

Development & Heritage Standing Committee Meeting

Date: Tuesday, July 4, 2023

Time: 4:30 o'clock p.m.

Location: Council Chambers, 1st Floor, Windsor City Hall

All members will have the option of participating in person in Council Chambers or electronically and will be counted towards quorum in accordance with Procedure By-law 98-2011 as amended, which allows for electronic meetings. The minutes will reflect this accordingly. Any delegations have the option to participate in person or electronically.

MEMBERS:

Ward 1 – Councillor Fred Francis

Ward 4 – Councillor Mark McKenzie

Ward 7 – Councillor Angelo Marignani

Ward 9 – Councillor Kieran McKenzie

Ward 10 – Councillor Jim Morrison (Chairperson)

Anthony Arbour

Joseph Fratangeli

Daniel Grenier

John Miller

Charles Pidgeon

Robert Polewski

Khassan Saka

William Tape

ORDER OF BUSINESS

Item # Item Description
1. **CALL TO ORDER**

Reading of Land Acknowledgement

We [] would like to begin by acknowledging that the land on which we gather is the traditional territory of the Three Fires Confederacy of First Nations, which includes the Ojibwa, the Odawa, and the Potawatomi. The City of Windsor honours all First Nations, Inuit and Métis peoples and their valuable past and present contributions to this land.

2. **DISCLOSURES OF PECUNIARY INTEREST AND THE GENERAL NATURE THEREOF**

3. **REQUEST FOR DEFERRALS, REFERRALS OR WITHDRAWALS**

4. **COMMUNICATIONS**

5. **ADOPTION OF THE *PLANNING ACT* MINUTES**

5.1. Minutes of the June 5, 2023 Development & Heritage Standing Committee - *Planning Act* Matters (**SCM 180/2023**)

6. **PRESENTATION DELEGATIONS (*PLANNING ACT* MATTERS)**

7. **PLANNING ACT MATTERS**

7.1. Draft Plan of Condominium with Exemption under Section 9(3) of the Condominium Act –CDM 005-23 [CDM-7029] 2481939 Ontario Inc. 3817 Howard Ave Ward 9 (**S 72/2023**)

7.2. Zoning By-law Amendment Application for the property at the southwest corner of Tecumseh Road West and Mark Avenue; Applicant: 2832765 Ontario Inc.; File No. Z-036/2022, ZNG/6897; Ward 10. (**S 73/2023**)

7.3. Z 015-22 [ZNG-6738] 2356976 Ontario Inc 0 & 845 Riverside E and 856 & 864 Chatham E (**S 74/2023**)

8. **ADOPTION OF THE MINUTES**

8.1. Adoption of the Development & Heritage Standing Committee meeting minutes held June 5, 2023 (**SCM 170/2023**)

9. **PRESENTATIONS AND DELEGATIONS (COMMITTEE ADMINISTRATIVE MATTERS)**

10. **HERITAGE ACT MATTERS**

10.1. 1148 Victoria Avenue, Kathleen Henderson House - Heritage Permit & Community Heritage Fund Request (Ward 3) (**S 76/2023**)

10.2. 1982 Norman Road, St Jules School – Heritage Evaluation Report (Ward 8) (**S 37/2023**)

11. **ADMINISTRATIVE ITEMS**

11.1. Municipal Sewer Servicing Charges for Residential Properties - Septic System Changeover - City Wide (**S 78/2023**)

11.2. Closure of east/west alley between closed Fifth Street R.O.W. and E. C. Row Expressway westbound onramp; Closure of Hudson Avenue R.O.W. between 4505 Fourth Street and E. C. Row Expressway westbound onramp; Amend Alley Closing By-law 10354, Ward 2, SAA-6688 (**S 66/2023**)

11.3. Closure of east/west alley between Rockwell Avenue and Closed Woodland Avenue; east/west alley between Closed Woodland Avenue and Granada Avenue West, Ward 1, SAA-6921 (**S 69/2023**)

11.4. Riverside Drive Streetscape Standards Manual, Spans Wards 2 through 7. (**S 67/2023**)

11.5. Main Street CIP/Ford City CIP Application, 1000 Drouillard Road, Owner: SPOTVIN INC. (C/O: Shane Potvin) (**S 77/2023**)

12. **COMMITTEE MATTERS**

12.1. Minutes of the Property Standards Committee of its meeting held May 2, 2023
(SCM 161/2023)

12.2. Minutes of the International Relations Committee of its meeting held May 10, 2023
(SCM 169/2023)

13. **QUESTION PERIOD**

14. **ADJOURNMENT**

Item No. 5.1



Committee Matters: SCM 180/2023

Subject: Minutes of the June 5, 2023 Development & Heritage Standing Committee - Planning Act Matters

**Development & Heritage Standing Committee
(Planning Act Matters)**

**Date: Monday, June 05, 2023
Time: 4:30 o'clock p.m.**

Members Present:

Councillors

Ward 1 - Councillor Fred Francis
Ward 4 – Councillor Mark McKenzie
Ward 7 – Councillor Angelo Marignani
Ward 9 – Councillor Keiran McKenzie
Ward 10 - Councillor Jim Morrison (Chairperson)

Members

Member Anthony Arbour
Member Daniel Grenier
Member Charles Pidgeon
Member Robert Polewski
Member William Tape

Members Regrets

Member Khassan Saka
Member John Miller

PARTICIPATING VIA VIDEO CONFERENCE ARE THE FOLLOWING:

Member Joseph Fratangeli
Sandra Gebauer, Council Assister

**ALSO PARTICIPATING IN COUNCIL CHAMBERS ARE THE FOLLOWING FROM
ADMINISTRATION:**

Jelena Payne, Commissioner – Economic Development & Innovation
Wira Vendrasco, Deputy City Solicitor – Legal & Real Estate
Kevin Alexander, Planner II – Special Projects

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Neil Robertson – Manager Urban Design/ Deputy City Planner
Adam Szymczak – Planner III – Zoning
Brian Nagata – Planner II – Development Review
Jim Abbs – Planner III – Subdivisions
Greg Atkinson – Planner III – Economic Development
Justina Nwaesei – Planner III – Subdivisions
Frank Garardo – Planner III – Policy and Special Studies
Rob Perissinotti – Development Engineer
Shannon Deehan – Transportation Planning Coordinator
Tracy Tang - Planner II – Revitalization & Policy Initiatives
Anna Ciacelli – Deputy City Clerk

Delegations—participating via video conference

Item 7.2 Bassem Mazloum, area resident
Item 7.2 Jacky Ng, Project & Architectural Designer, Avant Group Inc.
Item 7.6 Tracey Pillon-Abbs, Principal Planner, representing the applicant

Delegations—participating in Council Chambers

Item 7.2 Bruce Sheardown, area resident
Item 7.2 Michael Chen, area resident
Item 7.2 Justine Nakigozi, area resident
Item 7.2 Mrs. Antonia Laberi, area resident
Item 7.2 Mr. Laberi, area resident
Item 7.5 Amy Farkas, Planner, Dillion Consulting Limited
Item 7.5 Theresa O'Neill, Planner, Dillion Consulting Limited
Item 10.1 Micheal Schmidlin, Owner/applicant

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1. CALL TO ORDER

Following the reading of the Land Acknowledgement, the Chairperson calls the meeting of the Development & Heritage Standing Committee to order at 4:30 o'clock p.m.

2. DISCLOSURES OF PECUNIARY INTEREST AND THE GENERAL NATURE THEREOF

None disclosed.

3. REQUEST FOR DEFERRALS, REFERRALS OR WITHDRAWALS

None requested.

4. COMMUNICATIONS

None presented.

5. ADOPTION OF THE PLANNING ACT MINUTES

5.1. Minutes of the April 5, 2023 Development & Heritage Standing Committee (*Planning Act Matters*)

Moved by: Councillor Mark McKenzie
Seconded by: Councillor Angelo Marignani

That the *Planning Act* minutes of the Development & Heritage Standing Committee meeting held April 5, 2023 **BE ADOPTED** as presented.
Carried.

Report Number: SCM 128/2023

5.2. May 1, 2023 Development & Heritage Standing Committee Minutes (*Planning Act Matters*)

Moved by: Councillor Mark McKenzie
Seconded by: Councillor Angelo Marignani

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That the *Planning Act* minutes of the Development & Heritage Standing Committee meeting held May 1, 2023 **BE ADOPTED** as presented.
Carried.

Report Number: SCM 148/2023

6. PRESENTATION DELEGATIONS (*PLANNING ACT* MATTERS)

See Items 7.2, 7.5, 7.6

3. REQUEST FOR DEFERRALS, REFERRALS OR WITHDRAWALS (*PLANNING ACT* MATTERS)

7.1. Application for Approval of the Conversion of an existing Non-Residential Building at 1500 Ouellette Avenue to a Residential and Commercial Plan of Condominium; Applicant - Ryan Michael Solcz Prof. Corp; File No. CDM-004/22 [CDM/6939]; Ward 3

Moved by: Councillor Kieran McKenzie
Seconded by: Councillor Angelo Marignani

That the report of the Senior Planner, Subdivisions dated May 12, 2023 entitled “Application for Approval of the Conversion of an existing Non-Residential Building at 1500 Ouellette Avenue to a Residential and Commercial Plan of Condominium; Applicant - Ryan Michael Solcz Prof. Corp; File No. CDM-004/22 [CDM/6939]; Ward 3” **BE DEFERRED** to the August 1, 2023 Development and Heritage Standing Committee meeting to allow for the applicant to review some of the conditions.
Carried.

Report Number: S 62/2023
Clerk’s File: Z/14543

7.2. Zoning By-law Amendment Application for 0 & 1466 St. Patrick Avenue, Z-037/22 [ZNG-6899], Ward 10

Brian Nagata, Planner (author) presents item.

Bassem Mazloum (area resident) – 1598 Rankin Avenue – has concerns with the cost for the mandatory sanitary sewer connection and alleyway being purchased.

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Bruce Sheardown (area resident) – 1469 Askin Avenue – has various concerns like; with the possibility of the development becoming a lodging house, parking shortage, the possibility of flooding. Mr. Sheardown presents a signed petition from neighbors who are not in favour of the development.

Michael Chen (area resident) – 1429 St. Patrick's Avenue – Has various concerns like; the cost for the mandatory sanitary sewer connection, the possibility of sewer backups due to the age and size of the sewers, the density of the development, parking shortages and noise pollution.

Justine Nakigozi (area resident) – 1450 St. Patrick's Ave. – has concerns with the density being too high for the neighborhood, safety and the maintenance of the development.

Lorenzo Lombardi (area resident) – 1665 Partington – has concerns with the quality of life for the current residents and value of the neighborhood

Antonia Laberi (area resident) – 1665 Partington – has concerns with the cost of the mandatory sanitary sewer connection and the possibility of the development becoming a lodging house.

Jacky Ng (architect) – Avant Group – is available for questions.

Councilor Francis asks Mr. Ng if there is updated floor plan that was presented to the City of Windsor. Mr. Ng answers that an updated floor plan will be submitted within two weeks.

Councilor Francis asks Mr. Ng how many bedrooms there will be in the 2 semi-detached dwellings. Mr. Ng answers that the first floor will have 2 bedrooms and a bonus room above the garage.

Councilor Francis asks if there will be a total of 3 bedrooms in each semi-detached. Mr. Ng confirms.

Councilor Francis asks Administration to provide more information on the cost of the sanitary sewer connection. Mr. Perissinotti answers that they do not have a cost assessment at this time.

Councilor Marignani asks Mr. Ng if the basement will have its own entrance. Mr. Ng confirms that it will.

Councilor Kieran Mackenzie asks Mr. Perissinotti if the mandatory sanitary sewer connection will have a 1 year payment period for the residents. Mr. Perissinotti answers that according to Property Standards By-law 9-2019 the residents have 1 year to connect to the sanitary sewer and pay but they are looking into making a change to the policy. The Policy will be similar to another development on Northway Ave. which will allow the same policies as a local improvement. Mr. Perissinotti explains that this would make it much more beneficial to the residents. This has not yet been approved by Council.

Councilor Kieran Mackenzie asks Mr. Ng for the total number of bedrooms that are being proposed. Mr. Ng answers that each unit would have 3 bedrooms.

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Councilor Kieran Mackenzie asks Administration if they are confident in the analysis that has been provided regarding sewer capacity. Mr. Perissinotti answers that they are comfortable with what was provided but the final details have not been confirmed at this time.

Councilor Kieran Mackenzie asks about the building height. Mr. Nagata answers that the maximum building height provisions under the Residential District 1.2 (RD1.2) zoning district were recently reduced from 10m to 9m. Mr. Nagata adds that the applicant is not requesting an increase to the maximum building height provision under the RD1.2 zoning. Mr. Nagata lastly notes that the other properties on the block are also within the RD1.2 zoning district.

Councilor Kieran Mackenzie asks Mr. Nagata if purchasing the alley is a requirement in order for the buildings to be developed. Mr. Nagata answers that the Applicant requested a larger building area through a reduction in the minimum rear yard depth provision under the RD1.2 zoning, which Administration was not in support of. Mr. Nagata adds that the Applicants have made a concurrent application to close the alley for the purchase of achieving a larger building area. If they do not obtain the alley way they will have to scale back their building lot area.

Councilor Kieran Mackenzie asks if the parking that is being purposed is sufficient for the development. Ms. Amicarelli answers that the parking complies with the Zoning By-Law 8600 they require 8 spaces and are proposing 8 spaces. An option could be Residential Permit parking or request through the parking change policy where they can put limitations on the parking signs. These options could alleviate any issues with overflow parking.

Member Arbour asks how many residents will potentially have to connect into the sanitary sewer. Mr. Nagata answers that this will affect 6 possibly 7 properties. Mr. Arbour asks what the cost will be for the property owners. Mr. Perissinotti answers that they have not yet received a cost estimate from the developer.

Councilor Morrison asks for confirmation on the amount of bedrooms. Mr. Ng answers that there will be 20 bedrooms if this development becomes an ADU. If it is not an ADU, 12 bedrooms total.

Moved by: Councillor Kieran McKenzie

Seconded by: Councillor Angelo Marignani

That the report of the Planner II – Development Review dated May 2, 2023 entitled “Zoning By-law Amendment Application for 0 & 1466 St. Patrick Avenue, Z-037/22 [ZNG-6899], Ward 10” **BE DEFERRED** to the August 1, 2023 Development & Heritage Standing Committee Meeting to allow for Administration to provide information related to costs for sewer infrastructure that will be required for the proposed development and the costing options that will be available for surrounding residents, as well as issues related to costing for permit parking if required.

Carried.

Report Number: S 56/2023

Clerk's File: Z/14541

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Moved by: Councillor Kieran McKenzie
Seconded by: Councillor Angelo Marignani

That the report of the Planner II – Development Review dated May 2, 2023 entitled “Zoning By-law Amendment Application for 0 & 1466 St. Patrick Avenue, Z-037/22 [ZNG-6899], Ward 10” **BE DEFERRED** to the August 1, 2023 Development & Heritage Standing Committee Meeting to allow for Administration to provide information related to costs for sewer infrastructure that will be required for the proposed development and the costing options that will be available for surrounding residents, as well as issues related to costing for permit parking if required.

Carried.

Report Number: S 56/2023
Clerk’s File: Z/14541

7. PLANNING ACT MATTERS

7.3. Zoning Z42-22 [ZNG/6937] - 1155 California Ave Ward 2

Frank Garado, Planner (author) is available for questions.

Moved by: Councillor Angelo Marignani
Seconded by: Councillor Mark McKenzie

Decision Number: **DHSC 510**

- I. That Zoning By-law 8600 **BE AMENDED** for Plan 1120 N PT Lot 76; S PT Lot 77 (known municipally as 1155 California Avenue); by adding a site-specific exception to Section 20(1) as follows:

4xx. **WEST SIDE OF CALIFORNIA AVENUE BETWEEN GIRARDOT STREET
AND PELLETIER STREET**

For the lands comprising of lots N PT Lot 76; S PT Lot 77 Registered Plan 1120, a *semi-detached dwelling* shall be an additional permitted use and shall be subject to the following additional provisions:

- | | |
|------------------------|----------------------|
| a) Lot Width – minimum | 12.0 m |
| b) Lot Area – minimum | 398.0 m ² |

Further, for a *semi-detached dwelling*, two *dwelling units* in a *semi-detached dwelling unit* and one *dwelling unit* in an *accessory building* which is accessory to a *semi-detached dwelling* shall be additional permitted uses and shall be subject to the provisions in Sections 5.99.80.3 and 5.99.80.5. [ZDM4; ZNG/6937]

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Carried.

Report Number: S 54/2023

Clerk's File: Z/14573

7.4. Housekeeping Official Plan and Zoning By-law Amendments initiated by the City of Windsor to permit additional dwelling units (City-wide) - File No. OPA 172 and Z16-23

Frank Garado, Planner (author) presents the item.

Councillor Kieran Mackenzie asks if there is sufficient capacity to withhold storm water and other mitigation factors in the floodplain area why would there be a blanket restriction. Mr. Garado answers the provisions will regulate the additional dwelling units in the basement or cellars. Mr. Robertson adds that it mainly addresses overland flooding and having a blanket prohibition allows each application to be looked at individually.

Councillor Morrison asks about the size of additional dwelling units. Mr. Garado answers that if the additional dwelling unit is located in a main dwelling, there will be no minimum or maximum in size.

Moved by: Councillor Kieran McKenzie

Seconded by: Councillor Angelo Marignani

Decision Number: **DHSC 511**

- I. That Official Plan Amendment No. 172 as shown in Appendix A regarding a housekeeping amendment on new policies authorizing the use of additional dwelling units **BE ADOPTED**; and,
- II. That a housekeeping amendment to Zoning By-law 8600 as shown in Appendix B implementing additional dwelling units **BE PASSED**; and,
- III. That a housekeeping amendment to Zoning By-Law 85-18 as shown in Appendix C implementing additional dwelling units **BE PASSED**;

Carried.

Report Number: S 64/2023

Clerk's File: ZO/13470 & Z/14558

7.5. Zoning Bylaw Amendment Z 013-23 [ZNG-7000] - 1027458 Ontario Inc. - 0 Clover Avenue - SW Corner Clover & Wyandotte - Ward 7

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Jim Abbs, Planner (author) is available for questions.

Amy Farkas, Planner (Dillon Consulting Limited) is available for questions.

Theresa O'Neill, Planner (Dillon Consulting Limited) is available for questions.

Moved by: Councillor Angelo Marignani

Seconded by: Councillor Mark McKenzie

Decision Number: **DHSC 512**

- I That an amendment to City of Windsor Zoning By-law 8600 **BE APPROVED**, changing the zoning of Lands Identified as Part 1 in Figure 5: Areas to be rezoned from Residential District (RD) 1.1 to Residential District (HRD) 2.3;
- II That a Hold provision **BE APPLIED** to Lands Identified as Part 1 in Figure 5: Areas to be rezoned and that it be removed when the conditions contained in Section 5.4 HOLDING ZONE PROVISIONS of City of Windsor Zoning By-law 8600 have been met;
- III That subsection 1 of Section 20 of the City of Windsor Zoning By-law 8600 **BE AMENDED** for the lands described as Lands Identified as Part 1 in Figure 5: Areas to be rezoned by adding site specific regulations as follow:

4XX. **South West Corner Clover Avenue & Wyandotte Street**

For the lands described as Lands Identified as Part 1 in Figure 5: Areas to be rezoned, the total area of the *required front yard* occupied by a hard surface for the purpose of a walkway, driveway, *access area*, *parking space*, or any combination thereof, exceeding 60% of the *required front yard* shall be prohibited, and,

Lot Coverage – Maximum 50%
(ZDM 14; ZNG/7000)

- IV That an amendment to City of Windsor Zoning By-law 8600 **BE APPROVED**, changing the zoning of Lands Identified as Part 2 in Figure 5: Areas to be rezoned from Development Reserve District (DRD) 1.1 to Green District (GD) 1.1;

Carried.

Report Number: S 65/2023

Clerk's File: Z/14572

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7.6. OPA & Rezoning – 1998308 Ontario Inc. – 0 Russell Street - OPA 168 OPA/6975 Z-007/23 ZNG/6974 – Ward 2

Adam Szymczak, Planner (author) is available for questions.

Tracey Pillon-Abbs, Planner (agent) is available for questions

Councillor Kieran McKenzie asks if there are any concerns from existing area residents regarding dust and noise pollution. Mr. Szymczak answers that he is not aware of any concerns.

Councillor Kieran McKenzie

Moved by: Member Anthony Arbour

Seconded by: Councillor Mark McKenzie

Decision Number: **DHSC 513**

1. That Schedule “D” of Volume 1: The Primary Plan of the City of Windsor Official Plan **BE AMENDED** by changing the land use designation of Lot 13, E/S Russell St, Registered Plan 40, Town of Sandwich (PIN 01591-0155; 0 Russell Street; Roll No. 050-160-12400), situated at Russell Street, Chippawa Street, and Wilkinson Lane, from “Business Park” to “Residential”.

2. That Zoning By-law 8600 **BE AMENDED** by changing the zoning of Lot 13, E/S Russell St, Registered Plan 40 Town of Sandwich (PIN 01591-0155; 0 Russell Street; Roll No. 050-160-12400), situated at Russell Street, Chippawa Street, and Wilkinson Lane, from Manufacturing District 1.4 (MD1.4) to Residential District 3.3 (RD3.3) and by adding a site specific exception to Section 20(1) as follows:

471. **CORNER OF RUSSELL STREET, CHIPPAWA STREET AND WILKINSON LANE**

For the lands comprising of Lot 13, E/S Russell Street, Registered Plan 40, Town of Sandwich (PIN 01591-0155) the following shall apply:

1. A *townhome dwelling* is an additional permitted use and shall be subject to the additional provisions in paragraph a) below, the provisions in Section 11.2.5, and all other applicable provisions:
 - a) Subject to the satisfaction of the Chief Building Official, the following noise mitigation measures are required at time of building permit:
 - i) Exterior building components shall meet the minimum Acoustic Insulation Factors in Tables 2 and 3 of the Detailed Noise Control Study prepared by SS Wilson Associates, Consulting Engineers and dated November 22, 2022, subject to any revisions.
 - ii) Installation of central air conditioning
2. For any permitted use in RD3.3, the following additional provisions shall apply:
 - a) Notwithstanding Sections 12.3.5.4 and 21(3), *building height* as measured from Russell Street in excess of 4 *storeys* and *building height* as measured from

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Wilkinson Lane in excess of 3 *storeys* shall be prohibited. For the purpose of this provision, grade means the average elevation of the crown of That part of the street identified above and abutting the subject lot.

- b) *Parking spaces* forming part of a *parking area* shall be permitted in a *front yard* and are subject to the Parking Area Provisions in Section 25.

[ZDM 4; ZNG/6975]

3. That Lot 13, E/S Russell St, Registered Plan 40, Town of Sandwich (PIN 01591-0155; 0 Russell Street; Roll No. 050-160-12400), situated at the corner of Russell Street, Chippawa Street, and Wilkinson Lane, **BE CLASSIFIED** as a Class 4 area pursuant to MOECP Environmental Noise Guideline Publication NPC-300; and, that the City Planner or their designate **BE DIRECTED** to provide a copy of the Council Resolution approving the Class 4 area classification and a copy of any development agreement or site plan agreement for the subject lands that incorporates noise mitigation measures to Windsor Port Authority (3190 Sandwich St), Lafarge Canada (3450 Russell St) and St. Mary's CBM Ready-Mix Plant (3510 Russell St).

4. That, when Site Plan Control is applicable:

- A. Prior to the submission of an application for site plan approval, at the discretion of the City Planner, Deputy City Planner, or Site Plan Approval Officer, those documents submitted in support of the applications for amendments to the Official Plan and Zoning By-law 8600 **BE UPDATED** to reflect the site plan for which approval is being sought.
- B. The Site Plan Approval Officer **BE DIRECTED** to incorporate the following, subject to any updated information, into an approved site plan attached to an executed and registered site plan agreement:
 - i) Mitigation measures, including warning clauses, identified in the Detailed Noise Control Study prepared by SS Wilson Associates, Consulting Engineers, dated November 22, 2022, subject to the approval of the City Planner, Deputy City Planner, or Site Plan Approval Officer.
 - ii) Requirements of the City of Windsor – Engineering Department – Right-of-Way Division and City of Windsor – Transportation Planning contained in Appendix E of Report S 53/2023, subject to the approval of the City Engineer.
- C. The Site Plan Approval Officer **CONSIDER** the comments from municipal departments and external agencies contained in Appendix E of Report S 53/2023.

Carried.

Report Number: S 53/2023
Clerk's File: Z/14559 & Z/14558

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8. ADJOURNMENT

There being no further business, the meeting of the Development & Heritage Standing Committee is adjourned at 6:34 o'clock p.m.

Ward 10 - Councillor Jim Morrisson
(Chairperson)

Deputy City Clerk / Supervisor
of Council Services

Subject: Draft Plan of Condominium with Exemption under Section 9(3) of the Condominium Act –CDM 005-23 [CDM-7029] 2481939 Ontario Inc. 3817 Howard Ave Ward 9

Reference:

Date to Council: July 4, 2023
Author: Jim Abbs
Senior Planner - Subdivisions
519 255 6543 x6317
jabbs@citywindsor.ca

Planning & Building Services
Report Date: June 8, 2023
Clerk's File #: Z/14590

To: Mayor and Members of City Council

Recommendation:

THAT the application of 2481939 Ontario Inc. for an exemption under Section 9(3) of the Condominium Act for approval of a plan of condominium (Standard Condominium), comprised of a total of 78 dwelling units and 6 commercial unit and 14 parking units, as shown on the attached Map No. CDM-005/23-1 and CDM-005/23-2 on a parcel legally described as; Part of Lot 85, Concession 3, Lot 28 and Part of Lot 29, Plan 1431; Part 2, 12R-29025 located at 3817 Howard Ave **BE APPROVED** for a period of three (3) years.

Executive Summary:

N/A

Background:

Application Information:

Location: 3817 Howard Ave

Ward: 9 **Planning District:** South Windsor **ZDM:** 09

Applicant: 2481939 Ontario Inc.

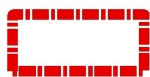
Proposal:

The applicant is applying for an exemption under Section 9(3) of *the Condominium Act* for approval of a plan of condominium for an existing Building to create 78 dwelling units and 6 commercial units in a mixed use building. 14 parking units are also included in the Plan of Condominium.

The subject site has received Site Plan Approval (File R006.20) registered as instrument CE1023406, on July 30, 2021 which permitted a total of 78 dwelling units and 6 commercial units in a mixed use building. The Site Plan agreement covers a range of municipal and agency requirements to be completed by the owner including items such as the provision of landscaping, parking, parkland conveyance, levies and lighting, all required prior to the issuance of a construction permit.



AERIAL MAP - CDM-005/23, CDM-7029

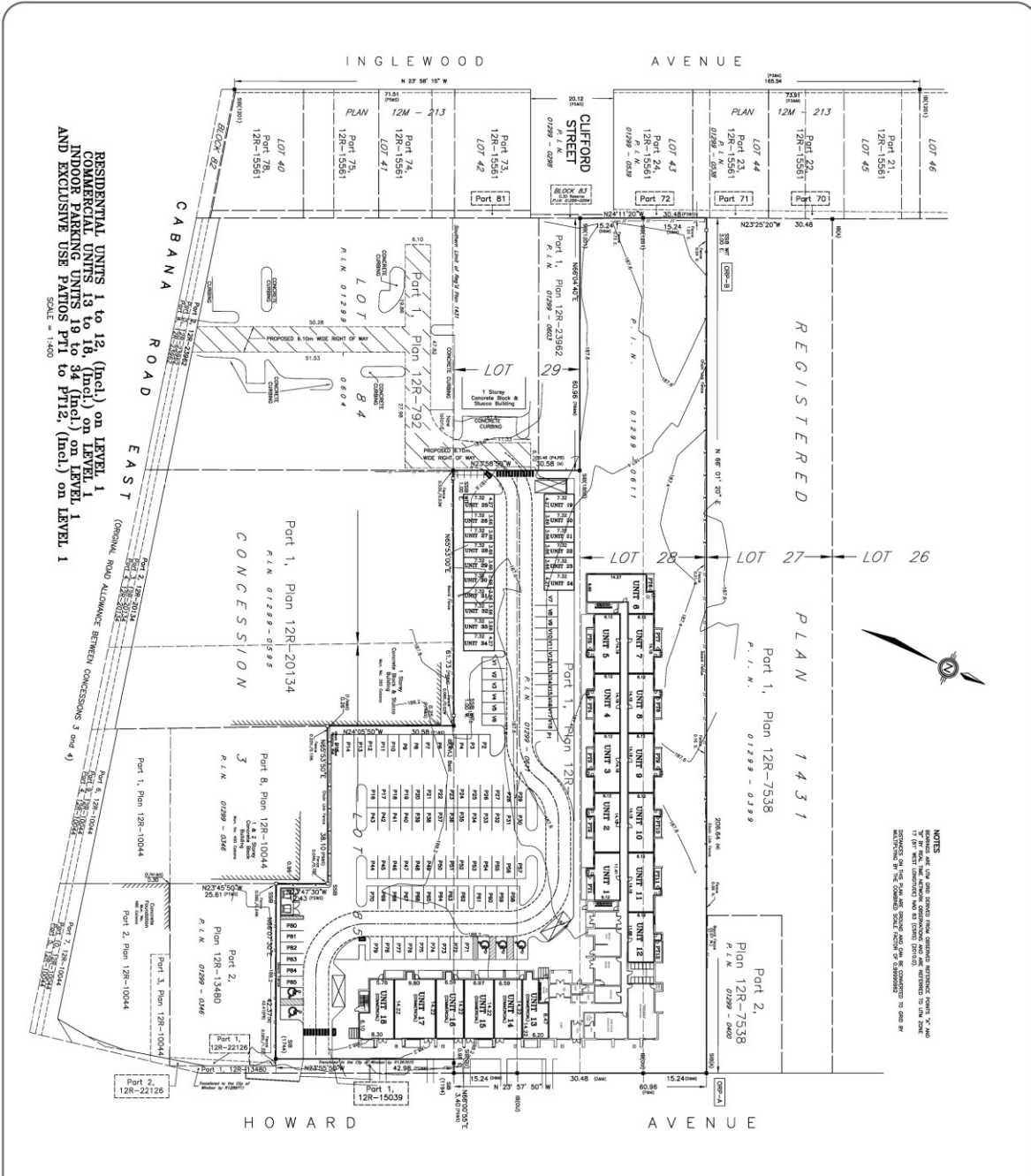


SUBJECT LANDS



Figure 1: Location Map

Plan of Condominium:

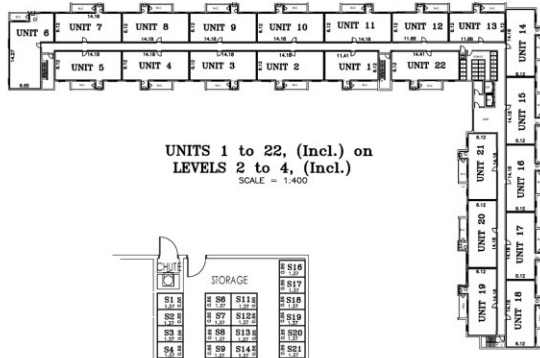


PLAN OF CONDOMINIUM

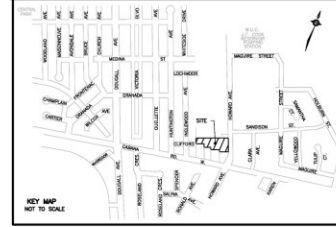
APPLICANT: 2481939 ONTARIO INC.
 ADDRESS: 3817 HOWARD AVENUE

SCALE: N.T.S.
 DATE: JUN. 2023
 FILE NO: CDM-005/23-1

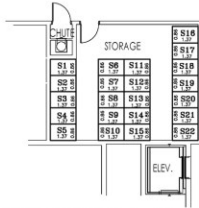
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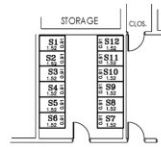
UNITS 1 to 22, (Incl.) on
LEVELS 2 to 4, (Incl.)
SCALE = 1:400



DRAFT PLAN OF CONDOMINIUM
OF
PART OF LOTS 28 and 29,
REGISTERED PLAN 1431
AND
PART OF LOT 85, CONCESSION 3
GEOGRAPHIC TOWNSHIP OF SANDWICH WEST
NOW IN THE
CITY OF WINDSOR
COUNTY OF ESSEX, ONTARIO
VERHAEGEN LAND SURVEYORS
SCALE = 1:400



STORAGE UNITS 1 to 22, (Incl.) on
LEVELS 2 to 4, (Incl.)
(NOT TO SCALE)



STORAGE UNITS 1 to 11, (Incl.)
on LEVEL 1
(NOT TO SCALE)

"METRIC" DISTANCES AND COORDINATES SHOWN ON THIS PLAN.
SEE ME METRES AND CAN BE CONVERTED TO FEET BY
DIVIDING BY 0.3048

LEGEND

ALL DIMENSIONS SHOWN THICKLY OR ARE FROM BARS UNLESS OTHERWISE NOTED.
S1 DENOTES 30mm x 20mm x 1.20m STANDARD IRON BAR
S2 DENOTES 30mm x 20mm x 1.20m SCOTCH STANDARD IRON BAR
S3 DENOTES 30mm x 20mm x 2.00m SCOTCH STANDARD IRON BAR
S4 DENOTES 30mm x 20mm x 2.00m SCOTCH IRON BAR
S5 DENOTES 40mm x 20mm x 2.00m SCOTCH IRON BAR
S6 DENOTES 40mm x 20mm x 2.00m SCOTCH IRON BAR
S7 DENOTES 40mm x 20mm x 2.00m SCOTCH IRON BAR
S8 DENOTES 40mm x 20mm x 2.00m SCOTCH IRON BAR
S9 DENOTES 40mm x 20mm x 2.00m SCOTCH IRON BAR
S10 DENOTES 40mm x 20mm x 2.00m SCOTCH IRON BAR
S11 DENOTES 40mm x 20mm x 2.00m SCOTCH IRON BAR
S12 DENOTES 40mm x 20mm x 2.00m SCOTCH IRON BAR
S13 DENOTES 40mm x 20mm x 2.00m SCOTCH IRON BAR
S14 DENOTES 40mm x 20mm x 2.00m SCOTCH IRON BAR
S15 DENOTES 40mm x 20mm x 2.00m SCOTCH IRON BAR
S16 DENOTES 40mm x 20mm x 2.00m SCOTCH IRON BAR
S17 DENOTES 40mm x 20mm x 2.00m SCOTCH IRON BAR
S18 DENOTES 40mm x 20mm x 2.00m SCOTCH IRON BAR
S19 DENOTES 40mm x 20mm x 2.00m SCOTCH IRON BAR
S20 DENOTES 40mm x 20mm x 2.00m SCOTCH IRON BAR
S21 DENOTES 40mm x 20mm x 2.00m SCOTCH IRON BAR
S22 DENOTES 40mm x 20mm x 2.00m SCOTCH IRON BAR
D1 DENOTES 30mm x 20mm STEEL PIPE
D2 DENOTES 40mm x 20mm STEEL PIPE
D3 DENOTES 50mm x 25mm STEEL PIPE
D4 DENOTES 60mm x 30mm STEEL PIPE
D5 DENOTES 75mm x 38mm STEEL PIPE
D6 DENOTES 90mm x 45mm STEEL PIPE
D7 DENOTES 100mm x 50mm STEEL PIPE
D8 DENOTES 125mm x 63mm STEEL PIPE
D9 DENOTES 150mm x 75mm STEEL PIPE
D10 DENOTES 175mm x 87.5mm STEEL PIPE
D11 DENOTES 200mm x 100mm STEEL PIPE
D12 DENOTES 225mm x 112.5mm STEEL PIPE
D13 DENOTES 250mm x 125mm STEEL PIPE
D14 DENOTES 275mm x 137.5mm STEEL PIPE
D15 DENOTES 300mm x 150mm STEEL PIPE
D16 DENOTES 325mm x 162.5mm STEEL PIPE
D17 DENOTES 350mm x 175mm STEEL PIPE
D18 DENOTES 375mm x 187.5mm STEEL PIPE
D19 DENOTES 400mm x 200mm STEEL PIPE
D20 DENOTES 425mm x 212.5mm STEEL PIPE
D21 DENOTES 450mm x 225mm STEEL PIPE
D22 DENOTES 475mm x 237.5mm STEEL PIPE
D23 DENOTES 500mm x 250mm STEEL PIPE
D24 DENOTES 525mm x 262.5mm STEEL PIPE
D25 DENOTES 550mm x 275mm STEEL PIPE
D26 DENOTES 575mm x 287.5mm STEEL PIPE
D27 DENOTES 600mm x 300mm STEEL PIPE
D28 DENOTES 625mm x 312.5mm STEEL PIPE
D29 DENOTES 650mm x 325mm STEEL PIPE
D30 DENOTES 675mm x 337.5mm STEEL PIPE
D31 DENOTES 700mm x 350mm STEEL PIPE
D32 DENOTES 725mm x 362.5mm STEEL PIPE
D33 DENOTES 750mm x 375mm STEEL PIPE
D34 DENOTES 775mm x 387.5mm STEEL PIPE
D35 DENOTES 800mm x 400mm STEEL PIPE
D36 DENOTES 825mm x 412.5mm STEEL PIPE
D37 DENOTES 850mm x 425mm STEEL PIPE
D38 DENOTES 875mm x 437.5mm STEEL PIPE
D39 DENOTES 900mm x 450mm STEEL PIPE
D40 DENOTES 925mm x 462.5mm STEEL PIPE
D41 DENOTES 950mm x 475mm STEEL PIPE
D42 DENOTES 975mm x 487.5mm STEEL PIPE
D43 DENOTES 1000mm x 500mm STEEL PIPE
D44 DENOTES 1025mm x 512.5mm STEEL PIPE
D45 DENOTES 1050mm x 525mm STEEL PIPE
D46 DENOTES 1075mm x 537.5mm STEEL PIPE
D47 DENOTES 1100mm x 550mm STEEL PIPE
D48 DENOTES 1125mm x 562.5mm STEEL PIPE
D49 DENOTES 1150mm x 575mm STEEL PIPE
D50 DENOTES 1175mm x 587.5mm STEEL PIPE
D51 DENOTES 1200mm x 600mm STEEL PIPE
D52 DENOTES 1225mm x 612.5mm STEEL PIPE
D53 DENOTES 1250mm x 625mm STEEL PIPE
D54 DENOTES 1275mm x 637.5mm STEEL PIPE
D55 DENOTES 1300mm x 650mm STEEL PIPE
D56 DENOTES 1325mm x 662.5mm STEEL PIPE
D57 DENOTES 1350mm x 675mm STEEL PIPE
D58 DENOTES 1375mm x 687.5mm STEEL PIPE
D59 DENOTES 1400mm x 700mm STEEL PIPE
D60 DENOTES 1425mm x 712.5mm STEEL PIPE
D61 DENOTES 1450mm x 725mm STEEL PIPE
D62 DENOTES 1475mm x 737.5mm STEEL PIPE
D63 DENOTES 1500mm x 750mm STEEL PIPE
D64 DENOTES 1525mm x 762.5mm STEEL PIPE
D65 DENOTES 1550mm x 775mm STEEL PIPE
D66 DENOTES 1575mm x 787.5mm STEEL PIPE
D67 DENOTES 1600mm x 800mm STEEL PIPE
D68 DENOTES 1625mm x 812.5mm STEEL PIPE
D69 DENOTES 1650mm x 825mm STEEL PIPE
D70 DENOTES 1675mm x 837.5mm STEEL PIPE
D71 DENOTES 1700mm x 850mm STEEL PIPE
D72 DENOTES 1725mm x 862.5mm STEEL PIPE
D73 DENOTES 1750mm x 875mm STEEL PIPE
D74 DENOTES 1775mm x 887.5mm STEEL PIPE
D75 DENOTES 1800mm x 900mm STEEL PIPE
D76 DENOTES 1825mm x 912.5mm STEEL PIPE
D77 DENOTES 1850mm x 925mm STEEL PIPE
D78 DENOTES 1875mm x 937.5mm STEEL PIPE
D79 DENOTES 1900mm x 950mm STEEL PIPE
D80 DENOTES 1925mm x 962.5mm STEEL PIPE
D81 DENOTES 1950mm x 975mm STEEL PIPE
D82 DENOTES 1975mm x 987.5mm STEEL PIPE
D83 DENOTES 2000mm x 1000mm STEEL PIPE
D84 DENOTES 2025mm x 1012.5mm STEEL PIPE
D85 DENOTES 2050mm x 1025mm STEEL PIPE
D86 DENOTES 2075mm x 1037.5mm STEEL PIPE
D87 DENOTES 2100mm x 1050mm STEEL PIPE
D88 DENOTES 2125mm x 1062.5mm STEEL PIPE
D89 DENOTES 2150mm x 1075mm STEEL PIPE
D90 DENOTES 2175mm x 1087.5mm STEEL PIPE
D91 DENOTES 2200mm x 1100mm STEEL PIPE
D92 DENOTES 2225mm x 1112.5mm STEEL PIPE
D93 DENOTES 2250mm x 1125mm STEEL PIPE
D94 DENOTES 2275mm x 1137.5mm STEEL PIPE
D95 DENOTES 2300mm x 1150mm STEEL PIPE
D96 DENOTES 2325mm x 1162.5mm STEEL PIPE
D97 DENOTES 2350mm x 1175mm STEEL PIPE
D98 DENOTES 2375mm x 1187.5mm STEEL PIPE
D99 DENOTES 2400mm x 1200mm STEEL PIPE
D100 DENOTES 2425mm x 1212.5mm STEEL PIPE
D101 DENOTES 2450mm x 1225mm STEEL PIPE
D102 DENOTES 2475mm x 1237.5mm STEEL PIPE
D103 DENOTES 2500mm x 1250mm STEEL PIPE
D104 DENOTES 2525mm x 1262.5mm STEEL PIPE
D105 DENOTES 2550mm x 1275mm STEEL PIPE
D106 DENOTES 2575mm x 1287.5mm STEEL PIPE
D107 DENOTES 2600mm x 1300mm STEEL PIPE
D108 DENOTES 2625mm x 1312.5mm STEEL PIPE
D109 DENOTES 2650mm x 1325mm STEEL PIPE
D110 DENOTES 2675mm x 1337.5mm STEEL PIPE
D111 DENOTES 2700mm x 1350mm STEEL PIPE
D112 DENOTES 2725mm x 1362.5mm STEEL PIPE
D113 DENOTES 2750mm x 1375mm STEEL PIPE
D114 DENOTES 2775mm x 1387.5mm STEEL PIPE
D115 DENOTES 2800mm x 1400mm STEEL PIPE
D116 DENOTES 2825mm x 1412.5mm STEEL PIPE
D117 DENOTES 2850mm x 1425mm STEEL PIPE
D118 DENOTES 2875mm x 1437.5mm STEEL PIPE
D119 DENOTES 2900mm x 1450mm STEEL PIPE
D120 DENOTES 2925mm x 1462.5mm STEEL PIPE
D121 DENOTES 2950mm x 1475mm STEEL PIPE
D122 DENOTES 2975mm x 1487.5mm STEEL PIPE
D123 DENOTES 3000mm x 1500mm STEEL PIPE
D124 DENOTES 3025mm x 1512.5mm STEEL PIPE
D125 DENOTES 3050mm x 1525mm STEEL PIPE
D126 DENOTES 3075mm x 1537.5mm STEEL PIPE
D127 DENOTES 3100mm x 1550mm STEEL PIPE
D128 DENOTES 3125mm x 1562.5mm STEEL PIPE
D129 DENOTES 3150mm x 1575mm STEEL PIPE
D130 DENOTES 3175mm x 1587.5mm STEEL PIPE
D131 DENOTES 3200mm x 1600mm STEEL PIPE
D132 DENOTES 3225mm x 1612.5mm STEEL PIPE
D133 DENOTES 3250mm x 1625mm STEEL PIPE
D134 DENOTES 3275mm x 1637.5mm STEEL PIPE
D135 DENOTES 3300mm x 1650mm STEEL PIPE
D136 DENOTES 3325mm x 1662.5mm STEEL PIPE
D137 DENOTES 3350mm x 1675mm STEEL PIPE
D138 DENOTES 3375mm x 1687.5mm STEEL PIPE
D139 DENOTES 3400mm x 1700mm STEEL PIPE
D140 DENOTES 3425mm x 1712.5mm STEEL PIPE
D141 DENOTES 3450mm x 1725mm STEEL PIPE
D142 DENOTES 3475mm x 1737.5mm STEEL PIPE
D143 DENOTES 3500mm x 1750mm STEEL PIPE
D144 DENOTES 3525mm x 1762.5mm STEEL PIPE
D145 DENOTES 3550mm x 1775mm STEEL PIPE
D146 DENOTES 3575mm x 1787.5mm STEEL PIPE
D147 DENOTES 3600mm x 1800mm STEEL PIPE
D148 DENOTES 3625mm x 1812.5mm STEEL PIPE
D149 DENOTES 3650mm x 1825mm STEEL PIPE
D150 DENOTES 3675mm x 1837.5mm STEEL PIPE
D151 DENOTES 3700mm x 1850mm STEEL PIPE
D152 DENOTES 3725mm x 1862.5mm STEEL PIPE
D153 DENOTES 3750mm x 1875mm STEEL PIPE
D154 DENOTES 3775mm x 1887.5mm STEEL PIPE
D155 DENOTES 3800mm x 1900mm STEEL PIPE
D156 DENOTES 3825mm x 1912.5mm STEEL PIPE
D157 DENOTES 3850mm x 1925mm STEEL PIPE
D158 DENOTES 3875mm x 1937.5mm STEEL PIPE
D159 DENOTES 3900mm x 1950mm STEEL PIPE
D160 DENOTES 3925mm x 1962.5mm STEEL PIPE
D161 DENOTES 3950mm x 1975mm STEEL PIPE
D162 DENOTES 3975mm x 1987.5mm STEEL PIPE
D163 DENOTES 4000mm x 2000mm STEEL PIPE
D164 DENOTES 4025mm x 2012.5mm STEEL PIPE
D165 DENOTES 4050mm x 2025mm STEEL PIPE
D166 DENOTES 4075mm x 2037.5mm STEEL PIPE
D167 DENOTES 4100mm x 2050mm STEEL PIPE
D168 DENOTES 4125mm x 2062.5mm STEEL PIPE
D169 DENOTES 4150mm x 2075mm STEEL PIPE
D170 DENOTES 4175mm x 2087.5mm STEEL PIPE
D171 DENOTES 4200mm x 2100mm STEEL PIPE
D172 DENOTES 4225mm x 2112.5mm STEEL PIPE
D173 DENOTES 4250mm x 2125mm STEEL PIPE
D174 DENOTES 4275mm x 2137.5mm STEEL PIPE
D175 DENOTES 4300mm x 2150mm STEEL PIPE
D176 DENOTES 4325mm x 2162.5mm STEEL PIPE
D177 DENOTES 4350mm x 2175mm STEEL PIPE
D178 DENOTES 4375mm x 2187.5mm STEEL PIPE
D179 DENOTES 4400mm x 2200mm STEEL PIPE
D180 DENOTES 4425mm x 2212.5mm STEEL PIPE
D181 DENOTES 4450mm x 2225mm STEEL PIPE
D182 DENOTES 4475mm x 2237.5mm STEEL PIPE
D183 DENOTES 4500mm x 2250mm STEEL PIPE
D184 DENOTES 4525mm x 2262.5mm STEEL PIPE
D185 DENOTES 4550mm x 2275mm STEEL PIPE
D186 DENOTES 4575mm x 2287.5mm STEEL PIPE
D187 DENOTES 4600mm x 2300mm STEEL PIPE
D188 DENOTES 4625mm x 2312.5mm STEEL PIPE
D189 DENOTES 4650mm x 2325mm STEEL PIPE
D190 DENOTES 4675mm x 2337.5mm STEEL PIPE
D191 DENOTES 4700mm x 2350mm STEEL PIPE
D192 DENOTES 4725mm x 2362.5mm STEEL PIPE
D193 DENOTES 4750mm x 2375mm STEEL PIPE
D194 DENOTES 4775mm x 2387.5mm STEEL PIPE
D195 DENOTES 4800mm x 2400mm STEEL PIPE
D196 DENOTES 4825mm x 2412.5mm STEEL PIPE
D197 DENOTES 4850mm x 2425mm STEEL PIPE
D198 DENOTES 4875mm x 2437.5mm STEEL PIPE
D199 DENOTES 4900mm x 2450mm STEEL PIPE
D200 DENOTES 4925mm x 2462.5mm STEEL PIPE
D201 DENOTES 4950mm x 2475mm STEEL PIPE
D202 DENOTES 4975mm x 2487.5mm STEEL PIPE
D203 DENOTES 5000mm x 2500mm STEEL PIPE
D204 DENOTES 5025mm x 2512.5mm STEEL PIPE
D205 DENOTES 5050mm x 2525mm STEEL PIPE
D206 DENOTES 5075mm x 2537.5mm STEEL PIPE
D207 DENOTES 5100mm x 2550mm STEEL PIPE
D208 DENOTES 5125mm x 2562.5mm STEEL PIPE
D209 DENOTES 5150mm x 2575mm STEEL PIPE
D210 DENOTES 5175mm x 2587.5mm STEEL PIPE
D211 DENOTES 5200mm x 2600mm STEEL PIPE
D212 DENOTES 5225mm x 2612.5mm STEEL PIPE
D213 DENOTES 5250mm x 2625mm STEEL PIPE
D214 DENOTES 5275mm x 2637.5mm STEEL PIPE
D215 DENOTES 5300mm x 2650mm STEEL PIPE
D216 DENOTES 5325mm x 2662.5mm STEEL PIPE
D217 DENOTES 5350mm x 2675mm STEEL PIPE
D218 DENOTES 5375mm x 2687.5mm STEEL PIPE
D219 DENOTES 5400mm x 2700mm STEEL PIPE
D220 DENOTES 5425mm x 2712.5mm STEEL PIPE
D221 DENOTES 5450mm x 2725mm STEEL PIPE
D222 DENOTES 5475mm x 2737.5mm STEEL PIPE
D223 DENOTES 5500mm x 2750mm STEEL PIPE
D224 DENOTES 5525mm x 2762.5mm STEEL PIPE
D225 DENOTES 5550mm x 2775mm STEEL PIPE
D226 DENOTES 5575mm x 2787.5mm STEEL PIPE
D227 DENOTES 5600mm x 2800mm STEEL PIPE
D228 DENOTES 5625mm x 2812.5mm STEEL PIPE
D229 DENOTES 5650mm x 2825mm STEEL PIPE
D230 DENOTES 5675mm x 2837.5mm STEEL PIPE
D231 DENOTES 5700mm x 2850mm STEEL PIPE
D232 DENOTES 5725mm x 2862.5mm STEEL PIPE
D233 DENOTES 5750mm x 2875mm STEEL PIPE
D234 DENOTES 5775mm x 2887.5mm STEEL PIPE
D235 DENOTES 5800mm x 2900mm STEEL PIPE
D236 DENOTES 5825mm x 2912.5mm STEEL PIPE
D237 DENOTES 5850mm x 2925mm STEEL PIPE
D238 DENOTES 5875mm x 2937.5mm STEEL PIPE
D239 DENOTES 5900mm x 2950mm STEEL PIPE
D240 DENOTES 5925mm x 2962.5mm STEEL PIPE
D241 DENOTES 5950mm x 2975mm STEEL PIPE
D242 DENOTES 5975mm x 2987.5mm STEEL PIPE
D243 DENOTES 6000mm x 3000mm STEEL PIPE
D244 DENOTES 6025mm x 3012.5mm STEEL PIPE
D245 DENOTES 6050mm x 3025mm STEEL PIPE
D246 DENOTES 6075mm x 3037.5mm STEEL PIPE
D247 DENOTES 6100mm x 3050mm STEEL PIPE
D248 DENOTES 6125mm x 3062.5mm STEEL PIPE
D249 DENOTES 6150mm x 3075mm STEEL PIPE
D250 DENOTES 6175mm x 3087.5mm STEEL PIPE
D251 DENOTES 6200mm x 3100mm STEEL PIPE
D252 DENOTES 6225mm x 3112.5mm STEEL PIPE
D253 DENOTES 6250mm x 3125mm STEEL PIPE
D254 DENOTES 6275mm x 3137.5mm STEEL PIPE
D255 DENOTES 6300mm x 3150mm STEEL PIPE
D256 DENOTES 6325mm x 3162.5mm STEEL PIPE
D257 DENOTES 6350mm x 3175mm STEEL PIPE
D258 DENOTES 6375mm x 3187.5mm STEEL PIPE
D259 DENOTES 6400mm x 3200mm STEEL PIPE
D260 DENOTES 6425mm x 3212.5mm STEEL PIPE
D261 DENOTES 6450mm x 3225mm STEEL PIPE
D262 DENOTES 6475mm x 3237.5mm STEEL PIPE
D263 DENOTES 6500mm x 3250mm STEEL PIPE
D264 DENOTES 6525mm x 3262.5mm STEEL PIPE
D265 DENOTES 6550mm x 3275mm STEEL PIPE
D266 DENOTES 6575mm x 3287.5mm STEEL PIPE
D267 DENOTES 6600mm x 3300mm STEEL PIPE
D268 DENOTES 6625mm x 3312.5mm STEEL PIPE
D269 DENOTES 6650mm x 3325mm STEEL PIPE
D270 DENOTES 6675mm x 3337.5mm STEEL PIPE
D271 DENOTES 6700mm x 3350mm STEEL PIPE
D272 DENOTES 6725mm x 3362.5mm STEEL PIPE
D273 DENOTES 6750mm x 3375mm STEEL PIPE
D274 DENOTES 6775mm x 3387.5mm STEEL PIPE
D275 DENOTES 6800mm x 3400mm STEEL PIPE
D276 DENOTES 6825mm x 3412.5mm STEEL PIPE
D277 DENOTES 6850mm x 3425mm STEEL PIPE
D278 DENOTES 6875mm x 3437.5mm STEEL PIPE
D279 DENOTES 6900mm x 3450mm STEEL PIPE
D280 DENOTES 6925mm x 3462.5mm STEEL PIPE
D281 DENOTES 6950mm x 3475mm STEEL PIPE
D282 DENOTES 6975mm x 3487.5mm STEEL PIPE
D283 DENOTES 7000mm x 3500mm STEEL PIPE
D284 DENOTES 7025mm x 3512.5mm STEEL PIPE
D285 DENOTES 7050mm x 3525mm STEEL PIPE
D286 DENOTES 7075mm x 3537.5mm STEEL PIPE
D287 DENOTES 7100mm x 3550mm STEEL PIPE
D288 DENOTES 7125mm x 3562.5mm STEEL PIPE
D289 DENOTES 7150mm x 3575mm STEEL PIPE
D290 DENOTES 7175mm x 3587.5mm STEEL PIPE
D291 DENOTES 7200mm x 3600mm STEEL PIPE
D292 DENOTES 7225mm x 3612.5mm STEEL PIPE
D293 DENOTES 7250mm x 3625mm STEEL PIPE
D294 DENOTES 7275mm x 3637.5mm STEEL PIPE
D295 DENOTES 7300mm x 3650mm STEEL PIPE
D296 DENOTES 7325mm x 3662.5mm STEEL PIPE
D297 DENOTES 7350mm x 3675mm STEEL PIPE
D298 DENOTES 7375mm x 3687.5mm STEEL PIPE
D299 DENOTES 7400mm x 3700mm STEEL PIPE
D300 DENOTES 7425mm x 3712.5mm STEEL PIPE
D301 DENOTES 7450mm x 3725mm STEEL PIPE
D302 DENOTES 7475mm x 3737.5mm STEEL PIPE
D303 DENOTES 7500mm x 3750mm STEEL PIPE
D304 DENOTES 7525mm x 3762.5mm STEEL PIPE
D305 DENOTES 7550mm x 3775mm STEEL PIPE
D306 DENOTES 7575mm x 3787.5mm STEEL PIPE
D307 DENOTES 7600mm x 3800mm STEEL PIPE
D308 DENOTES 7625mm x 3812.5mm STEEL PIPE
D309 DENOTES 7650mm x 3825mm STEEL PIPE
D310 DENOTES 7675mm x 3837.5mm STEEL PIPE
D311 DENOTES 7700mm x 3850mm STEEL PIPE
D312 DENOTES 7725mm x 3862.5mm STEEL PIPE
D313 DENOTES 7750mm x 3875mm STEEL PIPE
D314 DENOTES 7775mm x 3887.5mm STEEL PIPE
D315 DENOTES 7800mm x 3900mm STEEL PIPE
D316 DENOTES 7825mm x 3912.5mm STEEL PIPE
D317 DENOTES 7850mm x 3925mm STEEL PIPE
D318 DENOTES 7875mm x 3937.5mm STEEL PIPE
D319 DENOTES 7900mm x 3950mm STEEL PIPE
D320 DENOTES 7925mm x 3962.5mm STEEL PIPE
D321 DENOTES 7950mm x 3975mm STEEL PIPE
D322 DENOTES 7975mm x 3987.5mm STEEL PIPE
D323 DENOTES 8000mm x 4000mm STEEL PIPE
D324 DENOTES 8025mm x 4012.5mm STEEL PIPE
D325 DENOTES 8050mm x 4025mm STEEL PIPE
D326 DENOTES 8075mm x 4037.5mm STEEL PIPE
D327 DENOTES 8100mm x 4050mm STEEL PIPE
D328 DENOTES 8125mm x 4062.5mm STEEL PIPE
D329 DENOTES 8150mm x 4075mm STEEL PIPE
D330 DENOTES 8175mm x 4087.5mm STEEL PIPE
D331 DENOTES 8200mm x 4100mm STEEL PIPE
D332 DENOTES 8225mm x 4112.5mm STEEL PIPE
D333 DENOTES 8250mm x 4125mm STEEL PIPE
D334 DENOTES 8275mm x 4137.5mm STEEL PIPE
D335 DENOTES 8300mm x 4150mm STEEL PIPE
D336 DENOTES 8325mm x 4162.5mm STEEL PIPE
D337 DENOTES 8350mm x 4175mm STEEL PIPE
D338 DENOTES 8375mm x 4187.5mm STEEL PIPE
D339 DENOTES 8400mm x 4200mm STEEL PIPE
D340 DENOTES 8425mm x 4212.5mm STEEL PIPE
D341 DENOTES 8450mm x 4225mm STEEL PIPE
D342 DENOTES 8475mm x 4237.5mm STEEL PIPE
D343 DENOTES 8500mm x 4250mm STEEL PIPE
D344 DENOTES 8525mm x 4262.5mm STEEL PIPE
D345 DENOTES 8550mm x 4275mm STEEL PIPE
D346 DENOTES 8575mm x 4287.5mm STEEL PIPE
D347 DENOTES 8600mm x 4300mm STEEL PIPE
D348 DENOTES 8625mm x 4312.5mm STEEL PIPE
D349 DENOTES 8650mm x 4325mm STEEL PIPE
D350 DENOTES 8675mm x 4337.5mm STEEL PIPE
D351 DENOTES 8700mm x 4350mm STEEL PIPE
D352 DENOTES 8725mm x 4362.5mm STEEL PIPE
D353 DENOTES 8750mm x 4375mm STEEL PIPE
D354 DENOTES 8775mm x 4387.5mm STEEL PIPE
D355 DENOTES 8800mm x 4400mm STEEL PIPE
D356 DENOTES 8825mm x 4412.5mm STEEL PIPE
D357 DENOTES 8850mm x 4425mm STEEL PIPE
D358 DENOTES 8875mm x 4437.5mm STEEL PIPE
D359 DENOTES 8900mm x 4450mm STEEL PIPE
D360 DENOTES 8925mm x 4462.5mm STEEL PIPE
D361 DENOTES 8950mm x 4475mm STEEL PIPE
D362 DENOTES 8975mm x 4487.5mm STEEL PIPE
D363 DENOTES 9000mm x 4500mm STEEL PIPE
D364 DENOTES 9025mm x 4512.5mm STEEL PIPE
D365 DENOTES 9050mm x 4525mm STEEL PIPE
D366 DENOTES 9075mm x 4537.5mm STEEL PIPE
D367 DENOTES 9100mm x 4550mm STEEL PIPE
D368 DENOTES 9125mm x 4562.5mm STEEL PIPE
D369 DENOTES 9150mm x 4575mm STEEL PIPE
D370 DENOTES 9175mm x 4587.5mm STEEL PIPE
D371 DENOTES 9200mm x 4600mm STEEL PIPE
D372 DENOTES 9225mm x 4612.5mm STEEL PIPE
D373 DENOTES 9250mm x 4625mm STEEL PIPE
D374 DENOTES 9275mm x 4637.5mm STEEL PIPE
D375 DENOTES 9300mm x 4650mm STEEL PIPE
D376 DENOTES 9325mm x 4662.5mm STEEL PIPE
D377 DENOTES 9350mm x 4675mm STEEL PIPE
D378 DENOTES 9375mm x 4687.5mm STEEL PIPE
D379 DENOTES 9400mm x 4700mm STEEL PIPE
D380 DENOTES 9425mm x 4712.5mm STEEL PIPE
D381 DENOTES 9450mm x 4725mm STEEL PIPE
D382 DENOTES 9475mm x 4737.5mm STEEL PIPE
D383 DENOTES 9500mm x 4750mm STEEL PIPE
D384 DENOTES 9525mm x 4762.5mm STEEL PIPE
D385 DENOTES 9550mm x 4775mm STEEL PIPE
D386 DENOTES 9575mm x 4787.5mm STEEL PIPE
D387 DENOTES 9600mm x 4800mm STEEL PIPE
D388 DENOTES 9625mm x 4812.5mm STEEL PIPE
D389 DENOTES 9650mm x 4825mm STEEL PIPE
D390 DENOTES 9675mm x 4837.5mm STEEL PIPE
D391 DENOTES 9700mm x 4850mm STEEL PIPE
D392 DENOTES 9725mm x 4862.5mm STEEL PIPE
D393 DENOTES 9750mm x 4875mm STEEL PIPE
D394 DENOTES 9775mm x 4887.5mm STEEL PIPE
D395 DENOTES 9800mm x 4900mm STEEL PIPE
D396 DENOTES 9825mm x 4912.5mm STEEL PIPE
D397 DENOTES 9850mm x 4925mm STEEL PIPE
D398 DENOTES 9875mm x 4937.5mm STEEL PIPE
D399 DENOTES 9900mm x 4950mm STEEL PIPE
D400 DENOTES 9925mm x 4962.5mm STEEL PIPE
D401 DENOTES 9950mm x 4975mm STEEL PIPE
D402 DENOTES 9975mm x 4987.5mm STEEL PIPE
D403 DENOTES 10000mm x 5000mm STEEL PIPE
D404 DENOTES 10025mm x 5012.5mm STEEL PIPE
D405 DENOTES 10050mm x 5025mm STEEL PIPE
D406 DENOTES 10075mm x 5037.5mm STEEL PIPE
D407 DENOTES 10100mm x 5050mm STEEL PIPE
D408 DENOTES 10125mm x 5062.5mm STEEL PIPE
D409 DENOTES 10150mm x 5075mm STEEL PIPE
D410 DENOTES 10175mm x 5087.5mm STEEL PIPE
D411 DENOTES 10200mm x 5100mm STEEL PIPE
D412 DENOTES 10225mm x 5112.5mm STEEL PIPE
D413 DENOTES 10250mm x 5125mm STEEL PIPE
D414 DENOTES 10275mm x 5137.5mm STEEL PIPE
D415 DENOTES 10300mm x 5150mm STEEL PIPE
D416 DENOTES 10325mm x 5162.5mm STEEL PIPE
D417 DENOTES 10350mm x 5175mm STEEL PIPE
D418 DENOTES 10375mm x 5187.5mm STEEL PIPE
D419 DENOTES 10400mm x 5200mm STEEL PIPE
D420 DENOTES 10425mm x 5212.5mm STEEL PIPE
D421 DENOTES 10450mm x 5225mm STEEL PIPE
D422 DENOTES 10475mm x 5237.5mm STEEL PIPE
D423 DENOTES 10500mm x 5250mm STEEL PIPE
D424 DENOTES 10525mm x 5262.5mm STEEL PIPE
D425 DENOTES 10550mm x 5275mm STEEL PIPE
D426 DENOTES 10575mm x 5287.5mm STEEL PIPE
D427 DENOTES 10600mm x 5300mm STEEL PIPE
D428 DENOTES 10625mm x 5312.5mm STEEL PIPE
D429 DENOTES 10650mm x 5325mm STEEL PIPE
D430 DENOTES 10675mm x 5337.5mm STEEL PIPE
D431 DENOTES 10700mm x 5350mm STEEL PIPE
D432 DENOTES 10725mm x 5362.5mm STEEL PIPE
D433 DENOTES 10750mm x 5375mm STEEL PIPE
D434 DENOTES 10775mm x 5387.5mm STEEL PIPE
D435 DENOTES 10800mm x 5400mm STEEL PIPE
D436 DENOTES 10825mm x 5412.5mm STEEL PIPE
D437 DENOTES 10850mm x 5425mm STEEL PIPE
D438 DENOTES 10875mm x 5437.5mm STEEL PIPE
D439 DENOTES 10900mm x 5450mm STEEL PIPE
D440 DENOTES 10925mm x 5462.5mm STEEL PIPE
D441 DENOTES 10950mm x 5475mm STEEL PIPE
D442 DENOTES 10975mm x 5487.5mm STEEL PIPE
D443 DENOTES 11000mm x 550

Site Information:

OFFICIAL PLAN	ZONING	CURRENT USE	PREVIOUS USE
Special Policy Area 147	Commercial District 2.2 (CD2.2) & S 20(1)378	Combined Use Building (Under Construction)	Commercial/Vacant Parcel
LOT WIDTH	LOT DEPTH	AREA	SHAPE
103m	+/-120m	1.38ha	Irregular
All measurements are for the entire parcel and are approximate.			

