land & Building Company, the real estate arm of the Walker Enterprise and main developer and landholder of the former Town of Walkerville
- Designed by internationally renowned and locally significant architect Albert Kahn

Attributes that contribute to the contextual value of the Strathcona Building:

- Landmark building located at the highly visible corner on the northwest of Devonshire Road and Wyandotte Street East, two historic “main streets”
- Supports the continuous commercial street frontage, physically, visually and historically linked to many heritage buildings along the Wyandotte Street East Main Street, forming a major part of the former Town of Walkerville streetscape