Neighbourhood Characteristics:

The site is located between a residential neighbourhood to the north, mainly occupied by single detached residential dwellings and vacant land zoned HRD1.1, commercial uses to the west and south, and commercial and residential across Howard Avenue to the east. Specifically, the following land uses are present:

North

Single Family Dwellings on deep lots. The rear portion of the properties is zoned HRD1.1. The 'H' symbol places a hold on the development of the subject lands until certain conditions are satisfied.

East (across Howard Avenue)

Single family dwellings, New condominium development under construction, commercial and institutional uses

South: Commercial Uses

West: Single Detached Dwellings and commercial uses

The closest existing transit route is Central 3. The closest existing bus stop is located on Erie at Louis SE Corner. This bus stop is approximately 40 metres from this property, falling well within 400 metre walking distance guideline to a bus stop. The bus stop will be maintained by Council approved Transit Master Plan.

Discussion:

Planning Analysis:

Statutory Regulations:

Under Section 9 of the Condominium Act, an owner may request approval of a plan of condominium subject to Section 51 of the Planning Act (subdivisions). As such, the usual approval process for plans of subdivision is invoked, i.e. review by municipal and provincial agencies, public notification, draft plan approval, a condominium agreement and final registration.

The Condominium Act also provides that owners can be exempted from the above mentioned Planning Act provision if the approval authority (i.e. the City of Windsor) is of the opinion that "such exemption is appropriate in the circumstances". The reasons for exemptions are not specified, but usually applicants can be exempted if the following conditions are satisfied:

- (i) that all municipal requirements and conditions have been addressed (for example by an approved application for rezoning and/or site plan control); and
- (ii) that the building(s) is suitable for a condominium by virtue of design and amenities.

The proposed condominium complies with the above conditions.

Official Plan:

The subject property is designated "Special Policy Area 147" on Schedule A of the City of Windsor Official Plan. The designation is intended to specifically permit this use. The proposed development conforms to this policy.

The City of Windsor has policies in the Official Plan for approval of a plan of condominium with exemption under Section 9 of the Condominium Act. Applications for exemptions may be considered by Council if:

- (a) a residential building is constructed or a building permit for its construction has been issued;
- (b) the development has received site plan control approval; and
- (c) the development (new proposed units) do not contain any occupied residential rental units.

The proposed condominium complies with the above conditions.

Zoning By-Law

The property is zoned Commercial District (CD) 2.2 with a site specific provision to provide specific regulations to permit ground floor dwelling units, a 10m building setback from the northerly lot line, restricting the location of non residential uses on the site, and requiring privacy fencing along the northerly lot line. Building permits have been issued and construction of the building is nearing completion.

Risk Analysis:

N/A

Climate Change Risks

Climate Change Mitigation:

N/A

Climate Change Adaptation:

N/A

Financial Matters:

N/A

Consultations:

Municipal and agency requirements have been addressed and implemented through the site plan control approval process, and the registration of a site plan agreement. (File R006.20) registered as instrument CE1023406, on July 30, 2021)

Public Notice:

No public notification is required where exemptions are requested and the development is properly zoned for the permitted use.

Conclusion:

The application has been processed and evaluated with regard to both the Planning Act and the Condominium Act, as well as the City of Windsor Official Plan. It conforms to the City of Windsor Official Plan and complies with the zoning regulations contained in Bylaw 8600. Municipal requirements regarding this development have been addressed in the site plan control agreement. The draft plan of condominium is consistent with the approved site plan (File R006.20) registered as CE1023406, on July 30, 2021)

It is recommended that this application for approval be exempted from Section 51 of the Planning Act (per Section 9(3) of the Condominium Act). This means that the owner can proceed directly to registration following submission of an approved final plan of condominium.

Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Thom Hunt, City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP JM

Approvals:

Name	Title
Neil Robertson for Thom Hunt	City Planner / Executive Director, Planning & Development Services

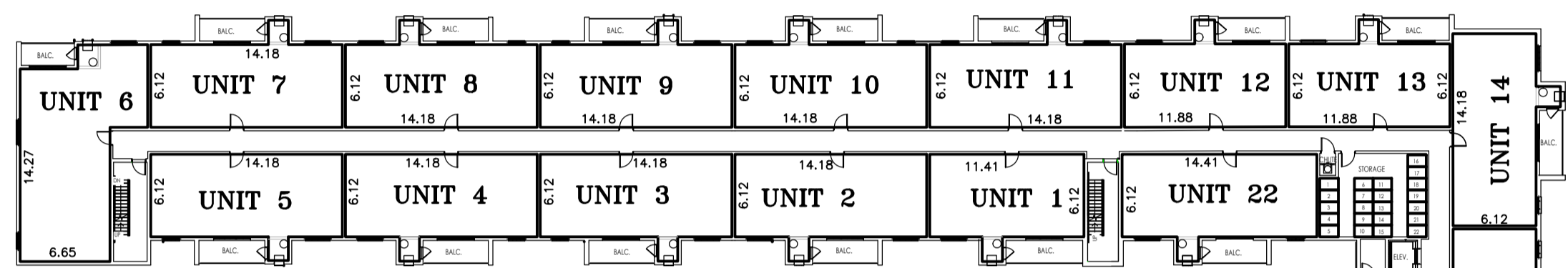
Wira Vendrasco	Deputy City Solicitor, Legal Services & Real Estate
Jelena Payne	Commissioner of Economic Development and Innovation
Joe Mancina	Chief Administrative Officer

Notifications:

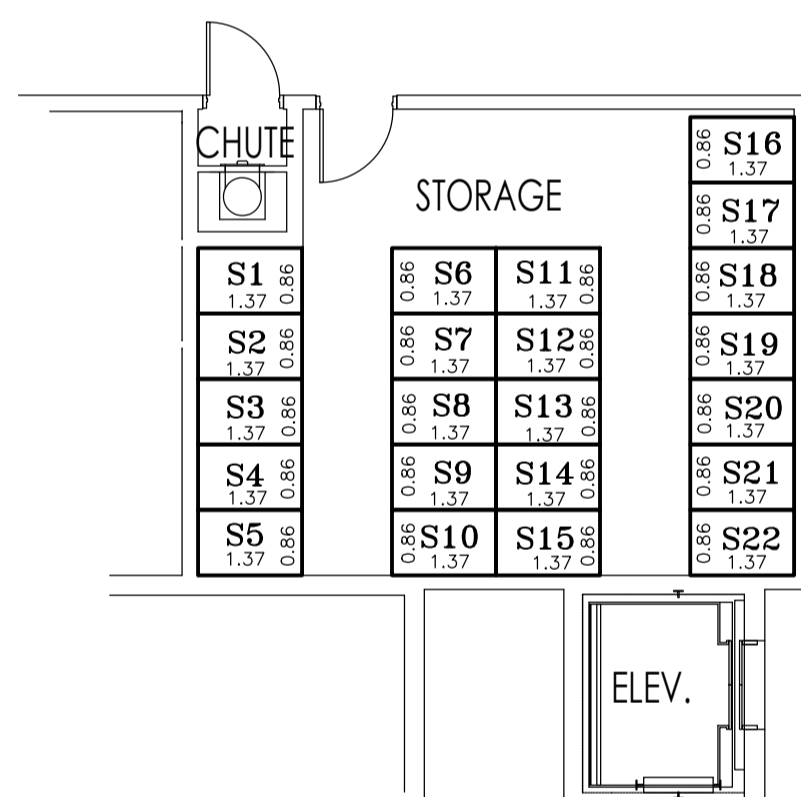
Name	Address	Email
2481939 Ontario Inc.	3070 Jefferson Boulevard, Windsor ON N8T 3G9	josh.burns@fortisgroup.ca
Mary-Ann Keefner	301-2510 Ouellette Avenue, Windsor ON N8X 1L4	mary-ann.keefner@shibleyrighton.com

Appendices:

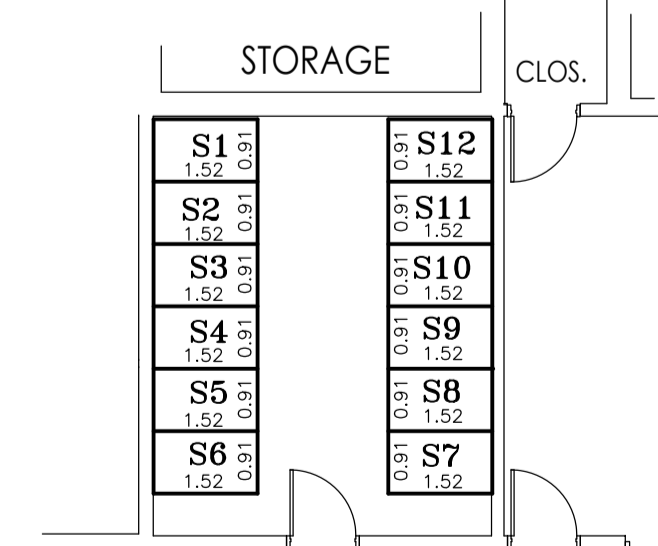
- 1 Proposed Draft Plan of Condominium



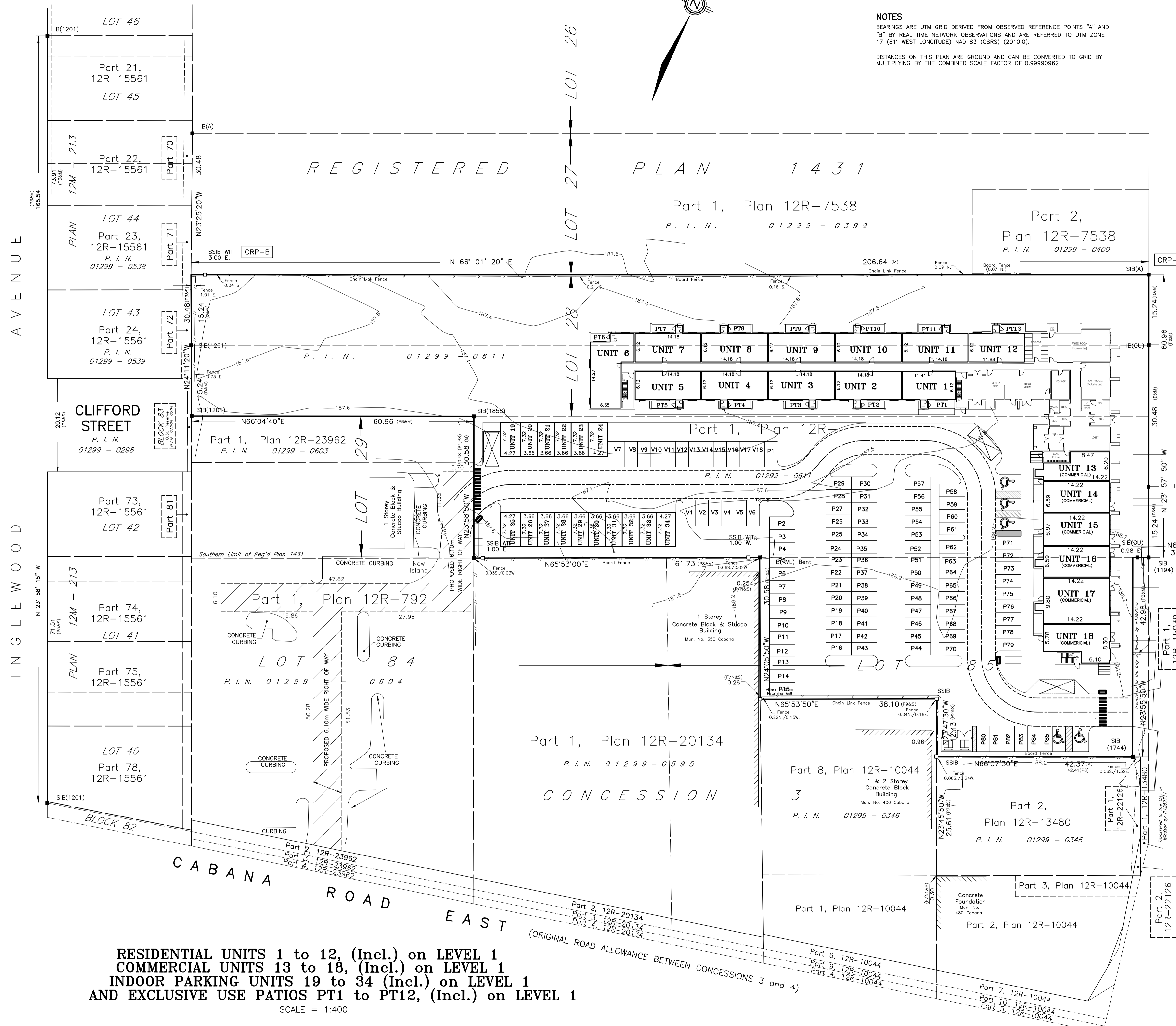
UNITS 1 to 22, (Incl.) on LEVELS 2 to 4, (Incl.)
SCALE = 1:400



STORAGE UNITS 1 to 22, (Incl.) on LEVELS 2 to 4, (Incl.)
(NOT TO SCALE)

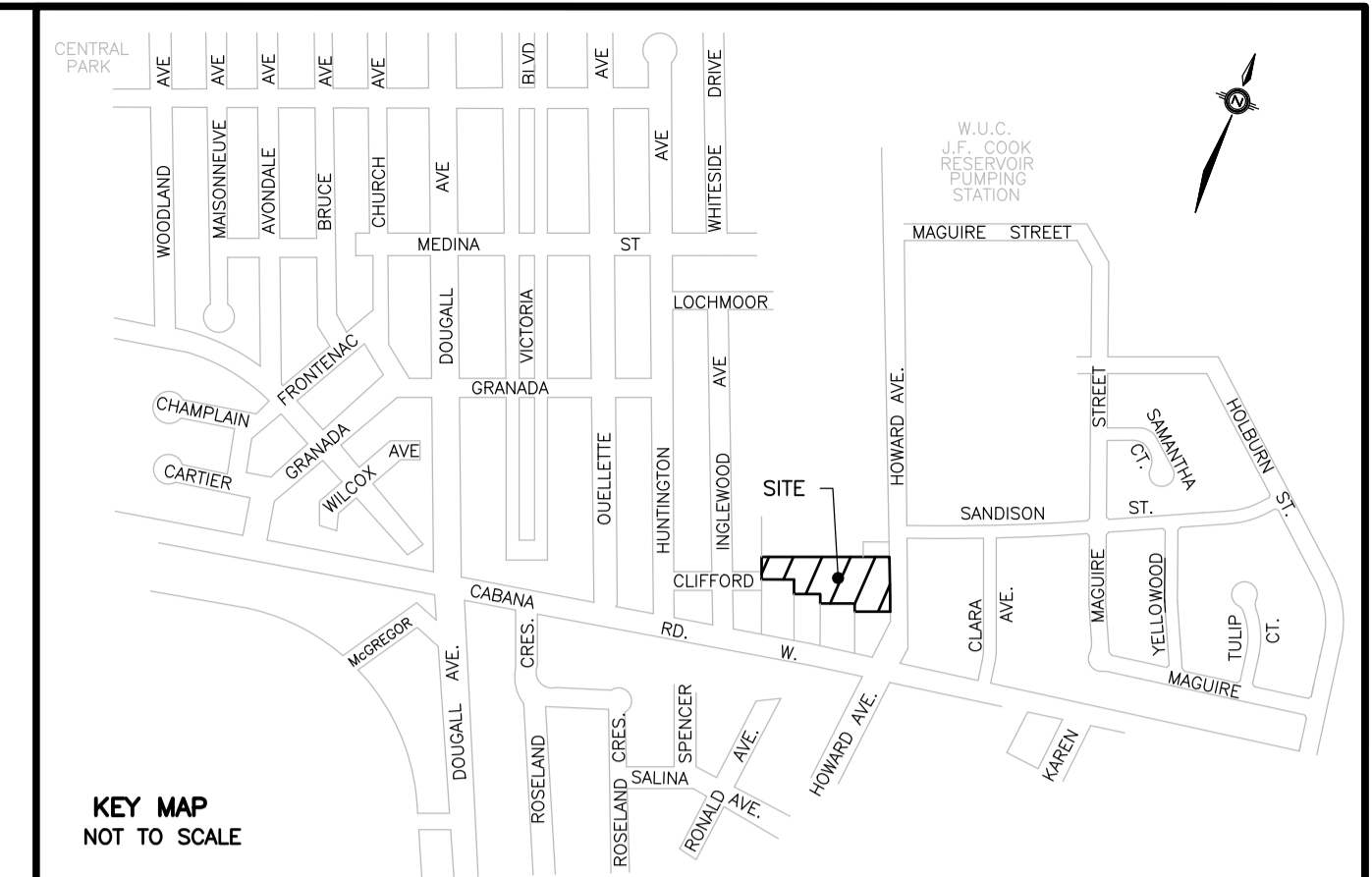


STORAGE UNITS 1 to 11, (Incl.) on LEVEL 1
(NOT TO SCALE)



RESIDENTIAL UNITS 1 to 12, (Incl.) on LEVEL 1
COMMERCIAL UNITS 13 to 18, (Incl.) on LEVEL 1
INDOOR PARKING UNITS 19 to 34 (Incl.) on LEVEL 1
AND EXCLUSIVE USE PATIOS PT1 to PT12, (Incl.) on LEVEL 1
SCALE = 1:400

NOTES
BEARINGS ARE UTM GRID DERIVED FROM OBSERVED REFERENCE POINTS "A" AND "B" BY REAL TIME NETWORK OBSERVATIONS AND ARE REFERRED TO UTM ZONE 17 (81° WEST LONGITUDE) NAD 83 (CSRS) (2010.0).
DISTANCES ON THIS PLAN ARE GROUND AND CAN BE CONVERTED TO GRID BY MULTIPLYING BY THE COMBINED SCALE FACTOR OF 0.99990962



DRAFT PLAN OF CONDOMINIUM OF PART OF LOTS 28 and 29, REGISTERED PLAN 1431 AND PART OF LOT 85, CONCESSION 3
GEOGRAPHIC TOWNSHIP OF SANDWICH WEST
NOW IN THE CITY OF WINDSOR
COUNTY OF ESSEX, ONTARIO
VERHAEGEN LAND SURVEYORS
SCALE = 1:400

"METRIC" DISTANCES AND COORDINATES SHOWN ON THIS PLAN ARE IN METRES AND CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048

- LEGEND**
- ALL MONUMENTS SHOWN THUSLY □ ARE IRON BARS (B) UNLESS OTHERWISE NOTED.
 - SB DENOTES 25mm X 25mm X 1.22m STANDARD IRON BAR
 - SSB DENOTES 25mm X 25mm X 0.61m SHORT STANDARD IRON BAR
 - SB DENOTES 16mm X 16mm X 0.61m IRON BAR
 - SB DENOTES 19mm diameter X 0.61m ROUND IRON BAR
 - CP DENOTES CUT-CROSS
 - CP DENOTES 5mm X 50mm STEEL PIN
 - SM DENOTES SURVEY MONUMENT SET AND MARKED 1744
 - WT DENOTES WITNESS
 - SE DENOTES SET
 - ME DENOTES MEASURED
 - ORP DENOTES OBSERVED REFERENCE POINT
 - SSIB'S SHOWN ON THIS PLAN HAVE BEEN SET IN LIEU OF SIB'S WHERE THE POSSIBILITY THAT UNDERGROUND UTILITIES EXIST.
 - (S/P) DENOTES SET PROPORTIONALLY
 - (P) DENOTES REGISTERED PLAN 1431
 - (P2) DENOTES PLAN 12R-20134
 - (P3) DENOTES PLAN 12R-15039
 - (P4) DENOTES PLAN 12R-7538
 - (P5) DENOTES REGISTERED PLAN 12M-213
 - (P6) DENOTES PLAN 12R-23962
 - (P7) DENOTES PLAN 12R-13480
 - (P7A) DENOTES FIELD NOTE BY (1744) DATED JUNE 9, 2003, (PROJECT: 4-21650)
 - (P7A1) DENOTES FIELD NOTE BY (1744) DATED MAY 6, 1994, (PROJECT: 4-11168)
 - (1744) DENOTES VERHAEGEN LAND SURVEYORS
 - (1201) DENOTES CLARKE SURVEYORS INC., O.L.S.
 - (A) DENOTES C.G.R. ARMSTRONG, O.L.S.
 - (1658) DENOTES TOTAL TECH SURVEYING INC., O.L.S.
 - (MVA) DENOTES VERHAEGEN AND BEZARE LIMITED, O.L.S.
 - (O) DENOTES ORIGIN UNKNOWN
 - (P1) DENOTES PLAN 12R-20134
 - (P2) DENOTES PLAN 12R-15039
 - (P3) DENOTES PLAN 12R-7538
 - (P4) DENOTES REGISTERED PLAN 12M-213
 - (P5) DENOTES PLAN 12R-23962
 - (P6) DENOTES PLAN 12R-13480
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 - (1658) DENOTES TOTAL TECH SURVEYING INC., O.L.S.
 - (MVA) DENOTES VERHAEGEN AND BEZARE LIMITED, O.L.S.

ADDITIONAL INFORMATION REQUIRED UNDER THE CONDOMINIUM ACT, 1998 AND SECTION 51 (17) OF THE PLANNING ACT, 1990:

- A) ON DRAFT PLAN
- B) ON DRAFT PLAN
- C) ON DRAFT PLAN
- D) RESIDENTIAL PARKING
- E) CONDOMINIUM UNITS
- F) ON DRAFT PLAN
- G) ON DRAFT PLAN
- H) MUNICIPAL WATER
- I) C/W
- J) ALL MUNICIPAL SERVICES PROVIDED
- K) ON DRAFT PLAN

SITE INFORMATION

LEVEL	BUILDING 'A'		BUILDING 'B'		BUILDING 'C'	
	NUMBER OF UNITS PER LEVEL	NUMBER OF UNITS PER LEVEL	NUMBER OF UNITS PER LEVEL	NUMBER OF UNITS PER LEVEL	NUMBER OF UNITS PER LEVEL	NUMBER OF UNITS PER LEVEL
LEVEL 1	6	12	16	85	5	18
LEVEL 2	-	22	-	-	-	-
LEVEL 3	-	22	-	-	-	-
LEVEL 4	-	22	-	-	-	-
TOTAL	6	78	16	85	5	18

OWNER'S AUTHORIZATION
I HEREBY AUTHORIZE THE FILING OF THIS DRAFT PLAN FOR APPROVAL.
2481839 ONTARIO INC.
TYLER BECKETT, VICE-PRESIDENT

DATE: JUNE 8, 2023
I HAVE THE AUTHORITY TO BIND THE CORPORATION

SURVEYOR'S CERTIFICATE
THE BOUNDARIES OF THE LANDS TO BE SUBDIVIDED AND THEIR RELATIONSHIP TO THE ADJACENT LANDS HAVE BEEN ACCURATELY AND CORRECTLY SHOWN.

DATE: JUNE 8, 2023
ANDREW S. MANTHA
ONTARIO LAND SURVEYOR

VERHAEGEN SURVEYING
LAND SURVEYORS MAPPING GIS
A Division of
J. D. Barnes Limited
944 OTTAWA STREET, WINDSOR, ON, N8X 2E1
T: (519) 258-1772 F: (519) 258-1791 www.jdbarnes.com
DRAWN BY: NMG CHECKED BY: A.S.M. REFERENCE NO.: 19-47-362-01A
CAD Date: June 8, 2023 2:23 PM
PAGE 27 of 464
FILE: E-1431-2
CAD File: 19-47-362-01B Draft Plan.dwg

Subject: Zoning By-law Amendment Application for the property at the southwest corner of Tecumseh Road West and Mark Avenue; Applicant: 2832765 Ontario Inc.; File No. Z-036/2022, ZNG/6897; Ward 10.

Reference:

Date to Council: July 4, 2023
Author: Justina Nwaesei, MCIP, RPP
Senior Planner – Subdivisions
519-255-6543, ext. 6165
jnwaesei@citywindsor.ca

Planning & Building Services
Report Date: June 9, 2023
Clerk's File #: Z/14571

To: Mayor and Members of City Council

Recommendation:

- I. THAT Zoning By-law 8600 **BE AMENDED** by changing the zoning of the land located on the southwest corner of Tecumseh Road West and Mark Avenue, described as Lots 131 and 132, Registered Plan 1342 [PIN 01208-0145 LT], from CD2.1 to CD2.2, subject to the following site-specific zoning provisions:

“474 SOUTHWEST CORNER OF TECUMSEH ROAD WEST AND MARK AVENUE

For the land comprising Lots 131 and 132, Registered Plan 1342 [PIN 01208-0145 LT], a *Multiple Dwelling with 11 or more dwelling units* shall be an additional permitted use and the following shall apply:

1. The provisions in section 15.2.5, save and except for sections 15.2.5.10 and 15.2.5.15
2. Building setback from an exterior lot line abutting Tecumseh Road West – minimum 5.0 m
3. Parking Area separation from the south lot line – minimum 1.2 m (to be maintained as a *landscaped open space*)
4. Parking - per Dwelling Unit – minimum 1 parking space per unit
5. Parking is prohibited in any front yard
[ZDM 4; ZNG/6897]”

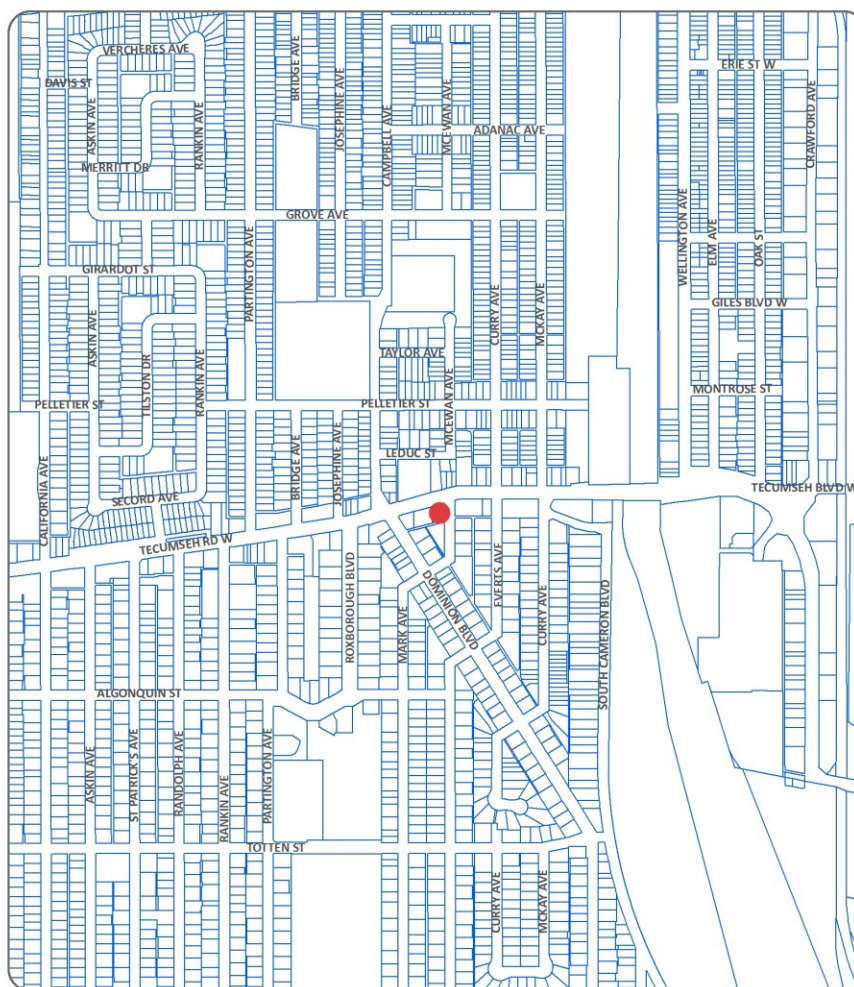
- II. THAT the Site Plan Approval Officer **BE DIRECTED** to incorporate the following, as required, in the Site Plan Approval and the Site Plan Agreement for the proposed development on the subject land:
 - a) Sanitary Sampling Manhole;
 - b) Corner Cut-Off - 4.6 m x 4.6 m (15' x 15') corner cut-off required at the intersection of Tecumseh Road West and Mark Avenue per City of Windsor Standard;

- c) Land Conveyance – convey approximately 3.5 metres along the entire Tecumseh Road West frontage of the subject land for road widening;
- d) Sanitary Servicing Study - retain a Consulting Engineer to provide a detailed servicing study report;
- e) Curbs and Gutters – construct new concrete curb and gutter along the entire Mark Avenue frontage of the subject land;
- f) Parkland dedication of 5% (cash-in-lieu) of the subject vacant parcel;
- g) Tree Survey to determine appropriate tree preservation for the site
- h) Noise mitigation measures as recommended in a Noise Impact Study that will be submitted at the time of Site Plan Control application;
- i) Railway Warning clause (s. 4.7.1.9 (d), South Cameron Planning Area, OP Vol. II);
- j) Enbridge Gas minimum separation requirements;
- k) Adequate clearance from existing ENWIN's pole lines and power lines;
- l) Canada Post multi-unit policy; and
- m) Record of Site Condition.

Executive Summary: N/A

Background:

1. KEY MAP



KEY MAP - Z-036/22, ZNG/6897



● SUBJECT LANDS

2. APPLICATION INFORMATION

LOCATION: 0 Tecumseh Road West (southwest corner of Tecumseh Road West and Mark Avenue)

APPLICANT: 2832765 ONTARIO INC. – GIOVANNI AND RITA MICELI (C/O GIOVANNI MICELI)

AGENT: LASSALINE PLANNING CONSULTANTS (C/O JACKIE LASSALINE)

REGISTERED OWNER: SAME AS APPLICANT

PROPOSAL: The applicant is requesting an amendment to Zoning By-law 8600 for the land located on the southwest corner of Tecumseh Rd. W. and Mark Avenue, described as Lots 131 and 132, Registered Plan 1342, [PIN 01208 0145 LT].

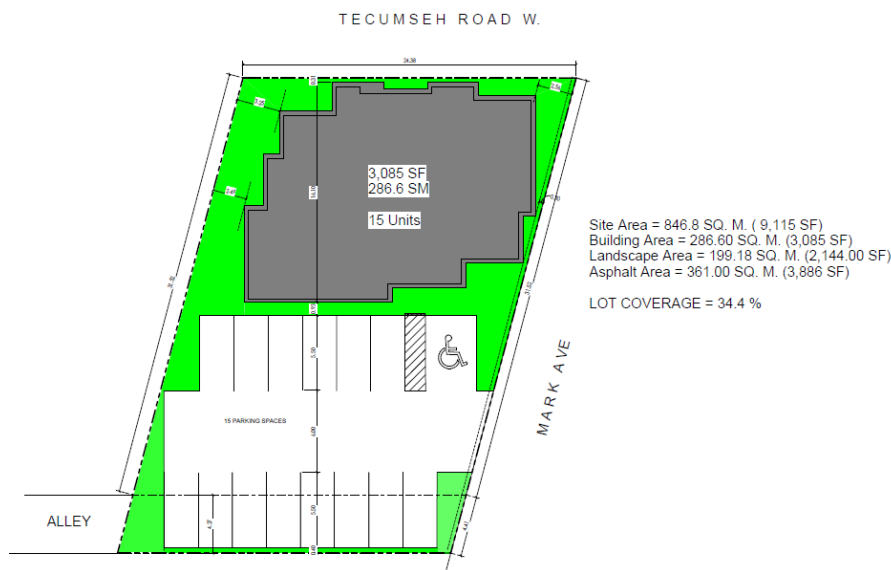
The subject land is designated Commercial Corridor on Schedule SC-1: Development Concept, South Cameron Planning Area, OP Vol. 2 and zoned Commercial District 2.1 (CD2.1) in Zoning By-law 8600. The applicant proposes to change the zoning of the property from CD2.1 to CD2.2 to facilitate the proposed development of a 4-storey apartment building with 15 dwelling units and a parking area with 14 regular parking spaces, one (1) Type A accessible parking space, and 10 bicycle parking spaces. The proposed parking area extends into the abutting east/west alley and access is proposed from Mark Avenue.

The subject amendment includes a request for the following site-specific zoning provisions:

1. To add Multiple dwelling as additional permitted use;
2. To reduce required minimum amenity area per dwelling unit from 12.0m² to 0m²; and
3. To reduce required minimum number of parking spaces from 19 spaces to 15 spaces.

Note: Section 4.7.7.2 of OP Vol 2 states that Commercial Corridors designated on Schedule SC-1 shall be subject to the policies contained within Volume 1 –The Primary Plan, and the special policy area provisions of Volume 2, where appropriate. OPA 159 changed the text “Commercial Corridor” in the Primary Plan (OP Vol.1) to “Mixed Use Corridor”; therefore, the proposed development shall be subject to the Mixed Use Corridor policies in OP Vol.1.

CONCEPT PLAN WITH LANDSCAPING DETAILS



The applicant submitted the following documents, which can be found on the city's website by copying this link: <https://www.citywindsor.ca/residents/planning/Land-Development/Development-Applications/current-development-applications/Pages/1609-Tecumseh-Road-West.aspx>

SUBMISSIONS BY APPLICANT:

- Zoning By-law Amendment Application Form
- Concept Development Plans (Site Plans and Floor Plans)
- Elevation Drawings
- Site Plan with proposed Landscaped Open Space
- Planning Rationale Report
- Parking Report
- Functional Servicing Report
- Civil Report – Site Removals & Erosion and Sediment Control Plan
- Energy Strategy
- PIN Sheet
- Topographic survey

3. SITE INFORMATION

3.1 Existing Site

OFFICIAL PLAN	ZONING & ZDM	CURRENT USE(S)	PREVIOUS USE(S)
COMMERCIAL CORRIDOR on Schedule SC-1: Development Concept, South Cameron Planning Area, OP Volume II.	COMMERCIAL DISTRICT 2.1 (CD2.1); ZDM 4	VACANT (<i>In 1980, a permit was issued to demolish existing old building known as 1609 Tecumseh Rd. W.</i>)	Unknown
FRONTAGE	DEPTH	AREA	SHAPE
24.38 m (along Tecumseh Rd W.)	31.52 (along Mark Avenue)	743 sq.m.	Parallelogram (4-sided)
Note: All measurements are in accordance with Topographic site survey dated April 5, 2022			

3.2 Additional Land (abutting rear alley) subject to closure and acquisition:

On November 9, 2022, City Council approved the closure of the abutting rear 4.27 m wide east/west alley and conveyance of the said alley to the owner of 0 Tecumseh Road W. (the subject property). See excerpts from council resolution (CR461/2022) below:

CR461/2022 DHSC 435

- I. That the 24.38 metre portion of the 4.27 metre wide east/west alley located immediately west of Mark Avenue and south of the property known municipally as 0 Tecumseh Road West (Roll No. 080-600-17600), and shown on Drawing No. CC-1814 attached hereto as Appendix "A", **BE ASSUMED** for subsequent closure;
- II. That the 24.38 metre portion of the 4.27 metre wide east/west alley located immediately west of Mark Avenue and south of the property known municipally as 0 Tecumseh Road West (Roll No. 080-600-17600), and shown on Drawing No. CC-1814 attached hereto as Appendix "A", **BE CLOSED AND CONVEYED** to the abutting property owner at 0 Tecumseh Road West (Roll No. 080-600-17600) and as necessary, in a manner deemed appropriate by the City Planner, subject to the following:
 - a. Easement, subject to there being accepted in the City's standard form and in accordance with the City's standard practice, be granted to:
 - i. Bell Canada to accommodate existing infrastructure;
 - ii. EnWin to accommodate existing overhead 16kV and 120/240 volt distribution, poles and down guy wires;
 - iii. MNSi to accommodate aerial plant on existing pole line; and
 - iv. The Corporation of the City of Windsor to accommodate existing circa 1955, 600.0 millimetre reinforced concrete storm sewer, to the satisfaction of the City Engineer.

Note: The subject alley is described as PART 1 on Reference Plan 12R-29399. Administration will prepare an alley closing by-law for Council's approval. Following Council's approval of the alley closing by-law, Administration will start the process of purchase and sale of the subject alley. If, and when, the applicant purchases the closed alley, the zoning boundary provision in section 1.20.40(c) of By-law 8600 will apply.

4. REZONING MAP



PART OF ZONING DISTRICT MAP 4

N.T.S.

REQUESTED ZONING AMENDMENT

Applicant: 2832765 Ontario Inc.



SUBJECT LANDS

PLANNING & BUILDING DEPARTMENT



DATE : MAY, 2023
FILE NO. : Z-036/22, ZNG/6897

5. NEIGHBOURHOOD CHARACTERISTICS

The neighbourhood is made up of a mix of uses (low and medium density residential, institutional, and commercial uses) fronting on Tecumseh Road West and backing onto mature residential neighbourhoods with low-density residential developments. There is a railyard on the east side of South Cameron Boulevard, roughly 320m from the subject land.



NEIGHBOURHOOD MAP - Z-036/22, ZNG/6897



SUBJECT LANDS

SURROUNDING LAND USE

North: Public R.O.W (Tecumseh Rd. W.); Clothing Store, Place of Worship (Redeemed Christian Church); Windsor Medical Clinic & family Health Centre (Clinic, Pharmacy and Chiropractor's office); Single unit dwellings; and Automobile Sales lot.

South: Public lane (4.27m wide east/west alley, shown on the concept plan); Residential uses (low profile residential developments, predominantly, single unit dwellings).

East: Public R.O.W (Mark Avenue); *Commercial uses* (Reliance Automobile Sales lot, Printing Shop, Piruzza Commercial Plaza – Piruzza Woodcraft, Southwest Ontario Aboriginal Health Access Centre, Bulls Eye Pizza).

West: Commercial uses (Retail Store – Used Appliances for less; Personal Service shop – First Choice Haircutters and Zee Tailor, and Automobile Sales lot)

Nearby Amenities:

- Schools within 1.0 km distance from the subject land
 - West Gate Public School (approx. 500 m distance);
 - West View Freedom Academy (Approx. 900 m distance); and
 - Ecole Elementaire Catholique Saint Edmond (approx. 900 m distance).
- Other nearby Schools
 - Assumption College Catholic Middle School (approx. 1.5 km distance).
- Public Libraries within 1.0 km distance from the subject land
 - Bridgeview Public Library
- Municipal Parks and Trails within 1.0 km distance from the subject land
 - Bridgeview Park and Superior Park (approx. 500 m distance); and
 - Shipping Back Trails, Oak/Elm Park and Girardot Park (approx. 950 m distance).
- Nearby Community Centre to the subject land
 - College Avenue Community Centre, approximately 3km distance

Attached to this report as **Appendix A** are site photos taken on June 6, 2023. The site photos show some of the surrounding land uses and the character of the neighbourhood.

MUNICIPAL INFRASTRUCTURE

- The City's records show the following municipal sewers are available to service the subject land: (i) a 300 mm diameter concrete pipe combined sewer located on the north side of Tecumseh Road West within the R.O.W, and (ii) a 600 mm diameter RCP storm sewer within the abutting east/west alley.
- There is an existing 300 mm municipal watermain located on the south side of Tecumseh Road West, in front of the subject land, and a 150mm municipal watermain located on the east side of Mark Avenue across from the subject land.
- Municipal fire hydrants are available at the southeast corner of Tecumseh and Mark, and within the nearby streets.
- Overhead pole lines with streetlights are available within Tecumseh Road West right-of-way and in some areas of the subject neighbourhood, but not on Mark Avenue.
- Overhead pole lines are available on Mark Avenue.
- There are bike lanes on Campbell Avenue, west of the subject land.

- There are concrete sidewalks, curbs and gutters on both sides of the street along Tecumseh Road West, Mark Avenue, and Campbell Avenue. Everts Avenue has sidewalk on one side and no curbs & gutter.
- Transit Windsor Bus routes (Central 3, Dominion 5 and Massey School Route Extras) are available to service the subject land and neighbourhood. Central transit buses run east-west along Tecumseh Road West, while Dominion 5 and Massey School Route Extras run north-south along Campbell Road less than 100m from the subject land.
- The closest existing bus stop to the subject land is located on Tecumseh Road West, at the southeast corner of Tecumseh and Campbell. The bus stop is approximately 50 metres away from the subject land.

Tecumseh Road West is classified as Class II Arterial Road and Mark Avenue is classified as local road.

Discussion:

PLANNING ANALYSIS:

1. PLANNING ACT

Section 2 – “The Minister, the council of a municipality, a local board, a planning board and the Tribunal, in carrying out their responsibilities under this Act, shall have regard to, among other matters, matters of provincial interest such as,

- (h) the orderly development of safe and healthy communities;
- (j) the adequate provision of a full range of housing, including affordable housing;
- (l) the protection of the financial and economic well-being of the Province and its municipalities;
- (o) the protection of public health and safety;
- (p) the appropriate location of growth and development;
- (s) the mitigation of greenhouse gas emissions and adaptation to a changing climate.”

The above noted matters are incorporated in the Provincial Policy Statement 2020 (PPS 2020).

2. PROVINCIAL POLICY STATEMENT (PPS) 2020

Provincial Policy Statement 2020 was issued under section 3 of the Planning Act and came into effect May 1, 2020, and replaces the Provincial Policy Statement issued April 30, 2014.

PPS 2020 provides policy direction on matters of provincial interest related to land use planning and development and sets the policy foundation for regulating the development and use of land in Ontario. The Provincial Policy Statement 2020 applies to all decisions in respect of the exercise of any authority that affects a planning matter made on or after May 1, 2020.

The zoning by-law amendment under consideration promotes residential intensification and infill in an area surrounded by commercial, residential, and institutional uses. Therefore, in evaluating the subject Zoning By-law Amendment for consistency with PPS 2020, the following policies of the PPS 2020 are relevant for discussion:

1.0 Building Strong Healthy Communities:

Ontario's long-term prosperity, environmental health and social well-being depend on wisely managing change and promoting efficient land use and development patterns. Efficient land use and development patterns support sustainability by promoting strong, liveable, healthy and resilient communities, protecting the environment and public health and safety, and facilitating economic growth.

1.1 Managing and Directing Land Use to Achieve Efficient and Resilient Development and Land Use Patterns

The PPS points to the important role of land use planning in achieving (i) an orderly development of strong, sustainable, safe and healthy communities, (ii) a healthy environment, and (iii) economic growth.

Policy 1.1.1 states that *Healthy, liveable and safe communities are sustained by:*

- a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*
- b) accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
- c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
- d) avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;*
- e) promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;*
- f) improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;*
- g) ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs;*
- h) promoting development and land use patterns that conserve biodiversity; and*
- i) preparing for the regional and local impacts of a changing climate*

With respect to 1.1.1(a) & (b) – This zoning by-law amendment facilitates infill residential development on the existing vacant subject land. The amendment will result in the construction of a 4-storey multiple dwelling on the vacant site; thereby, promoting efficient use of the existing infrastructure and land. The land within the immediate area accommodates a range and mix of commercial, residential, and institutional uses as noted already in the “Surrounding Land Uses” section of this report. The current residential zoning south of the subject land permits single detached dwellings, plus existing duplexes and semi-detached dwellings. Consequently, the existing residential developments south of the east/west alley are predominantly 2 or 1 storey single unit dwellings. This amendment will promote a mix in housing types and options without an Official Plan Amendment.

With respect to 1.1.1(c) –The existing overhead 27.6kV power lines on the northern limit of the subject property is a potential danger to public health and safety. To address such health and safety concern, the applicant is referred to the Building Code for (vertical and horizontal) clearance requirements for new building construction. Consultation with some City of Windsor Professional Engineers and ENWIN staff confirms that a 5m minimum horizontal clearance is required from conductors that are more than 750volts. Based on the proximity of the hydro pole lines to the north lot line of the subject property, this report recommends a minimum front yard depth of 5m, to help achieve compliance with minimum clearance requirement(s) from medium/high voltage overhead power lines.

There is no known environmental or public health and safety concern resulting from the recommended amendment. Railyard Noise is an existing environmental, health, and safety concern in the immediate area. Consequently, Railway Warning clause (s. 4.7.1.9 (d), South Cameron Planning Area, OP Vol. II shall apply to the subject development, and the proponent shall submit a Noise Study at the Site Plan Control application stage. The Noise Study will help

to address potential noise impact on the development. Any environment and/or public health and safety concern uncovered by the noise study will be addressed as part of the Site Plan review process.

With respect to 1.1.1 (d) - The subject land is surrounded by existing developments and is within a neighbourhood located away from City of Windsor's boundary with other settlement areas.

With respect to 1.1.1(e) – The subject amendment will promote intensification and a more compact development. The amendment will encourage the use of existing public transit and active transportation in area and help minimize land consumption and servicing costs.

With respect to 1.1.1 (f) – Sidewalks improve *accessibility for persons with disabilities and older persons*. As noted already in this report, there are existing concrete sidewalks on Tecumseh Road West, Mark Avenue, and other nearby rights-of-way. The Applicant's Planning Consultant indicates, in their May 8, 2023, PRR that an elevator is proposed within the building, which will also facilitate accessibility for persons with mobility challenges.

With respect to 1.1.1(g) – The subject land is in an area of the city that is serviced by necessary infrastructure (such as sanitary and storm sewers, watermains, electricity generation facilities and transmission and distribution systems) and public service facilities (such as public parks, police and fire protection).

With respect to 1.1.1(i) – This report contains zoning provisions, such as reduction in required number of parking spaces and increase in required landscape buffer at the south limit of the property, that will help to reduce the urban heat island effect created by extensive hard surface parking areas. In addition, Regional and local impacts of climate change will be further assessed at the Site Plan Control stage when lot-grading, stormwater management, servicing study, landscaping are carefully reviewed prior to Site Plan approval and agreement.

In summary, the subject amendment will facilitate an efficient development that will positively impact the financial well-being of the City of Windsor. This amendment will help minimize land consumption and servicing cost, and will increase the use of existing and planned public transit and active transportation services in the area. The recommended zoning by-law amendment is consistent with policy 1.1.1 of the PPS.

1.1.3.1 Settlement areas shall be the focus of growth and development.

1.1.3.2 Land use patterns within settlement areas shall be based on densities and a mix of land uses which:

- a) efficiently use land and resources;*
- b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
- c) minimize negative impacts to air quality and climate change, and promote energy efficiency;*
- d) prepare for the impacts of a changing climate;*
- e) support active transportation;*
- f) are transit-supportive, where transit is planned, exists or may be developed; and*
- g) are freight-supportive.*

Land use patterns within settlement areas shall also be based on a range of uses and opportunities for intensification and redevelopment in accordance with the criteria in policy 1.1.3.3, where this can be accommodated.

1.1.3.3 Planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.

1.1.3.4 Appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety.

The subject land is located within a settlement area (the City of Windsor) and this amendment creates opportunity for growth and development in the City. The recommended amendment promotes a land use that will make efficient use of land, resources, and existing infrastructure. There are existing active transportation options (such as sidewalks) and transit services adjacent to, or near the subject land. The proposed development has a scale, size, and location that are complementary to the existing building stock and area. The subject land represents an appropriate location for intensification. The proposed amendment will result in a zoning category and site-specific provisions that will help promote intensification and compact form of development. The subject amendment is consistent with policies 1.1.3.1, 1.1.3.2, 1.1.3.3 and 1.1.3.4 of the PPS.

1.4 Housing

1.4.3 Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs of current and future residents of the regional market area by:

b) permitting and facilitating:

1. all housing options required to meet the social, health, economic and well-being requirements of current and future residents, ...; and

2. all types of residential intensification, including additional residential units, and redevelopment in accordance with policy 1.1.3.3;

c) directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;

d) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed;

This amendment will

- promote a higher density residential development on the vacant subject land;
- create an opportunity for a more compact development in the border of an established residential area containing mostly low density developments;
- facilitate a net increase in residential units or accommodation within the subject area;
- result in intensification of the subject site and area;
- facilitate the City's ability to accommodate residential growth through intensification;
- provide a form of housing that is appropriate in terms of range and mix, and
- meet the social, health and well-being of current and future residents.

Appropriate level of infrastructure, active transportation and transit services are available or will be available in the subject area. This amendment is consistent with policy 1.4 of the PPS.

1.6 Infrastructure and Public Service Facilities

1.6.6.2 Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. Within settlement areas with existing municipal sewage services and municipal water services, intensification and redevelopment shall be promoted wherever feasible to optimize the use of the services.

The subject land is within an area that is serviced by municipal sewage services and municipal water services. This amendment will help optimize the use of existing municipal services and utilities in the area. The recommended amendment is consistent with policy 1.6.6.2 of the PPS.

1.6.6.7 Planning for stormwater management shall:

(e) maximize the extent and function of vegetative and pervious surfaces; and

f) *promote stormwater management best practices, including stormwater attenuation and re-use, water conservation and efficiency, and low impact development.*

With respect to 1.6.6.7(e) - The landscaped open space yard proposed by the applicant and the recommended 1.2m parking area separation from the south limit of the property will help to maximize the extent and function of vegetative and pervious surfaces on the subject site.

With respect to 1.6.6.7 (f) - The applicant submitted a Functional Servicing Report, dated Sep 15, 2022, prepared by D.C. McCloskey Engineering Ltd. The applicant's consulting Engineer states, "*the stormwater management scheme for this property is designed in accordance with the Windsor/Essex Region Stormwater Management Standards Manual.*" According to the applicant's Engineer, the stormwater management design proposed for this development has adequate capacity for stormwater detention and treatment of stormwater runoff during the minor and major storm events. The applicant's consulting Engineer concludes that this development will not have a negative impact on municipal services.

The recommended amendment is deemed consistent with policies 1.6.6.7 (e) & (f) of the PPS.

In summary, the above planning analysis demonstrates that the recommended zoning by-law amendment is consistent with relevant Policies of PPS 2020.

It should also be noted that the applicant's professional Planner (Lassaline Planning Consultants Inc.) prepared a Planning Rationale Report dated May 8, 2023, to support, explain and justify the subject zoning by-law amendment. The applicant's Planning Rationale Report concludes that the proposed development will:

- Promote a compact structure and a compact neighbourhood along the Tecumseh Road West 'Mixed-Use Corridor' which will improve walkability;
- Promote walking, biking, and the use of public transit;
- Demonstrate a pedestrian friendly orientation and visually attractive, high quality of building design.

The applicant's Planning Rationale Report also concludes that the subject zoning by-law amendment is consistent with the relevant Policies of the PPS.

3. OFFICIAL PLAN:

LAND USE DESIGNATION: COMMERCIAL CORRIDOR on Schedule SC-1: Development Concept, South Cameron Planning Area, Official Plan Volume II. Take note of the following:

- a) The South Cameron Planning District is proposed to be a largely residential community per Development Concept, section 4.4, OP Vol. II.
- b) Section 4.7.7.2 of OP Vol. II states that Commercial Corridors designated on Schedule SC-1 **shall be subject to the policies contained within Volume I – The Primary Plan, and the special policy area provisions of Volume 2**, where appropriate.
- c) OPA 159 changed the text "Commercial Corridor" in the Primary Plan (OP Vol.1) to "Mixed Use Corridor".
- d) The Mixed Use Corridors are expected to provide people-oriented employment and to accommodate higher density/intensity development, while maintaining a broad mix of land uses that support investment in transit and the achievement of complete communities (*new s.6.5.3, OP Vol. I, approved by OPA159*).
- e) **Permitted Uses** (*new s. 6.5.3.1, OP Vol. I, approved by OPA 159*): Uses permitted in the Mixed Use Corridor land use designation are primarily retail, wholesale store and service oriented uses and, to a lesser extent, office uses. Medium and High Profile

residential uses either as stand-alone buildings or as part of a commercial-residential mixed-use building are also permitted throughout the Mixed Use Corridors.

- f) **Street Presence** (*new s. 6.5.3.3, OP Vol. I, approved by OPA 159*): Council will encourage Mixed Use Corridor development to provide a continuous street frontage and presence. Development along Mixed Use Corridor shall be
- no more than 4 storeys in height, except on lands at an intersection of any combination of Class I Arterial Rd, Class II Arterial Rd, Class I Collector, or Class II Collector Road. The height of buildings shall generally not exceed the width of the road R.O.W. abutting the development site; (*new s.6.5.3.3(a), OP Vol. I*); and
 - encouraged to locate the buildings at the street frontage lot line with parking accommodated at the rear of the site. (*new s.6.5.3.3(c), OP Vol. I*).

Note: The applicant's concept plan shows conformity with the above street presence policies. A four-storey building is proposed with rear parking lot and the proposed building to be located at the street frontage. However, the recommendation in this report is to maintain a 5 m minimum building setback from the lot line abutting Tecumseh Road West R.O.W. This 5m minimum setback recommendation is necessary to address ENWIN and Building Code clearance requirements from overhead power lines and conductors, and achieve a safer environment for future occupants of the building.

- g) **Design Guidelines** (*new s. 6.5.3.8, OP Vol. I, approved by OPA 159*) have been considered in this report.

Note: This report recommends a 1.2m minimum landscape buffer from south lot line, maximum building height of 14m, and rear yard parking. Those recommendations are consistent with the design guideline in s.6.5.3.8, OP Vol. I. The design of the proposed development will be further evaluated at the Site Plan stage for conformity with s.6.5.3.8, OP Vol. I.

- h) **Noise Control Areas** on schedule SC-3 of the South Cameron Planning Area, OP Vol. II, confirm that Residential Development on the subject land would require Noise Study.
- i) Section 4.7.10.2, OP Vol. II requires the proponent to identify all noise sources affecting the subject land, assess their impact, and recommend noise abatement measures for the proposed residential development.

Note: This noise study requirement shall be fulfilled at the site plan control stage when any required abatement measure would be discussed as part of the site plan review and approval process and, most likely incorporated in the site plan agreement.

- j) **Locational Criteria** (s.6.5.3.6, OP Vol. I) is satisfied. The recommended zoning amendment will promote the location of a Mixed Use Corridor development where there is a Class II Arterial Road (Tecumseh Rd. W.), full municipal physical services can be provided, and no commercial related traffic is proposed.
- k) **Evaluation Criteria** (s.6.5.3.7, OP Vol. I) is satisfied. The proponent has, in their Planning Rationale Report, Functional Servicing Report and Parking Study demonstrated that the proposed development meets the evaluation criteria in s.6.5.3.7.
- l) Zoning By-law Amendment Evaluation Criteria - s.11.6.3.3, OP Vol. I. is satisfied.

Note: As noted already in this report, this amendment is consistent with the relevant policies of the PPS. This amendment maintains conformity with the relevant policies and objectives of OP Volumes I & II as discussed in this report. The Planning department received required support studies for the requested zoning by-law amendment and considered those studies in the preparation of this report. The requirements, comments and recommendations from municipal departments and circularized agencies were considered. The recommended zoning by-law amendment promotes opportunity for residential infill and intensification, which creates a compact form of neighbourhood and ensures continuation of an orderly development pattern in

the subject area. The amendment will provide additional housing opportunities in the area with no negative impact on the adjacent properties. Therefore, the criteria in s.11.6.3.3, OP Vol. I.

4. ZONING

The subject land is zoned Commercial District 2.1 (CD2.1) by City of Windsor Zoning By-law 8600. The CD2.1 zoning district does not permit residential developments. The applicant requests a zoning change from CD2.1 to CD2.2. The CD2.2 zoning district permits residential development in a combined use building with commercial use(s) located below the dwelling units. However, the proposed development is a stand-alone multiple dwelling, so the applicant requests site-specific provisions to permit a multiple dwelling as additional permitted use on the subject land. This report supports the applicant's requests noted above in this paragraph.

It should be noted that multiple dwellings with a maximum of ten units are exempt from Site Plan Control. Furthermore, it is noted that the applicant's proposal is for a 15-unit multiple dwelling at the subject location. However, to ensure that the items recommended for consideration at site plan control are achieved, this report recommends that a *multiple dwelling with 11 or more dwelling units* be permitted on the subject land.

The proposed four-storey building meets the Street Presence policy (s.6.5.3.3 (a)) of the Mixed Use Corridor land use designation. The 14m maximum building height is recommended to ensure consistency with the maximum building height requirement found under the current (CD2.1) and proposed (CD2.2) zoning categories.

As noted already in this report, there are potential public health and safety concerns arising from the existing overhead hydro pole lines located in close proximity to the north property line of the subject land. In order to address the situation, this report contains recommendation for a minimum Building setback requirement of 5.0m from the exterior lot line abutting Tecumseh Road West.

Note that there is also a municipal requirement for 3.5m land conveyance along the Tecumseh Rd frontage. Therefore, section 5.6 of zoning by-law 8600 shall apply and all zoning provisions, including the site-specific provisions recommended in this report shall be calculated using the original lands in existence immediately prior to the 3.5m conveyance.

The applicant's request for reduction in required minimum amenity area from 12 m² per dwelling unit to 0m² per dwelling unit is not supported for the following reason(s):

- Zoning by-law 8600 defines *Amenity Area* as "a landscaped open space yard or a recreational facility as an *accessory use* to a *dwelling* or a *dwelling unit* located on the same *lot*." Landscaped open space yard will be provided on the subject site.
- *Amenity Area* or/and *Landscaped Open Space Yard* improve quality of life for residents.
- The proposed 15 units is required to provide a minimum of 180m² of *amenity area* (i.e. 12 m² x 15 units). The applicant will provide landscape open space on site.
- PPS 2020 requires that vegetative covers be maximized to help in stormwater management. The proponent has the option of using landscaped open space yard as amenity area on the subject site. By so doing, the development will help maximize the extent and function of vegetative and pervious surfaces on the subject site.

Parking requirement for the proposed development is as follows:

Multiple Dwelling

- o Containing a maximum of 4 dwelling units 1 parking space for each dwelling unit
- o Containing a minimum of 5 dwelling units 1.25 parking spaces for each dwelling unit

The proponent provided a Parking Study dated October 2022, prepared by R.C. Spencer Associates Inc. The engineers' opinion, in the Parking Study, is that the proposed on-site parking supply of 15 spaces will adequately accommodate the anticipated peak parking demand generated by the proposed 16-unit mid-rise residential development. Although the engineers' opinion was based on a 16-unit multiple dwelling for the subject site, the zoning amendment is for a 15-unit multiple dwelling, which is less by one dwelling unit.

Based on the engineer's professional opinion in the Parking Study, and the availability of public transit in the subject area, the applicant's request for parking reduction is supported, provided parking spaces are provided at a ratio of 1 space per dwelling unit.

Site-specific Parking Area Separation requirement: The Planning Department's Landscape Architect is recommending a 1.2m landscape buffer running the length of the southern boundary of the property, as opposed to 0.9m required by the zoning by-law. Reason for the 1.2m buffer is to facilitate the provision of a row of trees that would not only provide a visual buffer for the existing residence at 1431 Mark Avenue, but will also help to reduce the increased urban heat island effect that will be created by the extensive hard surface parking area being proposed. Consequently, this report includes the 1.2m minimum landscape buffer requirement.

Based on the above analysis in this report, I am of the opinion that the recommended zoning by-law amendment maintains conformity with the Official Plan.

DRAFT BY-LAW: A draft by-law is attached as **Appendix E** to this report. The Planning Act, in subsection 24(1) requires that no by-law shall be passed for any purpose that does not conform with the Official Plan. As noted already in this report, the recommended amendment maintains conformity with the OP; therefore, the draft by-law can be passed at the appropriate time.

5. SITE PLAN

The recommended amendment will facilitate a development proposal (multiple dwelling) that meets the definition of a "development" per the Planning Act and the City of Windsor Site Plan Control By-law 1-2004. Site Plan Approval and Execution of a Site Plan Agreement are required for the proposed development.

A separate pre-submission consultation may be required for the site plan control application. The following supporting studies shall be included, among others, in the list of required supporting studies and information for submission of the Site Plan Control Application package:

- ❖ Noise Study per schedule SC-3: Noise Control Areas, South Cameron Planning Area, OP Vol. II, and shall be prepared in accordance with s. 4.7.10.2, OP Vol. II & s.10.2.11, OP Vol. I

The following issues found in the Studies submitted by the applicant, along with comments received from municipal departments and external agencies, are best addressed at the time of Site Plan Approval. These requirements, along with other conditions, will more correctly be included in the Site Plan Agreement:

- a) Sanitary Sampling Manhole;
- b) Corner Cut-Off - 4.6 m x 4.6 m (15' x 15') corner cut-off required at the intersection of Tecumseh Road West and Mark Avenue per City of Windsor Standard;
- c) Land Conveyance – convey approximately 3.5 metres along the entire Tecumseh Road West frontage of the subject land for road widening;
- d) Sanitary Servicing Study - retain a Consulting Engineer to provide a detailed servicing study report;
- e) Curbs and Gutters – construct new concrete curb and gutter along the entire Mark Avenue frontage of the subject land;
- f) Parkland dedication of 5% (cash-in-lieu) of the subject vacant parcel;

- g) Tree Survey to determine appropriate tree preservation for the site
- h) Noise mitigation measures as recommended in a Noise Impact Study that will be submitted at the time of Site Plan Control application;
- i) Railway Warning clause (s. 4.7.1.9 (d), South Cameron Planning Area, OP Vol. II);
- j) Enbridge Gas minimum separation requirements;
- k) Adequate clearance from existing ENWIN's pole lines and power lines;
- l) Canada Post multi-unit policy; and
- m) Record of Site Condition.

Risk Analysis: See Climate Change risk analysis below

Climate Change Risks

Climate Change Mitigation:

Residential intensification minimizes the impact on the community greenhouse gas emissions. Development within existing communities and neighbourhoods use available infrastructure such as sewers, sidewalks, and public transit and, as such, help to mitigate development impact. The proposed residential intensification on the subject land will encourage the use of existing and planned transit and active transportation in the neighbourhood, which will help to minimize the City's carbon footprint.

Climate Change Adaptation:

The proposed construction of a new dwelling provides an opportunity to increase resiliency for the development and surrounding area. Implementation of approved landscape plan, stormwater management measures and lot-grading plan for this proposed development will enhance the city's preparedness for climate change impact in the subject area.

In addition to the above, the applicant's Planning Rationale Report states, *"the apartment building is proposed to be constructed with 2 electric vehicle charging stations in the parking lot, and to include solar photovoltaic units. These adaptations will allow the residents to use renewable energy and reduce fossil fuel emissions."*

Financial Matters: N/A

Consultations:

1. DEPARTMENT AND AGENCIES

Attached as **Appendix D**, to this report, are comments from the municipal departments and external agencies. There are no objections to the proposed amendment. However, there are conditions for approval of the rezoning as noted in the municipal and agency comments in Appendix D. Summarized below are **some** of the conditions of approval.

Corner Cut-Off – Prior to the issuance of a construction permit, the owner(s) agrees to gratuitously convey a 4.6 m x 4.6 m (15' x 15') corner cut-off at the intersection of Tecumseh Road West and Mark Avenue in accordance with City of Windsor Standard Drawing AS-230.

Land Conveyance – Prior to the issuance of a construction permit, the owner(s) agrees to gratuitously convey to the Corporation, land sufficient to create a 33-metre-wide right-of-way on Tecumseh Road West. This conveyance shall be approximately 3.5 metres along the entire Tecumseh Road West frontage of the subject land.

Sanitary Servicing Study - The owner agrees, at its own expense, to retain a Consulting Engineer to provide a detailed servicing study report on the impact of the increased flow to the

existing municipal sewer system, satisfactory in content to the City Engineer and prior to the issuance of a construction permit. The study shall analyze the sanitary flow from the proposed development and recommend mitigation measures and implementation of those measures.

Curbs and Gutters – The Owner further agrees, at the discretion of the City Engineer, to construct at their own expense and according to City of Windsor Standard Specifications, a concrete curb and gutter along the entire Mark Avenue frontage of the subject land. All work to be to the satisfaction of the Corporation's City Engineer; it will be to the discretion of the City Engineer whether a cash contribution will be allowed in lieu of curb and gutter construction by the Owner.

Alley Closing – Prior to the issuance of a Building Permit, the owner shall apply to the Street and Alley Closing Committee to close the existing alley adjacent to the subject property. *[This requirement has already been satisfied. As noted earlier in this report, CR461/2022 approved the applicant's request for closure of part of the east/west alley abutting the subject property. The portion to be closed is described as PART 1 on Reference Plan 12R-29399.]*

Sanitary Sampling Manhole – The owner agrees for all non-residential uses, to install a sanitary sampling manhole accessible at the property line of the subject land. The determination of the requirement or interpretation if a sampling manhole exists or exceptions to such, will be to the satisfaction of the City Engineer.

Buffer requirement - 1.2m minimum landscape buffer running the length of the southern boundary of the property.

Tree Preservation - As part of the Site Plan control application, a tree survey is required to identify species, location, and condition of all trees on the subject lands, to assess any replacement and/or compensation for removals.

Parkland dedication – This requirement will be addressed in details at the time a Site Plan application is received.

Utility Clearance requirements -

- Enbridge Gas has active infrastructure in the proposed area and requires minimum horizontal and vertical separations from all its plant vital pipelines. See Appendix D for details of their requirements.
- ENWIN Ltd. Requires adequate clearance from existing power lines north and south of subject property.

2. PUBLIC NOTICE

The City will advertise the official notice in the Windsor Star Newspaper as prescribed by the Planning Act. In addition, the City will mail courtesy notice to all properties within 120m (400 feet) of the subject parcel, prior to the Development & Heritage Standing Committee (DHSC) meeting.

Conclusion:

As noted in this report, the applicant's request is for a change in the zoning of the subject land from CD2.1 to CD2.2 with site-specific provisions to facilitate the proposed development of a medium profile residential building (4-storey, 15-unit multiple dwelling) on the subject land.

As shown in this report, the relevant policies/provisions of the Provincial Policy Statement 2020, the City of Windsor Official Plan and the Zoning By-law 8600, along with the comments received from municipal departments and external agencies, required supporting studies and the

Planning Rationale Report provided by Lassaline Planning Consultants Inc., were all considered in the review of the applicant's request for the subject amendment.

Based on the analysis provided in this report, it is my professional opinion that the recommended amendment is consistent with the relevant policies of PPS 2020 and conforms with the City of Windsor Official Plan. The amendment is good planning.

Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Greg Atkinson
Acting Manager of Planning Policy/
Deputy City Planner

Thom Hunt, MCIP, RPP
City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP

JM

Approvals:

Name	Title
Greg Atkinson	Acting Manager of Planning Policy/Deputy City Planner
Neil Robertson for Thom Hunt	City Planner
Wira Vendrasco	Deputy City Solicitor
Jelena Payne	Commissioner of Economic Development & Innovation
Jelena Payne for Joe Mancina	Chief Administrative Officer

Notifications:

Name	Address	Email
Abutting property owners, tenants/occupants within 120 meter (400 feet) radius of the subject land		
Applicant / Registered Owner: 2832765 Ontario Inc., c/o Giovanni Miceli	3332 Gundy Park Crescent, Windsor, ON N9E 4R5	micelirentals@gmail.com
Agent: Lassaline Planning Consultants c/o Jackie Lassaline	P. O. Box 52, 1632 County, Road 31, St. Joachim, ON N7L 1Z9	jackie@lassalineplan.ca
Councillor Jim Morrison	350 City Hall Square West - Suite 510, Windsor, ON, N9A 6S1	jmorrison@citywindsor.ca

Appendices:

- 1 Appendix A - Site Photos
- 2 Appendix B - Concept Plan and floor plans
- 3 Appendix C - Concept Elevations
- 4 Appendix D – Consultations (comments from municipal departments & external agencies)
- 5 Appendix E – Draft By-law Amendment



Photo No. 1 – View of Subject vacant land and abutting south and west buildings



Photo No. 2 – Tecumseh Road streetview, looking west from subject land



Photo No. 3 – Tecumseh Road streetview, looking east from subject land



Photo No. 4 – View of the nearest building & use (existing automobile sales lot) on east of the subject land, located at the southeast corner of Tecumseh Rd W. & McKay Avenue



Photo No. 5 – View of the abutting building (First Choice Haircutters) on the west side of the subject land

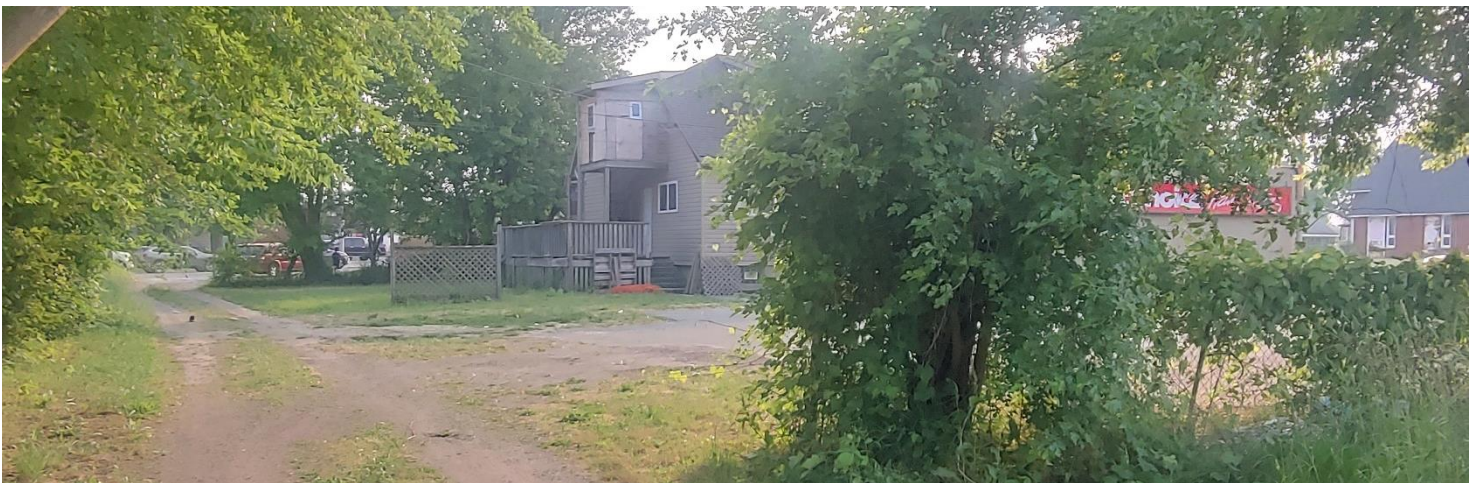


Photo No. 6 – View of the rear east/west alley and partial view of the south wall of the building abutting the west limit of the subject land



Photo No. 7 – View of the rear east/west alley and partial view of the SUD next south



Photo No. 8 – Street view of McKay Avenue, looking south from the subject property



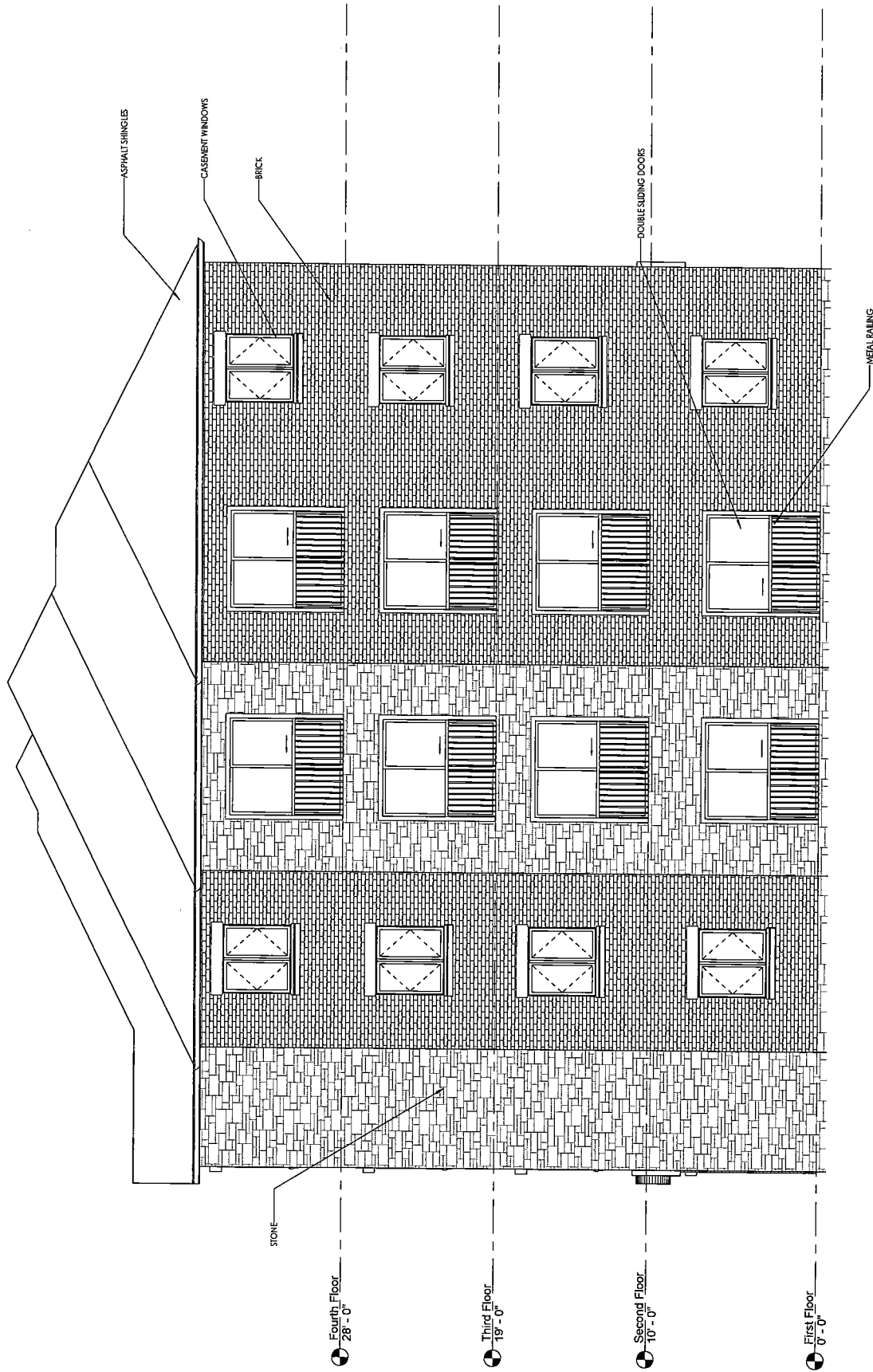
Photo No. 9 – Street view of McKay Avenue, looking north from the curve in the R.O.W. further south of the subject property



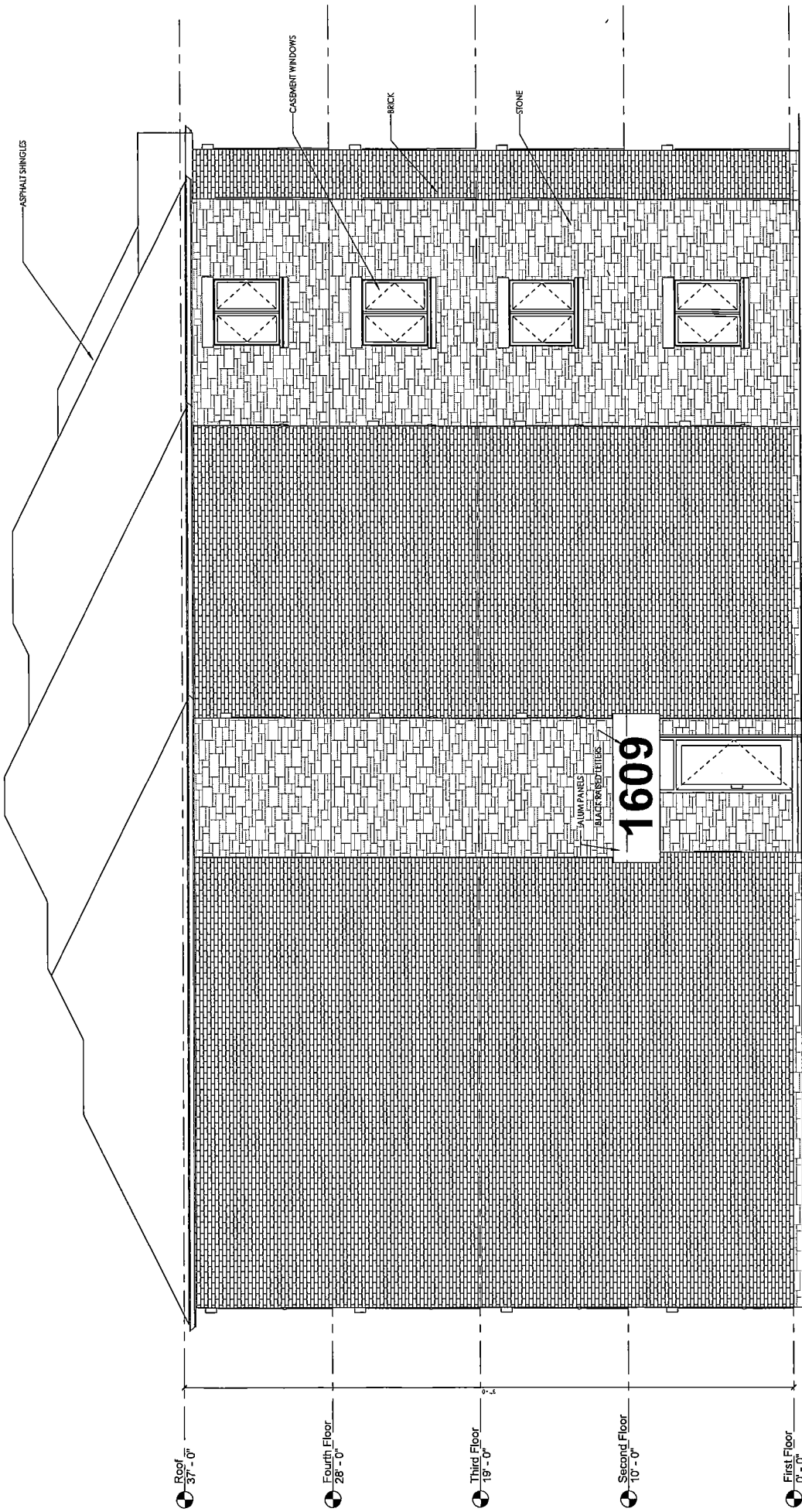
Photo No. 10 – Partial view of the subject land and properties to the north



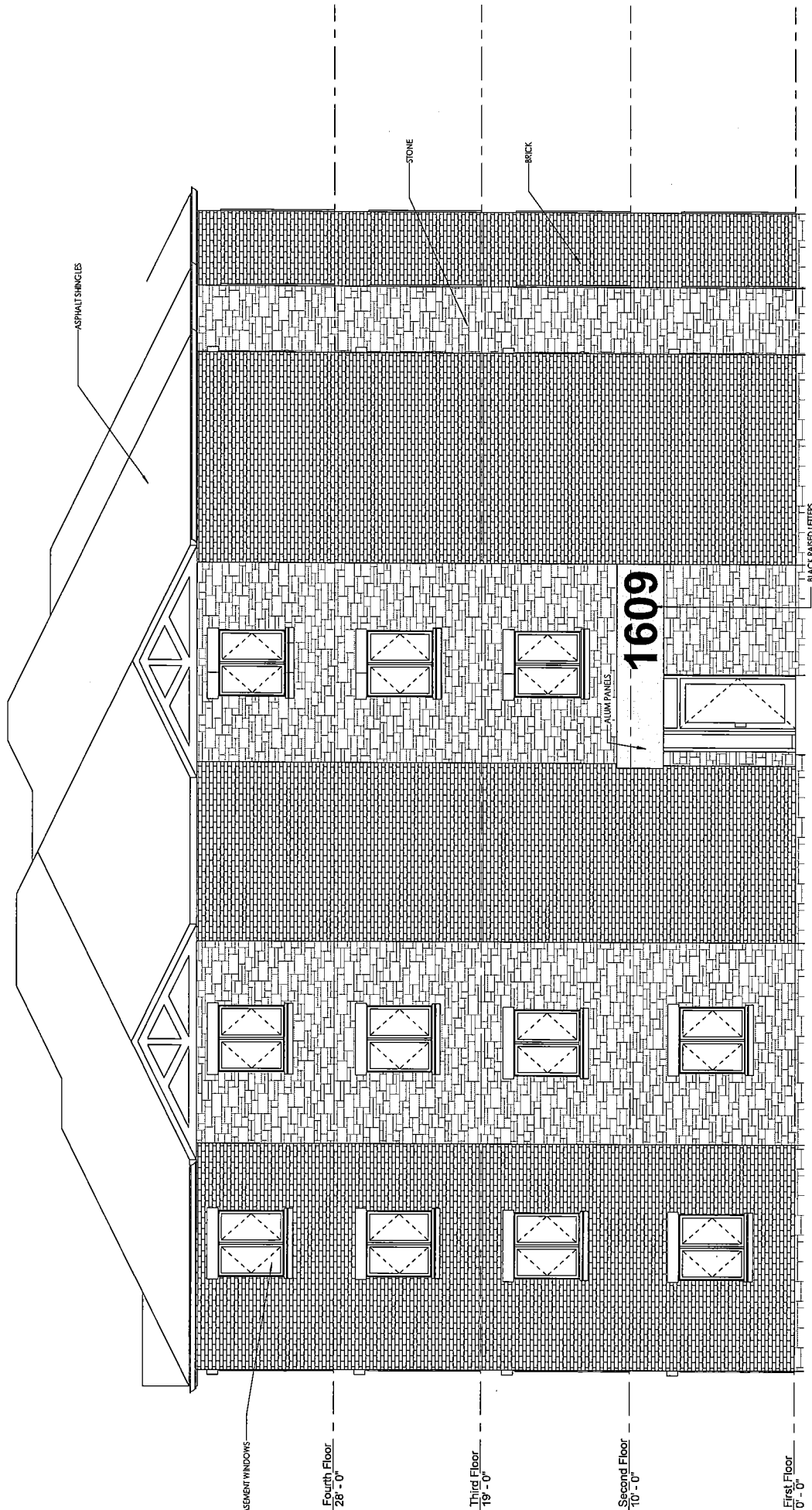
Photo No. 11 – Another partial view of the subject land and more properties to the north

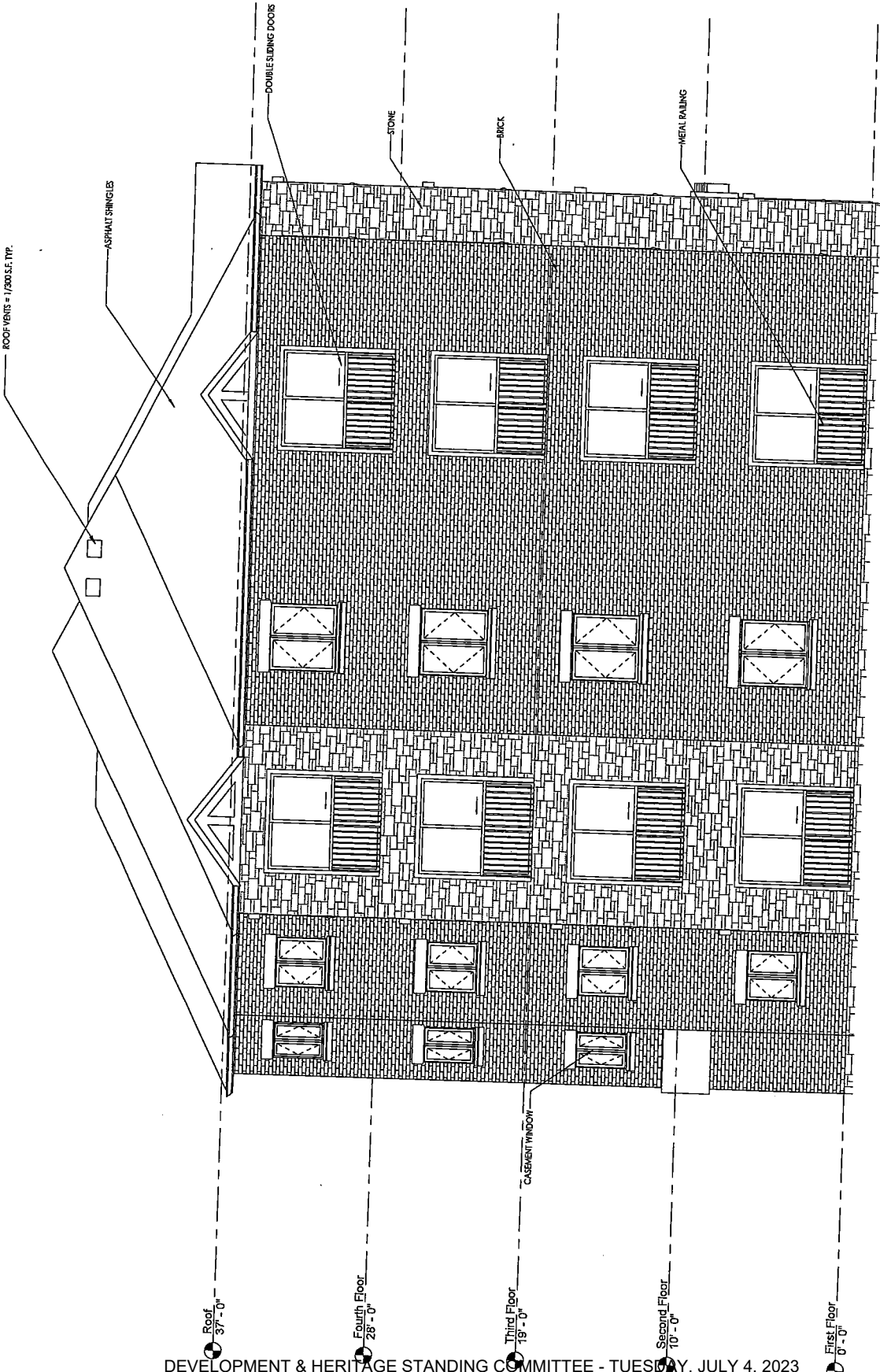


① East Elevation
1/4" = 1'-0"



① North Elevation
1/4" = 1'-0"





APPENDIX D- CONSULTATIONS

(Comments from Municipal Departments & External Agencies)

Enbridge – Kelly Buchanan

Enbridge Gas Inc, operating as Union Gas, does have service lines running within the area which may or may not be affected by the proposed Site Plan.

Should the proposed site plan impact these services, it may be necessary to terminate the gas service and relocate the line according to the new property boundaries. Any Service relocation required would be at the cost of the property owner.

If there is any work (i.e. underground infrastructure rebuild or grading changes...) at our easement and on/near any of our existing facilities, please contact us as early as possible (1 month in advance at least) so we can exercise engineering assessment of your work. The purpose is to ensure the integrity of our main is maintained and protected.

Confirmation of the location of our natural gas pipeline should be made through Ontario One Call 1-800-400-2255 for locates prior to any activity.

We trust the foregoing is satisfactory.

Enbridge – Gord Joynson

After reviewing the provided drawing at Tecumseh Rd W & Mark Ave. and consulting our mapping system, please note that Enbridge Gas has active infrastructure in the proposed area. A PDF drawing has been attached for reference.

Please Note:

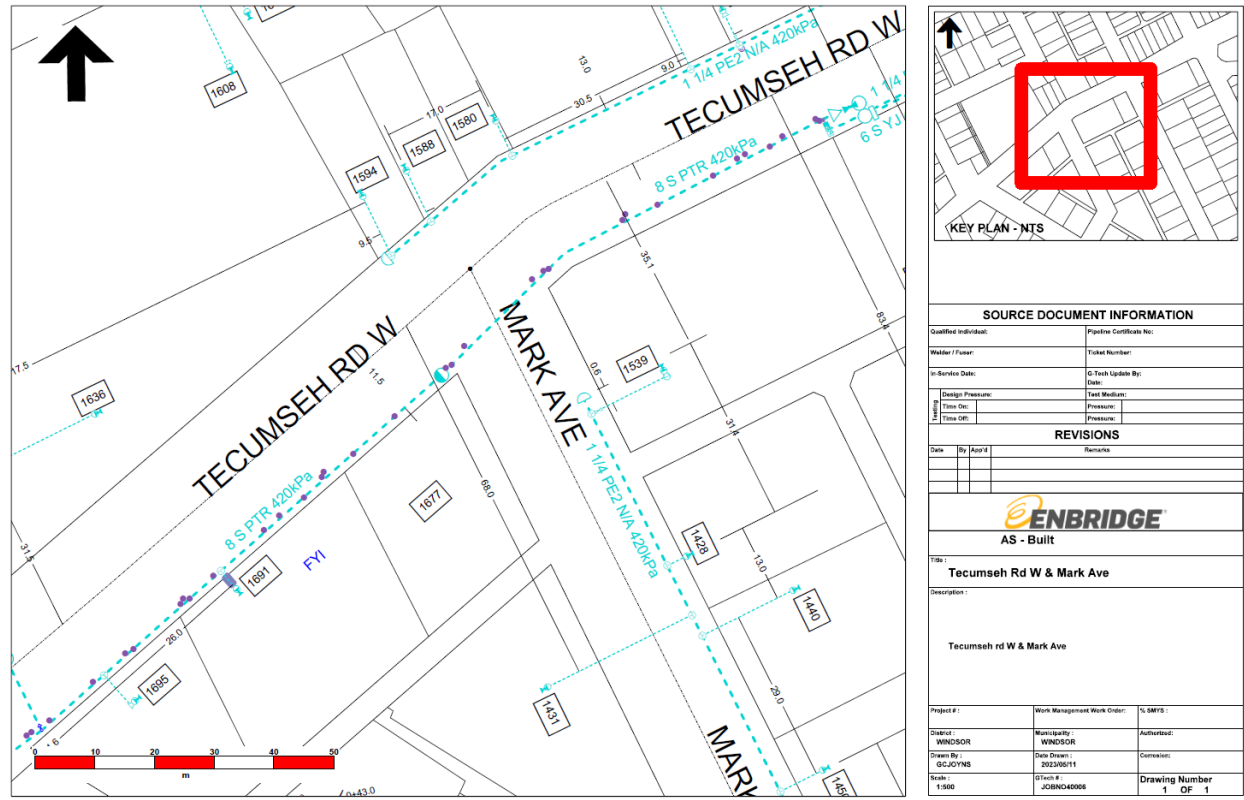
1. The shown piping locations are approximate and for information purposes only
2. The drawings are not to scale
3. This drawing does not replace field locates. Please contact Ontario One Call for onsite locates prior to excavating, digging, etc

Enbridge Gas requires a minimum separation of 0.6m horizontal and 0.3m vertical from all of our plant less than NPS 16 and a minimum separation 1.0m horizontal and 0.6m vertical between any CER-regulated and vital pipelines. For all pipelines (including vital pipelines), when drilling parallel to the pipeline, a minimum horizontal clearance measured from the edge of the pipeline to the edge of the final bore hole of 1 m (3.3 ft) is required. Please ensure that this minimum separation requirement is maintained, and that the contractor obtains locates prior to performing any work and utilizes safe excavation practices while performing any work in the vicinity.

Also, please note the following should you find any abandoned infrastructure in the area:

- Any pipe that is excavated, please assume that it is live
- If during the course of any job, any pipe is found that is not on the locate sheet and is in conflict with your work, please call our emergency number (1-877-969-0999), and one of our Union Gas representatives will respond to determine if that plant is in fact live or dead

- Please note that our Enbridge Gas representative will respond to the live or dead call within 1-4 hours, so please plan your work accordingly



Assessment Management Officer - Jose Mejalli

No objection to the zoning amendment from CD2.1 to CD2.2 to facilitate the proposed development of a 4-storey apartment building with 15 dwelling units and 15 parking spaces.

Transit Windsor - Jason Scott

Transit Windsor has no objections to this development. The closest existing bus routes to this property are with the Central 3 & Dominion 5. The closest existing bus stop to this property is located on Tecumseh at Campbell Southeast Corner. This bus stop is approximately 50 metres from this property falling well within our 400 metre walking distance guideline to a bus stop and providing direct transit access along Tecumseh Road West. This will be maintained with our City Council approved Transit Master Plan.

Canada Post – Bruno Desando

This development, as described, falls within our centralized mail policy.

I will specify the condition which I request to be added for Canada Post Corporation's purposes.

- a) Canada Post's multi-unit policy, which requires that the owner/developer provide the centralized mail facility (front loading lockbox assembly or rear-loading mailroom [mandatory for 100 units or more]), at their own expense, will be in effect for buildings and complexes with a common lobby, common indoor or sheltered space.

Should the description of the project change, I would appreciate an update in order to assess the impact of the change on mail service.

Lock-Box Assembly Requirements

The complete Canada Post Standards Manual for Builders & Developers can be downloaded at: https://www.canadapost.ca/cpo/mc/assets/pdf/business/standardsmanual_en.pdf

Compartments Size

- Horizontal lock-box models used in mailrooms must have the following minimums:
 - o Residential compartments must be at least 12.5 x 13.5 cm
 - o Commercial compartments at least 13.5 x 30.5 cm
 - o Parcel compartments at least 30.5 x 30.5 cm
- Vertical lock-box models must have min comp size of 25 x 12.5 cm. (Most models are 40 x 12.7 cm)

Heights

- All lock-box assemblies must be installed in a manner that will not require the delivery employee to reach higher than 170cm or lower than 45cm when delivering to the equipment. With respect to horizontal lock-boxes, the limits above will likely mean that maximum number of compartments that can be included in each column of residential compartments would be eight

Rear-loading Lock-boxes

- Projects with more than 100 units are required to be serviced via a rear-loading lock-box assembly.
- There must be a width of at least 100cm of working space from the back of the boxes to the wall.
- A ledge under the bottom row of boxes is also recommended in rear-loading designs. This ledge is to be directly under the bottom row of boxes (no space between ledge and bottom of boxes) and must stick out at least 20cm from the back of the boxes.
- Mailroom door is required to provide a minimum 81cm opening
- Lighting should be at least 100 lux (measured 75 cm from floor)

Access

- All buildings where the lock-boxes are required to be serviced from inside the building are required to install a Canada Post Crown lock in the building intercom. The intercom is pre-fabricated with an internal housing for the lock. The lock can be obtained from the local deliver supervisor.
- If the building has more than 100 units, a rear-loading lock-box assembly will be installed. The door to the Canada Post delivery area must be fitted with a specific model of deadbolt. This is because Canada Post will supply a key cylinder made specifically for the Canada Post key that will fit inside the deadbolt purchased by the developer.

Numbering

- Compartments should be numbered vertically and left to right on the delivery side of the boxes

Grade-level Components

- If the development includes grade level retail or residential units, please take note that door-to-door delivery will not be provided to these units. Canada Post is happy to install a Community Mailbox to provide service to these units. Please coordinate a location with the Canada Post Delivery Planner for the area. If there is no room on the property for the Community Mailbox, service can be provided via another Community Mailbox in the area. Options to service the units from the tower (lobby) lock-boxes or via a front-loading lock box erected on the outside of the building can also be discussed with the Delivery Planner.

ENWIN

HYDRO ENGINEERING:

No Objection.

Please be advised of the overhead 27.6kV power lines on the northern limit of the property

Please be advised of the overhead 120/240V secondary distribution conductor on the southern limit of the property

Prior to working in these areas, we suggest notifying your contractor and referring to the Occupational Health and Safety Act and Regulations for Construction Projects to confirm clearance requirements during construction. Also, we suggest referring to the Ontario Building Code for required clearances for New Building Construction.

WATER ENGINEERING:

Water Engineering has no objections.

Site Plan Control

The development proposal is subject to Site Plan Control pursuant to the Planning Act and City of Windsor By-law 1-2004. Where preceding development applications are required, inclusive of Official Plan and Zoning By-law Amendments, request for Site Plan Control Pre-Consultation may be made following completion of the requisite Development and Heritage Standing Committee meeting at <https://ca.cloudpermit.com/login>.

Building Department – Barbara Rusan

The Building Code Act, Section 8.(1) requires that a building permit be issued by the Chief Building Official for construction or demolition of a building.

The building permit review process occurs after a development application receives approval and once a building permit application has been submitted to the Building Department and deemed a complete application.

Due to the limited Ontario Building Code related information received, review of the proposed project for compliance to the Ontario Building Code has not yet been conducted.

It is strongly recommended that the owner and/or applicant contact the Building Department to determine building permit needs for the proposed project prior to building permit submission.

The City of Windsor Building Department can be reach by phoning 519-255-6267 or, through email at buildingdept@citywindsor.ca

Landscape Architect (Parks - Design & Development) - Sherif Barsom

No comments and no objections from Parks D&D.

Transportation Planning – Clare Amicarelli

- Tecumseh Rd W is classified as a Class II Arterial with a required right-of-way width of 33 meters per the Official Plan. The existing right-of-way along the frontage of the subject property is not sufficient; therefore, a conveyance of 3.5 metres is required.

- Mark Avenue is classified as a Local Road with a required right-of-way width of 20 meters per the Official Plan. The existing right-of-way along the frontage of the subject property is sufficient; therefore, a conveyance is not required.

- A corner cut-off of 4.6 meters x 4.6 metres is required at the corner of Tecumseh Rd W and Mark Ave for a non-signalized intersection.

- Transportation Planning has reviewed the Parking Study titled, “Mark Ave./Tecumseh Rd. W. Residential Development Windsor ON” conducted by RC Spencer Associates in October 2022. Transportation Planning has the following comments:
 - Report is satisfactory in its current form. Report states the proposed on-site parking supply of 15 spaces will adequately accommodate the anticipated peak parking demand generated by the proposed 16-unit mid-rise residential development.

- All accesses shall conform to the TAC Geometric Design Guide for Canadian Roads and the City of Windsor Standard Engineering Drawings.
 - Driveway proposed must be 7-9 metres total at the property line (minimum 3.5m/lane, maximum 4.5m/lane).

- All exterior paths of travel must meet the requirements of the Accessibility for Ontarians with Disabilities Act (AODA).

Engineering Department – Mark Schaffhauser

Sewers The site may be serviced by a 300mm concrete combined sewer located within the Tecumseh Road West right-of-way and a 600mm reinforced concrete pipe sewer located within rear alley. If possible existing connections should be utilized. Any redundant connections shall be abandoned in accordance with the City of Windsor Engineering Best Practice B.P 1.3.3.

A sanitary sampling manhole may be required on any new sanitary connection at the property line to the satisfaction of the City Engineer, if one does not already exist.

A functional servicing study is required to analyze the impact of the increased flow to the existing municipal combined sewer system.

The applicant will be required to submit site servicing drawings and storm detention calculations restricting storm water runoff to pre-development levels, as per the Windsor Essex Regional Stormwater Management Standards Manual:

<https://essexregionconservation.ca/wp-content/uploads/2018/12/WE-Region-SWM-Standards-Manual.pdf>.

Right of-Way

Tecumseh Road West is classified as Class II Arterial Road with a required right-of-way width of 33m according to the Official Plan. The current right-of-way width is 26m, therefore, a conveyance of 3.5 meters is required along the frontage of this property. Mark Avenue is classified as Local Road with a required right-of-way width of 20m according to the Official Plan. The current right-of-way width is sufficient, therefore, no conveyance is required. Furthermore, a 4.6m x 4.6m corner cut-off conveyance will be required at the corner of Tecumseh Road West and Mark Avenue intersection.

The alley at the rear of the property is unpaved and is not used by any of the adjacent properties for access. Owner shall apply to the Street and Alley Closing Committee to close the existing alley adjacent to the subject property as well as obtain any easements required due to the closure (storm sewer).

The curb and gutters along Mark Avenue do not meet current City of Windsor Standards. Owner to construct, at their own expense, new curb and gutters along entire Mark Avenue frontage as per AS-208.

In summary we have no objection to the proposed development, subject to the following requirements:

Corner Cut-Off – The owner(s) agrees, prior to the issuance of a construction permit, to gratuitously convey a 4.6 m x 4.6 m (15' x 15') corner cut-off at the intersection of Tecumseh Road West and Mark Avenue in accordance with City of Windsor Standard Drawing AS-230.

Land Conveyance – Prior to the issuance of a construction permit, the owner (s) shall agree to gratuitously convey to the Corporation, land sufficient to create a 33 metre wide right-of-way on Tecumseh Road West. This conveyance shall be approximately 3.5 metres along the entire Tecumseh Road West frontage of the subject lands.

Sanitary Servicing Study - The owner agrees, at its own expense, to retain a Consulting Engineer to provide a detailed servicing study report on the impact of the increased flow to the existing municipal sewer system, satisfactory in content to the City Engineer and prior to the issuance of a construction permit. The study shall analyze the sanitary flow from the proposed development and recommend mitigation measures and implementation of those measures.

Curbs and Gutters – The Owner further agrees, at the discretion of the City Engineer, to construct at their own expense and according to City of Windsor Standard Specifications, a concrete curb and gutter along the entire Mark Avenue frontage of the subject lands. All work to be to the satisfaction of the Corporation's City Engineer;

It will be up to the discretion of the City Engineer whether a cash contribution will be allowed in lieu of the curb and gutter construction by the Owner.

Alley Closing – Prior to the issuance of a Building Permit, the owner shall apply to the Street and Alley Closing Committee to close the existing alley adjacent to the subject property.

Sanitary Sampling Manhole – The owner agrees for all non-residential uses, to install a sanitary sampling manhole accessible at the property line of the subject lands to the City Engineer at all times. The determination of the requirement or interpretation if a sampling manhole exists or exceptions to such, will be to the satisfaction of the City Engineer

If you have any further questions or concerns, please contact Mark Schaffhauser, of this department at mschaffhauser@citywindsor.ca

Landscape Architect & Senior Urban Planner [A] (Planning Department)- Stefan Fediuk

Pursuant to the application for a zoning amendment (**Z 036/22**) to permit a 4-storey, 15 unit apartment building on the subject, please note no objections. Please also note the following comments:

Zoning Provisions for Parking Setback:

It is important to provide a buffer between the proposed development and the abutting residence immediately south at 1431 Mark Ave. The proposed site plan indicates a 200mm setback from the southern property line. Provision of a row of trees would not only provide a visual buffer for the existing residence at 1431 Mark Avenue, but will also help to reduce the increased urban heat island effect that will be created by the extensive hard surface parking area being proposed. It is therefore recommended that a site specific zoning provision be applied to the property for a 1.2m landscape buffer running the length of the southern boundary of the property.

Tree Preservation:

There is a remnant hedgerow of trees and scrub undergrowth running along the western boundary of the site. Require is a tree survey identifying species, location, and condition of all trees on the subject lands as part of the Site Plan control application to assess any replacement and/or compensation for removals.

Parkland Dedication:

Apart from Field of Dreams Park, 200m east of the proposed residential development, there are no accessible neighbourhood parks within a 0.8km (1/2mile) walking distance standard established by the Parks Master Plan, where children would not need to cross a Class 1 Collector (Campbell Avenue) or Class 2 Arterial (Tecumseh Road West) to access parkland. The reduction of amenity area to zero metres square per dwelling/development further increases the demand on Field of Dreams Park. Therefore, it is strongly recommended that as compensation, the owner provide additional cash-in-lieu to assist the Parks Department in improving the parkland resources to be found at Field of Dreams Park.

All detailed requirements will be determined at the time a Site Plan application is received.

APPENDIX E – DRAFT BY-LAW AMENDMENT

B Y - L A W N U M B E R -2023
A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600
CITED AS THE "CITY OF WINDSOR ZONING BY-LAW"

Passed the _____ day of _____, 2023.

WHEREAS it is deemed expedient to further amend By-law Number 8600 of the Council of The Corporation of the City of Windsor, cited as the "City of Windsor Zoning By-law" passed the 31st day of March, 1986, as heretofore amended:

THEREFORE, the Council of The Corporation of the City of Windsor enacts as follows:

1. By-law Number 8600 is further amended by changing the Zoning District Maps or parts thereof referred to in Section 1, of the by-law and made part thereof, so that the zoning district symbol of the lands described in Column 3 shall be changed from that shown in Column 5 to that shown in Column 6:

1. Item Number	2. Zoning District Map Part	3. Lands Affected	4. Official Plan Amendment Number	5. Zoning Symbol	6. New Zoning Symbol
1	4	Lots 131 and 132, Registered Plan 1342 [PIN 01208-0145 LT], located on the southwest corner of Tecumseh Road West and Mark Avenue	-	CD2.1	CD2.2

2. That subsection 1 of Section 20, of said by-law, is amended by adding the following paragraph:

“474 SOUTHWEST CORNER OF TECUMSEH ROAD WEST AND MARK AVENUE

For the land comprising Lots 131 and 132, Registered Plan 1342 [PIN 01208-0145 LT], a *Multiple Dwelling with 11 or more dwelling units* shall be an additional permitted use and the following shall apply:

- The provisions in section 15.2.5, save and except for sections 15.2.5.10 and 15.2.5.15
- Building setback from an exterior lot line abutting Tecumseh Road West – minimum 5.0 m
- Parking Area separation from the south lot line – minimum 1.2 m (to be maintained as a *landscaped open space*)
- Parking - per Dwelling Unit – minimum 1 parking space per unit
- Parking is prohibited in any front yard
[ZDM 4; ZNG/6897]”

3. The said by-law is further amended by changing the Zoning District Maps or parts thereof referred to in Column 2, of said by-law and made part thereof, so that the lands described in Column 3 are delineated by a broken line and further identified by the zoning symbol shown in Column 5:

1. Item Number	2. Zoning District Map Part	3. Lands Affected	4. Official Plan Amendment Number	5. Zoning Symbol
1	4	Lots 131 and 132, Registered Plan 1342 [PIN 01208-0145 LT], located on the southwest corner of Tecumseh Road West and Mark Avenue.	-	S.20(1)474

DREW DILKENS, MAYOR

CLERK

First Reading - , 2023
Second Reading - , 2023
Third Reading - , 2023

SCHEDULE 2

1. By-law _____ has the following purpose and effect:

To amend the zoning of the land located on the southwest corner of Tecumseh Road West and Mark Avenue, described as Lots 131 and 132, Registered Plan 1342 [PIN 01208-0145 LT], so as to permit the development of a 4-storey multiple dwelling with 15 dwelling units on the subject land.

The current CD2.1 zoning of the subject land will change to CD2.2 with site-specific zoning provision permitting the proposed development of a stand-alone residential dwelling on the subject land. This amending by-law will facilitate the development of an underutilized parcel situated in a prominent location within the subject neighbourhood. This amending by-law will also enhance the use of public transit and existing active transportation in the subject area.

2. Key map showing the location of the lands to which By-law _____ applies.



PART OF ZONING DISTRICT MAP 4

N.T.S.

SCHEDULE 2

Applicant: 2832765 Ontario Inc.



SUBJECT LANDS

PLANNING & BUILDING DEPARTMENT



DATE : MAY, 2023
FILE NO. : Z-036/22, ZNG/6897



Subject: Z 015-22 [ZNG-6738] 2356976 Ontario Inc 0 & 845 Riverside E and 856 & 864 Chatham E

Reference:

Date to Council: July 4, 2023

Author: Frank Garardo, MCIP, RPP
Senior Planner
Corporation of the City of Windsor
Planning and Building Services
350 City Hall Square West, Suite 210
Windsor, Ontario N9A 6S1
T. (519) 255-6543 x 6446
F. (519) 255-6544
E. fgarardo@citywindsor

Economic Development & Innovation
Report Date: June 9, 2023
Clerk's File #: Z/14430

To: Mayor and Members of City Council

Recommendation:

I. THAT Zoning By-law 8600 **BE AMENDED** by changing the zoning on the lands of Plan 140 W PT lot 7, Plan 143 E PT lot 1, Plan 140 W PT Lot 7, Plan 143 E PT lot 12 (Roll No: 3739-030-050-00200-0000, 3739-030-050-00100-0000, 3739-030-040-09400-0000, 3739-030-050-01400-0000, 3739-030-040-08800-0000), situated on the north side of Chatham Street east, west of Parent Avenue and known municipally as 0 & 845 Riverside Drive East, and 856, & 864 Chatham Street East by adding a site specific exception to Section 20(1) as follows:

X. SOUTH SIDE OF RIVERSIDE DRIVE EAST, NORTH OF CHATHAM STREET EAST, BETWEEN PARENT AVENUE AND MARENTETTE AVENUE

For the lands comprising of Plan 140 W PT lot 7, Plan 143 E PT lot 1, Plan 140 W PT Lot 7, Plan 143 E PT lot 12; a *multiple dwelling* shall be an additional permitted use subject to the following provisions apply:

.1 Lot Area – minimum

a) for the first 19 dwelling units – 1825.0 square metres

b) for each additional dwelling unit- 45.0 square metres per unit

.2 Lot Frontage – minimum- 45.0 m

.3 Lot Coverage – maximum - 35.0 %

.4 Landscape Open Space Yard – minimum - 35% of the *lot area*

.5 Main Building Height – maximum – 20.0 m

.6 Building Setback:

- a) *front yard depth*- from most northerly front lot line – minimum - 6.0m
- b) *rear yard depth* - north limit of Chatham Street east right of way- minimum- 7.5 m
- c) *side yard width*: minimum - 4.5 m

7. Parking:

- a) *Parking spaces* – minimum - 1.1 per unit
- b) A *parking space* is prohibited in any *front yard*
- c) *Parking spaces* shall be setback a minimum of 6.0 m from the most northerly front lot line abutting the Riverside Drive East right-of-way, and shall be screened from Riverside Drive East
- d) Vehicular *access* from Riverside Drive East is prohibited
- e) Indoor ground floor *amenity space* – minimum – 4.0 square metres per unit

II. That the Site Plan Approval Officer **BE DIRECTED** to incorporate the following, as required, in the site plan approval and site plan agreement:

- a) Provide an Energy Strategy as per the terms of reference from the Environmental and Sustainability Coordinator;
- b) Provide a Tree Preservation and Landscaping Plan as per the comments of the Landscape Architect;
- c) Noise warning clause(s) and other noise abatement measures identified in the Acoustical Study dated August 12, 2021;
- d) Provide a design brief in accordance with the urban design chapter of the City of Windsor Official Plan as part of site plan control (pedestrian connectivity, enclosure of front facade);and,
- e) The requirements and recommendations of municipal departments and agencies as noted in this report and detailed in Appendix F attached.

Executive Summary:

N/A

Background:

Application Information

Location: 0 & 845 Riverside Drive east; and 856 & 864 Chatham Street east

Ward: 4 **Planning District:** Walkerville **Zoning District Map:** 6

Applicant: 2356976 Ontario Inc., Garg Bhan

Owner: Same as Applicant

Agent: Pillon Abbs Inc.; c/o Tracey Pillon-Abbs, MCIP, RPP

Submitted Documents

Application Form, Conceptual Site Plan (attached as Appendix A), Conceptual Elevations (attached as Appendix B), Topographic Survey, Planning Rationale Report revised May 29, 2023 (attached as Appendix G), Stormwater Management Report (attached as Appendix H), Acoustical Report (attached as Appendix I), Stage 1 & Stage 2 Archaeological Assessment Report (attached as Appendix J).

Proposal:

The applicant is requesting an amendment to Zoning By-law 8600 by changing the zoning on the lands to facilitate construction of a multi-unit residential building with a total of 29 dwelling units. The proposed building would be a maximum of twenty (20) meters in height, approximately six (6) storeys, and include thirty-one (31) parking spaces on the ground floor. The dwelling units are located on the second through to the sixth floor.

The subject lands are designated as Residential on Schedule D: Land Use in the Official Plan and currently Zoned as Residential District 2.2 (RD2.2) in the Zoning by-law 8600. The RD2.2 zoning permits one multiple dwelling containing a maximum of four dwelling units. The applicant proposed to change the zoning to Residential District (RD3.3) to permit a multi-unit dwelling with five or more units on the subject lands.

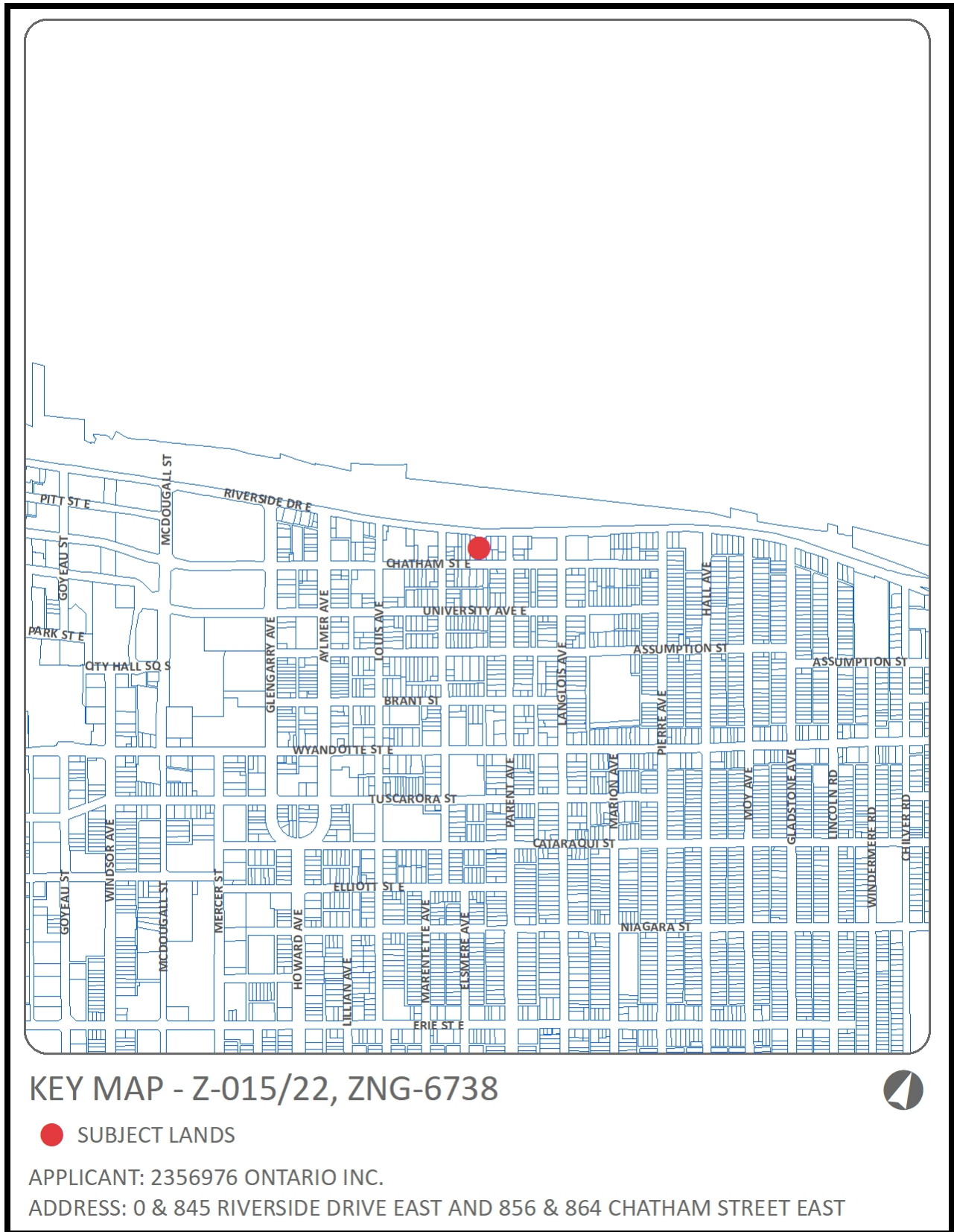
The proposed development is subject to site plan control.

Site Information

OFFICIAL PLAN	ZONING	CURRENT USE	PREVIOUS USE
Residential	Residential District 2.2 (RD2.2)	Vacant lands	Unknown
LOT FRONTAGE	LOT DEPTH	LOT AREA	LOT SHAPE
47.2 m	57.8 m	2285 m ²	Irregular
155 ft	189.6 ft	24595.5 sq. ft	

All measurements are provided by the applicant and are approximate.

Figure 1: Key Map



Neighbourhood Description:

The subject parcel is located on the south side of Riverside Drive East; north of Chatham Street East, between Marentette Avenue and Parent Avenue. The subject lands are located in the Walkerville Planning Area and immediately to the east of the City Centre Planning Area.

Site images are provided in Appendix G. The Planning Rational Report (PRR) attached as Appendix G also contains site images.

SURROUNDING LAND USE:

North: Riverside Drive East, Riverfront Parks, Riverfront Trail, Bert Weeks Memorial Gardens, and further north, the Detroit River.

East: Residential uses – low profile multi-unit dwellings, small commercial development at the corner of Parent Ave and Riverside Dr, and two high profile residential buildings 10-storey apartment building known as Riverside Heights, at 1070 Chatham Street E. and a 12-storey apartment building known as Royal Towers, at 101 Langlois Ave).

West: Residential uses-low profile multi unit, vacant and under-utilized lands, Kinsmen Park, and further west, the City Centre planning district.

South, Southeast and Southwest: mostly low profile housing developments including duplexes, and multi-dwelling units, University Ave. Park, and an institutional use (Frank W. Begley Public School).

Riverside Drive in this neighbourhood serves as the dividing line between the Central Riverfront park system on the north side and the developed area to the south.

Riverside Drive East is a two-way four lane cross-section which travels east-west and has sidewalks on both sides of the street. There is no on-street parking on Riverside Drive East.

The City Centre planning area to the west of the subject lands consist of several vacant and under-utilized parcels. The area to the west of Marentette includes a policy framework in the Official Plan which supports the consolidation of parcels to facilitate high density development.

Downtown Windsor which includes facilities of the University of Windsor and St. Clair College, Caesars Windsor (500 m to the West) and various municipal, provincial and federal offices, is located 1 km (Ouellette Avenue) to the west.

An elementary school (Frank W. Begley Public School) is located 325 m to the southeast.

Public transit is currently available via the Walkerville 8 bus route located on Riverside Drive. The closest bus stop is located within less than 50m at the intersection of Riverside Drive at Parent Ave. The nearest bicycle network facilities are located on the Riverfront Trail System.

MUNICIPAL INFRASTRUCTURE:

- The City's records show that there are municipal storm and sanitary sewers within the abutting roadways, available to service the subject land.
- The closest fire hydrant is located at the southeast corner of Parent Avenue and Riverside Drive East.
- Street lights are located on the south side of Riverside Drive East.
- There are sidewalks on the south side of Riverside Drive East.
- The nearest bicycle network facilities are located on the Riverfront Trail System.
- There are multi-use trails along the north side of Riverside Dr. E. and within the waterfront parks.
- ENWIN has overhead power distribution wires in the subject area. ENWIN has provided further information on further requirements during site plan control and construction of the proposal, including potential easements and relocation of existing services.
- The closest existing transit route to this property is the Walkerville 8. Bus stop is located at the southwest corner of Riverside and Parent, within 50 metres of the subject property.
- Riverside Drive East is classified as a Scenic Drive in the Official Plan;
- Nearby Class II Collector Roads – Parent Ave. (approx. 50 m east of the subject land).

Figure 2: Subject Parcel - Rezoning

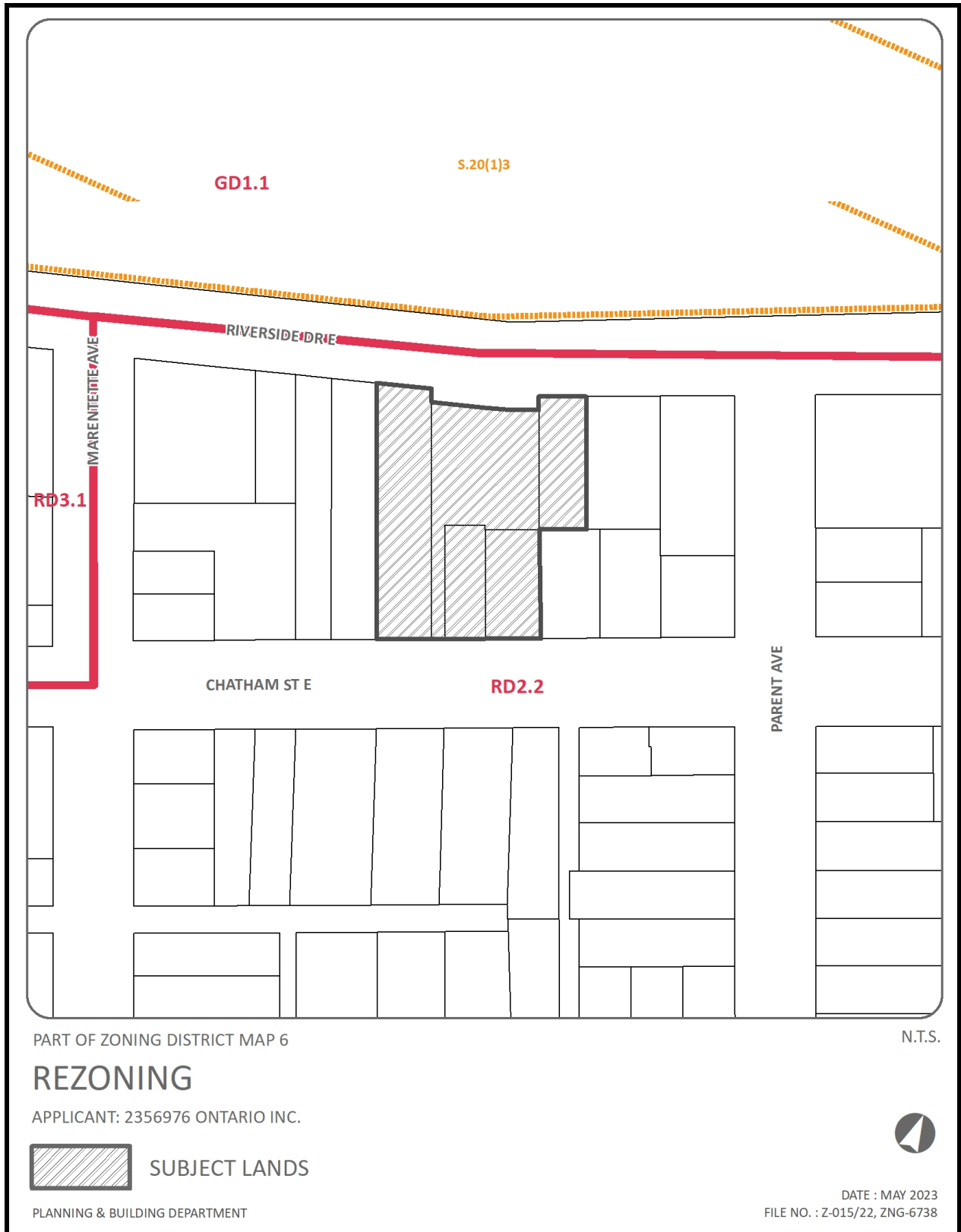


Figure 3: Neighbourhood Map



Discussion:

PROVINCIAL POLICY STATEMENT (PPS) 2020

The Provincial Policy Statement (PPS) provides direction on matters of provincial interest related to land use planning and development and sets the policy foundation for regulating the development and use of land in Ontario. The zoning bylaw amendment promotes residential intensification and infill and would result in a development on a site which is currently vacant and under-utilized. This is consistent with the Provincial Policy Statement in that the development promotes the efficient use of existing land, promotes cost-effective development patterns and standards to minimize land consumption and servicing costs. Related to this direction, the PPS states:

1.1 Managing and Directing Land Use to Achieve Efficient and Resilient Development and Land Use Patterns

1.1.1 Healthy, liveable and safe communities are sustained by:

a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;

b) accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;

c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;

f) improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;

g) ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs;

i) preparing for the regional and local impacts of a changing climate.

With respect to 1.1.1(a) – The requested multiple dwelling development promotes cost-effective development by redeveloping an under-utilized vacant site. The amendment will introduce a medium profile, higher density residential use on the subject land; thereby, resulting in an efficient use of land, municipal services and infrastructure. Furthermore, the amendment will promote efficient development and land use pattern that will positively impact the financial well-being of the City of Windsor.

With respect to 1.1.1(b) - There are medium and high profile residential developments located along Riverside Drive, east of the subject land. The City Centre is located west of the subject lands and includes policy framework to encourage high density and high

profile developments. The recommended amendment will bring about the accommodation of a new *multi-unit, multi-storey* housing type that will constitute an appropriate market-based range and mix of residential types.

With respect to 1.1.1(c) – There are no known environmental or public health & safety concerns.

With respect to 1.1.1(f) - Sidewalks improve *accessibility for persons with disabilities and older persons*. As noted already in this report, there are existing concrete sidewalks on Riverside Drive east. The concept plan shows proposed on-site sidewalks, which connect to City sidewalks.

With respect to 1.1.1(g) – The subject land is in an area of the City that is built-up and serviced by necessary infrastructure and public utilities.

With respect to 1.1.1(i) –The proposed development with 29 dwelling units will support the use of public transit and help to reduce carbon foot-print, causing a positive impact on climate change. The impacts of climate change can be further addressed at the time of site plan approval when the lot-grading provisions, stormwater management measures, servicing study, landscaping requirements and much more, can be discussed in details and incorporated in the site plan approval and site plan agreement.

In summary, the recommended zoning by-law amendment will facilitate an efficient development on the subject land and sustain a healthy, liveable and safe community. The recommended zoning by-law amendment is consistent with policy 1.1.1 of the PPS.

Policy 1.1.3.1 and Policy 1.1.3.2 state:

1.1.3.1 Settlement areas shall be the focus of growth and development.

1.1.3.2 Land use patterns within settlement areas shall be based on densities and a mix of land uses which:

- a) efficiently use land and resources;*
- b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
- c) minimize negative impacts to air quality and climate change, and promote energy efficiency;*
- d) prepare for the impacts of a changing climate;*
- e) support active transportation;*
- f) are transit-supportive, where transit is planned, exists or may be developed; and ...*

Land use patterns within settlement areas shall also be based on a range of uses and opportunities for intensification and redevelopment in accordance with the criteria in policy 1.1.3.3, where this can be accommodated.

1.1.3.3 Planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.

This amendment creates opportunity for growth and development within the City of Windsor settlement area. This amendment will facilitate the development of a medium profile housing option, which is both an infill development and promotes residential intensification. The amendment will facilitate a transit-supportive multi-unit residential development that will efficiently use land, resources, and existing infrastructure, including existing and planned active transportation options such as sidewalks, multi-use trails, and transit. The subject amendment is consistent with policies 1.1.3.1 and 1.1.3.2 of the PPS.

Policy 1.4 Housing states:

1.4 Housing

1.4.3 Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs of current and future residents of the regional market area by:

b) permitting and facilitating:

1. all housing options required to meet the social, health, economic and well-being requirements of current and future residents, ...; and

2. all types of residential intensification, including additional residential units, and redevelopment in accordance with policy 1.1.3.3;

c) directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;

d) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed;

The proposed zoning by-law amendment would facilitate a net increase in residential units and provide a form of housing that is appropriate in terms of range and mix. In terms of supporting active transportation and transit, the site of the proposed zoning amendment is directly served by Transit Windsor. The subject amendment is consistent with policies 1.4.3 of the PPS.

1.6 Infrastructure and Public Service Facilities

1.6.6.2 Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. Within settlement areas with existing municipal sewage services and municipal water services, intensification and redevelopment shall be promoted wherever feasible to optimize the use of the services.

The subject land is within an area that is serviced by municipal sewage services and municipal water services. The recommended zoning by-law amendment is consistent with policy 1.6.6.2 of the PPS.

1.6.6.7 Planning for stormwater management shall:

e) maximize the extent and function of vegetative and pervious surfaces; and

f) promote stormwater management best practices, including stormwater attenuation and re-use, water conservation and efficiency, and low impact development.

The applicant submitted an initial Storm Water Management (SWM) and Servicing Report dated December 3, 2021. The Site Plan Review process will further address storm water management and landscaping features. The recommended amendment is consistent with policy 1.6.6.7 (f) of the PPS.

1.7 Long-Term Economic Prosperity

1.7.1 Long-term economic prosperity should be supported by:

b) encouraging residential uses to respond to dynamic market-based needs and provide necessary housing supply and range of housing options for a diverse workforce;

c) optimizing the long-term availability and use of land, resources, infrastructure and public service facilities.

This amendment encourages residential intensification which provides additional housing supply to the City. This amendment, therefore, symbolizes an appropriate response to the housing needs in the City of Windsor. The proposed development of a 6-storey, 29-unit multiple dwelling will optimize the availability and use of land, infrastructure and public service facilities. The amendment is consistent with policy 1.7.1 of the PPS.

1.8 Energy Conservation, Air Quality and Climate Change

1.8.1 Planning authorities shall support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and preparing for the impacts of a changing climate through land use and development patterns which:

a) promote compact form and a structure of nodes and corridors;

b) promote the use of active transportation and transit in and between residential, employment (including commercial and industrial) and institutional uses and other areas; and

g) maximize vegetation within settlement areas, where feasible.

The amendment promotes a compact development, which is transit-supportive, in an area that promotes active transportation and connectivity through the existing and planned sidewalks and multi-use trails. The recommended amendment contains zoning provisions (building setbacks, lot coverage) that will help to maximize vegetation within the subject site and enhance air quality and positively impact storm management design for the site. The developer will be required to submit a landscaping plan and an energy study to consider energy efficiency in the building design during the Site Plan review process. This may include, but not be limited to increased insulation, energy efficient appliances and fixtures, high efficiency windows and doors.

OFFICIAL PLAN (OP)

A **safe, caring and diverse community** encourages a range of housing types to ensure that people have an opportunity to live in their neighbourhoods as they pass through the various stages of their lives. “As the city grows, more housing opportunities will mean less sprawl onto agricultural and natural lands.” S. 3.2.1.2 (Neighbourhood Housing variety), OP Vol. 1.

Land Use Designation: The site is designated “Residential” in Schedule D of City of Windsor Official Plan. The objectives and policies of the Residential land use designation establish the framework for development decisions in Residential areas within the City of Windsor.

Permitted Uses: s. 6.3.2.1, OP Vol. 1. “Uses permitted in the Residential land use designation identified on Schedule D: Land Use include Low Profile and Medium Profile dwelling units.” S. 6.2.1.2 of the Official Plan defines Medium profile developments as 6.2.1.2 (b) Medium Profile developments are buildings or structures generally no greater than six (6) storeys in height; The proposed amendment conforms to the Residential land use designation.

Development Strategy (chapter 3, OP Vol): The subject property is located in an area designated as a “Growth Centre” on schedule J: Urban Structure Plan. S. 3.3.1.1 Growth Centres are the highest in the hierarchy of nodes in Windsor due to their scale, density, range of uses, function and current or future identity. A multi-unit residential dwelling unit would be an appropriate development to facilitate a compact built-form

development within a Growth Centre and near to the City Centre, places of employment, public transportation, cycling, and available nearby amenities.

Residential Land Use (chapter 6, OP Vol): The Official Plan's objectives are to support a complementary range of housing forms, promote compact residential form for new developments and promote selective residential redevelopment, infill and intensification initiatives in the City of Windsor. Objective 6.1.1 is to achieve safe, caring and diverse neighbourhoods. Objective 6.1.2 seeks environmentally sustainable urban development. Objective 6.1.3 promotes housing suited to the needs of Windsor's residents.

Objective 6.3.1.1 supports a complementary range of housing forms and tenures in all neighbourhoods. Objective 6.3.1.2 seeks to promote compact neighbourhoods and balanced transportation systems. Objective 6.3.1.3 seeks to promote residential redevelopment, infill and intensification initiatives in appropriate locations in the City.

The proposed development will help to support a diverse neighbourhood that represents a sustainable community and will provide housing that is in demand. The proposed development will help to encourage residential uses in vacant and under-utilized areas. The proposed residential development represents a complementary and compact form of housing and intensification that is near sources of transportation.

Objective 6.3.2.4 of the Official Plan outlines the locational criteria for residential development to be located in areas with access to collector or arterials roads, full municipal services can be provided, adequate community services and open spaces are available or are planned for, and public transportation service can be provided. The proposed location is fronting on a four-lane cross-section (Riverside Drive) with a signalized intersection at Parent Avenue (Collector Road) located within 50 metres of the subject lands. The subject lands can be serviced by full municipal physical services and existing community services, open spaces and public transportation are available or planned for in the neighbourhood. Administration is satisfied the proposed amendment meets the evaluation criteria of the Official Plan. 6.3.2.4.

Objective 6.3.2.5c of the Official Plan outlines the evaluation criteria for zoning amendments to be i) compatible with the surrounding area, (ii) provided with adequate off street parking; (iii) capable of being provided with full municipal physical services and emergency services; (iv) and facilitating a gradual transition from Low Profile residential development to Medium and/or High profile development and vice versa, where appropriate.

The subject site is located in an area of transition. The development pattern of the existing area includes several vacant and under-utilized lot patterns which can support medium density development. The Concept site plan shows adequate off-street parking for the proposed development; and as noted already in this report, the subject land is within an area adjacent to the City Centre Planning District, therefore, the proposed residential intensification is capable of being provided with full municipal physical services and access to public transit and emergency services per s.6.3.2.5c of the Official Plan.

In terms of scale, massing, orientation, landscaping, setbacks, parking and amenity areas, these compatibility elements are further assessed at the time of Site Plan Approval when detailed designs are available for review. However, it is my opinion that with the recommended zoning provisions the amendment can result in a design that is compatible with the surrounding area as required under s.6.3.2.5 above. The term “Compatible” means the proposed development needs to be able to coexist with existing land uses; it does not mean the proposed development needs to be identical or similar to existing development in an area. As noted already in this report, there is a mix of low, medium and high profile residential developments co-existing along Riverside Drive.

Energy Conservation, s.8.5.2.8 of OP Vol. 1: The proposed infill redevelopment is a compact, transit-oriented development with increased density, making transit service a viable investment for the City, per s.8.5.2.8(b), OP Vol. 1. Landscaping can further assist in reducing heating and cooling requirements. Hence the recommended amendment is structured to conform with s.8.5.2.8(c), OP Vol. 1.

Infill Development, s.8.7.2.3 of OP Vol. 1: The proposed infill residential development on the subject land is capable of being designed to function as an integral and complementary part of the existing residential development pattern. The requirements under s.8.7.2.3 can be more appropriately addressed at the time of Site Plan Approval. If Council approves the recommended zoning by-law amendment, the next step in the development process is for the proponent to submit an application for site plan review and approval, which will ensure that the proposed residential development is in keeping with the Official Plan built form policy for infill developments as in section 8.7.2.3, OP Vol. 1.

Civic Way (Chapter 8 OP Vol 1, The City of Windsor Official Plan designates Riverside Drive East as “Civic Way” on Schedule G- Civic Image. Chapter 8 of the City of Windsor Official Plan provides design goals and objectives for buildings located within the City of Windsor and defined “Civic Way” areas.

Section 8.7.2.2 outlines the redevelopment goals of providing design which complementary to adjacent development, provides links with pedestrian, cycle, public transportation and road networks. Section 8.7.2.8 outlines that main entrances to buildings are to be street oriented and clearly visible from principal pedestrian approaches. Section 8.5.2.5 provides direction for encouraging the use of landscaping to assist in energy conservation, and section 8.7.1.7 provides direction to “*achieve external building designs that reflect high stands or character, appearance, design and sustainable design features*”. Section 8.11.2.19 encourages the partial screening of surface parking lots through the use of low fences, walls, berms and other landscape elements, and through the location of lots away from street view, while still permitting views for orientation and safety. The recommended zoning amendment will include provisions prohibiting parking in the front yard and require partial enclosure of the ground floor parking area.

The proposed Zoning by-law amendment is consistent with the objectives of the City of Windsor Official Plan.

Amendments Must Conform, s. 11.6.3.1 of OP Vol. 1: “All amendments to the Zoning By-law(s) shall conform with this Plan”. Based on the analysis provided in this report, the recommended zoning by-law amendment maintains conformity with the Official Plan.

Zoning By-Law:

The subject site is currently zoned Residential (RD2.2) in the City of Windsor Zoning By-law 8600. The current zoning permits multi-dwellings up to a maximum of four dwelling units; as such the proposed development requires a Zoning By-law amendment to include “multiple dwelling” as an additional permitted use. In consultation with the applicant administration is recommending site specific provisions. Relevant excerpts from Zoning By-law 8600 are attached as Appendix E.

The applicant’s requests for a zoning change to a higher density residential (RD3.3) provisions in the revised PRR dated May 29, 2023 have all been considered and are supported in this report; through site specific recommendations to interior side yard setbacks, rear yard setbacks, maximum height, parking, and amenity spaces. The site specific recommendations include provisions for setbacks which are typical for medium density residential development. The proposed setbacks will assist with the transition from low profile buildings located on Chatham Street East. **DRAFT BY-LAW:** A draft by-law is attached as **Appendix K**.

The following items were taken into consideration in drafting of a recommended site specific zoning by-law provision.

Zoning Regulations:

Minimum Lot Area and Dwelling units: The current zoning practice for comparable medium density developments ties the dwelling unit counts with the lot area. The recommended zoning provision requires 1825 square meters for the first 19 dwellings units and 45 square meters for each additional unit.

Building Height: The applicant’s initial request for the RD3.3 zone category would permit a maximum height of 24 meters. In consultation with the applicants a maximum height of 20 meters will be sufficient for a medium density development with a maximum of six (6) storeys. The 20-metre maximum height would conform to the objectives of the Official Plan for medium density development.

Lot Coverage/Landscaped Open Space: The applicant’s proposal identifies the current permitted thirty-five percent (35%) maximum lot coverage and 35% minimum landscape open space yard would be sufficient. Administration is in favor of a maximum 35% lot coverage and minimum 35 % landscaped open space to ensure a good ratio for the building envelope and to assist with site plan control principles in regards to design and landscaping.

Lane Access: The applicant’s concept plan identifies no vehicular access from Riverside Drive East. Administration is recommending no vehicular access and no parking area shall be located in the front yard adjacent to Riverside Drive East. This will promote an aesthetically pleasing landscaping area fronting Riverside Drive East.

Parking Provisions: The development as a whole will provide 31 parking spaces, including 2 barrier free spaces for the proposed 29 residential units (1.1 spaces/unit), which is slightly below the required 1.25 spaces/unit in the current provisions of the Zoning by-law. The subject parcel is located in close proximity to the City Centre which permits parking at a ratio of 1.0 spaces/unit. It is worth mentioning that a Bus stop is located within 50m from the subject parcel, a multi-use trail is located in front of the subject parcel; and the site is located within a short walking distance to the City Centre which includes access to multiple bus routes and a variety of amenities. Bicycle parking spaces will also be a requirement of site plan control. Administration is in favour of recommending the 1.1 parking spaces per unit.

Amenity Areas: The City of Windsor Zoning by-law requires multi-unit dwellings to include amenity areas to provide opportunities for recreational facilities and/or common spaces. The development will provide some amenity area for the future residents of the building. Those residents will also have access to exceptional public space and facilities adjacent the subject site such as the series of riverfront parks. The recommended zoning amendment includes provisions for an indoor amenity space to be located on the ground floor. The amenity space would also provide partial screening of the parking area from Riverside Drive East.

Setbacks: The current zoning setbacks have minimal side yard, rear yard, or front yard setbacks. The City of Windsor Intensification Guidelines require a minimum 7.5 metre rear yard setback were Medium and High Profile development abuts lot profile properties. Accordingly, administration is recommending site specific side yard, front yard, and rear yard setbacks including a minimum 7.5 m setback for the main building to assist with the separation from the low profile development on the south side of Chatham Street East. Furthermore, to assist with landscaping, massing, building separations, and the context of any future buildings on the subject lands, administration is recommending the following additional site specific setbacks to be included in the zoning by-law amendment:

- minimum rear yard width setback - 7.5m
- minimum front yard width setback - 6.0 m
- minimum side yard width setback - 4.5m

Site Plan Control: The proposed development will be subject to site plan control. The requirements and concerns of municipal departments will be considered during the site plan control process. Recommendation II provides additional direction concerning the circulation of any SPC application, the enhancing of landscaping features, and pedestrian connectivity to nearby amenities.

Consultations:

Comments received from municipal departments and external agencies are attached as Appendix F. Municipal departments have noted no objection to the proposed amendment subject to some requirements, which could be addressed at the time of site plan approval.

Public Notice: Statutory notice was advertised in the Windsor Star, a local daily newspaper. A courtesy notice was mailed to property owners and tenants within 120m of the subject lands.

Risk Analysis:

N/A

Climate Change Risks**Climate Change Mitigation:**

In general, residential intensification minimizes the impact on the Community greenhouse gas emissions as these developments create complete communities and neighbourhoods while using currently available infrastructure such as sewers, sidewalks, and public transit.

Climate Change Adaptation:

The development proposal incorporates landscaping and building design elements to improve energy efficiency and increase resiliency of the development and surrounding area.

Financial Matters:

N/A

Planner's Opinion:

The *Planning Act* requires that a decision of Council in respect of the exercise of any authority that affects a planning matter, "*shall be consistent with*" Provincial Policy Statement 2020. The requested zoning amendment has been evaluated for consistency with the Provincial Policy Statement 2020 and conformity with the policies of the City of Windsor Official Plan.

The proposed Multiple Dwelling represents a well positioned compact form of medium density development that meets the requirements of current and future residents. Furthermore, it represents a form of residential intensification, is set in a location with access to infrastructure, public service facilities, and is close to commercial land uses

Based on the information presented in this report, it is my opinion that the proposed zoning by-law amendment is consistent the PPS, conforms with the policy direction of the City of Windsor Official Plan, is compatible with existing and permitted uses in the surrounding neighbourhood and constitutes good planning.

Conclusion:

Staff recommend approval of an amendment to Zoning By-law 8600, changing the zoning on the lands to permit a multiple dwelling unit. Direction is also provided to the Site Plan Approval Officer in Recommendation II for matters raised from consultations with municipal departments and external agencies.

Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Thom Hunt, MCIP, RPP

City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP JM

Approvals:

Name	Title
Greg Atkinson	Manager, Policy Planning
Neil Robertson for Thom Hunt	City Planner
Wira Vendrasco	Deputy City Solicitor
Jelena Payne	Commissioner of Economic Development and Innovation
Joe Mancina	Chief Administrative Officer

Notifications:

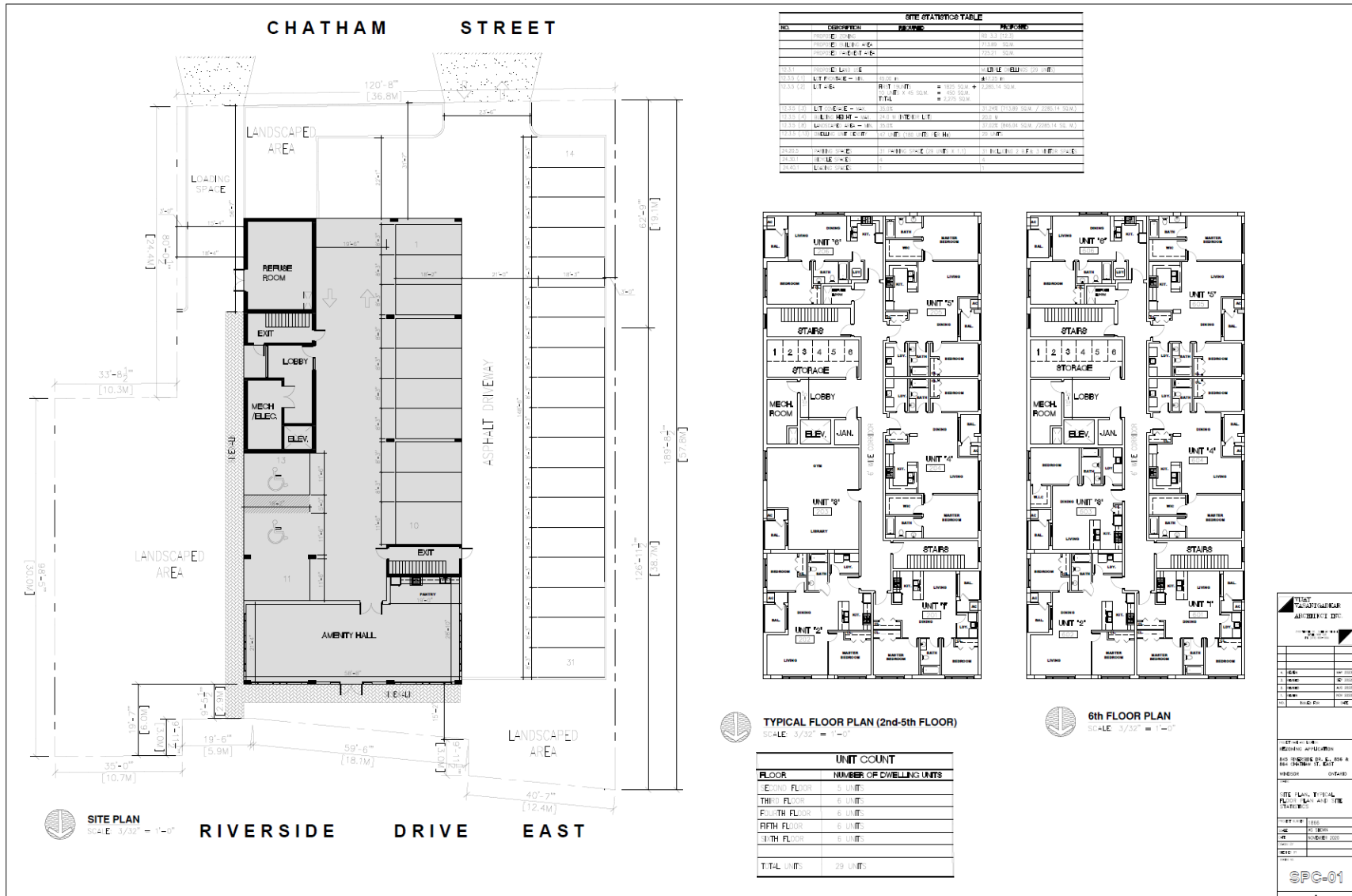
Name	Address	Email
Tracey Pillon-Abbs Pillon Abbs Inc.	23699 Prince Albert Road Chatham, ON N7M 5J7	tpillonabbs@gmail.com
Property owners and tenants within 120 m of the subject lands		

Appendices:

- 1 Appendix A - Concept Plan
- 2 Appendix B- Concept Elevations
- 3 Appendix C- Site Images
- 4 Appendix D- Excerpts from Official Plan
- 5 Appendix E- Excerpts from Zoning By-Law
- 6 Appendix F- Consultations
- 7 Appendix G- Planning Rationale Report
- 8 Appendix H- Stormwater Cover Letter
- 9 Appendix I- Acoustical Report
- 10 Appendix J- Archeological Report Stage II
- 11 Appendix K- Draft Amending By-Law

APPENDIX "A"

Concept Plan



APPENDIX "B" Concept Elevations



VIJAY VARSIANTGADKAR ARCHITECT INC.
1014 LEBANON RD. SUITE 200
 WINDSOR, ONT. N9A 1T3
 TEL: (519) 991-8888

PROJECT:
 REZONING APPLICATION
 845 RIVERSIDE DR. E., 856 &
 864 CHATHAM ST. EAST
 WINDSOR, ONTARIO

DATE:

DESCRIPTION:
 ALL DIMENSIONS ARE IN METERS UNLESS OTHERWISE SPECIFIED.
 ALL DIMENSIONS SHALL BE TO FACE UNLESS OTHERWISE SPECIFIED.
 ALL DIMENSIONS SHALL BE TO FACE UNLESS OTHERWISE SPECIFIED.

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PROJECT:
 Project No. 1866

DATE:
 ELEVATIONS

DATE:
A-1

**APPENDIX “C”
Site Images**



Image 1- Subject Parcel 0 & 845 Riverside Drive East; 856 & 864 Chatham Street East



Image 2- Subject Parcel (looking West on Riverside Drive East)



Image 3 – Subject Parcel (Front lot line - Riverside Drive East)



Image 4 - Subject parcel (Rear lot line fronting Chatham Street East)



Image 5 - Subject parcel (Rear lot line fronting on Chatham Street East- looking East)

APPENDIX “D”

Excerpts from Official Plan

3 Development Strategy

GROWTH CENTRE 3.3.3.1 Growth Centres are the highest in the hierarchy of nodes in Windsor due to their scale, density, range of uses, function and current or future identity.

6 Residential

The lands designated as “Residential” on Schedule D: Land Use provide the main locations for housing in Windsor . In order to develop safe, caring and diverse neighbourhoods, opportunities for a broad range of housing types and complementary services and amenities are provided.

PERMITTED USES 6.3.2.1 Uses permitted in the Residential land use designation identified on Schedule D: Land Use include Low Profile and Medium Profile dwelling units.

6.1 Goals

In keeping with the Strategic Directions, Council’s land use goals are to achieve:

- NEIGHBOURHOODS* 6.1.1 Safe, caring and diverse neighbourhoods.
- ENVIRONMENTALLY SUSTAINABLE* 6.1.2 Environmentally sustainable urban development.
- RESIDENTIAL* 6.1.3 Housing suited to the needs of Windsor’s residents.

6.3.1 Objectives

- RANGE OF FORMS & TENURES* 6.3.1.1 To support a complementary range of housing forms and tenures in all neighbourhoods.
- NEIGHBOURHOODS* 6.3.1.2 To promote compact neighbourhoods which encourage a balanced transportation system.
- ~~*INTENSIFICATION, INFILL & REDEVELOPMENT* 6.3.1.3 To promote selective residential redevelopment, infill and intensification initiatives. (Deleted by OPA #159 – APPROVED July 11, 2022, B/L #100-2022)~~
- INTENSIFICATION, INFILL & REDEVELOPMENT* 6.3.1.3 To promote residential redevelopment, infill and intensification initiatives in locations in accordance with this plan. **(Added by OPA159 - APPROVED July 11, 2022, B/L#100-2022)**
- MAINTENANCE & REHABILITATION* 6.3.1.4 To ensure that the existing housing stock is maintained and rehabilitated.
- SERVICE & AMENITIES* 6.3.1.5 To provide for complementary services and amenities which enhance the quality of residential areas.

HOME BASED OCCUPATIONS 6.3.1.6 To accommodate home based occupations.

SUFFICIENT LAND SUPPLY 6.3.1.7 To ensure that a sufficient land supply for residential and ancillary land uses is available to accommodate market demands over the 20 year period of this Plan.

LOCATIONAL CRITERIA 6.3.2.4 Residential development shall be located where:

- a) there is access to a collector or arterial road;
- b) full municipal physical services can be provided;
- c) adequate community services and open spaces are available or are planned; and
- d) public transportation service can be provided.

8.7 Built Form

8.7.1 Objectives

VARIED DEVELOPMENT PATTERN 8.7.1.1 To achieve a varied development pattern which supports and enhances the urban experience.

COMPLEMENTARY DESIGN 8.7.1.2 To achieve a complementary design relationship between new and existing development, while accommodating an evolution of urban design styles.

VISUAL INTEREST 8.7.1.3 To maximize the variety and visual appeal of building architecture.

ART AND LANDSCAPING 8.7.1.4 To integrate art and landscaping with the built form.

UNIQUE CHARACTER 8.7.1.5 To enhance the unique character of a district, neighbourhood, prominent building or grouping of buildings.

SIGNS 8.7.1.6 To ensure that signs respect and enhance the character of the area in which they are located.

8.7.1.7 To achieve external building designs that reflect high standards of character, appearance, design and sustainable design features.
(Added by OPA #66-11/05/07-B/L209-2007)

8.7.2 Policies

NEW DEVELOPMENT 8.7.2.1 ~~Council will ensure that the design of new development:~~
(Deleted by OPA #66-11/05/07-B/L209-2007)

NEW DEVELOPMENT 8.7.2.1 Council will ensure that the design of new development:
(Added by OPA #66-11/05/07-B/L209-2007)

- (a) ~~is complementary to adjacent development in terms of its overall massing, orientation and setback;~~
(Deleted by OPA #66-11/05/07-B/L209-2007)

- (a) is complementary to adjacent development in terms of its overall massing, orientation, setback and exterior design, particularly character, scale and appearance;
(Added by OPA #66–11/05/07-B/L209-2007)
- (b) provides links with pedestrian, cycle, public transportation and road networks; and
- (c) maintains and enhances valued heritage resources and natural area features and functions.
- (d) Encourages the creation of attractive residential streetscapes through architectural design that reduces the visual dominance of front drive garages, consideration of rear lanes where appropriate, planting of street trees and incorporation of pedestrian scale amenities. **(added by OPA #60–05/07/07-B/L85-2007–OMB Decision/Order No.2667, 10/05/2007)**

REDEVELOPMENT AREAS 8.7.2.2

Council will ensure that the design of extensive areas of redevelopment achieves the following:

- (a) provides a development pattern that support a range of uses and profiles;
- (b) defines the perimeter of such an area by a distinct edge which may be formed by roads, elements of the Greenway System or other linear elements;
- (c) contains activity centres or nodes which are designed to serve the area and which may be identified by one or more landmarks;
- (d) provides transportation links to adjacent areas; and
- (e) maintains and enhances valued historic development patterns or heritage resources.
- (f) is complementary to adjacent development in terms of overall massing, orientation, setback and exterior design, particularly character, scale and appearance.
(Added by OPA #66–11/05/07-B/L209-2007)

INFILL DEVELOPMENT 8.7.2.3

Council will ensure that proposed development within an established neighbourhood is designed to function as an integral and complementary part of that area’s existing development pattern by having regard for:

- (a) massing;
- (b) building height;
- (c) architectural proportion;
- (d) volumes of defined space;
- (e) lot size;

- (f) position relative to the road; and
- (g) building area to site area ratios.
- (h) the pattern, scale and character of existing development; and, **(Added by OPA #66–11/05/07-B/L209-2007)**
- (i) exterior building appearance **(Added by OPA #66–11/05/07-B/L209-2007)**
- (j) Council adopted Design Guidelines that will assist in the design and review of applications for development in accordance with the policies noted above

TRANSITION IN BUILDING HEIGHTS 8.7.2.4 Council will ensure a transition among Very High, High, Medium and Low Profile developments through the application of such urban design measures as incremental changes in building height, massing, space separation or landscape buffer.

CONTINUOUS BUILDING FACADES 8.7.2.5 Council will require new development to support the creation of continuous building facades along Mainstreets through the street level presence of:

- (a) community facilities, retail shops, and other frequently visited uses; and
- (b) architectural features and elements which can be experienced by pedestrians.

APPEALING STREET FACADES 8.7.2.6 Council will encourage the buildings facades to be visually interesting through extensive use of street level entrances and windows. Functions which do not directly serve the public, such as loading bays and blank walls, should not be located directly facing the street.

11.6.3 Zoning By-law Amendment Policies

AMENDMENTS MUST CONFORM 11.6.3.1 All amendments to the Zoning By-law(s) shall conform with this Plan. The Municipality will, on each occasion of approval of a change to the zoning by-law(s), specify that conformity with the Official Plan is maintained or that the change will be in conformity upon the coming into effect of an amendment to the Official Plan.

APPENDIX “E”
Excerpts from Zoning By-law 8600

11.2 RESIDENTIAL DISTRICT 2.2 (RD2.2)

11.2.1 PERMITTED USES

- One Double Duplex Dwelling
- One Duplex Dwelling
- One *Multiple Dwelling* containing a maximum of four *dwelling units*
- One *Semi-Detached Dwelling*
- One Single Unit Dwelling
- Townhome Dwelling*
- Any use accessory to any of the preceding uses

11.2.5 Provisions

.1 Duplex Dwelling

- .1 Lot Width – minimum 12.0 m
- .2 Lot Area – minimum 360.0 m²
- .3 Lot Coverage – maximum 45.0%
- .4 Main Building Height – maximum 9.0 m
- .5 Front Yard Depth – minimum 6.0 m
- .6 Rear Yard Depth – minimum 7.50 m
- .7 Side Yard Width – minimum 1.20 m
- .10 Gross Floor Area – Main Building – maximum 400 m²

.2 Semi-Detached Dwelling

- .1 Lot Width – minimum 15.0 m
- .2 Lot Area – minimum 450.0 m²
- .3 Lot Coverage – maximum 45.0%
- .4 Main Building Height – maximum 9.0 m
- .5 Front Yard Depth – minimum 6.0 m
- .6 Rear Yard Depth – minimum 7.50 m
- .7 Side Yard Width – minimum 1.20 m
- .10 Gross Floor Area – Main Building – maximum 400 m²

.3 Single Unit Dwelling

- .1 Lot Width – minimum 9.0 m
- .2 Lot Area – minimum 270.0 m²
- .3 Lot Coverage – maximum 45.0%
- .4 Main Building Height – maximum 9.0 m
- .5 Front Yard Depth – minimum 6.0 m
- .6 Rear Yard Depth – minimum 7.50 m
- .7 Side Yard Width – minimum 1.20 m
- .10 Gross Floor Area – Main Building – maximum 400 m²

.4 Double Duplex Dwelling or Multiple Dwelling

- .1 Lot Width – minimum 18.0 m
- .2 Lot Area – minimum 540.0 m²
- .3 Lot Coverage – maximum 45.0%
- .4 Main Building Height – maximum 9.0 m
- .5 Front Yard Depth – minimum 6.0 m

- .6 Rear Yard Depth – minimum 7.50 m
- .7 Side Yard Width – minimum 1.80 m

.5 Townhome Dwelling

- .1 Lot Width – minimum 20.0 m
- .2 Lot Area – per *dwelling unit* – minimum 200.0 m²
- .3 Lot Coverage – maximum 45.0%
- .4 Main Building Height – maximum 9.0 m
- .5 Front Yard Depth – minimum 6.0 m
- .6 Rear Yard Depth – minimum 7.50 m
- .7 Side Yard Width – minimum 1.50 m

(AMENDED by B/L 101-2022, July 11, 2022)

12.3 RESIDENTIAL DISTRICT 3.3 (RD3.3)

12.3.1 PERMITTED USES

Lodging House

Multiple Dwelling

Religious Residence

Residential Care Facility

Any of the following *existing* dwellings:

Double Duplex Dwelling

Duplex Dwelling

Semi-Detached Dwelling

Single Unit Dwelling

Any use accessory to any of the preceding uses

12.3.5 PROVISIONS

.1 Lot Frontage – minimum 45.0 m

.2 Lot Area – minimum

For a *corner lot* having a minimum *lot frontage* of 45.0 m on each of the *exterior lot lines*:

a) For the first 23 *dwelling units* 1,825.0 m²

b) For each additional *dwelling unit* 37.0 m² per unit

For any other lot:

c) For the first 19 *dwelling units* 1,825.0 m²

d) For each additional *dwelling unit* 45.0 m² per unit

.3 Lot Coverage – maximum 35.0%

.4 Main Building Height – maximum

Corner Lot 30.0 m

Interior Lot 24.0 m

.8 Landscaped Open Space Yard – minimum 35.0% of *lot area*

.13 Dwelling Unit Density – *dwelling units* per hectare – maximum

For a *corner lot* having a minimum *lot frontage* of 45.0 m on each of the *exterior lot lines* 225 units per ha

For any other *lot* 180 units per ha

.50 A *Lodging House* for the accommodation of 10 persons or less, and any use accessory thereto, shall comply with the *Single Unit Dwelling* provisions of Section 10.1.5 and further, the whole of the *building* shall be used for a *Lodging House*, including any *accessory use*.

[ZNG/5630]

(AMENDED by B/L 95-2019, Sept. 27/2019)

.55 An addition to an existing *Double Duplex Dwelling*, existing *Duplex Dwelling*, existing *Semi-Detached Dwelling* or an existing *Single Unit Dwelling* and any use accessory to the foregoing uses, shall comply with the provisions of Section 11.2.5.

APPENDIX “F” Consultations

Canada Post:

This development, as described, falls within our centralized mail policy. Canada Post's multi-unit policy, which requires that the owner/developer provide the centralized mail facility (front loading lockbox assembly or rear-loading mailroom [mandatory for 100 units or more]), at their own expense, will be in effect for buildings and complexes with a common lobby, common indoor or sheltered space.

ENWIN- Hydro engineering:

ENWIN has existing overhead pole lines with 27,600 volt primary, and 120/240 volt secondary hydro distribution along the South side of the property.

ENWIN has existing overhead 120/240 volt secondary service conductors to 835 & 839 Riverside Dr E along the West side of the development property from Chatham St E.

ENWIN has existing overhead 120/240 volt secondary service conductor to 889 Riverside Dr E along the East side of the zoning by-law amendment location, but through the planned parking lot development.

ENWIN has existing streetlight poles with overhead lines along the North limits with 120/240 volt streetlight distribution.

An easement named to Enwin Utilities Ltd may be required to accommodate the existing overhead 120/240 volt secondary service to the adjacent properties at 835 & 859 Riverside Dr E along the West side of the development property.

Additionally, the owner may be responsible to cover all costs associated with the relocation of the existing overhead 120/240 volt secondary service crossing the development property that services 889 Riverside Dr E.

Prior to working in these areas, we would suggest notifying your contractor and referring to the Occupational Health and Safety Act and Regulations for Construction Projects to confirm clearance requirements during construction.

Essex Region Conservation Authority (ERCA):

With the review of the background information provided and aerial photographs, the ERCA recommends that the stormwater management analysis be completed to the satisfaction of the City of Windsor at the time of Site Plan Control.

Landscape Architect:

please note no objections. Please also note the following comments:

Zoning Provisions for Parking Setback:

The applicant is reminded that the site has frontage on Riverside Drive and a Scenic Drive and Civic way it is also governed by the Riverside Drive Vista Project, which will have requirements for enhanced parking screening and setbacks in addition to those identified in the Zoning Bylaw. As the development will be subject to Site Plan Control, all detailed requirements and landscape comments will be made through that process.

Climate Change:

Applicant is to consider strongly that the need for shade and 'green infrastructure' are necessary to combat climate change. Opportunities will be identified through the Site Plan Control process.

Parkland Dedication:

All requirements will be determined at the time a Site Plan application is received.

Transportation Planning:

Riverside Dr E is classified as a Scenic Parkway according to the Official Plan with a required right-of-way width of 24 meters. The current right-of-way is insufficient, however, the Riverside Drive Vista Improvement Project EA does not require a land conveyance.

Chatham St E is classified as a Local Road according to the Official Plan with a required right-of-way width of 20 meters. The current right-of-way is sufficient, therefore, a land conveyance is not required.

The applicant will be responsible for removing any redundant curb cuts and accesses along the Chatham St E and Riverside Dr E frontage.

All parking should comply with zoning by-law 8600.

All accesses shall conform to the TAC Geometric Design Guide for Canadian Roads and the City of Windsor Standard Engineering Drawings.

- *Driveway width must comply with AS-203 and AS-204 with straight flares.*
- *Engineering Right-of-Way will need to be consulted on driveway compliance at the time of formal submission.*

All exterior paths of travel must meet the requirements of the Accessibility for Ontarians with Disabilities Act (AODA).

Engineering Department:

The site may be serviced by a 1675mmØ Reinforced concrete sanitary sewer on Riverside Dr E and a 525mmØ brick combined sewer on Riverside Dr E and a 300mmØ vitrified clay combined sewer on Chatham St E. A functional servicing study for full municipal services (sanitary, storm, water) is required. Existing connections should be utilized where possible to minimize work within the right-of-way, any redundant private drain connections are to be abandoned in accordance with the City of Windsor Engineering Best Practice B.P 1.3.3, to the satisfaction of the City Engineer. Storm water management completed in accordance with the Windsor/Essex Region Stormwater Standards Manual.

The Official Plan classifies Riverside Dr E as a Scenic Drive with a required right-of-way width of 24 meters. The current right-of-way width is 17.4 meters however, no conveyance is required at this time as per the Riverside Drive Vista Improvements Environmental Assessment.

A Right-of-Way permit is required for any work on the right of way. All driveways are to be constructed as per AS-221 or AS-222, complete with straight flares and no raised curbs within the right-of-way. Redundant curb cuts and sidewalks shall be removed and restored in accordance with City Standards to the satisfaction of the City Engineer.

In summary, we have no objections to this rezoning application, subject to the following: (Requirements can be imposed at the time of Site Plan Control):

Site Plan Control Agreement – The applicant enters into an agreement with the City of Windsor for all requirements under the General Provisions of the Site Plan Control Agreement for the Engineering Department.

Site Servicing Plans – The owner agrees to submit a site servicing plan for the subject lands to the satisfaction of the Chief Building Official, the City Engineer, and ERCA in regulated areas, prior to the issuance of any construction permits for the subject lands.

Servicing Study – The owner agrees, at its own expense, to retain a Consulting Engineer to provide a detailed servicing study report on the impact of the increased flow to the existing municipal sewer system, satisfactory in content to the City Engineer and prior to the issuance of a construction permit. The study shall review the proposed impact and recommend mitigating measures and implementation of those measures. The Study is required to be finalized to the satisfaction of the City Engineer prior to the issuance of construction permits.

Enbridge:

Enbridge Gas requires a minimum separation of 0.6m horizontal and 0.3m vertical from all of our plant less than NPS 16 and a minimum separation 1.0m horizontal and 0.6m vertical between any CER-regulated and vital pipelines. For all pipelines (including vital pipelines), when drilling parallel to the pipeline, a minimum horizontal clearance measured from the edge of the pipeline to the edge of the final bore hole of 1 m (3.3 ft) is required. Please ensure that this minimum separation requirement is maintained, and that the contractor obtains locates prior to performing any work and utilizes safe excavation practices while performing any work in the vicinity.

Transit Windsor:

Transit Windsor has no objections to this development. The closest existing transit route to this property is with the Walkerville 8. The closest existing bus stop to this property is located on Riverside at Parent Southwest Corner. This bus stop is approximately 50 metres from this property falling well within our 400 metre walking distance guideline to a bus stop. This will be maintained with our Council approved Transit Master Plan.

Environment and Sustainability:

The City of Windsor looks forward to reviewing the Energy Strategy that will be submitted as part of the SPC. The review panel hopes to see energy calculations toward zero emissions development using the three scenarios of baseline, higher performance and near zero emissions. These scenarios should include opportunities for efficient building envelopes and EV charging infrastructure.

Building Department:

The Building Code Act, Section 8.(1) requires that a building permit be issued by the Chief Building Official for construction or demolition of a building.

The building permit review process occurs after a development application receives approval and once a building permit application has been submitted to the Building Department and deemed a complete application.

Due to the limited Ontario Building Code related information received, review of the proposed project for compliance to the Ontario Building Code has not yet been conducted.

It is strongly recommended that the owner and/or applicant contact the Building Department to determine building permit needs for the proposed project prior to building permit submission.

PLANNING RATIONALE REPORT

ZONING BY-LAW AMENDMENT

For proposed

RESIDENTIAL DEVELOPMENT

0 Riverside Drive Drive East, 845 Riverside Drive East, 856 Chatham Street East, and 864 Chatham Street East

City of Windsor, Ontario

May 29, 2023

Prepared by:



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1.0 INTRODUCTION

I have been retained by 2356976 Ontario Inc. (c/o Bhan Garg), the applicant/owner, to provide a land use Planning Rationale Report (PRR) in support of a proposed residential development for property located at 0 Riverside Drive Drive East, 845 Riverside Drive East, 856 Chatham Street East and 864 Chatham Street East (herein the “Site”) in the City of Windsor, Province of Ontario.

The Site is presently vacant.

The applicant is proposing to construct a six storey multiple dwelling with 29 residential (rental) dwelling units, as well as parking on-site for 31 vehicles, including 2 barrier free spaces, 3 visitor spaces, 1 loading space and 4 bicycle parking spaces.

The proposed development will provide for more housing.

A site specific Zoning By-law Amendment (ZBA) is required in support of the proposed development. Council for the City of Windsor is the approval authority.

The proposed development will also be subject to Site Plan Control (SPC) prior to the issuance of any building permits.

The purpose of this report is to review the relevant land use documents, including the Provincial Policy Statement (PPS) 2020, the City of Windsor Official Plan (OP) and the City of Windsor Zoning By-law (ZBL) as it pertains to the ZBA application.

Pre-submission was completed by the applicant/owner (City File #PS-111/21). Comments dated November 16, 2021, were received and have been incorporated into the proposed application.

This PRR will show that the proposed development represents good planning addressing the need for the City to provide residential infilling development in the form of a multiple dwelling, which contributes to affordability and intensification requirements.

2.0 SITE AND SURROUNDING LAND USES

2.1 Legal Description and Ownership

The Site has been owned by 2356976 Ontario Inc. (c/o Bhan Garg) since 2018 and has been vacant for over 8 years.

The Site is made up of five (5) parcels located in Ward 4 on the south side of Riverside Drive East and the north side of Chatham Street East, between Marentette Avenue and Parent Avenue (see pin located on Figure 1 – Air Photo).

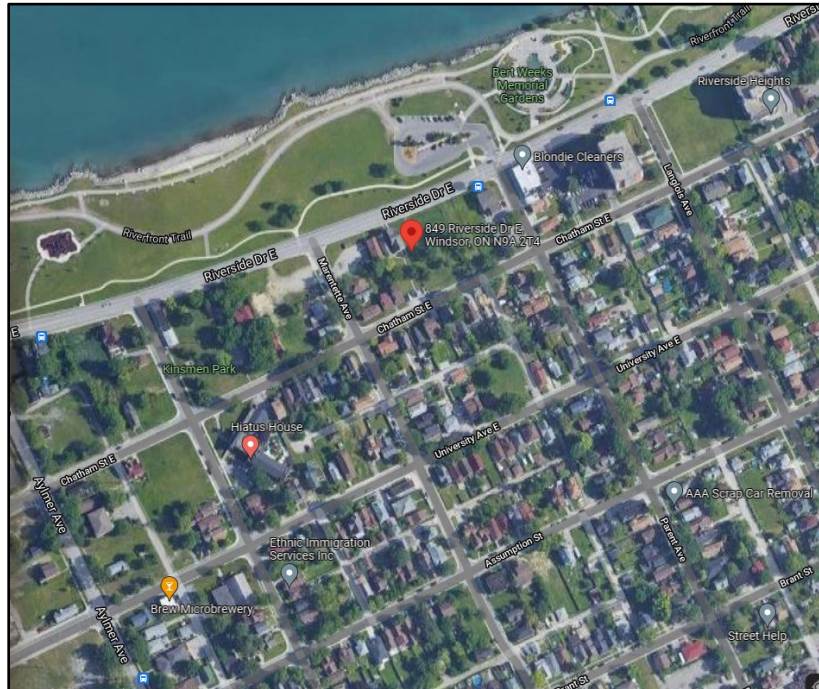


Figure 1 – Air Photo (Google 2021)

The Site is legally described and locally known as follows:

Address	ARN	Legal
0 Riverside Drive East	030-050-00100	Plan 140; Pt Lot 7; Plan 143; Pt Lot 1
0 Riverside Drive East	030-040-09400	Plan 143; Pt Lot 1
845 Riverside Drive East	030-050-00200	Plan 140; Pt Lot 7
856 Chatham Street East	030-050-01400	Plan 140; Pt Lot 7
864 Chatham Street East	030-040-08800	Plan 140; Pt Lot 7; Plan 143; Pt Lot 12

It is the intent of the owner/applicant to merge the 5 parcels of land together as one in order to accommodate the proposed development (see Figure 2 – GIS).

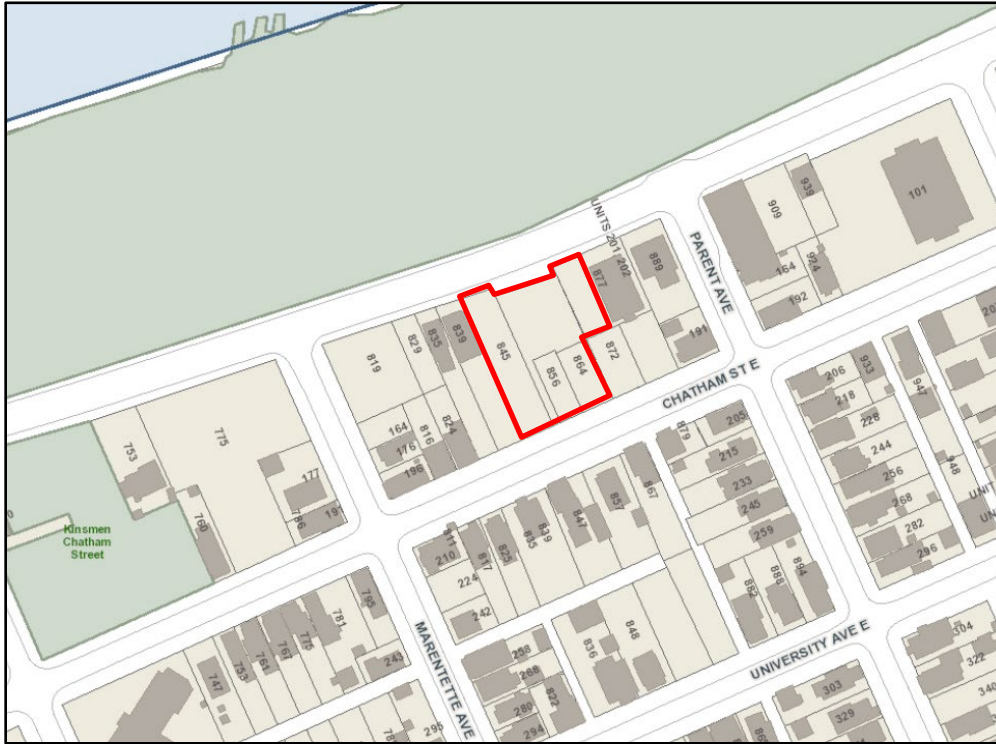


Figure 2 – GIS (City of Windsor Map My City)

2.2 Physical Features of the Site

2.2.1 Size and Site Dimension

The Site consists of a total area of approximately 2,285.14 square metres.

The Site is an irregularly shaped interior lot with an approximate depth of 51.86 m and an approximate width of 47.25 m along the south side of Riverside Drive East.

2.2.2 Vegetation and Soil

The Site currently has a maintained lawn with 2 mature trees.

The soil is made up Brookston Clay Loam (Bcl).

2.2.3 Topography

The Site is flat and is outside the regulated area of the Essex Region Conservation Authority (ERCA).

The Site is not impacted by the ERCA Source Water Protection area.

The Site is part of the Windsor Area Drainage subwatershed area.

2.2.4 Other Physical Features

There is currently fencing around the entire perimeter of the Site.

2.2.5 Municipal Services

The property has access to municipal water, storm and sanitary services.

The closest fire hydrant is located southeast corner of Parent Avenue and Riverside Drive East.

Streetlights are located on the south side of Riverside Drive East.

There are sidewalks on both sides of Riverside Drive East.

Riverside Drive East is a 2-way 4 lane roadway that travels east-west. There is no on-street parking along Riverside Drive East; however, there is a municipal parking lot on the north side of Riverside Drive East directly across the Site. There is on-street parking along Chatham Street East.

2.2.6 Nearby Amenities

There are several schools close to the Site, including Frank W. Begley Public School.

There are many parks and recreation opportunities in proximity of the Site, including Riverfront Trail, Bert Weeks Memorial Gardens and University Ave. Park.

There are nearby commercial uses, such as food service, personal service shops, and retail. There are also nearby employment lands, places of worship, and local/regional amenities.

The Site has access to transit, with the nearest bus stop located at the corner of Riverside Drive East and Parent Ave (Stop ID 1919) on the City of Windsor 8 bus line.

2.3 Surrounding Land Uses

Overall, the Site is located in an established residential area and waterfront area.

A site visit was conducted on December 19, 2021. Photos were taken by Pilon Abbs Inc.

North – The lands to the north of the Site are used waterfront recreation including trails and parks south of the Detroit River (see Photo 1 - North).



Photo 1 – North

East – The lands to the east of the Site are used for low, medium and high profile residential with frontage along Riverside Drive East (see Photo 2 – East).



Photo 2 – East

South – The lands to the south of the Site are used for low profile residential with frontage along Chatham Street East (see Photo 3 - South).



Photo 3 – South

West – The lands to the west of the Site are used for low, medium and high profile residential with frontage along Riverside Drive East (see Photo 4 – West).



Photo 4 – West

3.0 PROPOSAL AND CONSULTATION

.1 Development Proposal

The applicant is proposing to construct a multiple dwelling with 29 residential (rental) dwelling units.

A concept plan has been prepared by Vijay Vasantgadkar Architect Inc. dated May 2023, as revised (see Figure 3a – Site Plan).

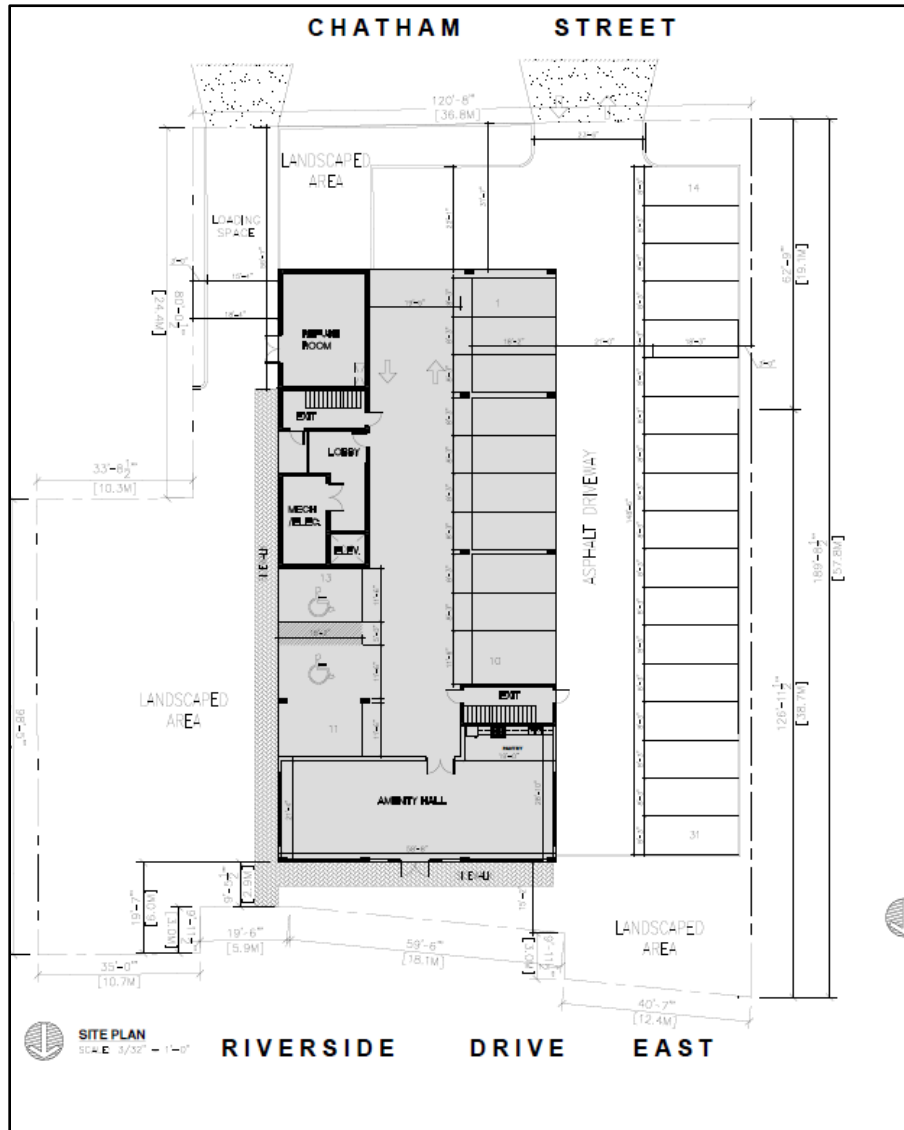


Figure 3a – Site Plan

The proposed building will be six storey in height.

The building will be facing Riverside Drive East. The main entrance into the building will be from the east into a lobby area (see Figure 3b – Floor Plans).

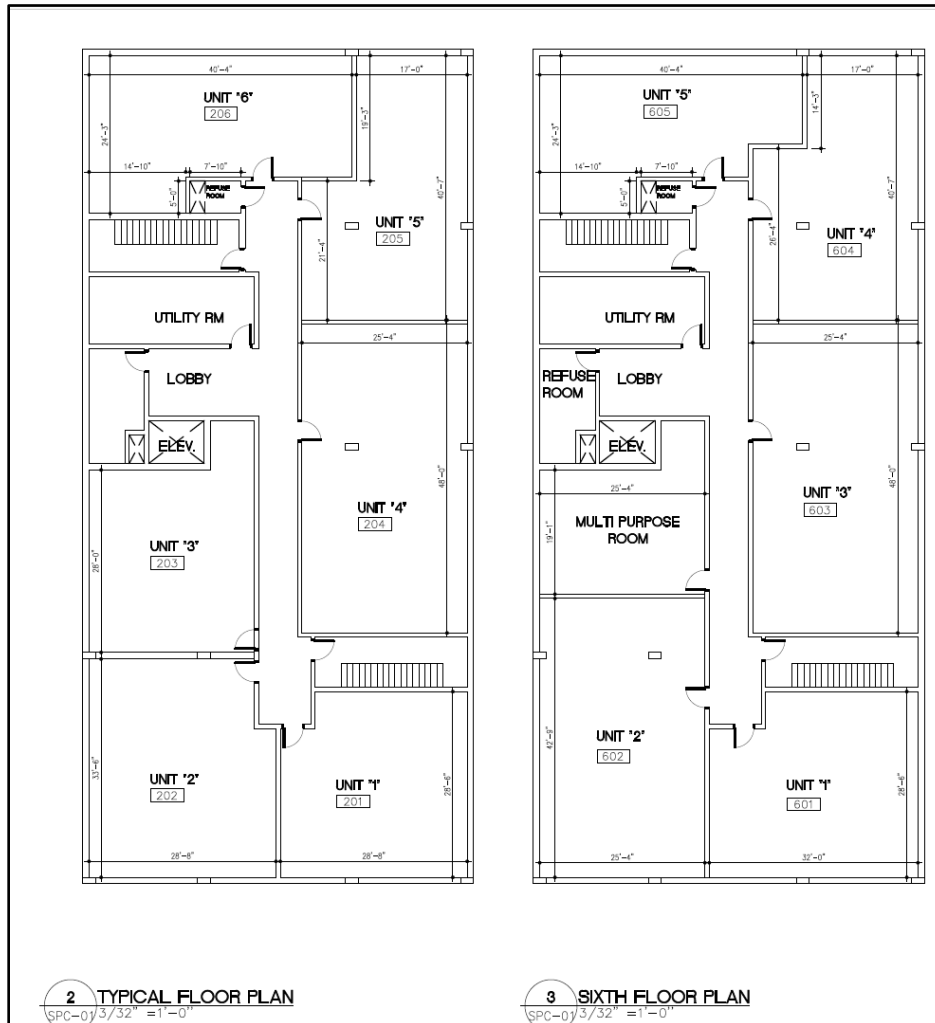


Figure 3b – Floor Plans

The first floor will include an elevator, refuse room and mechanical/electrical room.

The second and fifth floors each have 6 units, and the sixth floor will have 5 units for a total of 29 residential dwelling units.

The proposed building area will be 725.21 m² in size, which is a total lot coverage of 31.24 %.

The development will result in a net density of 127.193 units per hectare.

Sidewalks will connect pedestrians to the proposed building from Riverside Drive East and Chatham Street East.

Landscaped area is proposed.

Amenity spaces is provided include an interior amenity hall and multi purpose rooms.

Vehicle access to the parking area will be from 2 new entrances along Chatham Street East.

Parking will be located under the proposed building and on the east side of the proposed building.

Parking is provided on-site for 31 vehicles, including 2 barrier free spaces, 3 visitor, 1 loading space and 4 bicycle parking spaces.

3.2 Public Consultation Strategy

The Planning Act requires that the applicant submit a proposed strategy for public consultation with respect to an application as part of the complete application requirements.

As part of a public consultation strategy, the applicant proposes that the required public meeting will be sufficient as the size of the development is small scale.

At this time, no informal public open house is proposed to be held by the applicant.

4.0 APPLICATIONS AND STUDIES

4.1 Zoning By-Law Amendment (ZBA)

A site specific Zoning By-law Amendment (ZBA) is required to permit the proposed residential development.

The Site is currently zoned “Residential District 2.2 (RD2.2)” Zone on Map 6 of the City of Windsor Zoning By-Law 8600.

A site specific zoning is required for the Site to permit the proposed multiple dwelling with 5 or more dwelling units.

It is proposed to change the zoning of the Site to a site specific “Residential District 3.3 (RD3.3 – S.20(1) (XXX))” and to provide relief from zone provisions set out in Section 12.3.

Further analysis is provided in Section 5.1.3 of this PRR.

4.2 Other Application

Once the ZBA has been approved, the applicant will proceed with a Site Plan Control (SPC) Application prior to the issuance of a building permit.

The proposed development will be subject to a Development Agreement, which will include any required fees or securities, lighting, buffering, landscaping, signage, etc.

4.3 Supporting Studies

Pre-submission was completed by the applicant/owner (City File #PS-111/21). Comments dated November 16, 2021, were received and have been incorporated into the proposed application.

The following studies have been completed as part of this PRR in support of the application for the zoning amendment.

4.3.1 Archeological

A Stage 1 Archaeological assessment was prepared by AMICK Consultants Limited dated June 5, 2021, and a Stage 2 Archaeological assessment was prepared by AMICK Consultants Limited dated February 12, 2023.

The purpose of the assessments was to review any potentially affected lands by the proposed development.

It was concluded that the proposed undertaking is clear of any concerns and no further assessment of the study area is warranted.

The assessment for Stage 1 has been filed with the Ministry and entered into the Ontario Register. The assessment for Stage 2 has been filed with the Ministry and pending being entered into the Ontario Register.

4.3.2 Energy Study

An Energy Study is not available at this time and will be submitted as part of SPC. The details of the proposed development are not yet completed.

Construction will minimize negative impacts to air quality and climate change, and promote energy efficiency as set out in Section 1.1.3.2 of the City of Windsor Official Plan (OP).

Transportation systems are provided which are safe, energy efficient, facilitate the movement of people and goods, and are appropriate to address projected needs as set out in Section 1.6.7.1 of the OP.

The proposed development supports a compact form within an existing built-up area of the City, which supports Section 1.8 of the OP which requires planning authorities shall support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and preparation for the impacts of a changing climate through land use and development patterns.

The intensification can be accommodated for the proposed development as it is an infilling opportunity within an existing land use pattern.

4.3.3 Infrastructure

A Stormwater Management and Servicing Report was prepared by Haddad Morgan & Associates Ltd dated December 3, 2021.

The purpose of the report was to provide the requirements of infrastructure services in accordance with City guidelines for the proposed development.

It was concluded that there is sufficient services.

4.3.4 Noise

A Noise Assessment Report was prepared by akoustik engineering limited, dated August 12, 2021.

The assessment pertains to the noise impacts caused by the adjacent Riverside Drive roadway.

It was concluded that the proposed development is within manageable limits.

It was recommended that;

- Noise control measures are implemented; and
- A warning clause is included on the title of the property.

4.3.5 Parking

A Parking Study has not been completed as parking relief is minor.

The Site has access to transit, active transportation and a public parking area nearby.

4.3.6 Tree Inventory and Preservation

A Natural Site Features Inventory and Preservation Study has not been completed.

The Site is presently vacant with a manicured lawn.

A total of 2 mature trees are located on the Site.

New landscaping will be proposed as part of Site Plan Control (SPC) approval.

There are no natural heritage features on the Site as shown on Schedule 'D' of the OP.

5.0 PLANNING ANALYSIS

5.1 Policy and Regulatory Overview

5.1.1 Provincial Policy Statement

The Provincial Policy Statement, 2020 (PPS) provides policy direction on matters of provincial interest related to land use planning and development, providing for appropriate development while protecting resources of provincial interest, public health and safety, and the quality of the natural and built environments.

The PPS is issued under Section 3 of the Planning Act and came into effect on May 1, 2020. It applies to all land use planning matters considered after this date.

The PPS supports improved land use planning and management, which contributes to a more effective and efficient land use planning system.

The following provides a summary of the key policy considerations of the PPS as it relates to the proposed development.

PPS Policy #	Policy	Response
1.0Ontario's long-term prosperity, environmental health and social well-being depend on wisely managing change and promoting efficient land use and development patterns.....	The proposed development provides a new housing choice.
1.1.1	Healthy, liveable and safe communities are sustained by: a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term; b) accommodating an appropriate affordable and market-based range and mix of residential types, employment, institutional, recreation, park and open space, and other uses to meet long-term needs;	The proposed development is consistent with the policy to build strong, healthy and livable communities as it provides for a new housing choice in the form of a multiple dwelling. There are no environmental or public health and safety concerns as the area is well established. The development pattern does not require expansion of the settlement area as it is

PPS Policy #	Policy	Response
	<p>c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;</p> <p>d) avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;</p> <p>e) promoting.....cost-effective development patterns and standards to minimize land consumption and servicing costs;</p> <p>f) improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;</p> <p>h) promoting development and land use patterns that conserve biodiversity.</p>	<p>a use of an existing vacant parcel of land.</p> <p>The Site has access to full municipal services and is close to nearby local parks, places of worship and schools.</p> <p>Accessibility of units will be addressed at the time of the building permit application.</p> <p>Public service facilities are available, such as local schools.</p> <p>The development pattern is proposed to be an efficient use of the Site.</p>
1.1.2	<p>Sufficient land shall be made available to accommodate an appropriate range and mix of land uses to meet projected needs for a time horizon of up to 25 years.</p> <p>Within settlement areas, sufficient land shall be made available through intensification and redevelopment and, if necessary, designated growth areas.</p>	<p>The proposed development will help the City of Windsor meet the full range of current and future residential needs through intensification.</p> <p>The intensification can be accommodated for the proposed development as it is a development opportunity within an existing land use pattern.</p> <p>The Site will provide for residential infilling within an existing settlement area in the form of a multiple dwelling.</p>
1.1.3.1	Settlement areas shall be the focus of growth and development.	The proposal enhances the vitality of the Municipality, as

PPS Policy #	Policy	Response
		<p>the proposal is within the City's settlement area.</p> <p>The Site will provide for a range of housing choices consistent with developments in the area.</p>
1.1.3.2	<p>Land use patterns within settlement areas shall be based on densities and a mix of land uses which:</p> <ul style="list-style-type: none"> a) efficiently use land and resources; b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion; c) minimize negative impacts to air quality and climate change, and promote energy efficiency; d) prepare for the impacts of a changing climate; e) support active transportation; f) are transit-supportive, where transit is planned, exists or may be developed; and g) are freight-supportive. 	<p>The total density of the proposed development is considered appropriate as most of the surrounding area is residential.</p> <p>The Site offers an opportunity for intensification by creating new residential units on an underutilized site.</p> <p>The intensification can be accommodated for the proposed development as it is an infilling opportunity within an existing land use pattern.</p> <p>The proposed design and style of the proposed multiple dwelling will blend with the residential uses in the area.</p> <p>Residents will have immediate access to shopping, employment, transit, active transportation, recreational areas and institutional uses.</p> <p>Transit is available for the area.</p>
1.1.3.3	<p>Planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range</p>	<p>The intensification can be accommodated for the proposed residential development as it is an appropriate development of the vacant parcel of land.</p>

PPS Policy #	Policy	Response
	of housing options through intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.	
1.1.3.4	Appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety.	The intensification can be accommodated for the proposed development as it is a development opportunity within an existing land use pattern. There will be no risks to the public.
1.1.3.5	Planning authorities shall establish and implement minimum targets for intensification and redevelopment within built-up areas, based on local conditions.	The City has established targets for intensification and redevelopment. The proposed development will assist in meeting those targets as the Site is located in an existing built-up area and will add new residential units.
1.1.3.6	New development taking place in designated growth areas should occur adjacent to the existing built-up area and should have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities.	The proposed development does have a compact form. The medium profile density will allow for the efficient use of land, infrastructure and public services.
1.4.1	To provide for an appropriate range and mix of housing options and densities required to meet projected requirements of current and future residents of the regional	The proposed development will provide for a new housing choice and density in the existing built-up area.

PPS Policy #	Policy	Response
	<p>market area, planning authorities shall:</p> <p>a) maintain at all times the ability to accommodate residential growth for a minimum of 15 years through residential intensification and redevelopment and, if necessary, lands which are designated and available for residential development; and</p> <p>b) maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned to facilitate residential intensification and redevelopment, and land in draft approved and registered plans.</p>	<p>The intensification can be accommodated for the proposed development as it is a development opportunity within an existing land use pattern.</p> <p>The area is pedestrian-friendly, allowing people to access nearby amenities, such as public spaces, commercial nodes, and recreational activities. The proposed density offers an opportunity to efficiently use municipal infrastructure.</p> <p>Full municipal services are available.</p>
1.4.3	<p>Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs of current and future residents of the regional market area.</p>	<p>The proposed medium profile density is compatible with the surrounding area and will provide affordable intensification and infilling through the efficient use of the Site.</p> <p>The Site is close to nearby amenities.</p> <p>There is suitable and has access to full municipal infrastructure.</p>
1.6.1	<p>Infrastructure and public service facilities shall be provided in an efficient manner that prepares for the impacts of a changing climate while accommodating projected</p>	<p>The development is already on full municipal services.</p> <p>Access to public transit is available.</p>

PPS Policy #	Policy	Response
1.6.6.2	<p>needs.</p> <p>Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. Within settlement areas with existing municipal sewage services and municipal water services, intensification and redevelopment shall be promoted wherever feasible to optimize the use of the services.</p>	<p>The proposed development will be serviced by municipal sewer, water and storm, which is the preferred form of serving for settlement areas.</p> <p>There will be no anticipated impacts on the municipal system, and will not add to the capacity in a significant way.</p>
1.6.6.7	<p>Planning for stormwater management shall:</p> <ul style="list-style-type: none"> a) be integrated with planning for sewage and water services and ensure that systems are optimized, feasible and financially viable over the long term; b) minimize, or, where possible, prevent increases in contaminant loads; c) minimize erosion and changes in water balance, and prepare for the impacts of a changing climate through the effective management of stormwater, including the use of green infrastructure; d) mitigate risks to human health, safety, property and the environment; e) maximize the extent and function of vegetative and pervious surfaces; and 	<p>There will be no risk to health and safety.</p>

PPS Policy #	Policy	Response
	f) promote stormwater management best practices, including stormwater attenuation and re-use, water conservation and efficiency, and low impact development.	
1.6.7.1	Transportation systems should be provided which are safe, energy efficient, facilitate the movement of people and goods, and are appropriate to address projected needs.	The subject property is near major roadways and has access to transit.
1.6.7.2	Efficient use should be made of existing and planned infrastructure, including through the use of transportation demand management strategies, where feasible.	The proposed development contributes to the City's requirements for development within a built-up area. The area is serviced by transit.
1.6.7.4	A land use pattern, density and mix of uses should be promoted that minimize the length and number of vehicle trips and support current and future use of transit and active transportation.	The proposed development contributes to the City's requirement for infilling within a built-up area. Parking is provided on-site. Minor relief is requested. The area is pedestrian-friendly, allowing people to access nearby amenities, such as public spaces, commercial nodes, and recreational activities. The proposed density offers an opportunity to efficiently use municipal infrastructure.
1.8	Planning authorities shall support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and preparing	The proposed development supports a compact form within an existing built-up area of the City.

PPS Policy #	Policy	Response
	for the impacts of a changing climate through land use and development patterns.	The Site has access to transit and local amenities.
2.1.1	Natural features and areas shall be protected for the long term.	There are no natural features that apply to this Site. There are no trees on the Site.
2.2.1	Planning authorities shall protect, improve or restore the quality and quantity of water.	Full municipal services are available.
2.6.1	Significant built heritage resources and significant cultural heritage landscapes shall be conserved.	An assessment was completed.
3.0	Development shall be directed away from areas of natural or human-made hazards where there is an unacceptable risk to public health or safety or of property damage, and not create new or aggravate existing hazards.	There are no natural or human-made hazards that apply to this Site. The Site is outside of the ERCA regulated area.

Therefore, the proposed development is consistent with the PPS.

5.1.2 Official Plan

The City of Windsor Official Plan (OP) was adopted by Council on October 25, 1999, approved in part by the Ministry of Municipal Affairs and Housing (MMAH) on March 28, 2000 and the remainder approved by the Ontario Municipal Board (OMB) on November 1, 2002. Office consolidation version is dated September 7, 2012.

The OP implements the PPS and establishes a policy framework to guide land use planning decisions related to development and the provision of infrastructure and community services throughout the City.

The lands are designated “Residential” according to Schedule “D – “Land Use” attached to the OP for the City of Windsor (see Figure 4 – City of Windsor OP, Schedule “D”).

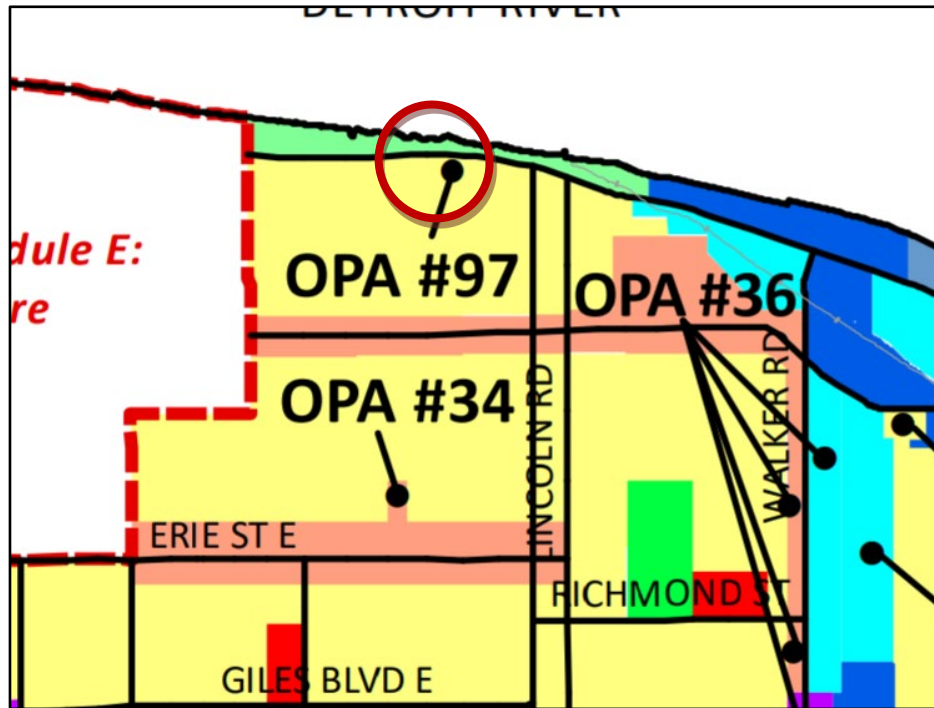


Figure 4 – City of Windsor OP, Schedule “D”

The following provides a summary of the key policy considerations of the OP as it relates to the proposed development.

OP Policy #	Policy	Response
3.2.1.2	Encouraging a range of housing types will ensure that people have an opportunity to live in their neighbourhoods as they pass through the various stages of their lives.	The proposed residential development supports one of the City’s overall development strategies of providing for a range of housing types. The proposed development will be rental units close to transit and local amenities.
3.3.3	Neighbourhoods are the most basic component of Windsor’s urban structure and occupy the greatest proportion of the City. Neighbourhoods are stable, low-to-medium-density residential areas and are	The proposed residential development is in an existing built-up area. The proposed building will blend with the surrounding built-up area.

OP Policy #	Policy	Response
	<p>comprised of local streets, parks, open spaces, schools, minor institutions and neighbourhood and convenience scale retail services.</p> <p>The three dominant types of dwellings in Windsor's neighbourhoods are single detached, semi-detached and townhouses.</p> <p>The density range for Windsor's neighbourhoods is between 20 to 35 units per net hectare.</p> <p>This density range provides for low and some medium-density intensification to occur in existing neighbourhoods. Multiple dwelling buildings with medium and high-densities are encouraged at nodes identified in the Urban Structure Plan.</p>	<p>The Site is not in a node; however, it offers appropriate infilling in the existing neighbourhood.</p> <p>The area is pedestrian-friendly, allowing people to access nearby amenities, such as public spaces, commercial nodes, and recreational activities.</p> <p>The proposed density offers an opportunity to efficiently use municipal infrastructure.</p> <p>The Site will provide for a range of housing options.</p>
4.0	<p>The implementing healthy community policies are interwoven throughout the remainder of the Plan, particularly within the Environment, Land Use, Infrastructure and Urban Design chapters, to ensure their consideration and application as a part of the planning process.</p>	<p>The proposed development will support the City's goal of promoting a healthy community (live, work and play).</p> <p>The proposed development is close to nearby transit, employment, shopping, local/regional amenities and parks.</p>
5.0	<p>A healthy and sustainable environment represents a balance between human activities and natural features and functions. In order to attain this balance, Council will enhance the quality of</p>	<p>The proposed development will support the City's goal of a healthy and sustainable environment.</p> <p>The Site is pedestrian-friendly, as there are sidewalks that will</p>

OP Policy #	Policy	Response
	Windsor's natural environment and manage development in a manner that recognizes the environment as the basis of a safe, caring and diverse community and a vibrant economy.	<p>link to the surrounding amenities.</p> <p>The Site is level, which is conducive to easy vehicular movements.</p> <p>There are no anticipated traffic concerns, no environmental concerns, and no expected hazards.</p>
6.0 - Preamble	A healthy and livable city is one in which people can enjoy a vibrant economy and a sustainable healthy environment in safe, caring and diverse neighbourhoods. In order to ensure that Windsor is such a city, Council will manage development through an approach which balances environmental, social and economic considerations.	<p>The proposed development supports the policy set out in the OP as it is suited for the residential needs of the City.</p> <p>The Site will provide for a new housing choice.</p>
6.1 - Goals	<p>In keeping with the Strategic Directions, Council's land use goals are to achieve:</p> <p>6.1.1 Safe, caring and diverse neighbourhoods.</p> <p>6.1.3 Housing suited to the needs of Windsor's residents.</p> <p>6.1.10 Pedestrian oriented clusters of residential, commercial, employment and institutional uses.</p>	The proposed development supports the goals set out in the OP as it provides for housing that is suited to residents in this area of Windsor, is pedestrian-oriented, and is close to employment and schooling opportunities.
6.1.14 (OPA 159)	i) To direct residential intensification to those areas of the City where transportation, municipal services, community facilities	The proposed development supports this policy.

OP Policy #	Policy	Response
	and goods and services are readily available.	
6.2.1.2 – General Policies	<p>For the purpose of this Plan, Development Profile refers to the height of a building or structure. Accordingly, the following Development Profiles apply to all land use designations on Schedule D: Land Use unless specifically provided elsewhere in this Plan:</p> <p>(a) Low Profile developments are buildings or structures generally no greater than three (3) storeys in height;</p> <p>(b) Medium Profile developments are buildings or structures generally no greater than six (6) storeys in height; and</p> <p>(c) High Profile developments are buildings or structures generally, no greater than fourteen (14) storeys in height.</p>	The structure is considered a medium profile building.
6.3.1.3 (OPA 159)	To promote residential redevelopment, infill and intensification initiatives in locations in accordance with this plan.	The proposed development supports this policy.
6.3.2.5	<p>At the time of submission, the proponent shall demonstrate to the satisfaction of the Municipality that a proposed residential development within an area having a Neighbourhood development pattern is:</p> <p>(a) feasible having regard to the other provisions of this Plan, provincial legislation,</p>	<p>This PRR has addressed these requirements.</p> <p>All RD3.3 zone provisions shall comply, except minor zoning relief is requested for parking.</p> <p>The proposed development is compatible with the surrounding area and pedestrian friendly.</p>

OP Policy #	Policy	Response
	<p>policies and appropriate guidelines and support studies for uses: (i) within or adjacent to any area identified on Schedule C: Development Constraint Areas and described in the Environment chapter of this Plan; (ii) adjacent to sources of nuisance, such as noise, odour, vibration and dust; (iii) within a site of potential or known contamination; (iv) where traffic generation and distribution is a provincial or municipal concern; and (v) adjacent to heritage resources. (b) in keeping with the goals, objectives and policies of any secondary plan or guideline plan affecting the surrounding area; In existing neighbourhoods, compatible with the surrounding area in terms of scale, massing, height, siting, orientation, setbacks, parking and amenity areas. In Mature Neighbourhoods as shown on Schedule A-1, compatible with the surrounding area, as noted above, and consistent with the streetscape, architectural style and materials, landscape character and setback between the buildings and streets; (d) provided with adequate off street parking; (e) capable of being provided with full municipal physical services and emergency services; and f) facilitating a gradual transition from Low Profile residential development to Medium</p>	<p>The proposed development provides for amenity space and is close to nearby community facilities and services.</p> <p>The Site is not in a mature neighbourhood.</p> <p>Support studies have been completed, including noise, servicing and archaeology.</p>

OP Policy #	Policy	Response
	and/or High Profile development and vice versa, where appropriate, in accordance with Design Guidelines approved by Council.	
6.3.1.1 (Residential)	To support a complementary range of housing forms and tenures in all neighbourhoods.	The proposed development provides for a new housing choice in an existing built-up area.
6.3.1.2	To promote compact neighbourhoods which encourage a balanced transportation system.	The design is compact. All RD3.3 zone provisions shall comply, except minor zoning relief is requested for parking reductions.
6.3.1.3	To promote selective residential redevelopment, infill and intensification initiatives.	The intensification can be accommodated for the proposed development as it is a development opportunity within an existing land use pattern.
6.3.2.3	For the purposes of this Plan, Low Profile housing development is further classified as follows: (a) small scale forms: single detached, semi-detached, duplex and row and multiplexes with up to 8 units; and (b) large scale forms: buildings with more than 8 units.	The proposed development is considered large scale medium profile with a total of 29 units proposed.
6.3.2.4	Residential development shall be located where: (a) there is access to a collector or arterial road; (b) full municipal physical services can be provided; (c) adequate community services and open spaces are	The Site has pedestrian access to Riverside Drive East and pedestrian and vehicle access to Chatham Street East. Full municipal services are available.

OP Policy #	Policy	Response
	available or are planned; and (d) public transportation service can be provided.	Access to transit is available.
6.3.2.5	Official Plan policy 6.3.2.5 outlines that PRR is required at the time of submission to outline land use compatibility.	<p>Section 5.1.2 of the PRR, provided a reference to how the justification for this policy was provided throughout the entire report.</p> <p>The profile of the building is considered a medium profile building. The proposed development does have a compact form and uses the Site as efficiently as possible.</p> <p>The proposed development is compatible with the surrounding area and will provide affordable intensification and infilling through the efficient use of the Site. It provides a transition from Riverside Drive East to the low profile density located to the south. There are other similar types of structures along Riverside Drive East.</p> <p>Appropriate buffering can be provided as part of SPC approval through fencing and landscaping.</p> <p>Urban design features are not available as the details of the proposed development are not yet completed.</p> <p>A concept plan was provided in section 3.1 of the PRR.</p>
7.0 - Infrastructure	The provision of proper infrastructure provides a safe, healthy and efficient living environment. In order to	The Site is close to nearby transit, off a major roadway and has access to full municipal services.

OP Policy #	Policy	Response
	<p>accommodate transportation and physical service needs in Windsor, Council is committed to ensuring that infrastructure is provided in a sustainable, orderly and coordinated fashion.</p>	<p>An assessment of services has been completed.</p> <p>An assessment of noise from the roadway has been completed, and mitigation measures are recommended.</p> <p>There will be no negative impacts on the municipal system as the proposed buildings will be limited to medium profile and will not add to the capacity in a significant way.</p>
8 – Urban Design	<p>A memorable, attractive and liveable city is one where people feel comfortable and are inspired by their surroundings. The physical systems and built form of the City are also designed to protect, maintain and improve the quality of life for present and future generations by integrating the principles of sustainability and place making. In order for Windsor to be such a city, Council is committed to urban design principles that enhance the enjoyment and image of Windsor and its people</p>	<p>The design of the proposed multiple dwelling will blend with the surrounding residential area.</p> <p>The proposed building will be limited to 6 storeys in the zoning by-law regulations, is pedestrian-friendly, has a clean façade and is a safe place for people to live.</p> <p>The Site is compatible with the surrounding area in terms of scale, massing, height and siting.</p> <p>The proposed building will integrate well with the surrounding area.</p>
8.11.12.12 and 8.11.12.13 – Civic Way	<p>Council will promote the development of Civic Ways at the locations identified on Schedule G: Civic Image. Such Civic Ways will be designed to : (a) promote and present an attractive and unifying image of Windsor; (b) maintain a sense of</p>	<p>The subject lands are designated as a Civic Way in the Official Plan (Schedule G), and adjacent to the Greenway System (Schedule 'B') of Central Riverfront Lands.</p> <p>Development along Riverside Drive is to be complementary</p>

OP Policy #	Policy	Response
	<p>welcome and arrival for travelers; (c) create a memorable impression of Windsor; and (d) complement and enhance the Municipality's capital investment in major infrastructure.</p> <p>Council will recognize the significance of roads designated as Civic Ways on Schedule G: Civic Image by:</p> <p>(a) enhancing the public rights-of-way along major entry points into Windsor consistent with a highly attractive and distinctive image using unifying elements such as landscaping, fixtures and boulevard and median treatments; and (b) protecting and enhancing significant views and vistas, public space and heritage resources along the Civic Way.</p>	<p>to those areas as identified in the Official Plan sections 8.11.12.12 and 8.11.12.13, which require the provision of enhanced landscape and urban design for the frontages of the development along Civic Ways.</p> <p>The details of the proposed development are not yet completed.</p> <p>Development is subject to SPC, and the design of the building can be assessed at that time.</p> <p>The Greenway System is across the street on the north side of Riverside Drive East and is not directly abutting. The proposed development will have no direct impact on the system.</p> <p>The Site is currently zoned "Residential District 2.2 (RD2.2)" Zone on Map 6 of the City of Windsor Zoning By-Law 8600. It is proposed to change the zoning of the Site to a site specific "Residential District 3.3 (RD3.3 – S.20(1) (XXX))" to permit the proposed multiple dwelling and to provide relief from zone provisions set out in Section 12.3.</p> <p>The requested zone change will complement the civic image along Riverside Drive East as it increases the density of the Site and</p>

OP Policy #	Policy	Response
		provides for the intensification of a vacant and underutilized parcel of land.

Therefore, the proposed development conforms to the City of Windsor OP, and no amendment is required.

5.1.3 Zoning By-law

The City of Windsor Zoning By-law (ZBL) #8600 was passed by Council on July 8, 2002, and then a further Ontario Municipal Board (OMB) decision was issued on January 14, 2003.

A ZBL implements the PPS and the City OP by regulating the specific use of the property and provides for its day-to-day administration.

The Site is currently zoned “Residential District 2.2 (RD2.2)” Zone on Map 6 of the City of Windsor Zoning By-Law 8600 (see Figure 5 – City of Windsor Zoning Map 6).

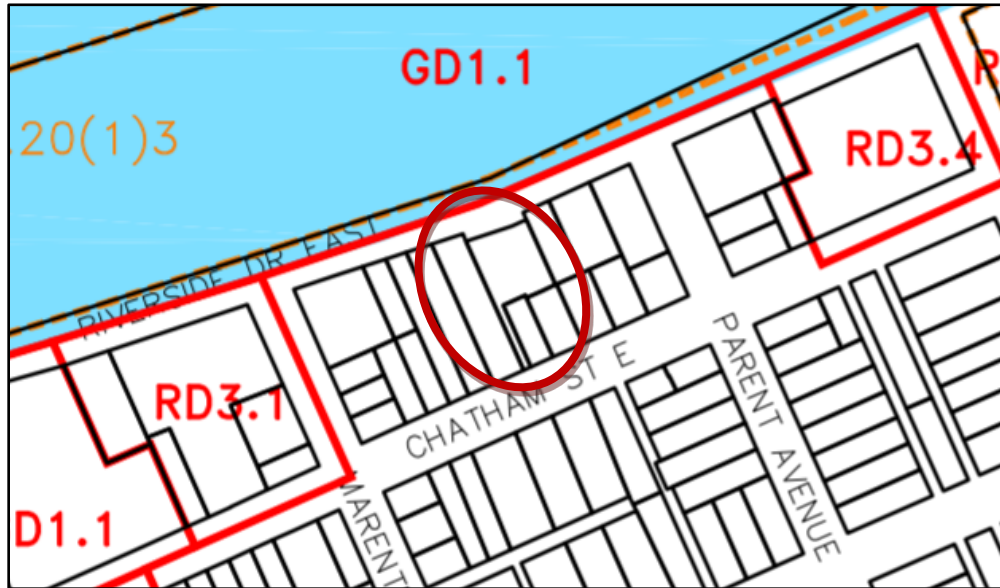


Figure 5 – City of Windsor Zoning Map 6

A site specific zoning is required for the Site in order to permit the proposed multiple dwelling with 5 or more dwelling units.

MULTIPLE DWELLING means one dwelling containing a minimum of three dwelling units. A double duplex dwelling, semi-detached dwelling, stacked dwelling, or townhome dwelling is not a multiple dwelling.

It is proposed to change the zoning of the Site to a site specific “Residential District 3.3 (RD3.3 – S.20(1) (XXX))” and to provide relief from zone provisions set out in Section 12.3.

A review of the RD3.3 zone provisions, as set out in Section 12.3 of the ZBL are as follows:

Zone Regulations Sec 12.3	Required RD2.5	Proposed	Compliance and/or Relief Requested Justification
Permitted Uses	Lodging House Multiple Dwelling Religious Residence Residential Care Facility	Multiple Dwelling (29 units)	Complies, subject to the ZBL application.

	Any of the following existing dwellings: Double Duplex Dwelling Duplex Dwelling Semi-Detached Dwelling Single Unit Dwelling Any use accessory to any of the preceding uses		
Minimum Lot Frontage	45.0 m	47.25 m	Complies
Minimum Lot Area	For any other lot: c) For the first 19 dwelling units 1,825.0 m ² d) For each additional dwelling unit 45.0 m ² per unit 1,825 m ² + 450 m ² = 2,275 m²	2,285.14 m ²	Complies
Maximum Lot Coverage	35 %	31.24 %	Complies
Maximum Building Height	Interior Lot 24.0 m	20.0 m	Complies
Landscaped Open Space Yard – minimum	35.0 % of lot area	37.02 %	Complies

Dwelling Unit Density – dwelling units per hectare – maximum	For any other lot 180 units per ha = 47 units	29 units	Complies
Minimum Parking Requirements 24.20.5.1	Multiple Dwelling containing a minimum of 5 Dwelling units - 1.25 for each dwelling unit 29 units x 1.25 = 36.25 (36 rounded down)	31	Relief request of 5 spaces. The Site has access to transit and active transportation. There is also a public parking area across the street.
Minimum Visitor Parking 24.22.1	a minimum of 15 percent of parking spaces shall be marked as visitor parking = 4.65 (4 rounded down)	3	Relief request of 1 space. The Site has access to transit and active transportation.
Minimum Accessible Parking Spaces 24.24.1	26 to 100 – 2 percent of parking spaces (Types A and B) = 2% of 31 parking spaces is 0.62 parking spaces	2	Complies
Minimum Bicycle Parking Spaces 24.30.1.1	20 or more - 2 for the first 19 spaces plus 1 for each additional 20 parking spaces 2 + 0.6 = 2.6 (2 rounded down)	4	Complies

Minimum Loading 24.20.1.5	GFA - 1,000 m ² or less = 0	1	Complies
Parking Area Separation 25.5.20.1	Any other street – 3.00 m	>3.00 m	Complies
25.5.20.3	An interior lot line or alley – 0.90 m	0.91 m	Complies
25.5.20.5	A building wall in which is located a main pedestrian entrance facing the parking area – 2.00 m	>2.00 m	Complies
25.5.20.6	A building wall containing a habitable room window or containing both a main pedestrian entrance and a habitable room window facing the parking area where the building is located on the same lot as the parking area – 4.50 m	>4.50 m	Complies

Therefore, the proposed development will require a site specific zoning RD3.3 - S.20(1)(XXX) in order to permit the proposed multiple dwelling with 5 or more dwelling units.

All other RD3.3 zone provisions shall comply with the exception of the following requested relief:

1. *decrease the required minimum parking requirement from 36 to 31 spaces, and*
2. *decrease the required minimum visitor parking requirement from 4 to 3.*

6.0 SUMMARY AND CONCLUSION

6.1 Context and Site Suitability Summary

6.1.1 Site Suitability

The Site is ideally suited for residential development for the following reasons:

- The land area is sufficient to accommodate the proposed development,
- The Site is generally level, which is conducive to easy vehicular movements,
- The Site already accommodates municipal water, storm and sewer systems,
- There are no anticipated traffic concerns,
- There are no environmental concerns,
- There are no hazards, and
- The location of the proposed development is appropriate in that it will blend well with the residential uses in the surrounding area.

6.1.2 Compatibility of Design

The proposed development provides efficient ease of access to the proposed parking area.

The Site is compatible with the surrounding area in terms of scale, massing, height and siting.

6.1.3 Good Planning

The proposal represents good planning as it addresses the need for the City to provide residential infilling development.

The additional units will contribute toward affordability and intensification requirements.

Residential use on the Site represents an efficient development pattern that optimizes the use of land in an existing built-up area which has residential uses surrounding the Site.

The additional residential units will not put any additional stress on municipal Infrastructure or the current Site.

6.1.4 Natural Environment Impacts

The proposal does not have any negative natural environmental impacts, as there are no natural heritage features on the Site.

6.1.5 Municipal Services Impacts

There will be no negative impacts on the municipal system as the residential development is limited to a medium profile and will not add to the capacity in a significant way.

6.1.6 Social and/or Economic Conditions

The proposed development does not negatively affect the social environment as the Site is in close proximity to major transportation corridors, transit, places of worship and community amenities.

Adding additional residential units on an underutilized site in an area with residential uses contributes toward the goal of 'live, work and play' where citizens share a strong sense of belonging and a collective pride of place.

The proposed development promotes efficient development and land use pattern, which sustains the financial well-being of the Municipality.

The proposal does not cause any public health and safety concerns. The proposal represents a cost-effective development pattern that minimizes land consumption and servicing costs.

There will be no urban sprawl as the proposed development is within the existing settlement area and is an ideal development opportunity.

6.2 Conclusion

The proposal to add a multiple dwelling on the Site is appropriate and should be approved by the City of Windsor.

This PRR has shown that the proposed development is suitable intensification of affordable residential use, is consistent with the PPS, conforms with the intent and purpose of the City of Windsor OP and represents good planning.

The report components for this PRR have set out the following, as required under the City of Windsor OP:

10.2.13.2 Where a Planning Rationale Report is required, such a study should:

- (a) Include a description of the proposal and the approvals required;*
- (b) Describe the Site's previous development approval history;*
- (c) Describe major physical features or attributes of the Site including current land uses(s) and surrounding land uses, built form and contextual considerations;*
- (d) Describe whether the proposal is consistent with the provincial policy statements issued under the Planning Act.*
- (e) Describe the way in which relevant Official Plan policies will be addressed, including both general policies and site-specific land use designations and policies;*
- (f) Describe whether the proposal addresses the Community Strategic Plan;*

(g) Describe the suitability of the Site and indicate reasons why the proposal is appropriate for this Site and will function well to meet the needs of the intended future users;

(h) Provide an analysis of the compatibility of the design and massing of the proposed developments and land use designations;

(i) Provide an analysis and opinion as to why the proposal represents good planning, including the details of any methods that are used to mitigate potential negative impacts;

(j) Describe the impact on the natural environment;

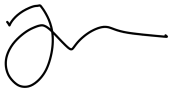
(k) Describe the impact on municipal services;

(l) Describe how the proposal will affect the social and/or economic conditions using demographic information and current trends; and,

(m) Describe areas of compliance and non-compliance with the Zoning By-law.

Planner's Certificate:

I hereby certify that this report was prepared by Tracey Pillon-Abbs, a Registered Professional Planner, within the meaning of the Ontario Professional Planners Institute Act, 1994.



**Tracey Pillon-Abbs, RPP
Principal Planner**



Stormwater Management
and Servicing Report
Proposed Residential Building
845 Riverside Drive East
Windsor, Ontario

Our Project No. 21-232

December 3, 2021

Catherine Girgis, P.Eng.
Project Engineer

SUBMITTED TO:

The City of Windsor

The principal objective of this report is to provide the proposed site servicing for a new residential building located at 845 Riverside Drive East. This shall include the stormwater storage requirement, in accordance with the storm water management guidelines.

Available Infrastructure

Based on the City of Windsor Sewer Atlas and the topographic survey, the current property is serviced by the following:

- 1650 mm diameter combined pipe sanitary sewer
- 525 x 700 mm diameter brick pipe combined sewer
- Existing watermain
- Existing private drain connections to the property shall be determined prior to construction

Current Conditions

The site has been vacant for a minimum of 8 years and is covered in dense well-maintained grass. The property covers an area of 2.521 square metres (27,140 square feet / 0.62 acres) with a runoff coefficient of 0.20.

Based on our analysis (attached) and applying an overland routing of 54 m at a 1.9% slope and using a Manning's "n" of 0.24, it was found that the time of concentration would be approximately 25 minutes, which translates to a release rate of 0.0092 cubic metres per second.

The existence of private drain connections to the site are not known. During construction, if the contractor becomes aware of an existing connection, the service will be videoed, reviewed by the City of Windsor, and re-used if feasible. City of Windsor service abandonment standards shall be followed for any redundant services.

STORM

Proposed Development

The proposed project includes the construction of a multi-storey residential building and curbed parking lots. In the developed condition, the surface condition breakdown will be as follows:

Building Area	831 square metres
Paved/Hardscape Area	708 square metres
Landscaped Area	982 square metres

The introduction of the new on-site storm network would change the overland flow distance, in addition to accounting for the flow on paved infrastructure. The expected time of concentration for both these values results is approximately 25 minutes. Based on the attached calculations, the anticipated runoff for the evaluated storm events would be:

5-year Event	0.0092 cubic metres per second
100-year Event	0.0152 cubic metres per second

Based on the calculated values, the pre-development run-off of 0.0092 cubic metres per second will govern; and will be the basis for determining the storage requirement.

Runoff Control

Based on the establishing of the 5-year and 100-year event flood storage levels and an anticipated elevation of the outlet, it was found that an equivalent of a 51 mm diameter opening is required. Assuming a minimum orifice size of 4" (100 mm) (City of Windsor standard), a Tempest ® Inlet Control Devices will be introduced to control the outflow to a maximum release rate of 9.2 l/s.

Storage Requirements

Based on the requirements already noted in this report, it was found that the site would require a minimum storage capacity of 46 cubic metres for the 1:5-year storm and 112 cubic metres for the 1:100-year storm.

The storage for the 5-year storm shall be accommodated in an ADS Stormtech stormwater chamber system, as well as underground in pipes, manhole and catch basin. The high-water level shall be no greater than 0.30 m below the parking lot catch basin elevation. For the 1:100-year storm event, the high-water level will be 182.15 m; with minimum underground storage capacity of 76 cubic metres.

SANITARY

The site shall be serviced with a single private drain connection, complete with sampling manhole located on the property line.

Closing

Should you have any questions or comments regarding this report, please feel free to contact our office.





HADDAD, MORGAN AND ASSOCIATES LTD.

Consulting Engineers

1316 Ouellette Avenue, Windsor, Ontario N8X 1J8

Phone: (519) 973-1177 Fax: (519) 253-2740 e-mail: hma@haddadmorgan.com

Stormwater Management Calculations

The following storm calculations have been provided for the proposed development in accordance with the Windsor/Essex Regional Stormwater Management Standards Manual.

Project : Proposed Residential Building

Project No. 21-232

Location: 845 Riverside Drive East, Windsor, Ontario

Date: December 3, 2021

Pre- Development Condition

		C value (5 yr)	AC value
Total Area	2,521 sq.m.		
Building Area	- sq.m.	0.95	0.00
Pavement Area	- sq.m.	0.90	0.00
Gravel Area	- sq.m.	0.70	0.00
Landscape	2,521 sq.m.	0.20	504.20

C pre-development 0.20
***C Undeveloped** 0.20

Developed Condition

		C value	AC value
Total Area	2,521 sq.m.		
Building Area	831 sq.m.	0.95	789.45
Pavement Area	708 sq.m.	0.95	672.60
Gravel Area	- sq.m.	0.70	-
Landscape	982 sq.m.	0.20	196.40

C developed 100 yr (see below) 0.85
C developed 5 yr 0.64

* Pre-development C value selected based on historical development standards

Sheet Flow Analysis

Runoff Distance (overland)	54	m
Surface Type	Grass - Dense	
Manning's n	0.24	
Slope	1.90%	
User input add. flow time	0	minutes
24 Hour rainfall event SCS Type II*	68.0	minutes
Sheet flow Overtop and Meadows	25.09	minutes
+ shallow time	0.00	minutes
Total time	25.09	minutes

Runoff Distance (overland)	26	
Surface Type	Grass - Dense	
Manning's n	0.24	
Slope	0.5%	
User input add. flow time	0	
24 Hour rainfall event SCS Type II*	68.0	
actual time per storm network design	24.45	minutes
+ pipe flow time	0.96	minutes
Total time	25.41	minutes

C 100 Year Calculation

Impervious %	61%		Hydraulic Soil Group	D
Impervious adjust	0	sq.m	Storage Depth (mm)	92.15
			C 100 year (eq. 3.3.2.2)	0.85
Zero release Storage Volume	232.3	CM		

Design Storage I values (a/(T+b)^c)

2 year	50.03	mm/hr	49.62	mm/hr
5 year	65.74	mm/hr	65.22	mm/hr
100 year	108.33	mm/hr	107.50	mm/hr

Rational Method Calculation

2 year Discharge rate	7.01	L/s	Restricted Flow Rate	22.39	L/s
5 year Discharge rate	9.21	L/s		29.43	L/s
100 Year Discharge rate	15.18	L/s		64.28	L/s

Orifice/Restrictor Calculation

Center of orifice	179.60	m	Cd	0.62	0.62 = sharp Orifice
Surface elevation	181.70	m			0.80 = tube
Calculated Orifice Size (5 year)	54.46	mm	Elevation difference	2.074	m
Orifice Diameter	51	mm			
Discharge Rate	8.08	L/s	Control Rate Required	9.21	L/s
					O.K.

An IPEX flow control device shall be installed to control the outflow to the restricted rate.

100 year release:

Free surface Elevation	182.15	m	Elevation difference	2.52	m	0.45
Discharge Rate	8.91	L/s				O.K.

5 year storage Calculation

**

td min.	Intensity mm/hr	Flow Rate L/s	Total Vol. L	Release L	Storage L
5	139.57	62.98	18,893	-	-
10	107.72	48.60	29,161	-	-
15	88.40	39.89	35,898	-	-
20	75.35	34.00	40,796	-	-
25	65.89	29.73	44,592	-	-
30	58.69	26.48	47,669	2,224	45,445
35	53.02	23.92	50,242	4,648	45,594
40	48.43	21.85	52,447	7,072	45,375
45	44.63	20.14	54,371	9,496	44,875
50	41.43	18.69	56,077	11,920	44,157
55	38.69	17.46	57,607	14,344	43,263
60	36.32	16.39	58,994	16,768	42,225
65	34.24	15.45	60,261	19,192	41,068
70	32.41	14.63	61,427	21,617	39,811
75	30.79	13.89	62,508	24,041	38,467
80	29.33	13.23	63,515	26,465	37,050
85	28.01	12.64	64,456	28,889	35,568
90	26.82	12.10	65,342	31,313	34,029
91	26.59	12.00	65,512	31,798	33,715

Developed Time of Concentration	25.41
Applied Runoff Coefficient	0.64

Storage Requirement	45,594 L
	45.59 c.m.

Drainage Structures			
underground			
			CM
	Total	0.00	c.m.
MH	1	1.41	
CB	2	0.86	
	Total	2.28	c.m.
Piping		1.87	
	Total	1.87	c.m.

Storage Provided	4.15	c.m.
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Volume Required in Chambers	41.45
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** release rate is based on time interval less time of concentration (no release considered prior to Tc)

100 year storage Calculation

**

td min.	Intensity mm/hr	Flow Rate L/s	Total Vol. L	Release L	Storage L
5	218.23	130.49	39,148	-	-
10	172.68	103.25	61,952	-	-
15	143.67	85.91	77,318	-	-
20	123.48	73.84	88,603	-	-
25	108.56	64.92	97,375	-	-
30	97.06	58.04	104,471	2,452	102,019
35	87.91	52.57	110,387	5,126	105,261
40	80.44	48.10	115,432	7,799	107,633
45	74.21	44.38	119,814	10,472	109,342
50	68.94	41.23	123,676	13,145	110,531
55	64.42	38.52	127,121	15,818	111,303
60	60.50	36.17	130,227	18,492	111,735
65	57.05	34.12	133,050	21,165	111,886
70	54.01	32.29	135,636	23,838	111,798
75	51.29	30.67	138,020	26,511	111,509
80	48.86	29.21	140,229	29,184	111,045
85	46.66	27.90	142,287	31,858	110,430
90	44.66	26.71	144,213	34,531	109,682
95	42.84	25.62	146022.07	37,204	108,818
100	41.18	24.62	147727.1	39,877	107,850
105	39.64	23.70	149339.34	42,550	106,789
110	38.23	22.86	150868.21	45,224	105,645
115	36.92	22.08	152321.79	47,897	104,425
120	35.70	21.35	153707.09	50,570	103,137

Developed Time of Concentration	25.41
Applied Runoff Coefficient	0.85

Storage Requirement	111,886 L
	111.89 c.m.

Surface Storage			
		31.75	c.m.
	Total	31.75	c.m.
MH	1	1.41	
CB	2	0.86	
	Total	2.28	c.m.
Piping		1.87	
	Total	1.87	c.m.

Surface Storage Provided	0.00	c.m.
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Storage Provided	4.15	c.m.
CB & Pipes		

Volume Required in Chambers	75.99
-----------------------------	-------



August 12, 2021

Dr. Bhan Garg
Phone: (519)257-1774
Email: bcgarg@gmail.com

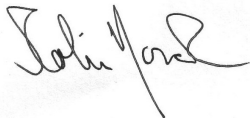
Re: Environmental Noise Assessment for the Proposed Highrise Residential Development at 845
Riverside Drive East, City of Windsor

Dear Dr. Garg,

Please find enclosed a noise assessment report for the proposed highrise residential development located at 845 Riverside Drive East, in the City of Windsor. This assessment pertains to the noise impacts caused by the adjacent Riverside Drive roadway which is near to the proposed site.

I trust that the enclosed information meets your requirements. Please do not hesitate to contact me if you have any questions.

Sincerely,



Colin Novak PhD, PEng

**Environmental Noise Assessment for the proposed
Highrise Residential Development at 845 Riverside
Drive East, City of Windsor**



Dr. Bhan Garg
Phone: (519)257-1774
Email: bcgarg@gmail.com

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Statement of Liability

Akoustik Engineering Limited prepared this report for Dr. Bhan Garg. The material in it reflects Dr. Helen Ule and Dr. Colin Novak's judgement considering the information available to them and Akoustik Engineering Limited at the time of the measurements, under the stated test conditions. Any use that a Third Party makes of this report, or any reliance on decisions made based on it, is the responsibility of such Third Parties. Akoustik Engineering Limited accepts no responsibility for damages, if any, suffered by any Third Party resulting from decisions made or actions based on this report.

Introduction

This report is a noise assessment for the proposed highrise residential development to be located at 845 Riverside Drive East in the City of Windsor. The assessment pertains to the environmental noise impacts from nearby road traffic. An illustration of the geographical area with the proposed development area identified, as well as the proposed lot layout is given in Appendix A: Site Location. The expected noise impacts have been predicted using the Ministry of the Environment, Conservation and Parks (MECP) prediction software STAMSON and are based on available road traffic volumes, which have been projected 10 years forward. Given that the road and rail traffic occurs during all periods of a 24-hour day, as defined by the applicable MECP's Noise Pollution Control documents (NPC-300), the assessment has been carried out for the entire 24-hour period. Any recommended abatement to control the noise is included in this report.

Identification of Noise Sources

A zoning map of the area is provided in Appendix B: Land-use Zoning Map of Area. The proposed development is surrounded by park lands to the north and residential housing to the east, south and west of the property. The Ministry of the Environment, Conservation and Parks (MECP) specifications for the identification of existing or future major sources of noise impact on a development is whether they are within 500 metres of the site. The identified noise source which could possibly produce an impact on the proposed development is Riverside Drive East, located on the north property line of the development. There are no other significant sources of noise which are expected to have an impact on the proposed development.

Ministry of the Environment, Conservation and Parks Noise Criteria

In accordance to the MECP Guideline NPC-300, the following sound level limits for residential developments of Class 2 have been set for roadway noise and is shown in Table 1. Select pages from the NPC-300 guideline have been included in Appendix C: NPC-300 Reference Pages for reference. The proposed development is classified as a Class 2 area, given that the region exhibits features of both a Class 1 and a Class 3 area, based on the environmental noise characteristics. It is worth noting that when the sound level limits presented in Table 1 are exceeded, noise control abatement is required.

Error! Reference source not found.

From Table 1, daytime refers to the period from 07:00 to 23:00 and nighttime refers to the period from 23:00 to 07:00 hours. An outdoor living area (OLA) refers to a location such as a patio, yard or barbeque area.

The limits presented in Table 1 are the limits before noise control measures are required. The noise level limit in an OLA may be exceeded by up to 5 dBA if proper warning clauses are inserted in the titles, deeds, and any tenancy agreements relating to the property and only after barriers or other noise control measures have been found to be impractical or unfeasible. A noise attenuation barrier is required to protect and bring the sound level down to 55 dBA in the OLA if the noise level exceeds 60 dBA. Only in cases where the

required noise control measures are not feasible for technical, economic or administrative reasons would an excess above the limit (55 dBA) be acceptable with the appropriate warning clause; in this situation, any excess above the noise limit will not be deemed acceptable if it exceeds 5 dB.

The guideline also recommends the provision for the installation of central air conditioning when the noise level outside the plane of a window exceeds 55 dBA for the daytime or 50 dBA for the nighttime. If the noise level exceeds 65 dBA for the daytime or 60 dBA for the nighttime, the installation of central air conditioning should be implemented.

Noise Source Data

Prior to modelling the noise, road traffic volume data was obtained from the City of Windsor who provided average annual daily traffic (AADT) for a location near to the proposed development (Riverside East of Glengarry). The projected road traffic volumes along with the breakdown of vehicle types is given in Appendix D: Road Traffic Volume Data. Also, given in Appendix D are the distances between the representative receptor locations and the road noise source that are used in the noise propagation model.

The City of Windsor provided one year of historical traffic data for this location. For this information a growth rate of 2.5% per annum was assumed. Day and night traffic volumes were calculated using an assumed 90%-day and 10%-night split. A breakdown of cars, medium trucks and heavy trucks was assumed to be 95/3.0/2.0 %, based on previous experience.

Identification of the Representative Receptor Locations

Upon examination of the proposed high rise, the first residential floor (2nd storey) and top floor (7th storey) of each tower was evaluated at the north and west façades of the building facing Riverside Drive East. The ground floor will be parking and the lobby. The representative units were chosen as potential worst case for their proximity and orientation to the Riverside Drive roadway. The representative sensitive points of reception (PORs) are as follows:

- POR 2A on north façade, 4.5 m (second floor)
- POR 2B on west façade, 4.5 m (second floor)
- POR 7A on north façade, 19.5 m (seventh floor)
- POR 7B on west façade, 19.5 m (seventh floor)

The above representative units are identified on the proposed lot layout in Appendix A: Site Location.

Assessment Approach

The predicted sound levels from the nearby road traffic has been determined through the application of the prediction noise modelling software STAMSON 5.0. All input data pertaining to the points of reception was based on the building plan provided at the time of the study, as shown in Appendix A: Site Location.

The input data used to calculate the predicted sound level exposures for the selected points of reception impacted by the road traffic noise, and the resulting outputs, are given in Appendix E: Noise Model Printout.

The MECP noise software program STAMSON Version 5.0 was used to model the noise impact from the road traffic. A model of the predicted noise levels at the representative units was created to determine the extent of the noise control measures required (if any). The MECP requires the calculation of the noise impacts at the OLA and plane of window of the dwellings. Since the dimensions and orientation of the dwellings are not specified the layout for the units was assumed. The MECP requires the calculation of the noise impacts at the OLA and plane of window of the dwellings. Since the intended balconies are less than a minimum 4 metres in depth, they are not considered to be a dwelling OLA. The site plan does not have an outdoor communal OLA, therefore no noise impacts at an OLA is considered for this development; only the impacts at the plane of the residential windows is considered.

Results and Noise Control Requirements

The following is a summary and assessment of the predicted results at the representative PORs.

As specified by the MECP Environmental Noise Guideline NPC-300, the outdoor and indoor sound level limits (based on one-hour LA_{eq} values) at a residence for road traffic noise are categorized into three (3) limits, based on the type of space assessed. The document also specifies the recommended noise control measures, if required, that should be followed for the OLA, plane of a window (ventilation requirements) and the indoor living area (building components) noise assessments. Select pages from the NPC-300 guideline are given in Appendix C: NPC-300 Reference Pages, which includes the stated warning clauses and other requirements based on the predicted noise levels.

The predicted noise level impacts for the daytime and nighttime periods for the representative units with no control measures are given in Tables 2. From this table it is seen that the MECP noise limits are exceeded for all units. As such, noise control measures are required. Table 3 provides the required warning clauses given the values presented in Table 2.

Table 2: Predicted Noise Levels – Plane of Window- No Abatement

Unit #	Height (m)	Daytime Period LA _{eq} (dBA)	Nighttime Period LA _{eq} (dBA)
2A (Unit 201 north façade)	4.5	66.45	59.93
2B (Unit 201, west façade)	4.5	63.73	57.21
7A (Unit 701, north façade)	19.5	67.41	60.89
7B (Unit 701, west façade)	19.5	65.47	58.94

Table 3: Control Measures – Plane of Window

Unit #	Daytime Noise Control Measures (if Required)	Daytime Warning Clause (if required)	Nighttime Noise Control Measures (if Required)	Nighttime Warning Clause (if required)
2A (Unit 201 north façade)	See Note D	Type D	See Note C	Type C
2B (Unit 201, west façade)	See Note D	Type D	See Note C	Type C
7A (Unit 701, north façade)	See Note D	Type D	See Note C	Type C
7B (Unit 701, west façade)	See Note D	Type D	See Note C	Type C

Notes:

Note A: Noise control measures may be applied to reduce the sound level to 55 dBA. If measures are not provided, prospective purchasers or tenants should be informed of potential noise problems by a warning clause Type A.

Note B: Noise control measures should be implemented to reduce the level to 55 dBA. Only in cases where the required noise control measures are not feasible for technical, economic or administrative reasons would an excess above the limit (55 dBA) be acceptable with a warning clause Type B.

Note C: The dwelling should be designed with a provision for the installation of central air conditioning in the future, at the occupant’s discretion. Warning clause Type C is also recommended.

Note D: Installation of central air conditioning should be implemented with a warning clause Type D. In addition, building components including windows, walls and doors, where applicable, should be designed so that the indoor sound levels comply with the sound level limits in Table C-2.

Note E: Building components including windows, walls and doors, where applicable, need to be designed so that the indoor sound levels comply with the sound level limits in Table C-2. The acoustical performance of the building components (windows, doors and walls) needs to be specified. In addition, the exterior walls of the first row of dwellings next to railway tracks are to be built to a minimum of brick veneer or masonry equivalent construction, from the foundation to the rafters when the rail traffic Leq (24-hour), estimated at a location of a nighttime receptor, is greater than 60 dBA, and when the first row of dwellings is within 100 metres of the tracks.

It is required that the following warning clauses be implemented in all development agreements, offers to Purchase, and agreements of Purchase or Sale or Lease of each dwelling unit:

Warning Clause(s):

Type A: “Purchasers/tenants are advised that sound levels due to increasing road traffic (rail traffic) (air traffic) may occasionally interfere with some activities of the dwelling occupants as the sound levels exceed the sound level limits of the Municipality and the Ministry of the Environment.”

Type B: "Purchasers/tenants are advised that despite the inclusion of noise control features in the development and within the building units, sound levels due to increasing road traffic (rail traffic) (air traffic) may on occasions interfere with some activities of the dwelling occupants as the sound levels exceed the sound level limits of the Municipality and the Ministry of the Environment."

Type C: “This dwelling unit has been designed with the provision for adding central air conditioning at the occupant’s discretion. Installation of central air conditioning by the occupant in low and medium density developments will allow windows and exterior doors to remain closed, thereby ensuring that the indoor sound levels are within the sound level limits of the Municipality and the Ministry of the Environment.”

Type D: “This dwelling unit has been equipped with central air conditioning in order to allow windows and exterior doors to remain closed, thereby ensuring that the indoor sound levels are within the sound level limits of the Municipality and the Ministry of the Environment. Air conditioning units are to be installed in a noise insensitive area.”

STC Requirements

Given the excessive predicted noise levels at the plane of window for these units, the calculation of the minimum sound transmission class (STC) for the window and building wall components is required. Specifically, the STC building requirements are calculated for the north façade of unit 701 as this unit is most exposed to the roadway noise and is therefore considered worst-case. The sound level at the plane of window is taken as 67 dBA. The STC requirements are calculated for a 45 dBA daytime indoor sound level limit for the sleeping quarters with respect to the roadway noise. The windows are assumed to be operable thin double windows. As no specific building information was available, the bedroom was assumed to be a 10’ by 10’ bedroom with 8’ ceiling heights and a standard 36” by 36” window. The program inputs and results for the STC calculations and assumption are given in Appendix F: STC Output Results. The building component STC requirements are provided in Table 4. It is recommended that a qualified acoustical engineer inspect and approve any building plans to ensure compliance of the required STC rating prior to the issuing of a building permit.

Table 4: Building Componenets STC Requirements.

Unit #	Exterior Wall	Window, operable thin double
701	30	16

Results Summary



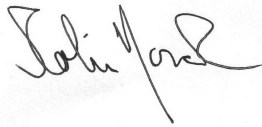
Table 5 summarizes the required warning clauses and building requirements, if any, for all units within the proposed development. It is required that any necessary warning clauses be implemented in all development agreements, offers to Purchase, and agreements of Purchase or Sale or Lease as identified in Table 5.

Table 5: Summary of Warning Clauses and Building Component Requirements

Units	Noise Barrier Requirement (Y/N)	Warning Clause(s)	Building Component Requirement(s)	Ventilation Requirement(s)
All units	N	Type D	Minimum STC Rating for the following Building Components: Exterior Wall – 30 Window (openable thin double) - 16	Supply of Central Air Conditioning System

Conclusion

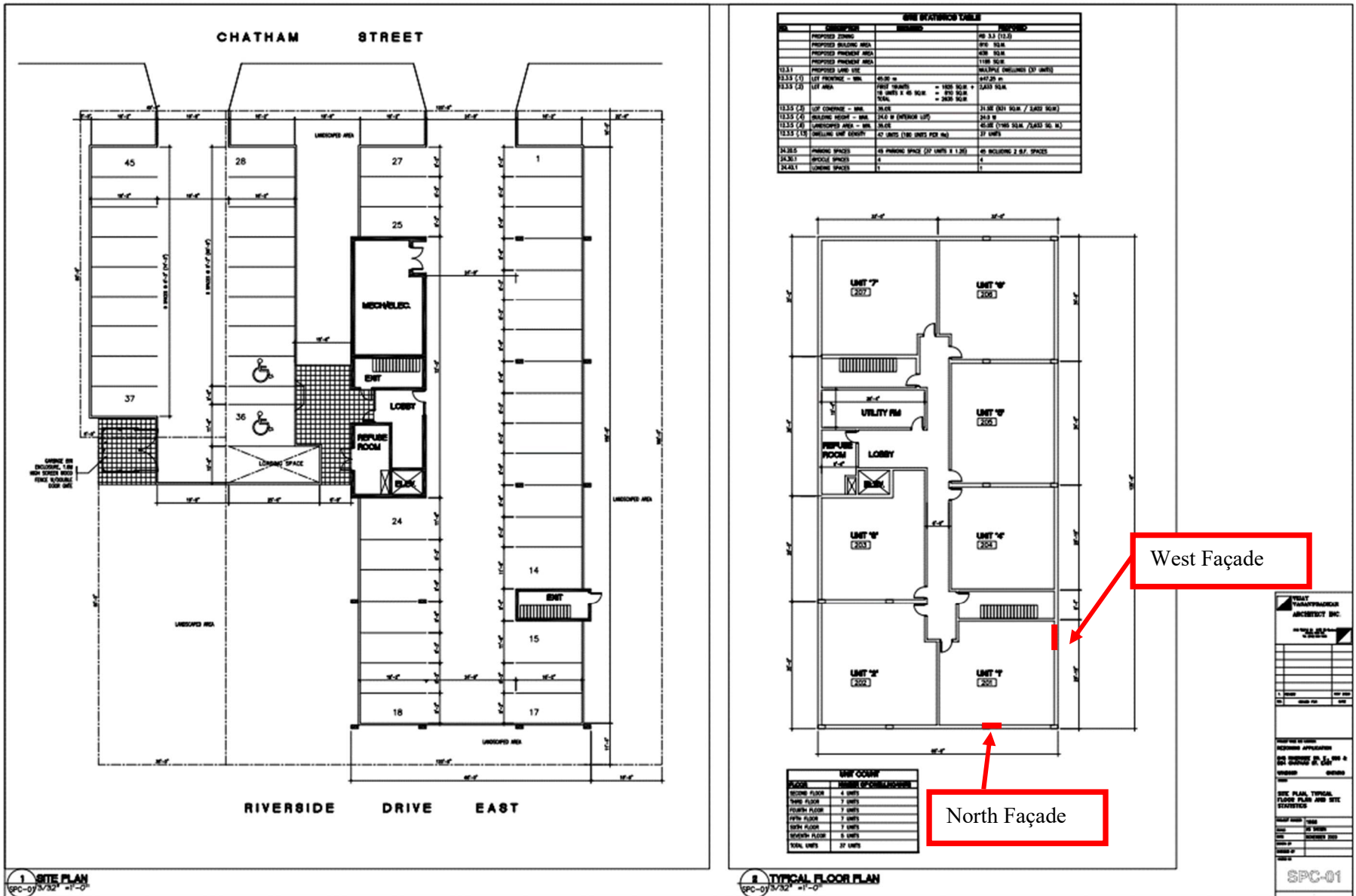
As the noise impact on the proposed development has been shown in this report to be within the manageable limits set by the Ontario Ministry of the Environment, Conservation and Parks, it is recommended that the development be given approval with the understanding that the stated noise control measures are implemented. These include the given ventilation and building requirements and the specified warning clauses.

<p>For</p> 		
	<p>Prepared by: Helen Ule, Ph.D., PEng</p>	<p>Reviewed by: Colin Novak, Ph.D., PEng</p>

Appendix A: Site Location



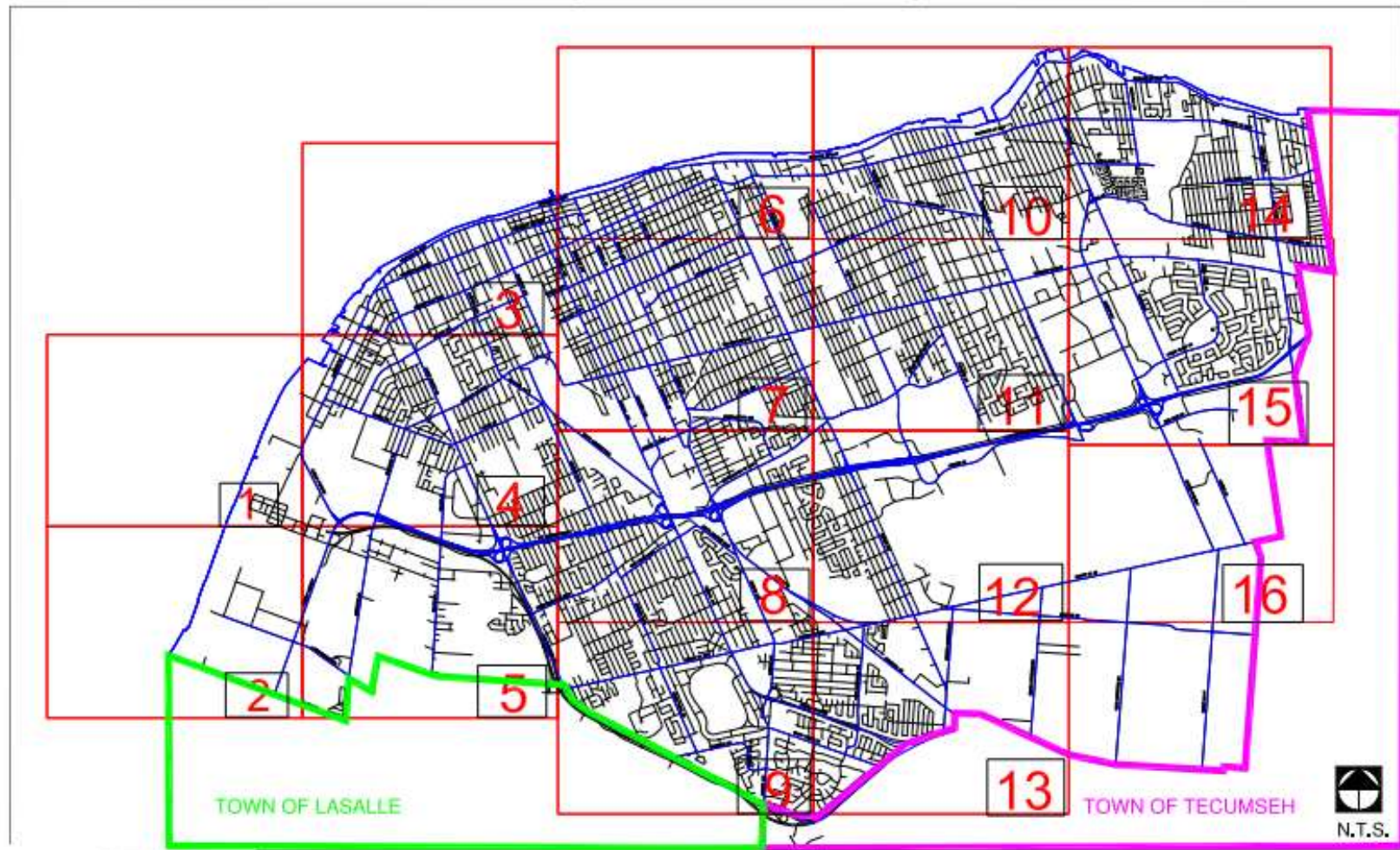
A 1: General Location of Proposed Development and Surrounding Area



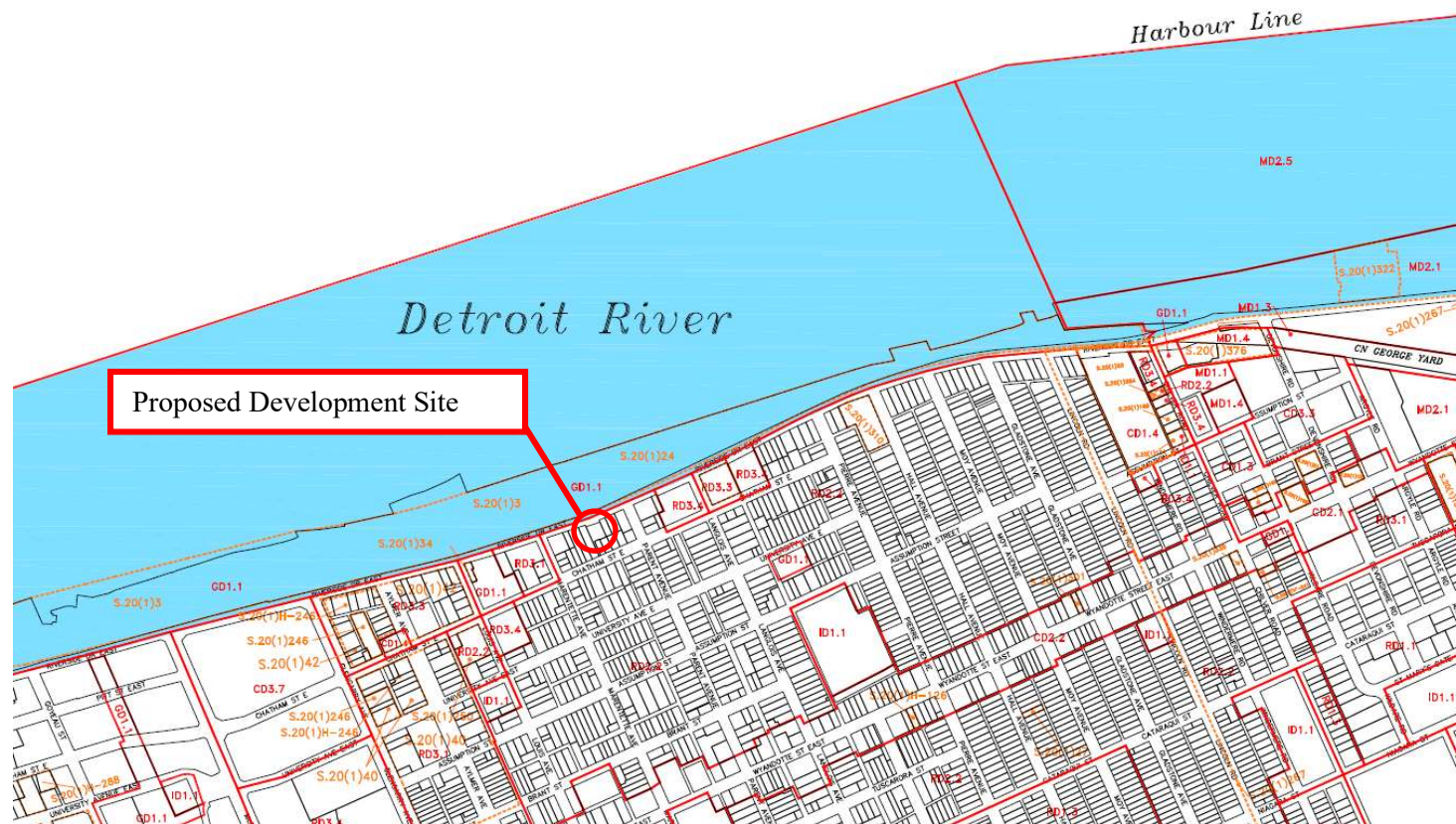
A 2: Proposed Development Layout

Appendix B: Land-use Zoning Map of Area

Zoning District Maps



B 1: The City of Windsor Zoning Map



B 2: Zone 6

Appendix C: NPC-300 Reference Pages

ENVIRONMENTAL NOISE GUIDELINE Stationary and Transportation Sources - Approval and Planning	NPC-300																		
<p>L_{eq} (8). For complete description on assessing road traffic impacts, refer to ORNAMENT. Other traffic noise prediction models have been and are being developed by various authorities and may be adopted from time to time for use in Ontario by the MOE.</p> <p>In order to be consistent with MOE guidelines, the sound level should be assessed in an OLA, such as a rear yard or a patio, and in indoor living areas, such as bedrooms and living rooms. Where the noise impact exceeds the applicable sound level limits, mitigation measures such as site planning, architectural design, noise barriers, building envelope elements (windows, exterior walls, doors) with upgraded sound isolation performance and/or central air conditioning may be required. Noise control measures are not required if the sound level estimated in the OLA is 55 dBA or less during the daytime and 50 dBA or less in the plane of bedroom windows during either daytime or nighttime.</p>																			
<p>C3.2.2 Daytime Outdoor Sound Level Limit</p> <p>Table C-1 gives the equivalent sound level (L_{eq}) limit for designated OLAs. The limit applies to the entire daytime period from 07:00 to 23:00.</p>																			
<p>Table C-1 Sound Level Limit for Outdoor Living Areas Road and Rail</p>																			
<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="width: 50%;">Time Period</th> <th style="width: 50%;">L_{eq} (16) (dBA)</th> </tr> </thead> <tbody> <tr> <td style="text-align: center;">16-hour, 07:00 – 23:00</td> <td style="text-align: center;">55</td> </tr> </tbody> </table>		Time Period	L_{eq} (16) (dBA)	16-hour, 07:00 – 23:00	55														
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16-hour, 07:00 – 23:00	55																		
<p>C3.2.3 Indoor Sound Level Limits</p> <p>Table C-2 gives the equivalent sound level (L_{eq}) limits and the applicable time periods for the indicated types of indoor spaces. The specified indoor sound level limits are maxima and apply to the indicated indoor spaces with windows and doors closed.</p>																			
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Sleeping quarters	07:00 – 23:00	45	40																
Ministry of the Environment, August 2013	38																		

C 1: Daytime Outdoor and Daytime/Nighttime Indoor Sound Level Limits

Type of Space	Time Period	L _{eq} (dBA)	
	23:00 – 07:00	40	35

C3.3 Rail Traffic Noise

C3.3.1 Method

The assessment of rail traffic noise impact should be conducted using a prediction method entitled STEAM, Sound from Trains Environmental Analysis Method, published in 1990 by MOE, Reference [34]. The descriptors used in the assessment are the 16-hour daytime and the 8-hour nighttime equivalent sound levels, L_{eq} (16) and L_{eq} (8). Other traffic noise prediction models have been and are being developed by various authorities and may be adopted from time to time for use in Ontario by the MOE.

The impact of railway traffic noise and the requirement for noise control measures should be assessed similarly to road traffic noise. The sound level should be assessed in an OLA, such as a rear yard or a patio, and in indoor living areas, such as bedrooms and living rooms, and compared with MOE guidelines. Noise control measures are not required if the sound level estimated in the OLA is 55 dBA or less during the daytime and 50 dBA or less in the plane of bedroom windows during daytime or nighttime.

C3.3.2 Daytime Outdoor Sound Level Limit

The outdoor noise impact should be assessed in the OLA during daytime hours, 07:00 to 23:00, considering a combination of only two sources of rail traffic noise, namely the locomotive and the wheel-rail interaction. Whistle noise is not included in the outdoor noise impact assessment. Table C-1 gives the equivalent sound level (L_{eq}) limit for OLAs.

C3.3.3 Indoor Sound Level Limits

The indoor assessment should consider the combination of all three railway noise sources, i.e., locomotive, wheel-rail and whistle. Table C-2 gives the equivalent sound level (L_{eq}) limits for the indicated types of indoor space. The specified indoor sound level limits are maxima and apply to the indicated indoor spaces with windows and doors closed.

A major characteristic of railway noise is its high pass-by sound level for short periods and a major low frequency component produced by the operation of the diesel locomotive. This special character of the sound should be taken into account, particularly when assessing the indoor sound levels. Consequently, in order to account for the special character of railway sound, the indoor sound level limits for rail noise, Table C-2, are 5 dBA lower than the indoor sound level limits for road traffic noise.

C 2: Rail Traffic Noise

This difference results in a requirement for acoustically superior architectural components such as windows and walls, for railway noise.

C3.4 Air Traffic Noise

C3.4.1 Method

Aircraft noise impact assessment is based on Noise Exposure Forecast/Noise Exposure Projection (NEF/NEP) contours determined by methods approved by Transport Canada.

Where the noise impact exceeds the applicable limits, warning clauses and mitigation measures for indoor spaces such as architectural design, special building components and/or central air conditioning may be required. The indoor NEF/NEP values, specified in Table C-4 and Table C-10, are related to the outdoor values and the acoustical insulation provided by the building. The indoor NEF values can be calculated by converting the indoor sound levels, expressed as $L_{eq}(24)$ (dBA), using the expression $NEF = L_{eq}(24) - 31$ dBA.

Section 1.6.7.2 of the 2005 Provincial Policy Statement, Reference [26], establishes the applicable development criterion. With the exception of redevelopment or infilling, Section 1.6.7.2 of the 2005 Provincial Policy Statement, Reference [26], prohibits new residential development and other sensitive land uses in aircraft noise zones above the NEF/NEP 30 contour. The noise impact on the proposed noise sensitive land use is determined based on the location of the noise sensitive land use with respect to the official NEF/NEP contours. NEF/NEP contours are usually available for major civil aviation airports from the airport authority. The more restrictive of the NEF and NEP contours would apply.

C3.4.2 Outdoor Limit

Table C-3 gives the aircraft noise limit in terms of an NEF/NEP value in any outdoor area, including the OLA. The limit applies to the entire 24-hour period. The distance separation from the airport and, consequently, the location of the noise sensitive land use with respect to the NEF/NEP contours, is the only measure that controls the outdoor noise impact.

Table C-3
Outdoor Aircraft Noise Limit

Time Period	NEF/NEP
24-hour	30

C 3: Rail Traffic Noise (Continued)

the emissions are not required to be included with the overall noise assessment of a stationary source facility.

In addition, sound level limits do not apply to emergency equipment operating in emergency situations.

C4.5.4 Sound Level Limits for Layover Sites

The sound level limit for noise from a layover site in any hour, expressed in terms of the One-Hour Equivalent Sound Level (L_{eq}) is the higher of either 55 dBA or the background sound level.

C5 Noise Impact Assessment – Multiple Sources

Impulse sources, non-impulse sources and emergency equipment are to be analyzed separately. Where there are multiple, non-impulse sources at a stationary source, the noise assessment should be based on the combined effect of all sources comprising the stationary source, added together on an energy basis.

C6 Noise Impact Assessment – Supplementary Noise Limits

Indoor limits for transportation sources applicable to noise sensitive land uses are specified in Table C-2 and Table C-4. Table C-9 and Table C-10 are expanded versions of Table C-2 and Table C-4, and present guidelines for acceptable indoor sound levels that are extended to land uses and developments which are not normally considered noise sensitive. The specified values are maximum sound levels and apply to the indicated indoor spaces with the windows and doors closed. The sound level limits in Table C-9 and Table C-10 are presented as information, for good-practice design objectives.

**Table C-9
Supplementary Indoor Sound Level Limits
Road and Rail**

Type of Space	Time Period	L_{eq} (Time Period) (dBA)	
		Road	Rail
General offices, reception areas, retail stores, etc.	16 hours between 07:00 – 23:00	50	45
Living/dining areas of residences, hospitals, schools, nursing/retirement homes, daycare centres, theatres, places of worship, libraries, individual or semi-private offices, conference rooms, reading rooms, etc.	16 hours between 07:00 – 23:00	45	40
Sleeping quarters of hotels/motels	8 hours between 23:00 – 07:00	45	40
Sleeping quarters of residences, hospitals, nursing/retirement homes, etc.	8 hours between 23:00 – 07:00	40	35

C 4: Noise Impact Assessment – Supplementary Noise Limits

**Table C-10
Supplementary Indoor Aircraft Noise Limits
(Applicable over 24-hour period)**

Type of Space	Indoor NEF/NEP*
General offices, reception areas, retail stores, etc.	15
Individual or semi-private offices, conference rooms, etc.	10
Living/dining areas of residences, sleeping quarters of hotels/motels, theatres, libraries, schools, daycare centres, places of worship, etc.	5
Sleeping quarters of residences, hospitals, nursing/retirement homes, etc.	0

* The indoor NEF/NEP values listed in Table C-10 are not obtained from NEF/NEP contour maps. The values are representative of the indoor sound levels and are used as assessment criteria for the evaluation of acoustical insulation requirements.

C7 Noise Control Measures

The following sections provide MOE guidance for appropriate noise control measures. These sections constitute requirements that are applied to MOE approvals for stationary sources. This information is also provided as guidance which land use planning authorities may consider adopting.

The definition in Part A describes the various types and application of noise control measures. All the noise control measures described in the definition are appropriate to address the impact of noise of transportation sources (road, rail and aircraft) on planned sensitive land uses. Only some of the noise control measures described in the definition are appropriate to address the noise impact of stationary sources on planned sensitive land uses.

C7.1 Road Noise Control Measures

C7.1.1 Outdoor Living Areas

If the 16-Hour Equivalent Sound Level, $L_{eq}(16)$ in the OLA is greater than 55 dBA and less than or equal to 60 dBA, noise control measures may be applied to reduce the sound level to 55 dBA. If measures are not provided, prospective purchasers or tenants should be informed of potential noise problems by a warning clause Type A.

If the 16-Hour Equivalent Sound Level, $L_{eq}(16)$ in the OLA is greater than 60 dBA, noise control measures should be implemented to reduce the level to 55 dBA. Only in cases where the required noise control measures are not feasible for technical, economic or administrative reasons would an excess above the limit (55 dBA) be acceptable with a warning clause Type B. In the above situations, any excess above the limit will not be acceptable if it exceeds 5 dBA.

C7.1.2 Plane of a Window – Ventilation Requirements

C7.1.2.1 Daytime Period, 07:00 – 23:00 Hours

Noise control measures may not be required if the L_{eq} (16) daytime sound level in the plane of a bedroom or living/dining room window is less than or equal to 55 dBA. If the sound level in the plane of a bedroom or living/dining room window is greater than 55 dBA and less than or equal to 65 dBA, the dwelling should be designed with a provision for the installation of central air conditioning in the future, at the occupant's discretion. Warning clause Type C is also recommended.

If the daytime sound level in the plane of a bedroom or living/dining room window is greater than 65 dBA, installation of central air conditioning should be implemented with a warning clause Type D. In addition, building components including windows, walls and doors, where applicable, should be designed so that the indoor sound levels comply with the sound level limits in Table C-2. The location and installation of the outdoor air conditioning device should comply with sound level limits of Publication NPC-216, Reference [32], and guidelines contained in Environmental Noise Guidelines for Installation of Residential Air Conditioning Devices, Reference [6], or should comply with other criteria specified by the municipality.

C7.1.2.2 Nighttime Period, 23:00 – 07:00 Hours

Noise control measures may not be required if the L_{eq} (8) nighttime sound level in the plane of a bedroom or living/dining room window is less than or equal to 50 dBA. If the sound level in the plane of a bedroom or living/dining room window is greater than 50 dBA and less than or equal to 60 dBA, the dwelling should be designed with a provision for the installation of central air conditioning in the future, at the occupant's discretion. Warning clause Type C is also recommended.

If the nighttime sound level in the plane of a bedroom or living/dining room window is greater than 60 dBA, installation of central air conditioning should be implemented, with a warning clause Type D. In addition, building components including windows, walls and doors, where applicable, should be designed so that the indoor sound levels comply with the sound level limits in Table C-2. The location and installation of the outdoor air conditioning device should comply with sound level limits of Publication NPC-216, Reference [32], and guidelines contained in Environmental Noise Guidelines for Installation of Residential Air Conditioning Devices, Reference [6], or should comply with other criteria specified by the municipality.

C7.1.3 Indoor Living Areas – Building Components

If the nighttime sound level outside the bedroom or living/dining room windows exceeds 60 dBA or the daytime sound level outside the bedroom or living/dining area windows exceeds 65 dBA, building components including windows, walls and doors, where applicable, should be designed so that the indoor sound levels comply with the

In Class 4 areas, where windows for noise sensitive spaces are assumed to be closed, the use of central air conditioning may be acceptable if it forms an essential part of the overall building designs.

C7.9 Verification of Noise Control Measures

It is recommended that the implementation of noise control measures be verified by qualified individuals with experience in environmental acoustics.

C8 Warning Clauses

The use of warning clauses or easements in respect of noise are recommended when circumstances warrant. Noise warning clauses may be used to warn of potential annoyance due to an existing source of noise and/or to warn of excesses above the sound level limits. Direction on the use of warning clauses should be included in agreements that are registered on title to the lands in question. The warning clauses would be included in agreements of Offers of Purchase and Sale, lease/rental agreements and condominium declarations. Alternatively, the use of easements in respect of noise may be appropriate in some circumstances. Additional guidance on the use of noise warning clauses is provided in Section C7.1.1, Section C7.1.2.1, Section C7.1.2.2, Section C7.3 and Section C7.4.

C8.1 Transportation Sources

The following warning clauses may be used individually or in combination:

TYPE A: (see Section C7.1.1)

“Purchasers/tenants are advised that sound levels due to increasing road traffic (rail traffic) (air traffic) may occasionally interfere with some activities of the dwelling occupants as the sound levels exceed the sound level limits of the Municipality and the Ministry of the Environment.”

TYPE B: (see Section C7.1.1 and Section C7.4)

“Purchasers/tenants are advised that despite the inclusion of noise control features in the development and within the building units, sound levels due to increasing road traffic (rail traffic) (air traffic) may on occasions interfere with some activities of the dwelling occupants as the sound levels exceed the sound level limits of the Municipality and the Ministry of the Environment.”

TYPE C: (see Section C7.1.2.1, Section C7.1.2.2 and Section C7.4)

“This dwelling unit has been designed with the provision for adding central air conditioning at the occupant’s discretion. Installation of

central air conditioning by the occupant in low and medium density developments will allow windows and exterior doors to remain closed, thereby ensuring that the indoor sound levels are within the sound level limits of the Municipality and the Ministry of the Environment.”

TYPE D: (see Section C7.1.2.1, Section C7.1.2.2 and Section C7.4)

“This dwelling unit has been supplied with a central air conditioning system which will allow windows and exterior doors to remain closed, thereby ensuring that the indoor sound levels are within the sound level limits of the Municipality and the Ministry of the Environment.”

C8.2 Stationary Sources

It is not acceptable to use warning clauses in place of physical noise control measures to identify an excess over the MOE sound level limits. Warning clause (Type E) for stationary sources may identify a potential concern due to the proximity of the facility but it is not acceptable to justify exceeding the sound level limits.

TYPE E: (see Section C7.6)

“Purchasers/tenants are advised that due to the proximity of the adjacent industry (facility) (utility), noise from the industry (facility) (utility) may at times be audible.”

C8.3 Class 4 Area Notification

TYPE F: (see Section B9.2 and Section C4.4.2)

“Purchasers/tenants are advised that sound levels due to the adjacent industry (facility) (utility) are required to comply with sound level limits that are protective of indoor areas and are based on the assumption that windows and exterior doors are closed. This dwelling unit has been supplied with a ventilation/air conditioning system which will allow windows and exterior doors to remain closed.”

C 8: Noise Control Measures – Warning Clauses (Continued)

Appendix D: Road Traffic Volume Data

D 1: Predicted Riverside Drive East Traffic Volumes (2.5% growth rate per annum)

Year	Road	Location	ADT
2018	Riverside Drive East	East of Glengarry	18000
2031	Riverside Drive East	East of Glengarry	24813

D 2: Predicted Traffic Volumes per Period and Breakdown of Cars, Medium Trucks and Heavy Trucks

Period	Auto Traffic Volume (Vehicles/hr)	Medium Truck Traffic Volume (Vehicles/hr)	Heavy Truck Traffic Volume (Vehicles/hr)
Day (16 hours)	1326	42	28
Night (8 hours)	295	9	6

D 3: Distance from Riverside Drive East to Selected Representative Lots

Unit#	Distance to Roadway to Plane of Window North Facade (m)	Distance to Roadway to Plane of Window West Facade (m)
2	13.8	22.3
7	13.8	22.3

- Note: the minimum source to receiver distance allowed in STAMSON is 15 m. As such, the PORs listed above that have distances less than 15 m have been calculated with the source to receiver distance to be 15 m.

Source height = 1.19 m

ROAD (0.00 + 59.93 + 0.00) = 59.93 dBA

Angle1	Angle2	Alpha	RefLeq	P.Adj	D.Adj	F.Adj	W.Adj	H.Adj	B.Adj
--------	--------	-------	--------	-------	-------	-------	-------	-------	-------

SubLeq

-90	90	0.58	61.25	0.00	0.00	-1.32	0.00	0.00	0.00
-----	----	------	-------	------	------	-------	------	------	------

59.93

Segment Leq : 59.93 dBA

Total Leq All Segments: 59.93 dBA

TOTAL Leq FROM ALL SOURCES (DAY): 66.45
(NIGHT): 59.93

Appendix F: STC Output Results

```
Indoor sound level in dB(A) is 45
Room absorption category is: high absorption
Outdoor sound level is 67 dB(A) (plus 0 dB from Table 2)

Components:
      After      From Table 3   From Table 4   From Table 5   STC
      Step 2     (% energy)    (% floor area) (spectrum)
-----
1. Window, openable thin do 22   +3 dB ( 50 %) -11 dB ( 9 %)  +2 dB         16
2. Exterior wall            22   +3 dB ( 50 %) -2 dB ( 80 %)  +7 dB         30

Calculation is for: Bedroom

-----
OPTIONS : 1. Find/change required STC for components
          2. Calculate noise reduction by specified components
          3. Load or save result file
          4. STOP
          ..... Choose action [ 1]: _
```

F 1: STC Results Unit 7A



12 FEBRUARY 2023

STAGE 2 ARCHAEOLOGICAL ASSESSMENT

*845 Riverside Drive East, Part of Lot 88, Concession 1 Petite Cote
(Geographic Township of Sandwich, County of Essex), City of Windsor
(AMICK Corporate File #2022-031/MCM File #P038-1215-2022)*

SUBMITTED TO:

Ontario Ministry of Citizenship and Multiculturalism
(MCM)

SUBMITTED BY:

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MCM FILE NUMBER: P038-1215-2022

CORPORATE PROJECT NUMBER: 2022-031

12 FEBRUARY 2023

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Marilyn Cornies (MCM Professional Archaeologist Licence #P038)

EXECUTIVE SUMMARY

This report describes the results of the 2022 Stage 2 Archaeological Property Assessment of *845 Riverside Drive East, Part of Lot 88, Concession 1 Petite Cote (Geographic Township of Sandwich, County of Essex), City of Windsor*, conducted by AMICK Consultants Limited. This assessment was undertaken as a requirement under the Planning Act (RSO 1990) and was conducted under Professional Archaeologist License #P038 issued to Marilyn Cornies by the Minister of Citizenship and Multiculturalism (MCM) for the Province of Ontario. All work was conducted in conformity with Ontario Ministry of Tourism and Culture (MTC) Standards and Guidelines for Consultant Archaeologists (MTC 2011) and the Ontario Heritage Act (RSO 1990a).

The entirety of the study area is approximately 0.25 hectares (ha) in area and includes within it mostly grass lawn and a few trees. The study area is bounded on the north by Riverside Drive East, on the east by existing residential development, on the south by Chatham Street East and on the west by Tenth Line West. AMICK Consultants Limited was engaged by the proponent to undertake a Stage 2 Archaeological Property Assessment of lands potentially affected by the proposed undertaking and was granted permission to carry out archaeological fieldwork. Following the criteria outlined by MCM (2011) for determining archaeological potential, portions of the study area were determined as having archaeological potential for Pre-contact and Post-contact archaeological resources. Consequently, this report is being prepared in advance of the planning process for this property.

The entirety of the study area was subject to property inspection and photographic documentation concurrently with the Stage 2 Property Assessment which consisted of test pit survey at a ten-metre interval to confirm disturbance on 10 November 2022. All records, documentation, field notes, photographs, and artifacts (as applicable) related to the conduct and findings of these investigations are held at the Lakelands District corporate offices of AMICK Consultants Limited until such time that they can be transferred to an agency or institution approved by the MCM on behalf of the government and citizens of Ontario.

As a result of the Stage 2 Property Assessment of the study area, no archaeological resources were encountered. Consequently, the following recommendations are made:

- 1. No further archaeological assessment of the study area is warranted.*
- 2. The Provincial interest in archaeological resources with respect to the proposed undertaking has been addressed.*
- 3. The proposed undertaking is clear of any archaeological concern.*

1.0 PROJECT CONTEXT

1.1 DEVELOPMENT CONTEXT

This report describes the results of the 2022 Stage 2 Archaeological Property Assessment of 845 Riverside Drive East, Part of Lot 88, Concession 1 Petite Cote (Geographic Township of Sandwich, County of Essex), City of Windsor, conducted by AMICK Consultants Limited. This assessment was undertaken as a requirement under the Planning Act (RSO 1990) and was conducted under Professional Archaeologist License #P038 issued to Marilyn Cornies by the Minister of Citizenship and Multiculturalism (MCM) for the Province of Ontario. All work was conducted in conformity with Ontario Ministry of Tourism and Culture (MTC) Standards and Guidelines for Consultant Archaeologists (MTC 2011) and the Ontario Heritage Act (RSO 1990a).

The entirety of the study area is approximately 0.25 hectares (ha) in area and includes within it mostly grass lawn and a few trees. The study area is bounded on the north by Riverside Drive East, on the east by existing residential development, on the south by Chatham Street East and on the west by existing residential development. AMICK Consultants Limited was engaged by the proponent to undertake a Stage 2 Archaeological Property Assessment of lands potentially affected by the proposed undertaking and was granted permission to carry out archaeological fieldwork. Following the criteria outlined by MCM (2011) for determining archaeological potential, portions of the study area were determined as having archaeological potential for Pre-contact and Post-contact archaeological resources. Consequently, this report is being prepared in advance of the planning process for this property.

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The proposed development of the study area includes an apartment building with asphalt driveway and parking areas with associated services and landscape modifications. A preliminary plan of the proposed development has been submitted together with this report to MCM for review and reproduced within this report as Map 4.

1.2 HISTORICAL CONTEXT

1.2.1 PRE-CONTACT LAND-USE OUTLINE

Table 1 illustrates the chronological development of cultures within southern Ontario prior to the arrival of European cultures to the area at the beginning of the 17th century. This general

cultural outline is based on archaeological data and represents a synthesis and summary of research over a long period of time. It is necessarily generalizing and is not necessarily representative of the point of view of all researchers or stakeholders. It is offered here as a rough guideline and as a very broad outline to illustrate the relationships of broad cultural groups and time periods.

TABLE 1 PRE-CONTACT CULTURAL CHRONOLOGY FOR SOUTHERN ONTARIO

Years ago	Period	Southern Ontario
250	Terminal Woodland	Ontario and St. Lawrence Iroquois Cultures
1000 2000	Initial Woodland	Princess Point, Saugeen, Point Peninsula, and Meadowood Cultures
3000 4000 5000 6000	Archaic	Laurentian Culture
7000 8000 9000 10000 11000	Palaeo-Indian	Plano and Clovis Cultures
		(Wright 1972)

What follows is an outline of Aboriginal occupation in the area during the Pre-Contact Era from the earliest known period, about 9000 B.C. up to approximately 1650 AD.

1.2.1.1 PALEO-INDIAN PERIOD (APPROXIMATELY 9000-7500 B.C.)

North of Lake Ontario, evidence suggests that early occupation began around 9000 B.C. People probably began to move into this area as the glaciers retreated and glacial lake levels began to recede. The early occupation of the area probably occurred in conjunction with environmental conditions that would be comparable to modern Sub-Arctic conditions. Due to the great antiquity of these sites, and the relatively small populations likely involved, evidence of these early inhabitants is sparse and generally limited to tools produced from stone or to by-products of the manufacture of these implements.

1.2.1.2 ARCHAIC PERIOD (APPROXIMATELY 8000-1000 B.C.)

By about 8000 B.C. the gradual transition from a post glacial tundra-like environment to an essentially modern environment was largely complete. Prior to European clearance of the landscape for timber and cultivation, the area was characterized by forest. The Archaic Period is the longest and the most apparently stable of the cultural periods identified through archaeology. The Archaic Period is divided into the Early, Middle and Late Sub-Periods, each represented by specific styles in projectile point manufacture. Many more sites of this period are found throughout Ontario, than of the Palaeo-Indian Period. This is probably a reflection of two factors: the longer period of time reflected in these sites, and a greater population density. The greater population was likely the result of a more diversified

subsistence strategy carried out in an environment offering a greater variety of abundant resources (Smith 2002:58-59).

Current interpretations suggest that the Archaic Period populations followed a seasonal cycle of resource exploitation. Although similar in concept to the practices speculated for the big game hunters of the Palaeo-Indian Period, the Archaic populations utilized a much broader range of resources, particularly with respect to plants. It is suggested that in the spring and early summer, bands would gather at the mouths of rivers and at rapids to take advantage of fish spawning runs. Later in the summer and into the fall season, smaller groups would move to areas of wetlands to harvest nuts and wild rice. During the winter, they would break into yet smaller groups probably based on the nuclear family and perhaps some additional relatives to move into the interior for hunting. The result of such practices would be to create a distribution of sites across much of the landscape (Smith 2002: 59-60).

The material culture of this period is much more extensive than that of the Palaeo-Indians. Stylistic changes between Sub-Periods and cultural groups are apparent, although the overall quality in production of chipped lithic tools seems to decline. This period sees the introduction of ground stone technology in the form of celts (axes and adzes), manos and metates for grinding nuts and fibres, and decorative items like gorgets, pendants, birdstones, and bannerstones. Bone tools are also evident from this time period. Their presence may be a result of better preservation from these more recent sites rather than a lack of such items in earlier occupations. In addition, copper and exotic chert types appear during the period and are indicative of extensive trading (Smith 2002: 58-59).

1.2.1.3 WOODLAND PERIOD (APPROXIMATELY 1000 B.C.-1650 A.D.)

The primary difference in archaeological assemblages that differentiates the beginning of the Woodland Period from the Archaic Period is the introduction of ceramics to Ontario populations. This division is probably not a reflection of any substantive cultural changes, as the earliest sites of this period seem to be in all other respects a continuation of the Archaic mode of life with ceramics added as a novel technology. The seasonally based system of resource exploitation and associated population mobility persists for at least 1500 years into the Woodland Period (Smith 2002: 61-62).

The Early Woodland Sub-Period dates from about 1000-400 B.C. Many of the artifacts from this time are similar to the late Archaic and suggest a direct cultural continuity between these two temporal divisions. The introduction of pottery represents an entirely new technology that was probably acquired through contact with more southerly populations from which it likely originates (Smith 2002:62).

The Middle Woodland Sub-Period dates from about 400 B.C.-800 A.D. Within the region including the study area, a complex emerged at this time termed "Point Peninsula." Point Peninsula pottery reflects a greater sophistication in pottery manufacture compared with the earlier industry. The paste and temper of the new pottery is finer and new decorative techniques such as dentate and pseudo-scallop stamping appear. There is a noted Hopewellian influence in southern Ontario populations at this time. Hopewell influences

from south of the Great Lakes include a widespread trade in exotic materials and the presence of distinct Hopewell style artifacts such as platform pipes, copper or silver panpipe covers and shark's teeth. The populations of the Middle Woodland participated in a trade network that extended well beyond the Great Lakes Region.

The Late Woodland Sub-Period dates from about 500-1650 A.D. The Late Woodland includes four separate phases: Princess Point, Early Ontario Iroquoian, Middle Ontario Iroquoian and Late Ontario Iroquoian.

The Princess Point phase dates to approximately 500-1000 A.D. Pottery of this phase is distinguished from earlier technology in that it is produced by the paddle method instead of coil and the decoration is characterized by the cord wrapped stick technique. Ceramic smoking pipes appear at this time in noticeable quantities. Princess Point sites cluster along major stream valleys and wetland areas. Maize cultivation is introduced by these people to Ontario. These people were not fully committed to horticulture and seemed to be experimenting with maize production. They generally adhere to the seasonal pattern of occupation practiced by earlier occupations, perhaps staying at certain locales repeatedly and for a larger portion of each year (Smith 2002: 65-66).

The Early Ontario Iroquoian stage dates to approximately 950-1050 A.D. This stage marks the beginning of a cultural development that led to the historically documented Ontario Iroquoian groups that were first contacted by Europeans during the early 1600s (Petun, Neutral, and Huron). At this stage formal semi-sedentary villages emerge. The Early stage of this cultural development is divided into two cultural groups in southern Ontario. The areas occupied by each being roughly divided by the Niagara Escarpment. To the west were located the Glen Meyer populations, and to the east were situated the Pickering people (Smith 2002: 67).

The Middle Ontario Iroquoian stage dates to approximately 1300-1400 A.D. This stage is divided into two sub-stages. The first is the Uren sub-stage lasting from approximately 1300-1350 A.D. The second of the two sub-stages is known as the Middleport sub-stage lasting from roughly 1350-1400 A.D. Villages tend to be larger throughout this stage than formerly (Smith 2002: 67).

The Late Ontario Iroquoian stage dates to approximately 1400-1650 A.D. During this time the cultural divisions identified by early European explorers are under development and the geographic distribution of these groups within southern Ontario begins to be defined.

1.2.2 POST-CONTACT LAND USE OUTLINE

Human occupation in Southern Ontario spans back to the Palaeo-Indian Period (9000-8500 BC). During this time, very small populations of hunter-gatherers would move across the land in search of food. Due to this way of life, archaeological resources are rare because of the lack of stationary living. The effect of the hunter-gatherer lifestyle of the Paleo-Indian Period greatly affected the large scatter of Archaic sites in Southern Ontario as people found fruitful places to acquire food. The archaeological resources from the Archaic period show an egalitarian based society with seasonal subsistence patterns of gathering, causing an increase

of population sizes especially during the spring and summer months. The archaeological resources from sites in Southern Ontario dated to the Woodland Period (1000 BC- AD 1650) show more complex societies, an introduction to pottery and distinctive complex lithic styles (Dieterman 2002).

Sandwich was one of the original towns in Essex County and grew up across the river from the fort on the Detroit side. Although settlement had begun earlier the town of Sandwich was established in 1796 when the British gave up Detroit in accordance with the Jay Treaty. Many of the early settlers were Loyalists who chose to remain loyal to the crown and settled therefore on the Canadian side of the river. In 1845 an act to better define counties and townships in Ontario defined the Boundaries of the Township of Sandwich (Connecting Windsor-Essex 2011).

Essex County was among the first areas of Ontario to be settled. The original settlers were primarily disbanded French soldiers or former fur traders. Permanent settlement began on what was to become the Canadian side of the Detroit River in 1747, at this time these lands were largely inhabited by native peoples, both the Huron and the Ottawas had villages in the area (Connecting Windsor-Essex 2011).

Areas along Lake St. Clair and the Puce, Belle, and Ruscom rivers were originally occupied by the Huron and Wyandot First Nations. Some French colonists associated with Fort Detroit and the fur trade settled in this area in the 18th century. Their descendants are known as Fort Detroit French. They also came from Sandwich, where colonists had developed farms at what was known as Petite Côte, a bend in the Detroit River (Wikipedia 2019).

Map 2 is a facsimile segment of The Historical County Map of Essex County (Tackabury 1877). Map 2 illustrates the location of the study area and environs as of 1877. The study area is not shown to belong to anyone and no structures are shown to be within the study area.

Map 3 is a facsimile segment from the Belden & Co. Essex supplement in Illustrated atlas of the Dominion of Canada (Belden & Co. 1881). Map 2 illustrates the location of the study area and environs as of 1881. The study area is not shown to belong to anyone and no structures are shown to be within the study area.

A plan of the study area is included within this report as Map 6. Current conditions encountered during the Stage 2 Property Assessment are illustrated in Maps 5 & 6.

1.2.3 SUMMARY OF HISTORICAL CONTEXT

The brief overview of readily available documentary evidence indicates that the study area is situated within an area that was close to historic transportation routes and in an area well populated during the nineteenth century and therefore has potential for sites relating to early Post-contact settlement in the region. Background research also indicates the property has potential for significant archaeological resources of Native origins based on proximity to a

natural source of potable water in the past. The study area is located approximately 110 metres from the Detroit River. The study areas proximity to this water body is depicted on the City of Windsor Archaeological Potential Map from The City of Windsor Archaeological Master Plan Study Report for the City of Windsor (CRM Group Limited et al. 2005).

1.3 ARCHAEOLOGICAL CONTEXT

The study area is located in present day Windsor and is bounded on the north by Riverside Drive East, on the east by existing residential development, on the south by Chatham Street East and on the west by existing residential development.

The present use of the study area is a vacant lot. The study area includes within it mostly grass lawn and a few trees.

1.3.1 PHYSIOGRAPHIC REGION

The study area is within the St. Clair Clay Plains. The St. Clair clay plains cover 2, 270 square miles including the Counties of Essex, Kent and Lambton. The region has little relief varying between 575 and 700 feet a.s.l. in most areas. The counties of Lambton and Essex are till plains which have been smoothed by deposits of lacustrine clay which has settled in depressions as a result of glacial lakes Whittlesey and Warren which covered the whole area. A deep cover of overburden lies on the bedrock creating good conditions for vegetation (Chapman and Putnam 1984: 147-151).

1.3.2 SURFACE WATER

Sources of potable water, access to waterborne transportation routes, and resources associated with watersheds are each considered, both individually and collectively to be the highest criteria for determination of the potential of any location to support extended human activity, land use, or occupation. Accordingly, proximity to water is regarded as the primary indicator of archaeological resource potential. The Standards and Guidelines for Consultant Archaeologists stipulates that undisturbed lands within 300 metres of a water source are considered to have archaeological potential (MTC 2011: 21).

The study area is located approximately 110 metres southeast of the Detroit River which is also shown on The Historical County Map of Essex County (Tackabury 1877) and the Belden & Co. Essex supplement in Illustrated atlas of the Dominion of Canada (Belden & Co. 1881).

1.3.3 REGISTERED ARCHAEOLOGICAL SITES

The Archaeological Sites Database administered by the MCM indicates that there are six (6) previously documented sites within 1 kilometre of the study area. However, it must be noted that this assumes the accuracy of information compiled from numerous researchers using different methodologies over many years. AMICK Consultants Limited assumes no

responsibility for the accuracy of site descriptions, interpretations such as cultural affiliation, or location information derived from the Archaeological Sites Database administered by MCM. In addition, it must also be noted that a lack of formerly documented sites does not indicate that there are no sites present as the documentation of any archaeological site is contingent upon prior research having been conducted within the study area.

1.3.4.1 PRE-CONTACT REGISTERED SITES

A summary of registered and/or known archaeological sites within a 1-kilometre radius of the study area was gathered from the Archaeological Sites Database, administered by MCM. As a result, it was determined that one (1) archaeological site relating directly to Pre-contact habitation/activity had been formally registered within the immediate vicinity of the study area. However, the lack of formally documented archaeological sites does not mean that Pre-contact people did not use the area; it more likely reflects a lack of systematic archaeological research in the immediate vicinity. Even in cases where one or more assessments may have been conducted in close proximity to a proposed landscape alteration, an extensive area of physical archaeological assessment coverage is required throughout the region to produce a representative sample of all potentially available archaeological data in order to provide any meaningful evidence to construct a pattern of land use and settlement in the past. One (1) of these sites (AbHs-11) is a multi-component site listed as both a Pre-contact and Post-contact site. All previously registered Pre-contact sites are briefly described below in Table 1:

TABLE 2 PRE-CONTACT SITES WITHIN 1KM

Borden #	Site Name	Time Period	Affinity	Site Type
AbHs-11	Great Western Park	Post-Contact, Woodland, Late	Aboriginal, Euro-Canadian	Burial

None of the above noted archaeological sites are situated within 300 metres of the study area. Therefore, they have no impact on determinations of archaeological potential for further archaeological resources related to Pre-contact activity and occupation with respect to the archaeological assessment of the proposed undertaking.

1.3.4.2 POST-CONTACT REGISTERED SITES

A summary of registered and/or known archaeological sites within a 1-kilometre radius of the study area was gathered from the Archaeological Sites Database, administered by MCM. As a result, it was determined that six (6) archaeological sites relating directly to Post-contact habitation/activity had been formally registered within the immediate vicinity of the study area. One (1) of these sites (AbHs-11) is a multi-component site listed as both a Pre-contact and Post-contact site. All previously registered Post-contact sites are briefly described below in Table 2:

TABLE 3 POST-CONTACT SITES WITHIN 1KM

Borden #	Site Name	Time Period	Affinity	Site Type
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AbHs-70	Walker Power Railroad Turntable	Post-Contact		Railway
AbHs-65		Post-Contact	Euro-Canadian	Homestead
AbHs-15	Senator David A.Croll Park	Post-Contact	Euro-Canadian	House, midden, settlement
AbHs-14	Francois Baby House	Post-Contact	Euro-Canadian, French	House, military
AbHs-13	Train Depot	Post-Contact	Euro-Canadian	Depot, railway
AbHs-11	Great Western Park	Post-Contact, Woodland, Late	Aboriginal, Euro-Canadian	Burial

One of the above noted archaeological sites (AbHs-65) is situated within 300 metres of the study area. Therefore, it demonstrates archaeological potential for further archaeological resources related to Post-contact activity and occupation with respect to the archaeological assessment of the proposed undertaking.

1.3.5 PREVIOUS ARCHAEOLOGICAL ASSESSMENTS

On the basis of information supplied by MCM, no archaeological assessments have been conducted within 50 metres of the study area. AMICK Consultants Limited assumes no responsibility for the accuracy of previous assessments, interpretations such as cultural affiliation, or location information derived from the Archaeological Sites Database administered by MCM. In addition, it must also be noted that the lack of formerly documented previous assessments does not indicate that no assessments have been conducted.

1.3.5.1 PREVIOUS REGIONAL ARCHAEOLOGICAL POTENTIAL MODELLING

The study area is situated within an area subject to an archaeological master plan or a similar regional overview study. *The City of Windsor Archaeological Master Plan* was adopted by Council on 19 October, 2005 (CRM Group Limited et al., 2005). According to the plan:

Due to differences in approach, separate models were developed for Precontact Native settlement and historic period settlement. The Native model is based primarily on environmental and geomorphological criteria which would have influenced Native peoples relationship to the landscape. Although social factors have also been taken into consideration, these are difficult to re-create or interpret given both the time and cultural differences that separate the researcher from the people who lived here in the more distant past. The Euro- Canadian model, which includes the post-contact Native occupation, is based on known settlement locations drawn from historic mapping and other archival sources. The archaeological potential map created through the combination of the two models was subsequently screened to identify areas for which the physical landscape had been extensively modified or disturbed as a result of development. Since land that has been extensively disturbed retains little

or no archaeological integrity, it was identified and excluded from the final archaeological potential map.

(CRM Group Limited et al., 2005: Executive Summary – 2)

Additionally, active archaeological sites were included in the modelling put forward by the plan (CRM Group Limited et al., 2005: Executive Summary – 2). The archaeological First Nations (“Native”) potential modelling considers soil type, glacial geomorphology, drainage and topography, proximity to water and aboriginal transportation networks (CRM Group Limited et al., 2005: Section 4.2). The Euro-Canadian site potential modelling considers historic maps and other historical documentation of settlement patterns, as well as the proximity to previously registered archaeological sites. The resulting potential map shows that the current study area is within an area of high/low composite archaeological potential.

1.3.6 HISTORIC PLAQUES

There are no relevant plaques associated with the study area, which would suggest an activity or occupation within, or near, the study area that may indicate potential for associated archaeological resources of significant CHVI.

1.3.7 SUMMARY OF ARCHAEOLOGICAL CONTEXT

The study area contains a mostly grass lawn area with a few trees.

Background research also indicates that the study area is situated in the St. Clair Clay Plains physiographic region, which is characterized by deposits of lacustrine clay which has settled in depressions.

A total of 6 previously registered archaeological sites have been documented within 1km of the study area. Of these, 1 is characterized as both Pre-contact and Post-contact, 5 are Post-contact and 0 are of unknown cultural affiliation. One of these sites (AbHs-65) is located within 300m of the study area and, therefore, do demonstrate archaeological potential for further archaeological resources of Pre-contact/Post-contact activity and occupation with respect to the archaeological assessment of the current study area.

The study area is situated within an area subject to an archaeological master plan or a similar regional overview study. There are no relevant plaques associated with the study area.

The study area has potential for archaeological resources of Native origins based on proximity to previously registered archaeological sites of Pre-contact origins and proximity to a source of potable water that was also used as a means of waterborne trade and communication. Background research also suggests potential for archaeological resources of Post-contact origins based on proximity to previously registered archaeological sites of Post-contact origins, proximity to a historic roadway, and proximity to areas of documented historic settlement.

2.0 FIELD WORK METHODS AND WEATHER CONDITIONS

2.1 INTRODUCTION

A property inspection was carried out in compliance with Standards and Guidelines for Consultant Archaeologists (MTC 2011) to document the existing conditions of the study area to facilitate the Stage 2 Property Assessment. All areas of the study area were visually inspected and select features were photographed as a representative sample of each area defined within Maps 5 and 6. Observations made of conditions within the study area at the time of the inspection were used to inform the requirement for Stage 2 Property Assessment for portions of the study area as well as to aid in the determination of appropriate Stage 2 Property Assessment strategies. The locations from which photographs were taken and the directions toward which the camera was aimed for each photograph are illustrated in Maps 5 & 6 of this report.

The Stage 2 Assessment of the study area was carried out on 10 November 2022 and consisted of test pit survey at a ten-metre interval to confirm disturbance, which was conducted in compliance with the Standards and Guidelines for Consultant Archaeologists, section 2.1.8: Property Survey to Confirm Previous Disturbance (MTC 2011). Weather conditions were appropriate for the necessary fieldwork required to complete the Stage 2 Property Assessment and to create the documentation appropriate to this study.

2.4 CONFIRMATION OF DISTURBANCE

Approximately 0.25 ha of the study area was subject to test pit survey at 10m intervals to confirm disturbance. The entirety of the study area is disturbed. AMICK Consultants Limited tested the suspected disturbed area at a 10-metre interval to confirm disturbance in a manner consistent with the objectives to ensure that the area is accurately delimited and properly identified. This procedure demonstrated that the entire disturbed portion of the study area consists of deep deposit of mixed topsoil fill overlying buried concrete. There is no archaeological potential within this area.

100% of the study area was disturbed grass lawn area test pit surveyed at 10 metre intervals to confirm disturbance. Maps 5& 6 of this report illustrate the Stage 2 Assessment methodology within the study area.

3.0 RECORD OF FINDS

3.1 INTRODUCTION

As a result of the Stage 2 Assessment of the study area, no archaeological resources of any description were encountered.

4.0 ANALYSIS AND CONCLUSIONS

4.1 STAGE 2 ANALYSIS AND CONCLUSIONS

No archaeological sites or resources were found during the Stage 2 survey of the study area.

In accordance with the definitions contained within the Standards and Guidelines for Consultant Archaeologists (MTC 2011), it has been concluded that no archaeological sites or resources were found during the Stage 2 survey of the study area.

5.0 RECOMMENDATIONS

5.1 STAGE 2 RECOMMENDATIONS

As a result of the Stage 2 Property Assessment of the study area, no archaeological resources were encountered. Consequently, the following recommendations are made:

1. *No further archaeological assessment of the study area is warranted;*
2. *The Provincial interest in archaeological resources with respect to the proposed undertaking has been addressed;*
3. *The proposed undertaking is clear of any archaeological concern.*

6.0 ADVICE ON COMPLIANCE WITH LEGISLATION

While not part of the archaeological record, this report must include the following standard advisory statements for the benefit of the proponent and the approval authority in the land use planning and development process:

- a. *This report is submitted to the Minister of Citizenship and Multiculturalism as a condition of licensing in accordance with Part VI of the Ontario Heritage Act, R.S.O. 1990, c. 0.18. The report is reviewed to ensure that it complies with the standards and guidelines issued by the Minister, and that the archaeological fieldwork and report recommendations ensure the conservation, protection and preservation of the cultural heritage of Ontario. When all matters relating to archaeological sites within the project area of a development proposal have been addressed to the satisfaction of the Ministry of Citizenship and Multiculturalism, a letter will be issued by the ministry stating that there are no further concerns with regard to alterations to archaeological sites by the proposed development.*
- b. *It is an offence under Sections 48 and 69 of the Ontario Heritage Act for any party other than a licensed archaeologist to make any alteration to a known archaeological site or to remove any artifact or other physical evidence of past human use or activity from the site, until such time as a licensed archaeologist has completed archaeological fieldwork on the site, submitted a report to the Minister stating that the site has no further cultural heritage value or interest, and the report has been filed in the Ontario Public Register of Archaeological Reports referred to in Section 65.1 of the Ontario Heritage Act.*

- c. Should previously undocumented archaeological resources be discovered, they may be a new archaeological site and therefore subject to Section 48 (1) of the Ontario Heritage Act. The proponent or person discovering the archaeological resources must cease alteration of the site immediately and engage a licensed archaeologist to carry out archaeological fieldwork, in compliance with sec. 48 (1) of the Ontario Heritage Act.*

- d. The Cemeteries Act, R.S.O. 1990, c. C.4 and the Funeral, Burial and Cremation Services Act, 2002, S.O. 2002, c.33 (when proclaimed in force) require that any person discovering human remains must notify the police or coroner and the Registrar of Cemeteries at the Ministry of Consumer Services.*

- e. Archaeological sites recommended for further archaeological fieldwork or protection remain subject to Section 48 (1) of the Ontario Heritage Act and may not be altered, or have artifacts removed from them, except by a person holding an archaeological licence.*

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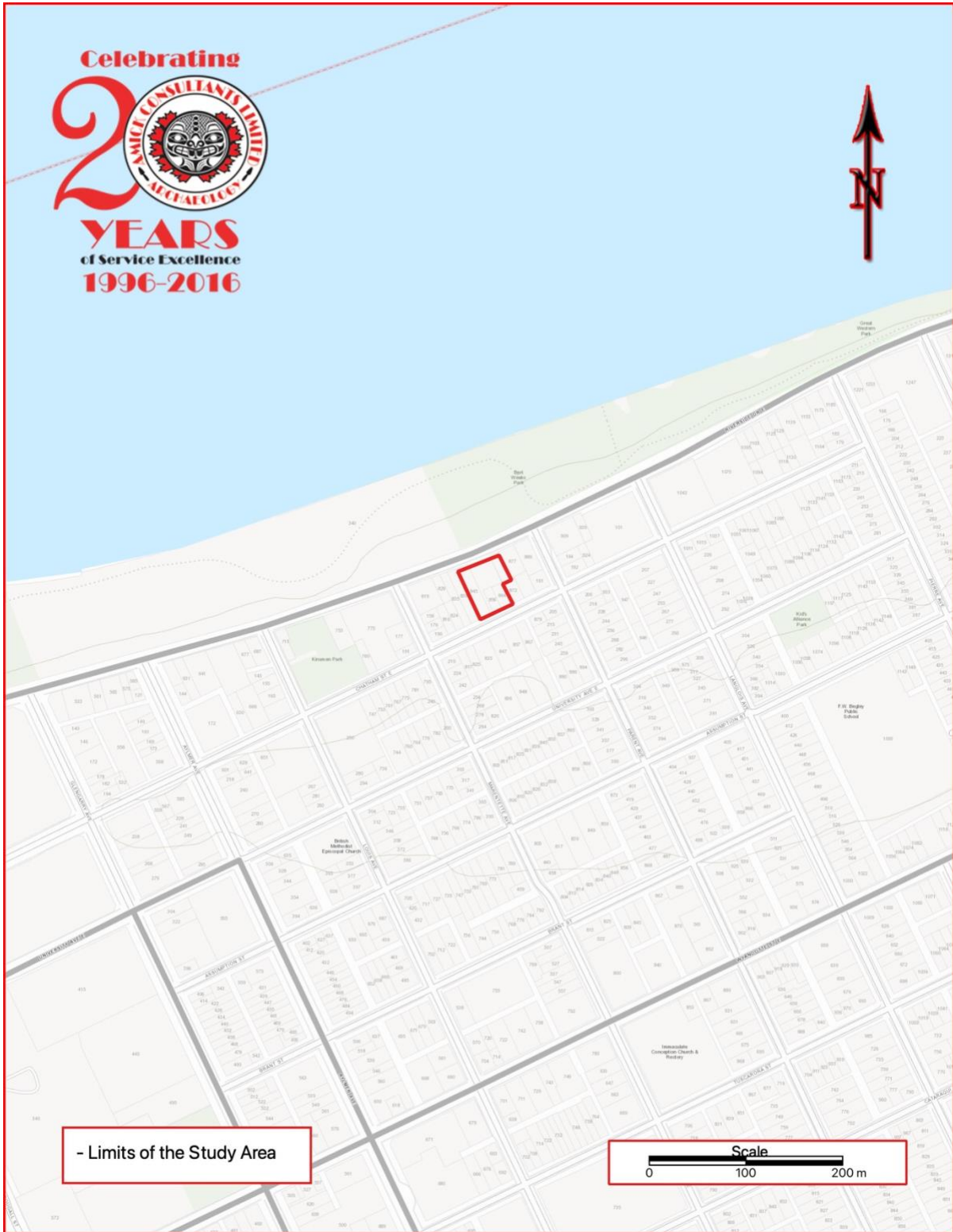
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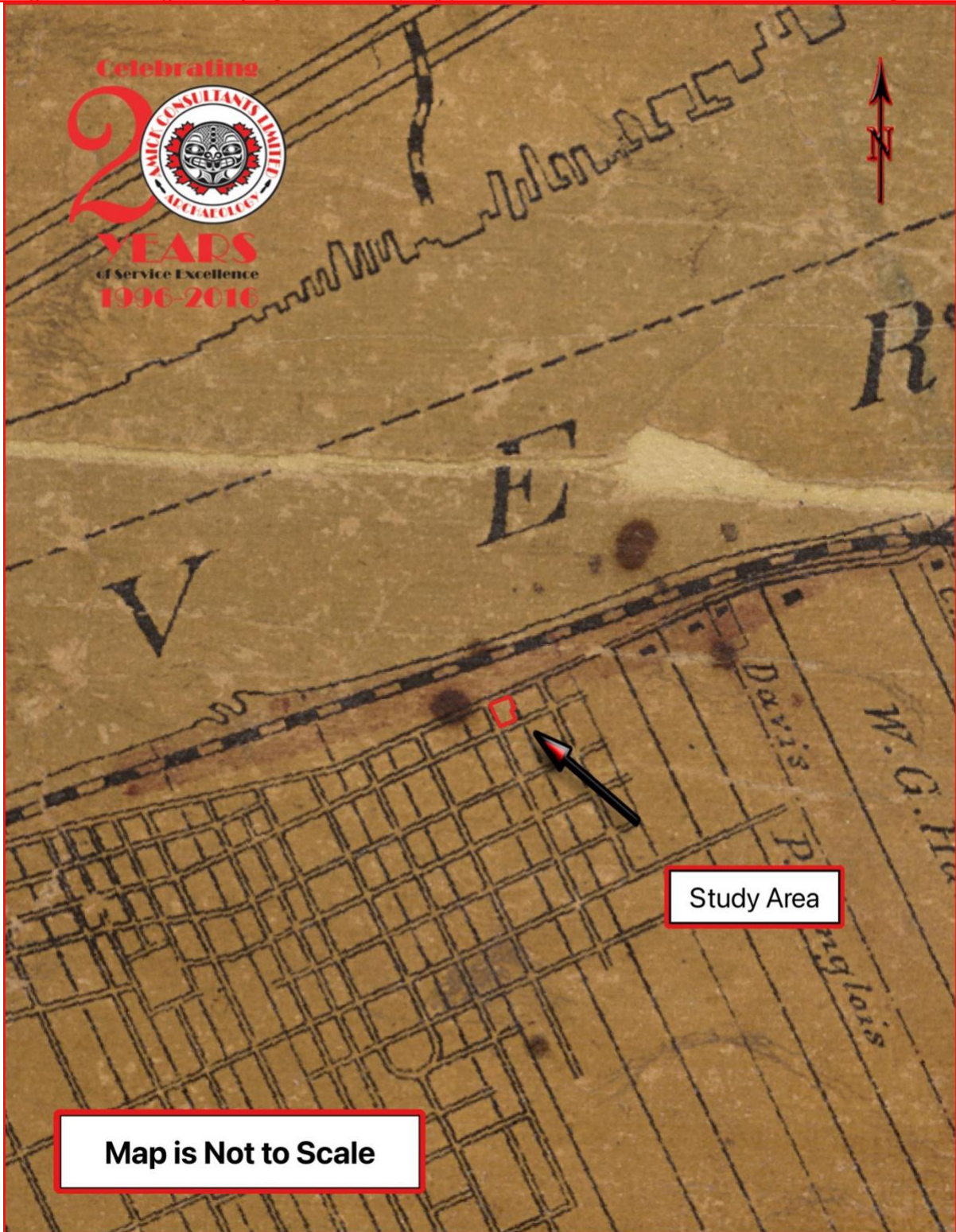
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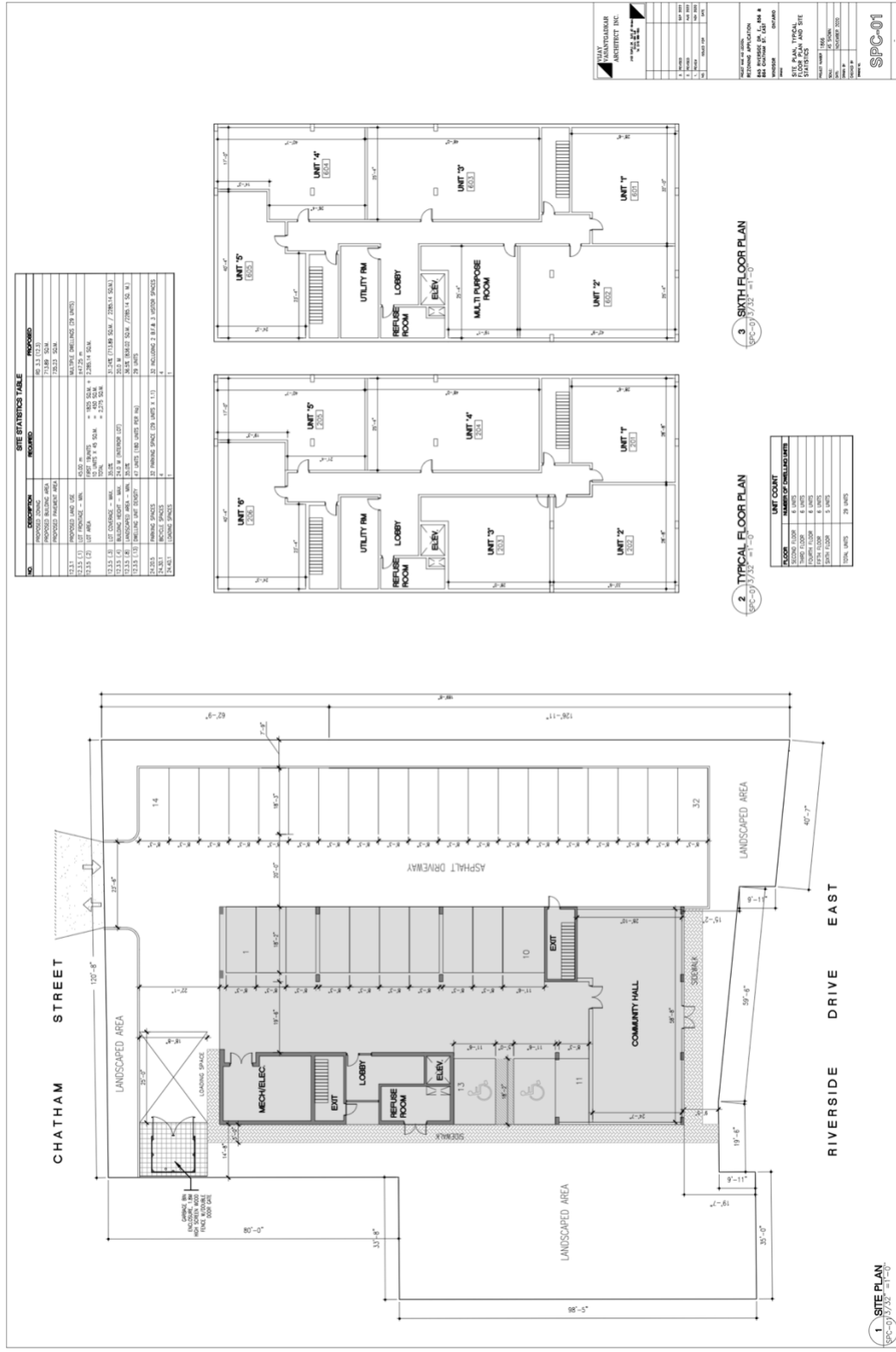
MAPS



MAP 1 LOCATION OF THE STUDY AREA (ESRI 2019)



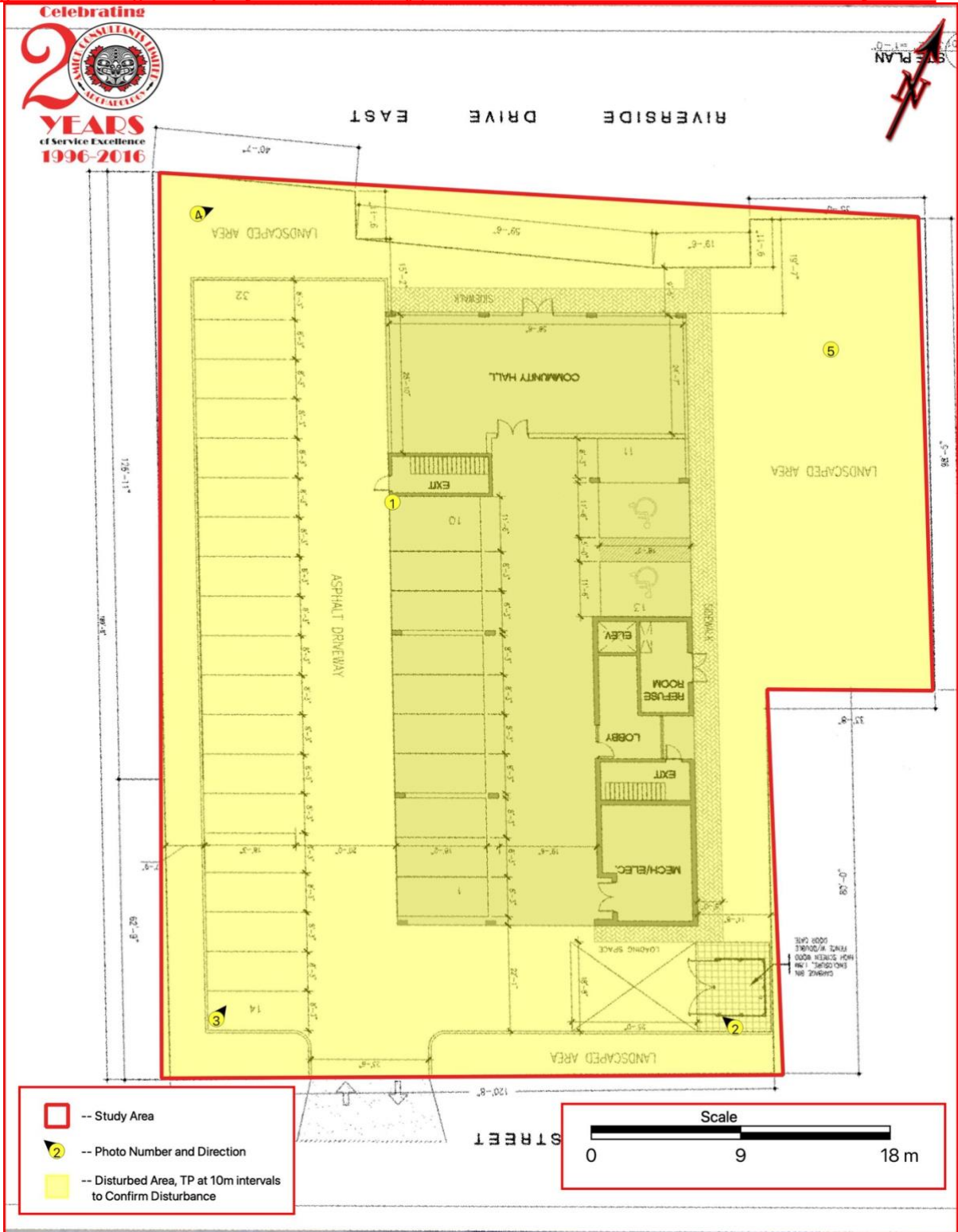
MAP 2 FACSIMILE SEGMENT OF TACKABURY'S MAP OF THE COUNTY OF ESSEX
(TACKABURY 1877)



MAP 4 SITE PLAN (VIJAY VASANTGADKAR ARCHITECT INC. 2020)



MAP 5 AERIAL PHOTO OF THE STUDY AREA (GOOGLE EARTH 2016)



MAP 6 DETAILED PLAN OF THE STUDY AREA

IMAGES



IMAGE 1 COMPLETED TEST PIT



IMAGE 2 TEST SURVEY AREA



IMAGE 3 TEST SURVEY AREA



IMAGE 4 TEST SURVEY AREA



IMAGE 5 COMPLETED TEST PIT

APPENDIX “K”
Draft Amending By-law

B Y - L A W N U M B E R -2023

A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600
CITED AS THE "CITY OF WINDSOR ZONING BY-LAW"

Passed the day of , 2023.

WHEREAS it is deemed expedient to further amend By-law Number 8600 of the Council of The Corporation of the City of Windsor, cited as the "City of Windsor Zoning By-law" passed the 31st day of March, 1986, as heretofore amended:

THEREFORE the Council of The Corporation of the City of Windsor enacts as follows:

1. That subsection 1 of Section 20, of said by-law, is amended by adding the following clause:

**X. SOUTH SIDE OF RIVERSIDE DRIVE EAST, NORTH OF CHATHAM STREET EAST,
 BETWEEN PARENT AVENUE AND MARENTETTE AVENUE**

For the lands comprising of Plan 140 W PT lot 7, Plan 143 E PT lot 1, Plan 140 W PT Lot 7, Plan 143 E PT lot 12; a *multiple dwelling* shall be an additional permitted use subject to the following provisions apply:

.1 Lot Area – minimum

- a) for the first 19 dwelling units – 1825.0 square metres
- b) for each additional dwelling unit- 45.0 square metres per unit

.2 Lot Frontage – minimum- 45.0 m

.3 Lot Coverage – maximum - 35.0 %

.4 Landscape Open Space Yard – minimum - 35% of the *lot area*

.5 Main Building Height – maximum – 20.0 m

.6 Building Setback:

- a) *front yard depth*- from most northerly front lot line - minimum - 6.0m
- b) *rear yard depth* - north limit of Chatham Street east right of way - minimum- 7.5 m
- c) *side yard width*: minimum - 4.5 m

.7 Parking:

- a) *Parking spaces* – minimum - 1.10 per unit
- b) A *parking space* is prohibited in any *front* yard
- c) *Parking spaces* shall be setback a minimum of 6.0 m from the most northerly front lot line abutting the Riverside Drive east right-of-way, and shall be screened from Riverside Drive East;
- d) Vehicular *access* from Riverside Drive East is prohibited
- e) Indoor ground floor amenity space – minimum – 4.0 square metres per unit

[ZDM 1xx; ZNG/xxx]

2 The said by-law is further amended by changing the Zoning District Maps or parts thereof referred to in Section 1, of said by-law and made part thereof, so that the lands described in Column 3 are delineated by a broken line and further identified by the zoning symbol shown in Column 5:

1. Item Number	2. Zoning District Map Part	3. Lands Affected	4. Official Plan Amendment Number	5. Zoning Symbol
1	6	Plan 140 W PT lot 7, Plan 143 E PT lot 1, Plan 140 W PT Lot 7, Plan 143 E PT lot 12 (known municipally as 0 & 845 Riverside Drive East, and 856, & 864 Chatham Street East; (Roll No: 3739-030-050-00200-0000, 3739-030-050-00100-0000, 3739-030-040- 09400-0000, 3739-030-050-01400-0000, 3739-030-040-08800-0000)		S.20(1)x

DREW DILKENS, MAYOR

CLERK

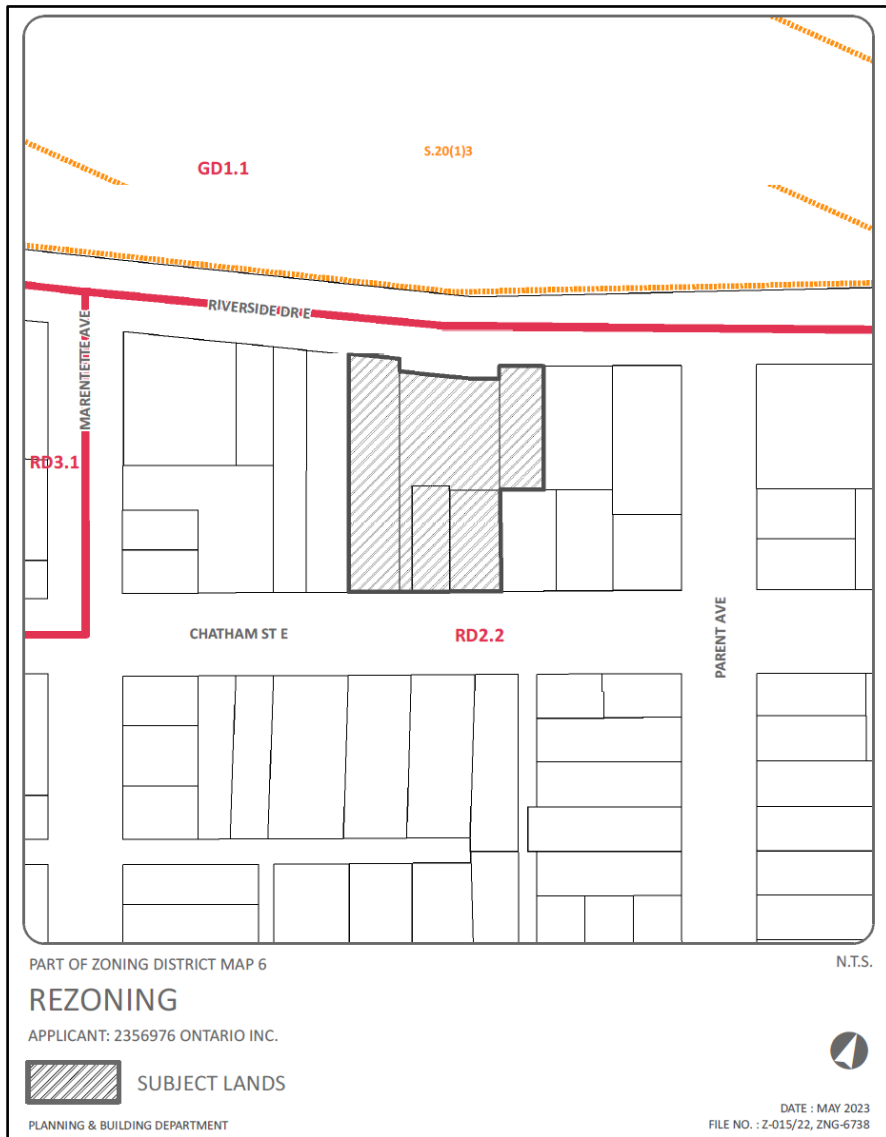
First Reading - , 2023
 Second Reading - , 2023
 Third Reading - , 2023

SCHEDULE 2

1. By-law _____ has the following purpose and effect:
 - I. To amend the zoning on Plan 140 W PT lot 7, Plan 143 E PT lot 1, Plan 140 W PT Lot 7, Plan 143 E PT lot 12 (Roll No: 3739-030-050-00200-0000, 3739-030-050-00100-0000, 3739-030-040-09400-0000, 3739-030-050-01400-0000, 3739-030-040-08800-0000), situated on the south side of Chatham Street east, west of Parent Avenue and known municipally as 0 & 845 Riverside Drive East, and 856, & 864 Chatham Street East by adding a site specific exception to Section 20(1) as follows:
 - X. **SOUTH SIDE OF RIVERSIDE DRIVE EAST, NORTH OF CHATHAM STREET EAST, BETWEEN PARENT AVENUE AND MARENTETTE AVENUE**

For the lands comprising of Plan 140 W PT lot 7, Plan 143 E PT lot 1, Plan 140 W PT Lot 7, Plan 143 E PT lot 12; a *multiple dwelling* shall be an additional permitted use subject to the following provisions apply:

 - .1 Lot Area – minimum
 - a) for the first 19 dwelling units – 1825.0 square metres
 - b) for each additional dwelling unit- 45.0 square metres per unit
 - .2 Lot Frontage – minimum- 45.0 m
 - .3 Lot Coverage – maximum - 35.0 %
 - .4 Landscape Open Space Yard – minimum - 35% of the *lot area*
 - .5 Main Building Height – maximum – 20.0 m
 - .6 Building Setback:
 - d) *front yard depth*- from most northerly front lot line - minimum - 6.0m
 - e) *rear yard depth* - north limit of Chatham Street east right of way - minimum- 7.5 m
 - f) *side yard width*: minimum - 4.5 m
 - .7 Parking:
 - f) *Parking spaces* – minimum - 1.10 per unit
 - g) A *parking space* is prohibited in any *front yard*
 - h) *Parking spaces* shall be setback a minimum of 6.0 m from the most northerly front lot line abutting the Riverside Drive east right-of-way, and shall be screened from Riverside Drive East;
 - i) Vehicular *access* from Riverside Drive East is prohibited
 - j) Indoor ground floor amenity space – minimum – 4.0 square metres per unit
2. Key map showing the location of the lands to which By-law _____ applies.



Item No. 8.1



Committee Matters: SCM 170/2023

Subject: Adoption of the Development & Heritage Standing Committee meeting minutes held June 5, 2023

Development & Heritage Standing Committee Meeting Minutes

Date: Monday, June 05, 2023

Time: 4:30 o'clock p.m.

Members Present:

Councillors

Ward 1 - Councillor Fred Francis
Ward 4 – Councillor Mark McKenzie
Ward 7 – Councillor Angelo Marignani
Ward 9 – Councillor Keiran McKenzie
Ward 10 - Councillor Jim Morrison (Chairperson)

Members

Member Anthony Arbour
Member Daniel Grenier
Member Charles Pidgeon
Member Robert Polewski
Member William Tape

Members Regrets

Member Khassan Saka
Member John Miller

PARTICIPATING VIA VIDEO CONFERENCE ARE THE FOLLOWING:

Member Joseph Fratangeli
Sandra Gebauer, Council Assister

ALSO PARTICIPATING IN COUNCIL CHAMBERS ARE THE FOLLOWING FROM ADMINISTRATION:

Jelena Payne, Commissioner – Economic Development & Innovation
Wira Vendrasco, Deputy City Solicitor – Legal & Real Estate
Kevin Alexander, Planner II – Special Projects

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Neil Robertson – Manager Urban Design/ Deputy City Planner
Adam Szymczak – Planner III – Zoning
Brian Nagata – Planner II – Development Review
Jim Abbs – Planner III – Subdivisions
Greg Atkinson – Planner III – Economic Development
Justina Nwaesei – Planner III – Subdivisions
Frank Garardo – Planner III – Policy and Special Studies
Rob Perissinotti – Development Engineer
Shannon Deehan – Transportation Planning Coordinator
Tracy Tang - Planner II – Revitalization & Policy Initiatives
Anna Ciacelli – Deputy City Clerk

Delegations—participating via video conference

Item 7.2 Bassem Mazloum, area resident
Item 7.2 Jacky Ng, Project & Architectural Designer, Avant Group Inc.
Item 7.6 Tracey Pillon-Abbs, Principal Planner, representing the applicant

Delegations—participating in Council Chambers

Item 7.2 Bruce Sheardown, area resident
Item 7.2 Michael Chen, area resident
Item 7.2 Justine Nakigozi, area resident
Item 7.2 Mrs. Antonia Laberi, area resident
Item 7.2 Mr. Laberi, area resident
Item 7.5 Amy Farkas, Planner, Dillion Consulting Limited
Item 7.5 Theresa O'Neill, Planner, Dillion Consulting Limited
Item 10.1 Micheal Schmidlin, Owner/applicant

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Development & Heritage Standing Committee Monday, June 05, 2023

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1. CALL TO ORDER

Following the reading of the Land Acknowledgement, the Chairperson calls the meeting of the Development & Heritage Standing Committee to order at 4:30 o'clock p.m.

2. DISCLOSURES OF PECUNIARY INTEREST AND THE GENERAL NATURE THEREOF

None disclosed.

3. REQUEST FOR DEFERRALS, REFERRALS OR WITHDRAWALS

None requested.

4. COMMUNICATIONS

None presented.

8. ADOPTION OF THE MINUTES

8.1. Adoption of the Development & Heritage Standing Committee minutes held March 6, 2023

Moved by: Councillor Angelo Marignani
Seconded by: Councillor Mark McKenzie

That the minutes of the Development & Heritage Standing Committee meeting held March 6, 2023 **BE ADOPTED** as presented.
Carried.

Report Number: SCM 90/2023

8.2. Adoption of the Development & Heritage Standing Committee minutes of its meeting held April 5, 2023

Moved by: Councillor Angelo Marignani
Seconded by: Councillor Mark McKenzie

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That the minutes of the Development & Heritage Standing Committee meeting held April 5, 2023
BE ADOPTED as presented.

Carried.

Report Number: SCM 127/2023

8.3. May 1, 2023 Development & Heritage Standing Committee minutes

Moved by: Councillor Angelo Marignani

Seconded by: Councillor Mark McKenzie

That the minutes of the Development & Heritage Standing Committee meeting held May 1, 2023
BE ADOPTED as presented.

Carried.

Report Number: SCM 136/2023

10. HERITAGE ACT MATTERS

10.1. 793 Devonshire Road, Bell-Coulter House - Heritage Permit & Community Heritage Fund Request (Ward 4)

Michael Schmidlin, Owner/applicant

Michael Schmidlin, Owner/applicant appears before the Development and Heritage Standing Committee Meeting regarding the administrative report “793 Devonshire Road, Bell-Coulter House-Heritage Permit & Community Heritage Fund Request (Ward 4)”and is available for questions.

Councillor Kieran McKenzie inquires about the financial matters in the administrative report and the loan component. Tracy Tang, Planner II-Revitalization & Policy Initiatives appears before the Development & Heritage Standing Committee Meeting regarding the administrative report “793 Devonshire Road, Bell-Coulter House-Heritage Permit & Community Heritage Fund Request (Ward 4)” and provides the rationale related to the grant component request by the applicant, being 28% instead of the suggested 15% including, inflation costs, increase in material, labour costs increasing, and heritage specialized trades persons shortage. Ms. Tang indicates when all those factors were reviewed with the request, administration recommends approval of the increased amount.

Councillor Kieran McKenzie inquires whether administration is considering formal direction around the thresholds, due to the outlined circumstances and whether updated guidelines are required. Ms. Tang indicates that discussions with Heritage planning staff and management are taking place, reviewing the existing Heritage guidelines and procedures.

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Moved by: Councillor Fred Francis

Seconded by: Councillor Kieran McKenzie

Decision Number: **DHSC 514**

- I. That a Heritage Permit at 793 Devonshire Rd, Bell-Coulter House, **BE GRANTED** for restoration of the chimneys and eavestroughs system.
- II. That the City Planner or designate **BE DELEGATED** the authority to approve any further proposed changes associated with the restoration.
- III. That a total grant of 28% of the cost of the chimney repairs and eavestroughs system repairs, to an upset amount of \$15,924 from the Community Heritage Fund (Reserve Fund 157) **BE GRANTED** to the Owners of the Bell-Coulter House, at 793 Devonshire Rd, subject to:
 - a. Submission of conservation details, technical details and samples, to the satisfaction of the City Planner or designate prior to work start;
 - b. Determination by the City Planner that the work is completed to heritage conservation standards;
 - c. Owner's submission of paid receipts for work completed;
 - d. That the Community Heritage Fund (Reserve Fund 157), grants approved shall lapse if the applicant has not completed the work and fulfilled the conditions within 2 years of the approval date.

Carried.

Report Number: S 63/2023
Clerk's File: MBA/12592

There being no further business the meeting of the Development & Heritage Standing Committee (*Heritage Act* Matters) portion is adjourned at 4:35 o'clock p.m.

The Chairperson calls the *Planning Act* Matters portion of the Development & Heritage Standing Committee meeting to order at 4:37 o'clock p.m.

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Development & Heritage Standing Committee
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5. ADOPTION OF THE PLANNING ACT MINUTES

5.1. Minutes of the April 5, 2023 Development & Heritage Standing Committee (*Planning Act Matters*)

Moved by: Councillor Mark McKenzie
Seconded by: Councillor Angelo Marignani

That the *Planning Act* minutes of the Development & Heritage Standing Committee meeting held April 5, 2023 **BE ADOPTED** as presented.
Carried.

Report Number: SCM 128/2023

5.2. May 1, 2023 Development & Heritage Standing Committee Minutes (*Planning Act Matters*)

Moved by: Councillor Mark McKenzie
Seconded by: Councillor Angelo Marignani

That the *Planning Act* minutes of the Development & Heritage Standing Committee meeting held May 1, 2023 **BE ADOPTED** as presented.
Carried.

Report Number: SCM 148/2023

6. PRESENTATION DELEGATIONS (*PLANNING ACT MATTERS*)

See Items 7.2, 7.5, 7.6

3. REQUEST FOR DEFERRALS, REFERRALS OR WITHDRAWALS (*PLANNING ACT MATTERS*)

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7.1. Application for Approval of the Conversion of an existing Non-Residential Building at 1500 Ouellette Avenue to a Residential and Commercial Plan of Condominium; Applicant - Ryan Michael Solcz Prof. Corp; File No. CDM-004/22 [CDM/6939]; Ward 3

Moved by: Councillor Kieran McKenzie
Seconded by: Councillor Angelo Marignani

That the report of the Senior Planner, Subdivisions dated May 12, 2023 entitled “Application for Approval of the Conversion of an existing Non-Residential Building at 1500 Ouellette Avenue to a Residential and Commercial Plan of Condominium; Applicant - Ryan Michael Solcz Prof. Corp; File No. CDM-004/22 [CDM/6939]; Ward 3” **BE DEFERRED** to the August 1, 2023 Development and Heritage Standing Committee meeting to allow for the applicant to review some of the conditions.
Carried.

Report Number: S 62/2023
Clerk’s File: Z/14543

7.2. Zoning By-law Amendment Application for 0 & 1466 St. Patrick Avenue, Z-037/22 [ZNG-6899], Ward 10

Moved by: Councillor Kieran McKenzie
Seconded by: Councillor Angelo Marignani

That the report of the Planner II – Development Review dated May 2, 2023 entitled “Zoning By-law Amendment Application for 0 & 1466 St. Patrick Avenue, Z-037/22 [ZNG-6899], Ward 10” **BE DEFERRED** to the August 1, 2023 Development & Heritage Standing Committee Meeting to allow for Administration to provide information related to costs for sewer infrastructure that will be required for the proposed development and the costing options that will be available for surrounding residents, as well as issues related to costing for permit parking if required.
Carried.

Report Number: S 56/2023
Clerk’s File: Z/14541

7. PLANNING ACT MATTERS

7.3. Zoning Z42-22 [ZNG/6937} - 1155 California Ave Ward 2

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Moved by: Councillor Angelo Marignani

Seconded by: Councillor Mark McKenzie

Decision Number: **DHSC 510**

- I. That Zoning By-law 8600 **BE AMENDED** for Plan 1120 N PT Lot 76; S PT Lot 77 (known municipally as 1155 California Avenue); by adding a site-specific exception to Section 20(1) as follows:

4xx. **WEST SIDE OF CALIFORNIA AVENUE BETWEEN GIRARDOT STREET AND PELLETIER STREET**

For the lands comprising of lots N PT Lot 76; S PT Lot 77 Registered Plan 1120, a *semi-detached dwelling* shall be an additional permitted use and shall be subject to the following additional provisions:

- | | |
|------------------------|----------------------|
| a) Lot Width – minimum | 12.0 m |
| b) Lot Area – minimum | 398.0 m ² |

Further, for a *semi-detached dwelling*, two *dwelling units* in a *semi-detached dwelling unit* and one *dwelling unit* in an *accessory building* which is accessory to a *semi-detached dwelling* shall be additional permitted uses and shall be subject to the provisions in Sections 5.99.80.3 and 5.99.80.5.

[ZDM4; ZNG/6937]

Carried.

Report Number: S 54/2023

Clerk's File: Z/14573

7.4. Housekeeping Official Plan and Zoning By-law Amendments initiated by the City of Windsor to permit additional dwelling units (City-wide) - File No. OPA 172 and Z16-23

Moved by: Councillor Kieran McKenzie

Seconded by: Councillor Angelo Marignani

Decision Number: **DHSC 511**

- I. That Official Plan Amendment No. 172 as shown in Appendix A regarding a housekeeping amendment on new policies authorizing the use of additional dwelling units **BE ADOPTED**; and,
- II. That a housekeeping amendment to Zoning By-law 8600 as shown in Appendix B implementing additional dwelling units **BE PASSED**; and,
- III. That a housekeeping amendment to Zoning By-Law 85-18 as shown in Appendix C implementing additional dwelling units **BE PASSED**;

Carried.

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Report Number: S 64/2023
Clerk's File: ZO/13740 & ZB/13739

7.5. Zoning Bylaw Amendment Z 013-23 [ZNG-7000] - 1027458 Ontario Inc. - 0 Clover Avenue - SW Corner Clover & Wyandotte - Ward 7

Moved by: Councillor Angelo Marignani
Seconded by: Councillor Mark McKenzie

Decision Number: **DHSC 512**

- I That an amendment to City of Windsor Zoning By-law 8600 **BE APPROVED**, changing the zoning of Lands Identified as Part 1 in Figure 5: Areas to be rezoned from Residential District (RD) 1.1 to Residential District (HRD) 2.3;
- II That a Hold provision **BE APPLIED** to Lands Identified as Part 1 in Figure 5: Areas to be rezoned and that it be removed when the conditions contained in Section 5.4 HOLDING ZONE PROVISIONS of City of Windsor Zoning By-law 8600 have been met;
- III That subsection 1 of Section 20 of the City of Windsor Zoning By-law 8600 **BE AMENDED** for the lands described as Lands Identified as Part 1 in Figure 5: Areas to be rezoned by adding site specific regulations as follow:
 - 4XX. **South West Corner Clover Avenue & Wyandotte Street**
For the lands described as Lands Identified as Part 1 in Figure 5: Areas to be rezoned, the total area of the *required front yard* occupied by a hard surface for the purpose of a walkway, driveway, *access area*, *parking space*, or any combination thereof, exceeding 60% of the *required front yard* shall be prohibited, and,
Lot Coverage – Maximum 50%
(ZDM 14; ZNG/7000)
- IV That an amendment to City of Windsor Zoning By-law 8600 **BE APPROVED**, changing the zoning of Lands Identified as Part 2 in Figure 5: Areas to be rezoned from Development Reserve District (DRD) 1.1 to Green District (GD) 1.1;

Carried.

Report Number: S 65/2023
Clerk's File: Z/14572

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7.6. OPA & Rezoning – 1998308 Ontario Inc. – 0 Russell Street - OPA 168 OPA/6975 Z-007/23 ZNG/6974 – Ward 2

Moved by: Member Anthony Arbour

Seconded by: Councillor Mark McKenzie

Decision Number: **DHSC 513**

1. That Schedule “D” of Volume 1: The Primary Plan of the City of Windsor Official Plan **BE AMENDED** by changing the land use designation of Lot 13, E/S Russell St, Registered Plan 40, Town of Sandwich (PIN 01591-0155; 0 Russell Street; Roll No. 050-160-12400), situated at Russell Street, Chippawa Street, and Wilkinson Lane, from “Business Park” to “Residential”.

2. That Zoning By-law 8600 **BE AMENDED** by changing the zoning of Lot 13, E/S Russell St, Registered Plan 40 Town of Sandwich (PIN 01591-0155; 0 Russell Street; Roll No. 050-160-12400), situated at Russell Street, Chippawa Street, and Wilkinson Lane, from Manufacturing District 1.4 (MD1.4) to Residential District 3.3 (RD3.3) and by adding a site specific exception to Section 20(1) as follows:

471. **CORNER OF RUSSELL STREET, CHIPPAWA STREET AND WILKINSON LANE**

For the lands comprising of Lot 13, E/S Russell Street, Registered Plan 40, Town of Sandwich (PIN 01591-0155) the following shall apply:

1. A *townhome dwelling* is an additional permitted use and shall be subject to the additional provisions in paragraph a) below, the provisions in Section 11.2.5, and all other applicable provisions:
 - a) Subject to the satisfaction of the Chief Building Official, the following noise mitigation measures are required at time of building permit:
 - i) Exterior building components shall meet the minimum Acoustic Insulation Factors in Tables 2 and 3 of the Detailed Noise Control Study prepared by SS Wilson Associates, Consulting Engineers and dated November 22, 2022, subject to any revisions.
 - ii) Installation of central air conditioning
2. For any permitted use in RD3.3, the following additional provisions shall apply:
 - a) Notwithstanding Sections 12.3.5.4 and 21(3), *building height* as measured from Russell Street in excess of 4 *storeys* and *building height* as measured from Wilkinson Lane in excess of 3 *storeys* shall be prohibited. For the purpose of this provision, grade means the average elevation of the crown of That part of the street identified above and abutting the subject lot.
 - b) *Parking spaces* forming part of a *parking area* shall be permitted in a *front yard* and are subject to the Parking Area Provisions in Section 25.

[ZDM 4; ZNG/6975]

3. That Lot 13, E/S Russell St, Registered Plan 40, Town of Sandwich (PIN 01591-0155; 0 Russell Street; Roll No. 050-160-12400), situated at the corner of Russell Street, Chippawa Street, and Wilkinson Lane, **BE CLASSIFIED** as a Class 4 area pursuant to MOECP Environmental Noise

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Guideline Publication NPC-300; and, that the City Planner or their designate **BE DIRECTED** to provide a copy of the Council Resolution approving the Class 4 area classification and a copy of any development agreement or site plan agreement for the subject lands that incorporates noise mitigation measures to Windsor Port Authority (3190 Sandwich St), Lafarge Canada (3450 Russell St) and St. Mary's CBM Ready-Mix Plant (3510 Russell St).

4. That, when Site Plan Control is applicable:

- A. Prior to the submission of an application for site plan approval, at the discretion of the City Planner, Deputy City Planner, or Site Plan Approval Officer, those documents submitted in support of the applications for amendments to the Official Plan and Zoning By-law 8600 **BE UPDATED** to reflect the site plan for which approval is being sought.
- B. The Site Plan Approval Officer **BE DIRECTED** to incorporate the following, subject to any updated information, into an approved site plan attached to an executed and registered site plan agreement:
 - i) Mitigation measures, including warning clauses, identified in the Detailed Noise Control Study prepared by SS Wilson Associates, Consulting Engineers, dated November 22, 2022, subject to the approval of the City Planner, Deputy City Planner, or Site Plan Approval Officer.
 - ii) Requirements of the City of Windsor – Engineering Department – Right-of-Way Division and City of Windsor – Transportation Planning contained in Appendix E of Report S 53/2023, subject to the approval of the City Engineer.
- C. The Site Plan Approval Officer **CONSIDER** the comments from municipal departments and external agencies contained in Appendix E of Report S 53/2023.

Carried.

Report Number: S 53/2023
Clerk's File: Z/14559 & Z/14558

There being no further business the meeting of the Development & Heritage Standing Committee (*Planning Act Matters*) portion is adjourned at 6:34 o'clock p.m.

The Chairperson calls the Administrative Items portion of the Development & Heritage Standing Committee meeting to order at 6:34 o'clock p.m.

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9. PRESENTATIONS AND DELEGATIONS (COMMITTEE ADMINISTRATIVE MATTERS)

None presented.

11. ADMINISTRATIVE ITEMS

11.1. Brownfield Redevelopment Community Improvement Plan (CIP) application submitted by Fife Veterinary Professional Corp. for 816 Hanna Street East (Ward 4)

Moved by: Councillor Angelo Marignani
Seconded by: Councillor Fred Francis

Decision Number: **DHSC 515**

- I. That the request made by Fife Veterinary Professional Corp. on behalf of the property owner to participate in the Environmental Site Assessment Grant Program **BE APPROVED** for the completion of a proposed Phase II Environmental Site Assessment Study for property located at 816 Hanna Street East pursuant to the City of Windsor Brownfield Redevelopment Community Improvement Plan;
- II. That the City Treasurer **BE AUTHORIZED** to issue payment up to a maximum of \$9,487.50 based upon the completion and submission of a Phase II Environmental Site completed in a form acceptable to the City Planner and City Solicitor;
- III. That the grant funds in the amount of \$9,487.50 under the Environmental Site Assessment Grant Program **BE TRANSFERRED** from the CIP Reserve Fund 226 to Brownfield Strategy Remediation (project 7069003) when the eligible work is completed to the satisfaction of the City Planner;
- IV. That should the proposed Phase II Environmental Site Assessment Study not be completed within two (2) years of Council approval, the approval **BE RESCINDED** and the funds be uncommitted and made available for other applications.
Carried.

Report Number: S 51/2023
Clerk's File: Z2023

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11.2. Brownfield Redevelopment Community Improvement Plan (CIP) application submitted by Dior Homes on behalf of Capital Plus Real Estate Corp. for 3841, 3843, and 3847 Howard Avenue (Ward 9)

Moved by: Councillor Angelo Marignani
Seconded by: Councillor Mark McKenzie

Decision Number: **DHSC 516**

- I. That the request made by Dior Homes on behalf of Capital Plus Real Estate Corp. to participate in the Environmental Site Assessment Grant Program **BE APPROVED** for the completion of a proposed Phase II Environmental Site Assessment Study for property located at 3841, 3843, and 3847 Howard Avenue pursuant to the City of Windsor Brownfield Redevelopment Community Improvement Plan;
- II. That the City Treasurer **BE AUTHORIZED** to issue payment up to a maximum of \$12,875 based upon the completion and submission of a Phase II Environmental Site completed in a form acceptable to the City Planner and City Solicitor;
- III. That the grant funds in the amount of \$12,875 under the Environmental Site Assessment Grant Program **BE TRANSFERRED** from the CIP Reserve Fund 226 to Brownfield Strategy Remediation (project 7069003) when the eligible work is completed to the satisfaction of the City Planner;
- IV. That should the proposed Phase II Environmental Site Assessment Study not be completed within two (2) years of Council approval, the approval **BE RESCINDED** and the funds be uncommitted and made available for other applications.

Carried.

Report Number: S 60/2023
Clerk's File: SPL2023

11.3. Amendment to CR58/2021 for Closure of east/west alley between Matthew Brady Boulevard and east limit of 7730 St. Rose Avenue, Ward 6, SAA-5947

Moved by: Councillor Angelo Marignani
Seconded by: Councillor Fred Francis

Decision Number: **DHSC 517**

- I. That CR58/2021, adopted on February 1, 2021, **BE AMENDED** as follows:
By **DELETING** the following wording under section IV to the council resolution:

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- IV. That the 4.86-metre-wide east/west alley between Matthew Brady Boulevard and St. Paul Avenue, north of St. Rose Avenue, and shown as “Part 2” on Drawing No. CC-1767 *attached* as Appendix ‘A’, **BE CLOSED AND CONVEYED** to the abutting property owners in equal halves along the centre line of the alley, and adjusted as necessary by the City Planner;

And **INSERTING:**

- IV. That the 4.86-metre-wide east/west alley between Matthew Brady Boulevard and St. Paul Avenue, north of St. Rose Avenue, and shown as “Part 2” on Drawing No. CC-1767 *attached* as Appendix ‘A’, **BE CLOSED AND CONVEYED** to the owner of the abutting property known municipally as 884 Matthew Brady Boulevard (legally described as Part of Lot 48 & Part of Closed Alley, Plan 1181) and as necessary, in a manner deemed appropriate by the City Planner, subject to the following:
- a. Easement, subject to there being accepted in the City’s standard form and in accordance with the City’s standard practice, be granted to:
 - i. Owner of the property known municipally as 7720 St. Rose Avenue (legally described as Lot 50, Plan 1181) to enter, construct, maintain, inspect, alter, repair, remove, replace, reconstruct and enlarge the existing private sanitary sewer; and
 - ii. Owner of the property known municipally as 7730 St. Rose Avenue (legally described as Lot 49, Plan 1181) to enter, construct, maintain, inspect, alter, repair, remove, replace, reconstruct and enlarge the existing private sanitary sewer.

Carried.

Report Number: S 61/2023
Clerk’s File: SAA2023

11.4. Closure of north/south alley located between Algonquin Street and 1429 Randolph Avenue, Ward 10, SAA-7025

Moved by: Councillor Angelo Marignani
Seconded by: Councillor Fred Francis

That the report of the Planner II – Development Review dated May 5, 2023 entitled “Closure of north/south alley located between Algonquin Street and 1429 Randolph Avenue, Ward 10, SAA-7025” **BE DEFERRED** to the August 1, 2023 Development & Heritage Standing Committee

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meeting to coincide with the item related to "Zoning By-law Amendment Application for 0 & 1466 St. Patrick Avenue, Z-037/22 [ZNG-6899], Ward 10".

Carried.

Report Number: S 57/2023

Clerk's File: SAA2023

11.5. Sandwich Town CIP Application, 3150 Peter Street; Owners South Rustico Holdings Inc, (c/o David Ly)-Ward 2

Moved by: Councillor Fred Francis

Seconded by: Councillor Kieran McKenzie

Decision Number: **DHSC 518**

- I. That the request for incentives under the Sandwich Incentive Program made by South Rustico Holdings Inc, (c/o David Ly), owner of the property located at 3150 Peter Street, **BE APPROVED** for the following programs when all work is complete:
 - i. *Development and Building Fees Grant* for 100% of the Development and Building Fees identified in the Sandwich CIP to a Maximum amount of **(+/- \$5,000)**;
 - ii. *Revitalization Grant Program* for 70% of the municipal portion of the tax increment for up to 10 years **(+/- \$2,051 per year)**; and
- II. That Administration **BE AUTHORIZED** to prepare the Sandwich Incentive Program Agreement for the *Revitalization Grant* in accordance with all applicable policies, requirements, and provisions contained within the Olde Sandwich Towne Community Improvement Plan to the satisfaction of the City Planner as to content, the City Solicitor as to legal form, and the CFO/City Treasurer as to financial implication;
- III. That the CAO and City Clerk **BE AUTHORIZED** to sign the agreement between the City and South Rustico Holdings Inc, (c/o David Ly) to implement the *Building/Property Improvement Tax Increment Grant Program* (only) in accordance with all applicable policies, requirements to the satisfaction of the City Planner as to content, the City Solicitor as to legal form, and the CFO/City Treasurer as to financial implications;
- IV. That funds in the maximum amount of **+/- \$5,000** under the *Development Building Fees Grant Program* **BE TRANSFERRED** from the CIP Reserve Fund 226 to the *Sandwich Community Development Plan Fund* (Account 7076176) when the work is complete;
- V. That grants **BE PAID** to South Rustico Holdings Inc, (c/o David Ly) upon completion of the interior/exterior renovations including the addition of a new dwelling unit in an existing space

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from the *Sandwich Community Development Plan Fund* (Project 7076176) to the satisfaction of the City Planner and Chief Building Official; and,

- VI. That grants approved **SHALL LAPSE** if the applicant has not completed the work and fulfilled the conditions within 2 years of the approval date. Extensions **SHALL BE** given at the discretion of the City Planner.

Carried.

Report Number: S 55/2023
Clerk's File: SPL2023

12. COMMITTEE MATTERS

None presented.

13. QUESTION PERIOD

None registered.

14. ADJOURNMENT

There being no further business the meeting of the Development & Heritage Standing Committee (Administrative Matters) is adjourned at 6:37 o'clock p.m.

Carried.

Ward 10 - Councillor Jim Morrisson
(Chairperson)

Deputy City Clerk / Supervisor
of Council Services

Subject: 1148 Victoria Avenue, Kathleen Henderson House - Heritage Permit & Community Heritage Fund Request (Ward 3)

Reference:

Date to Council: July 4, 2023
Author: Kristina Tang, MCIP, RPP
Heritage Planner
ktang@citywindsor.ca
519-255-6543 X 6179
Planning & Building Services
Planning & Building Services
Report Date: June 14, 2023
Clerk's File #: MBA2023

To: Mayor and Members of City Council

Recommendation:

- I. THAT a Heritage Permit at 1148 Victoria Ave, Kathleen Henderson House, **BE GRANTED** for reconstruction of the front porch.
- II. THAT the City Planner or designate **BE DELEGATED** the authority to approve any further proposed changes associated with the restoration.
- III. THAT a total grant of an upset amount of \$5,000 from the Community Heritage Fund (Reserve Fund 157) for the cost of the reconstruction of the front porch, **BE GRANTED** to the Owner of the Kathleen Henderson House (Gail Hargreaves), at 1148 Victoria Ave, subject to:
 - a. Submission of drawings, conservation details, technical details and samples, to the satisfaction of the City Planner or designate prior to work start;
 - b. Determination by the City Planner that the work is completed to heritage conservation standards and the City Building Official for building code compliance;
 - c. Owner's submission of paid receipts for work completed;
 - d. That the Community Heritage Fund (Reserve Fund 157), grants approved shall lapse if the applicant has not completed the work and fulfilled the conditions within 2 years of the approval date.

Executive Summary: N/A

Background:

The property at 1148 Victoria Avenue is known as the Kathleen Henderson House, which was constructed in 1922 on the east side of Victoria, at the terminus of Grove Ave. The dwelling is a two storey detached brick house in Colonial/Georgian Revival style, and was designated by City of Windsor Council through By-law No. 340-2000 on October 30, 2000. The Reasons for Designation from the Designation By-law is included as Appendix 'A'.



Front view of the property.

There have been discussions with Gail Hargreaves, Owner of the property about conducting repairs to the front porch steps since last year. The Owner had concentrated on restoring the wooden side porch using original design and materials (specially milled tongue and groove Douglas fir floor boards), and restoring the double-hung wood windows (re-puttied/ reglazed, repaired and repainted wood windows and shutters) in the past few years, before now turning attention to repairing the front porch step.



Before and after photos of the wood side porch restoration



Owner conducting maintenance of painting on wood windows.

The Owner made a Heritage Permit application for the front porch step work (See Appendix 'B') on June 14, 2023, and a Community Heritage Fund Application for a total grant of \$5,000 and included a rationale for the request (See Appendix 'C'). Administration is recommending approval of this request.

Legal Provisions:

Part IV, 39 (1) of the *Ontario Heritage Act* (OHA) provides that “The council of a municipality may pass by-laws providing for the making of a grant or loan to the owner of a property designated under this Part for the purpose of paying for the whole or any part of the cost of alteration of such designated property on such terms and conditions as the council may prescribe.” The City’s Community Heritage Fund (Reserve Fund 157) exists to provide grants to heritage conservation works on designated heritage properties.

The Ontario Heritage Act (OHA) requires the owner of a heritage designated property to apply to Council to alter the property. The designation by-law includes heritage attributes (see Appendix 'A'). In accordance with the OHA, changes to a designated property that affect heritage attributes must be considered by City Council after consulting with its municipal Heritage Committee. Council has the option of granting consent with or without terms and conditions, or refusing the application within 90 days of notice of complete application. Council also has the option to further delegate the item to an employee or official of the municipality. The delegation of final details would be more expediently handled through staff review and approval.

Discussion:

Proposal:



1988 photo of the front porch



2022 photo of the front porch and visible center crack

The front porch consists of three coloured concrete steps and flanking brick walls topped by stone, as well as compatible thin-profiled iron railing. The porch shows evident cracking in the center of the front steps, which Building Department staff have suggested is a result of the lack of footings below the porch. The Owner has solicited to receive quotes from a long list of companies. Quotes received include work for footings to be installed under the porch, salvage and reinstallation of the side flanking masonry walls, and for reconstruction of the concrete steps to match as closely as possible in colour. Contractors have expressed that the now “pink” steps would have been closer to the colour of the red bricks, and that the specialty colour for the concrete is part of the reason for the higher cost for the work.

The railings are not part of the quotes. The existing iron railings are likely not original to the porch but are a compatible type. The Owner has expressed the desire to reuse the railings.

Building Department staff have indicated that a Building Permit application with drawings of the elevation and site plan would be necessary. The drawing submitted was a rough preliminary sketch. Further staff review of the drawings is required, and is a condition of the Heritage Approvals and part of the Building Permit Application requirements that will be coordinated between City Planning & Building staff.

Official Plan Policy:

The Windsor Official Plan includes (9.3.6.1.), “Council will manage heritage resources by: (e) providing support and encouragement to organizations and individuals who undertake the conservation of heritage resources by private means”.

The Plan includes protection (9.3.4.1). “Council will protect heritage resources by: (c) Requiring that, prior to approval of any alteration, partial demolition, removal or change in use of a designated heritage property, the applicant demonstrate that the proposal will not adversely impact the heritage significance of the property ...”

Risk Analysis:

The risk of taking no action for this property is the potential deterioration of a designated heritage property. No city funds will be expended until the project is determined by Planning Staff to be completed according to good heritage practices and Building Department determination of Building Code compliance. Conditions of this determination include provision of drawings, technical information through details within the quote, and request for provision of material samples or mock-up, to ensure that the conservation work is heritage appropriate, prior to disbursement of the funds.

Climate Change Risks

Climate Change Mitigation: N/A

Climate Change Adaptation: N/A

Financial Matters:

The Community Heritage Fund (CHF) guidelines includes the following:

“As a general principle, awards will be limited to a maximum of \$50,000 unless the DHSC (Development & Heritage Standing Committee) so recommends and Council approves.” The award from the Community Heritage Fund is generally given according to the following formula: Grant: 15 percent of the award in the form of a grant & Low Cost Loan: 85 percent of the award”.

In this particular case, the applicant is asking for around 35% grant approval with no loan component. As well, the Community Heritage Fund asks for a minimum of two cost estimates, based on specifications approved by the DHSC and the City Planner or

designate, to be obtained by the owner for all restoration work to be done. The estimates will then be reviewed to ensure that all work specified is covered. Usually, the lower bid is to be recommended for funding.

The Owners made significant efforts for several months to solicit bids and quotes from local companies, and contacted the following and more for the proposed porch repairs:

- North Construction
- trswaterproofing
- Yahweyren
- JoeJax Foundation Specialists
- Ingratta Cement
- Winmar Windsor
- Alpine Construction

Ultimately, only Bricetin and JK Construction returned quotes. JK Construction contractor cost is \$14,249 with footings added (\$10,610 plus the \$2,000 plus HST). Bricetin's contractor cost is \$14,125. Both contractors indicated potential availability to begin work end of August or September and would provide drawings. The Owner's preferred contractor is Bricetin.

The Owner would still need to pay additional to cover Building Permit costs, as well cost for a compatible railing if reuse of the existing does not work out.

The Owner is requesting for around 35% of cost of the repair work to be covered by the CHF due to the high cost of conserving special heritage features, especially in today's climate of increasingly expensive trades work. Unfortunately, inflation has further exacerbated costs as reflected in the collection of quotes by the Owner. Council has approved a higher CHF grant approval percentage in recent years to other heritage property owners (of around 30%-35% for work ranging from wood windows to clay tile and other specialty roof related repairs). The cost of the repair work is encouraged to be conducted soon and is a significant cost for a private property owner. The Owner has continually stewarded this designated heritage property, and has most recently also expended significant efforts and finances in restoring the wood side porch and wood windows in recent years. Therefore, administration recommends approval of the request by the Owners for CHF of around 35% for the work for the restoration of the front porch.

The heritage financial incentive through the Community Heritage Fund (CHF) would provide support to the continued conservation of the heritage features of this designated building. As of May 31, 2023, the Community Heritage Fund (Reserve Fund 157) has an uncommitted balance of \$125,412 available (including the safeguard of having the minimum balance of \$50,000 in the Committed funds). Therefore, there is sufficient funds in Fund 157 to cover the cost of the grant project. Administration recommends that the amounts of around 35% to an upset amount of \$5,000 be provided for the front porch repair.

Consultations:

City staff have been consulting with the Owner since last year. Building Department staff were consulted. Josie Gualtieri, Financial Planning Administrator, assisted with confirmation of fund balance.

Conclusion:

A total grant amount of up to \$5,000 from the Community Heritage Fund (Reserve Fund 157) for the cost of the front porch restoration at 1148 Victoria Avenue is being recommended for approval, subject to conditions. Further heritage alteration approvals necessitated for this scope of work are recommended to be delegated to the City Planner or designate to direct further conservation details.

Planning Act Matters: N/A

Approvals:

Name	Title
Kristina Tang	Heritage Planner
Josie Gualtieri	Financial Planning Administrator
Neil Robertson for Thom Hunt	City Planner / Executive Director Planning & Building
Wira Vendrasco	Deputy City Solicitor
Janice Guthrie	Chief Financial Officer
Jelena Payne	Commissioner of Economic Development & Innovation
Joe Mancina	Chief Administrative Officer

Notifications:

Name	Address	Email
Gail Hargreaves		

Appendices:

- 1 Appendix A- Reasons for Designation
- 2 Appendix B- Heritage Permit Application
- 3 Appendix C- Community Heritage Fund Application

1148 Victoria Ave.-Kathleen Henderson House. By-law 340-2000 passed by council on October 30, 2000.

REASONS FOR DESIGNATION:

HISTORICAL

- Built in 1922 for Mrs. Agnes Henderson, widow of Ernest G. Henderson, President and General Manager of the Canadian Salt Co . Ltd. and prominent Windsor citizen.
- Home to daughter Kathleen Henderson for over fifty years.
- Fine example of the quality housing that developed on Victoria Avenue during the early decades of the 20th century.

ARCHITECTURAL

- Two-storey Colonial/Georgian Revival Style home
- Red brick and stucco exterior.
- Central entrance with columned semi-circle bracketed hood and 5 paned side lights.
- Double-hung 6/1 windows with shutters.
- Wooden side porch with bracketed overhand and balustrade.



HERITAGE PERMIT APPLICATION

Revised 12/2021

CORPORATION OF THE CITY OF WINDSOR

Planning Dept., Suite 320-350 City Hall Sq W, Windsor ON N9A 6S1
519-255-6543 | 519-255-6544 (fax) | planningdept@citywindsor.ca

1. Applicant, Agent and Registered Owner Information

Provide in full the name of the applicant, registered owner and agent, the name of the contact person, and address, postal code, phone number, fax number and email address. If the applicant or registered owner is a numbered company, provide the name of the principals of the company. If there is more than one applicant or registered owner, copy this page, complete in full and submit with this application.

APPLICANT

Contact Name(s) Gail D Hargreaves
Company or Organization _____
Mailing Address 1148 Victoria Ave.
Windsor, Ontario
Postal Code N9A 4N8
Email _____ Phone(s) _____

REGISTERED OWNER IF NOT APPLICANT

Contact Name(s) _____
Company or Organization _____
Mailing Address _____
Postal Code _____
Email _____ Phone(s) _____

AGENT AUTHORIZED BY REGISTERED OWNER TO FILE THE APPLICATION

Contact Name(s) _____
Company or Organization _____
Mailing Address _____
Postal Code _____
Email _____ Phone(s) _____

Who is the primary contact?

- Applicant Registered Owner Agent

2. SUBJECT PROPERTY

Municipal Address: 1148 Victoria Ave., Windsor, N9A 4N8

Legal Description (if known): Colonial Georgian Revival built 1922

Building/Structure Type:

- Residential
 Commercial
 Industrial
 Institutional

Heritage Designation:

- Part IV (Individual)
 Part V (Heritage Conservation District)

By-law #: 340-2000 District: City of Windsor

Is the property subject to a Heritage Easement or Agreement?

- Yes
 No

3. TYPE OF APPLICATION

Check all that apply:

- Demolition/Removal of heritage attributes
 Addition
 Erection
 Alteration*
 Demolition/Removal of building or structure
 Signage
 Lighting

*The Ontario Heritage Act's definition of "alter" means to change in any manner and includes to restore, renovate, repair or disturb.

4. HERITAGE DESCRIPTION OF BUILDING

Describe the existing design or appearance of buildings, structures, and heritage attributes where work is requested. Include site layout, history, architectural description, number of storeys, style, features, etc..

Two storey centre hall plan Georgian home.
First storey is Ohio red brick.
Second storey is a stone stucco called gunite
All windows are 6 over 1 double hung
There are two brick chimneys - one on the rear (east)
side and one on the north side.
It has a hip roof and a side porch (south side) off
the living room with original french doors.
There a high light windows on either side of the front
door.
Work is requested to repair the cracked front steps that
are currently original.

5. PROPOSED WORK

Provide a detailed written description of work to be done, including any conservation methods you plan to use. Provide details, drawings, and written specifications such as building materials, measurements, window sizes and configurations, decorative details, etc.. Attach site plans, elevations, product spec sheets, etc. to illustrate, if necessary.

The proposed work is to dismantle two brick retaining walls (saving and cleaning the bricks + top lentil of poured cement) Remove the original steps + landing. Properly replace any footings to allow new steps + landing, putting back the brick retaining walls as they were.

6. HERITAGE PERMIT RATIONALE

Explain the reasons for undertaking the proposed work and why it is necessary.

The work is necessary as the 100yr old original front steps are cracked from top to bottom (see photo) Water is seeping in underneath washing away the dirt and causing them to sink into the ground.

Describe the potential impacts to the heritage attributes of the property.

hopefully when these stairs are replaced the new ones will look exactly like the original steps did 100 years ago.

7. CHECKLIST OF MATERIALS SUBMITTED Check all that apply:

Required:

- Photographs (showing the current condition and context of existing buildings, structures, and heritage attributes that are affected by the application)
- Site plan/ Sketch (showing buildings on the property and location of proposed work) coming.
- Drawings of proposed work (e.g. existing and proposed elevations, floor plans, roof plans, etc., as determined by Heritage Planning staff)
- Specifications of proposed work (e.g. construction specification details)

Potentially required (to be determined by Heritage Planning staff):

- Registered survey
- Material samples, brochures, product data sheets etc.
- Cultural Heritage Evaluation Report
- Heritage Impact Assessment (HIA)
- Heritage Conservation Plan
- Building Condition Assessment

8. NOTES FOR DECLARATION

The applicant hereby declares that the statements made herein and information provided are, to the best of their belief and knowledge, a true and complete representation of the purpose and intent of this application.

The applicant agrees that the proposed work shall be done in accordance with this application, including attachments, and understands that the issuance of the Heritage Alteration Permit under the Ontario Heritage Act shall not be a waiver of any of the provisions of any By-Law of the Corporation of the City of Windsor, or the requirements of the Building Code Act, RSO 1980, c51.

The applicant acknowledges that in the event a permit is issued, any departure from the conditions imposed by the Council of the Corporation of the City of Windsor, or plans and specifications approved is prohibited and could result in the permit being revoked. The applicant further agrees that if the Heritage Alteration Permit is revoked for any cause of irregularity, in the relation to non-conformance with the said agreements, By-Laws, acts or regulations that, in consideration of the issuance of the permit, all claims against the City for any resultant loss or damage are hereby expressly waived.

APPLICANT Signature(s) _____

Sail D. Hargreaves

Date _____
Date _____

June 12, 2023

SCHEDULE A

A. Authorization of Registered Owner for Agent to Make the Application

If the applicant is not the registered owner of the land that is the subject of this application, the written authorization of the registered owner that the applicant is authorized to make the application must be included with this application form or the authorization below must be completed.

I, Gail D. Hargreaves, am the registered owner of the land that is
name of registered owner
subject of this application for a Heritage Alteration Permit and I authorize
_____ to make this application on my behalf.

name of agent

Gail D. Hargreaves.
Signature of Registered Owner

June 12, 2023
Date

If Corporation – I have authority to bind the corporation.

B. Consent to Enter Upon the Subject Lands and Premises

I, Gail D. Hargreaves, hereby authorize the members of the Windsor Heritage Committee and City Council and staff of the Corporation of the City of Windsor to enter upon the subject lands and premises described in Section 3 of the application form for the purpose of evaluating the merits of this application and subsequently to conduct any inspections on the subject lands that may be required as condition of approval. This is their authority for doing so.

Gail D. Hargreaves
Signature of Registered Owner

June 12, 2023
Date

If Corporation – I have authority to bind the corporation.

C. Acknowledgement of Applicant

I understand that receipt of this application by the City of Windsor Planning Department does not guarantee it to be a complete application. Further review of the application will occur and I may be contacted to provide additional information and/or resolve any discrepancies or issues with the application as submitted.

I further understand that pursuant to the provisions of the Ontario Heritage Act and the Municipal Freedom of Information and Protection of Privacy Act, this application and all material and information provided with this application are made available to the public.

Gail D. Hargreaves.
Signature of Applicant

June 12, 2023
Date

DO NOT COMPLETE BELOW – STAFF USE ONLY

Approval Record

Date Received by Heritage Planner: _____

Building Permit Application Date, if needed: _____

Application Approval (City Council):

Development & Heritage Standing Committee: _____

City Council: _____

Application Approval (City Planner):

Heritage Planner: _____

Staff Decision Appealed to City Council: _____

If so, Date to City Council: _____

Council Decision Appealed: _____

Additional Notes / Conditions:

DECISION

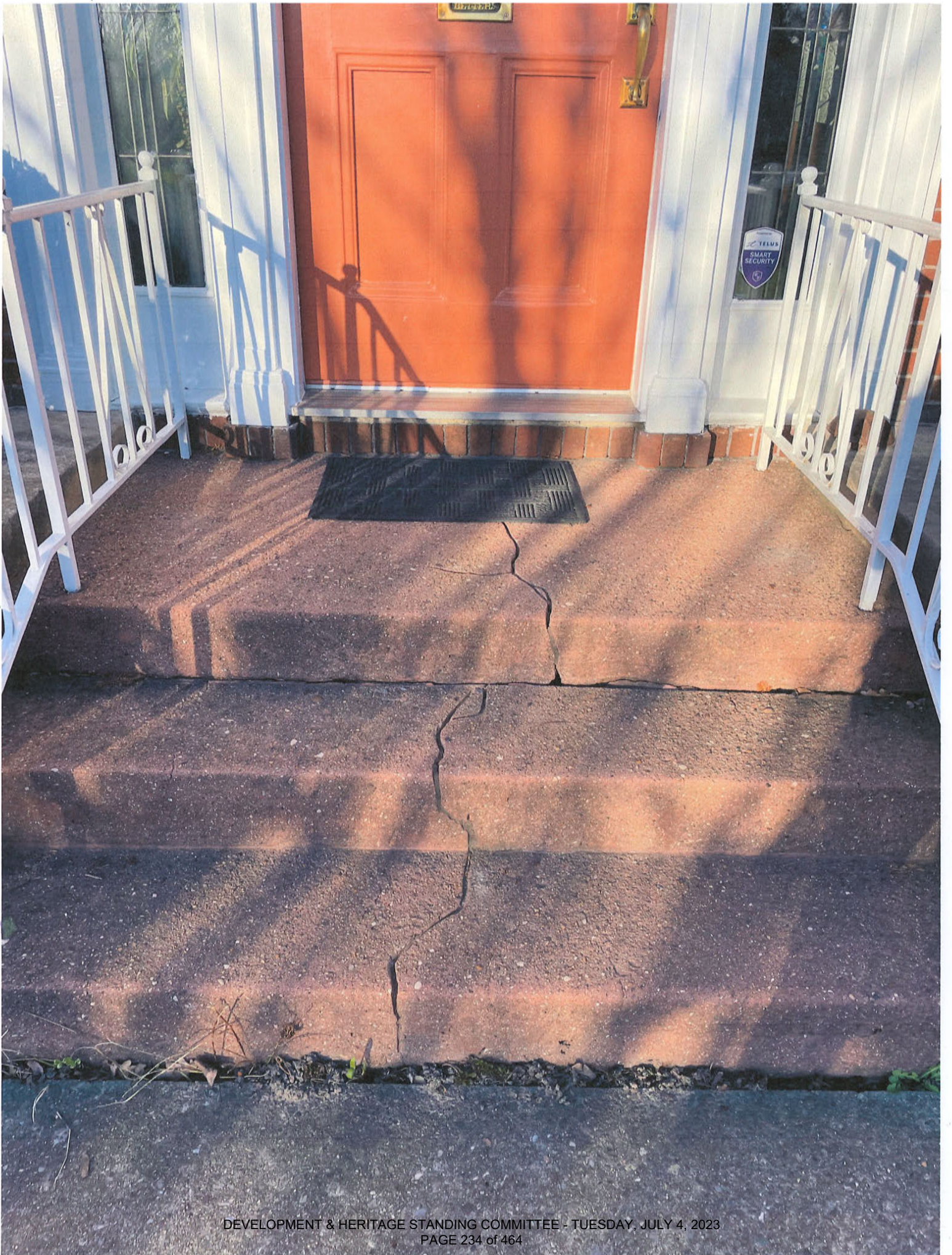
Heritage Permit No.: _____ Date: _____

Council Motion or City Planner's Signature: _____

Please contact Heritage Planning to request inspections at ktang@citywindsor.ca

CONTACT INFORMATION

Planning Department - Planning Policy
Corporation of the City of Windsor
Suite 320 - 350 City Hall Square West
Windsor ON N9A 6S1
planningdept@citywindsor.ca
519-255-6543 x 6179
519-255-6544 (fax)
http://www.citywindsor.ca



CITY OF WINDSOR
COMMUNITY HERITAGE FUND

APPLICATION FOR LOAN AND/OR GRANT
(To be completed in full)

APPLICANT/AGENT/OWNER INFORMATION

1. APPLICANT(S): Gail D. Hargreaves
Address, City: 1148 Victoria Ave. Postal Code: N9A 4N8
Email: [REDACTED] Telephone: [REDACTED]
2. AGENT/ARCHITECT/ENGINEER/SOLICITOR: _____
Address, City: _____ Postal Code: _____
Email: _____ Telephone: _____
3. REGISTERED OWNER(S): same as above
Address, City: _____ Postal Code: _____
Email: _____ Telephone: _____

PROPERTY FOR WHICH HERITAGE FUND ASSISTANCE IS REQUESTED

4. HERITAGE NAME OF PROPERTY Kathleen Henderson House
5. MUNICIPAL ADDRESS 1148 Victoria Ave.
6. LEGAL DESCRIPTION (Lot and Plan) Plan 581 Lot 172 PT LOT 171 LOT 173 P.T.
7. ASSESSMENT ROLL NUMBER(S) 373 904 037 002 6000000
8. EXISTING USE Private Residence
9. THIS PROPERTY IS DESIGNATED UNDER THE ONTARIO HERITAGE ACT
YES By-law No. 340-2000 Date October 30, 2000

REQUESTED HERITAGE FUND ASSISTANCE

10. AMOUNT AND TYPE OF FINANCIAL ASSISTANCE REQUESTED:
Loan \$ 0
Grant \$ 5,000 TOTAL \$ 5,000

NOTE: Details for loan/grant are set out in the attached pamphlet "Windsor's Community Heritage Fund".

11. If a loan is requested, please indicate your term of repayment: _____ years.
12. Are there any outstanding mortgages or liens against this property?
NO
YES Amount: _____ Institution: _____
13. If your application for a loan, grant or loan/grant combination is in an amount not to exceed \$15,000, you may be required to obtain a property appraisal from a real estate agent or certified appraiser. If your application is in an amount in excess of \$15,000, you may be required to obtain an appraisal from a certified appraiser.
14. Have you previously received assistance from the City for the property named above?
NO

YES Amount: _____ Date: _____

Source of funds: _____

REASONS FOR REQUESTING HERITAGE FUND ASSISTANCE

A. Rehabilitation of Owner's designated property:

15. Describe fully here (or on attached sheets) the proposed work to be undertaken:

Renovation of the original front landing and step area which is completely cracked down the centre.

16. The applicant is responsible for providing at least two written estimates from qualified contractors and/or qualified design consultants for the proposed restoration work. The estimates should contain sufficient detail to permit a review of individual components of the proposed work. Attach estimates to this form when filing. (Some specifications are available from the Heritage Planner – see contact information at the bottom of this page.)

It will involve removing the old steps including two brick retaining walls. Restoring + cleaning the bricks then putting everything back as it was.

17. Describe any new uses of the property, if different from the existing use.

0

18. The application shall include recent, dated photographs of the property, to clearly illustrate the areas of the property that are the subject of the proposed work. Any available architectural drawings should be included as well.

19. Early photographs or drawings showing the property's original appearance should be submitted, if available, to assist in the review of the application.

B. Purchase of designated property:

20. Indicate the full price of the property you wish to purchase: \$ _____ and include a copy of the "offer to purchase."

21. Indicate your other sources of funding, the amount you will receive, and any subsequent liens/mortgages.

C. Architectural/engineering study of Owner's designated property:

22. Indicate the full price of the architectural/engineering study by a restoration specialist: \$ _____ and include a copy of the estimate or invoice from the study.

SIGNATURES

APPLICANT OR AGENT _____ Date _____

REGISTERED OWNER(S) Gail D. Hargreaves Date June 14, 2023

CHECKED/RECEIVED BY _____ Date _____
HERITAGE PLANNER

This application should be completed and filed with the:
Planning Department
Suite 320 - 350 City Hall Square West
Windsor ON, N9A 6S1

For assistance and/or information on filing, please contact the Planning Department:
Telephone 519-255-6543 x 6179 Fax 519-255-6544

NOTICE WITH RESPECT TO COLLECTION OF PERSONAL INFORMATION:

I/We also acknowledge that the information requested on this form is required in order to process the application to the Development & Heritage Standing Committee. Please be advised that the information in this application form may be released to the public in an electronic form, i.e. web site and/or paper format, i.e. agenda or minutes.

Dated: June 14, 2023

Signed: Gail D. Hargreaves
Signature of Applicant

June 13, 2023

To whom it may concern;

My name is Gail Hargreaves and I have owned 1148 Victoria Ave. since February of 1988. The home was originally purchased by myself and my husband. In 1996 it became fully mine, as I was left to raise three young boys here, now all adults. In 2000 I was granted heritage status and it became the Kathleen Henderson home. (it actually has it's own Facebook page detailing all of my renovations)

It has been my life long goal to maintain this property in the era it was built. With a 30 year career in Family Studies at the High School level I was able to teach grade 11 students Housing and Interior Design. My passion has always been older homes and their decor. The Kathleen Henderson House has been on the Doors Open Windsor tour in the past and I have done walking tours of Victoria Ave. with my students.

Since retiring in 2006 I have spent many hours with a heat gun removing 100 years of paint from every window on the first floor and the basement. Almost every room has had plaster work redone instead of drywall and all lighting fixtures remain original and re-wired, including the maid's bell system. When an addition was added to the rear of the home a tool was made to cut baseboard trim in oak to match the trim in the rest of the home.

Being the sole owner of this home and trying to keep everything as original as possible has come at a great expense. I am actually quite shocked at the quotes to repair my front steps and landing, but realize to clean and reuse every brick will be time consuming.

At this point I am asking for as much help as I can get towards this latest project, something I am unable to do myself.

As a senior it is my hope to be able to stay in this home for many more years. Thank you for your consideration on this project.

Gail Hargreaves



Gail D. Hargreaves.

From: Bricetin <bricetinconcreterestoration@gmail.com>

Subject: quotation

Date: March 14, 2023 at 12:25:13 PM EDT

To: "Bluenotes ... Hargreaves" [REDACTED]



13300 Tecumseh Rd., E.

Unit #363

Tecumseh, Ontario

N8N 4R8

PH: (519) 979-5454

TOMORROW'S TECHNOLOGY... TODAY

TO: Gail Hargreaves

FROM: A. J. / Bricetin

March 14, 2023

RE: 1148 Victoria Ave. Windsor, On (Heritage House)

PHONE NUMBER: 519-981-4758

Upon inspection of the above address we are pleased to offer our quote for the following work:

Front Porch Steps

Scope of Work:

- Remove stone caps
- Remove bricks walls on right and left sides
 - Clean and salvage bricks and stone
 - Remove existing concrete steps
 - Excavate and truck away dirt
 - Pour footings
 - Install rebar cage
- Form and pour new steps using coloured concrete, match as close as possible
 - Rebuild brick walls to original size
 - Reinstall stone caps
 - Powerwash
 - Remove all waste
 - Supply all labour and materials

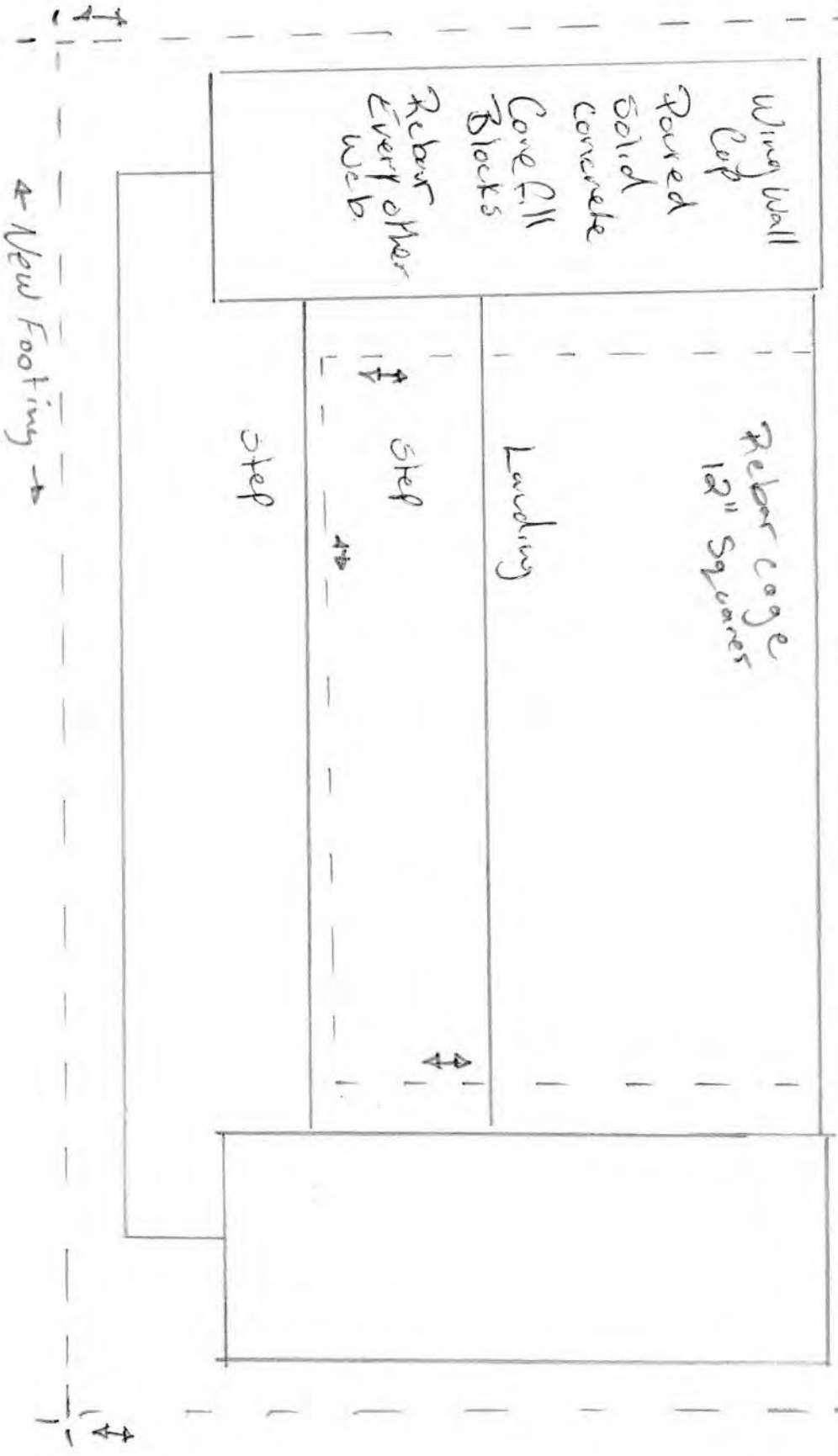
OUR PRICE \$12,500.00 +HST

Terms: Payment due upon completion. We accept Visa & Mastercard.

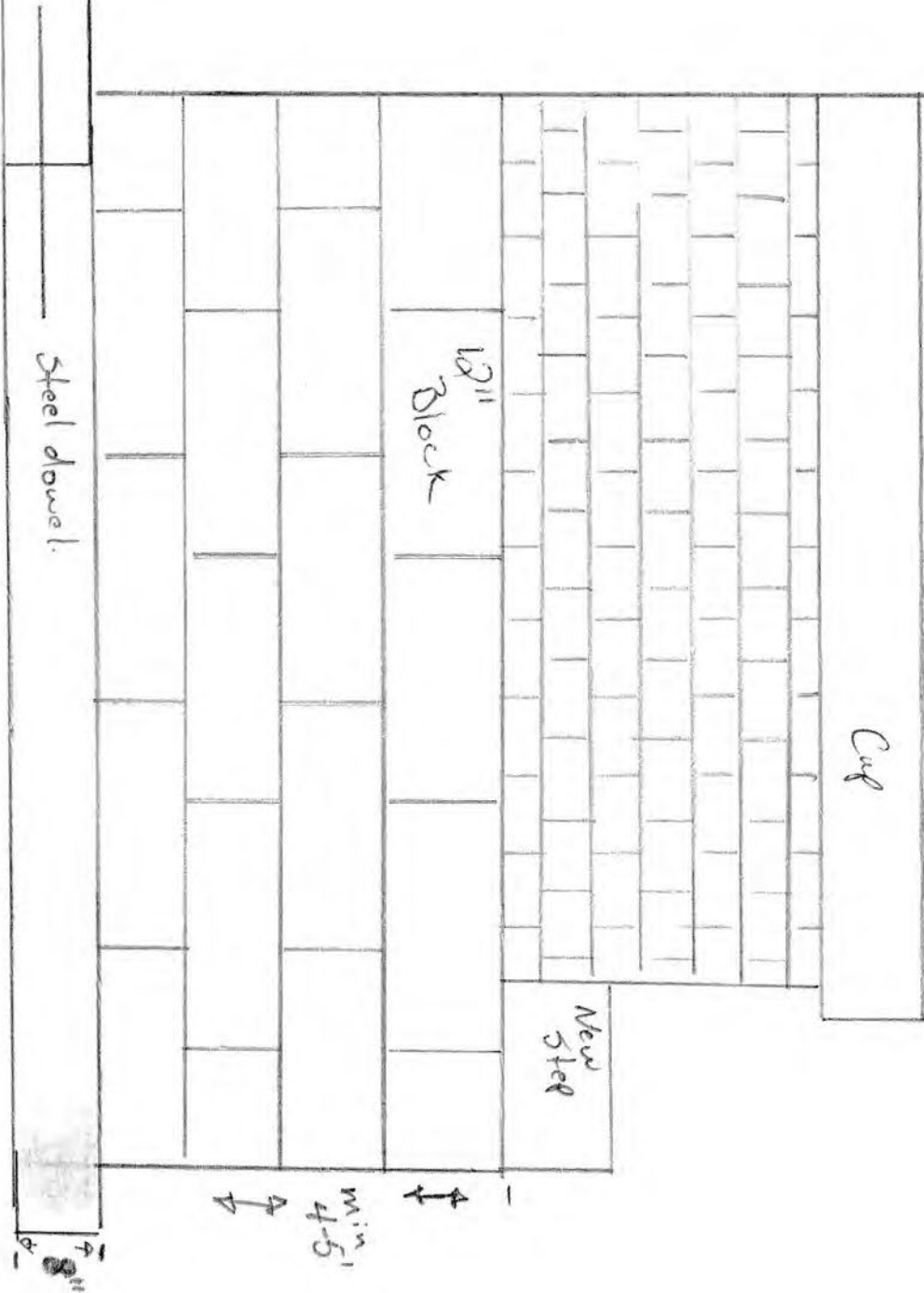
Note: Customer responsible for all permits and railings

Note: All excavation work needs a minimum 6 months for dirt to settle. Not responsible for any private gas, water or hydro lines.

Foundation Repairs - Brick, Block & Concrete Repairs
Leaky Basements - Waterproofing System
Epoxy/Polyurethane Injections
Brick Repointing - Decorative Cement Coatings & More...
www.bricetin.ca



House





ST# 766194518 RT0001

WS B reg stered

nfo@jkconstruct onrenovat on com

519-258-5098

2429 Douga Ave, W ndsor, On, N8X 1T3

fu y nsured for any project

BN 766194518

Estimate

JK Construction and Renovation Inc.

For

Ga unt ey

[REDACTED]

1148 V ctor a Ave, South Centra , W ndsor, N9A 4N8,
ON, Canada

[REDACTED]

Est mate No

1567

Date

05/01/2023

Description	Quantity	Rate	Amount
D SCLA MERS	1	CA\$10,610 00	CA\$10,610 00
<p>- There may be some landscap ng damage wh e perform ng the tasks to proper y and safe y comp ete the job We w do our best to m t th s damage but we cannot guarantee t due to the nature of the work requ red for th s project</p> <p>- There may be some grass damage wh e perform ng the tasks to proper y and safe y comp ete the job We w do our best to m t th s damage but we cannot guarantee t due to the nature of the work requ red for th s project</p> <p>- f any br cks or stone are broken on the porch wa s dur ng the remova process we w rep ace them at an add t ona cost We don't th nk any shou d break but we cannot guarantee t as they are very o d n the event we have to rep ace any br cks or stone we w try to match them as c ose y as poss b e</p> <p>We w supp y a abour and mater a s for the fo ow ng un ess otherw se stated</p> <p>- Remove and d spos of the ex st ng front porch wh te ra ngs We d d not quote to add new ra ngs as per customer request</p> <p>- Temporar y remove the ex st ng front porch s de br ck wa w th stone tops We w do our best to sa vage these br cks and stone n order to re- nsta them after</p> <p>- Remove and d spos of the ex st ng front porch p nk exposed aggregate concrete</p> <p>- nsta new exposed aggregate concrete front porch the same s ze and sty e as the ex st ng porch We w try to match the co our as c ose y as poss b e but t may end up more brown than the ex st ng co our as that shade of p nk/red s a most mposs b e to find</p> <p>- nsta mortar to fi a coup e ho es above the front door entry</p> <p>- nsta some exter or cau k ng to sea 2 cracks above the front door entry</p> <p>- Re- nsta the removed br cks and stone for both porch s de wa s</p> <p>- C ean up of the job area upon comp et on of the job</p> <p>- B n and d sposa fees are nc uded n th s est mate</p> <p>** P ease note that we d d not nc ude the cost to nsta foot ngs/sono tubes beneath the front porch as we don't know f they are needed unt the porch has been removed n the event we have to nsta foot ng/sono tubes the cost w ncrease by \$2000+hst A so, we w try to remove the front porch concrete w thout hav ng to remove the br ck/stone s dewa s n order to reduce the cost f we can avo d hav ng to remove the br ck/stone s dewa s, the cost w decrease accord ng y **</p>			
		Subtota	CA\$10,610 00
		ST 13% (CA\$10,610 00)	CA\$1,379 30
		Tota	CA\$11,989 30
		Total	CA\$11,989.30

Comments

P ease note that th s est mate s for the mater a s and abour for the job descr bed above Pr ce adjustments can be made f the job spec ficat ons and/or mater a s are changed due to unforeseen c rcumstances or c ent cho ce 50% of tota payment s requ red pr or to start date, an add t ona 25% w be requ red at the ha f way po nt The fina 25% w be due when the job has been comp eted

Late payments 3 5% per month (42% per annum) p us co ect on costs on overdue accounts Net 14

Date

Print Name

Signature

JK Construction and Renovation
Inc

Client's signature

Subject: 1982 Norman Road, St Jules School – Heritage Evaluation Report (Ward 8)

Reference:

Date to Council: July 4, 2023
Author: Kristina Tang, MCIP, RPP
Heritage Planner
Email: ktang@citywindsor.ca
Phone: 519-255-6543 X 6179

Edwin Chiu
former Planning Assistant (Co-op)
Planning & Building Services
Report Date: May 23, 2023
Clerk's File #: MBA2023

To: Mayor and Members of City Council

Recommendation:

- I. THAT Council **BE INFORMED** that the former St Jules School, at 1982 Norman Road, meets the O. Reg. 9/06 Criteria for Determining Cultural Heritage Value or Interest for designation in accordance with Part IV of the Ontario Heritage Act; and,
- II. THAT the former St Jules School, at 1982 Norman Road **REMAIN** on the Windsor Municipal Heritage Register.

OR ALTERNATIVELY

- I. THAT the City Clerk **BE AUTHORIZED** to publish a Notice of Intention to Designate the St Jules School, at 1982 Norman Road, in accordance with Part IV of the Ontario Heritage Act for the reasons attached in Appendix 'A'; and,
- II. THAT City Legal staff **PREPARE** the By-law for Council to designate the property.

Executive Summary:

N/A

Background:

The former St Jules School, located at 1982 Norman Road, was listed on the Windsor Municipal Heritage Register on June 4, 2013.

The Windsor Essex Catholic School Board sold the property to the current Owner Olivia Construction Homes Inc. in 2019. The Owner contacted the Planning and Engineering Departments and a meeting was held on November 19, 2019 to discuss the redevelopment of the site with a plan to demolish the former school building to create a typical subdivision. The Owner was notified that the existing sewers were operating at or above capacity. Besides the storm water management requirements, the Owner was also informed that the property is on the Windsor Municipal Heritage Register and would require a Heritage Impact Study as part of the redevelopment considerations of the site.

The Owner has since been contemplating different redevelopment options that incorporate the retention of the heritage structure. No formal Planning Act application has been submitted to-date though the Owner has discussed plans periodically with City staff.

The former school remains vacant at this time and subject to on-going enforcement action by the Building Department for non-compliance with the Property Standards By-law. Additionally, concerns related to trespassing/entry to the vacant building has necessitated numerous responses from various City resources (Building Bylaw enforcement Officers and Windsor Police Services); the issues are on-going. Surrounding property owners have expressed their concerns related to these issues with the City and their local Ward 8 Councillor. City staff have therefore initiated the heritage evaluation of this property to provide Council with an opportunity to consider the heritage merits of the property.

The Owner, Olivia Construction Homes Inc. owns other properties in the City, including 1920 Grove Avenue vacant lands (former St John School demolished by 2019). That former school site has not been redeveloped to-date.



Google Streetview photo of the property in May 2014



Google Streetview photo of the property in Jan 2021

Discussion:

Legal Provisions:

Part IV, Section 29(1)(a) of the *Ontario Heritage Act* provides the authority to the Council of a municipality to designate a property that is of cultural heritage value or interest, where the property meets the prescribed criteria. Ontario Regulation 9/06 provides criteria for designation under the *Ontario Heritage Act*. A property may be designated under Section 29 of the Act if it meets two or more of the following criteria for determining whether it is of cultural heritage value or interest:

1. The property has design value or physical value because it is a rare, unique, representative or early example of a style, type, expression, material or construction method.
2. The property has design value or physical value because it displays a high degree of craftsmanship or artistic merit.
3. The property has design value or physical value because it demonstrates a high degree of technical or scientific achievement.
4. The property has historical value or associative value because it has direct associations with a theme, event, belief, person, activity, organization or institution that is significant to a community.
5. The property has historical value or associative value because it yields, or has the potential to yield, information that contributes to an understanding of a community or culture.
6. The property has historical value or associative value because it demonstrates or reflects the work or ideas of an architect, artist, builder, designer or theorist who is significant to a community.
7. The property has contextual value because it is important in defining, maintaining or supporting the character of an area.
8. The property has contextual value because it is physically, functionally, visually or historically linked to its surroundings.
9. The property has contextual value because it is a landmark

Designation requires “a statement explaining the cultural heritage value or interest of the property and a description of the heritage attributes of the property”. Also, there are provisions for a municipal heritage committee to be consulted about a designation before the council makes a decision on it.

Should Council decide to designate a property, the first step is to initiate a Notice of Intention to Designate, which provides direction for the City Clerk to give notice to the owner, the Ontario Heritage Trust, and through the newspaper. After thirty days, if there is no objection, the Council may pass a by-law to designate the property in accordance with the *Ontario Heritage Act*.

The St Jules School at 1982 Norman Road meets more than two of the criteria for designation listed above.

Heritage Evaluation according to Ontario Regulation 9/06

The heritage evaluation of the property can be considered in the three broad categories of design/physical value, historical/associative value, and contextual value.

Design/Physical value:

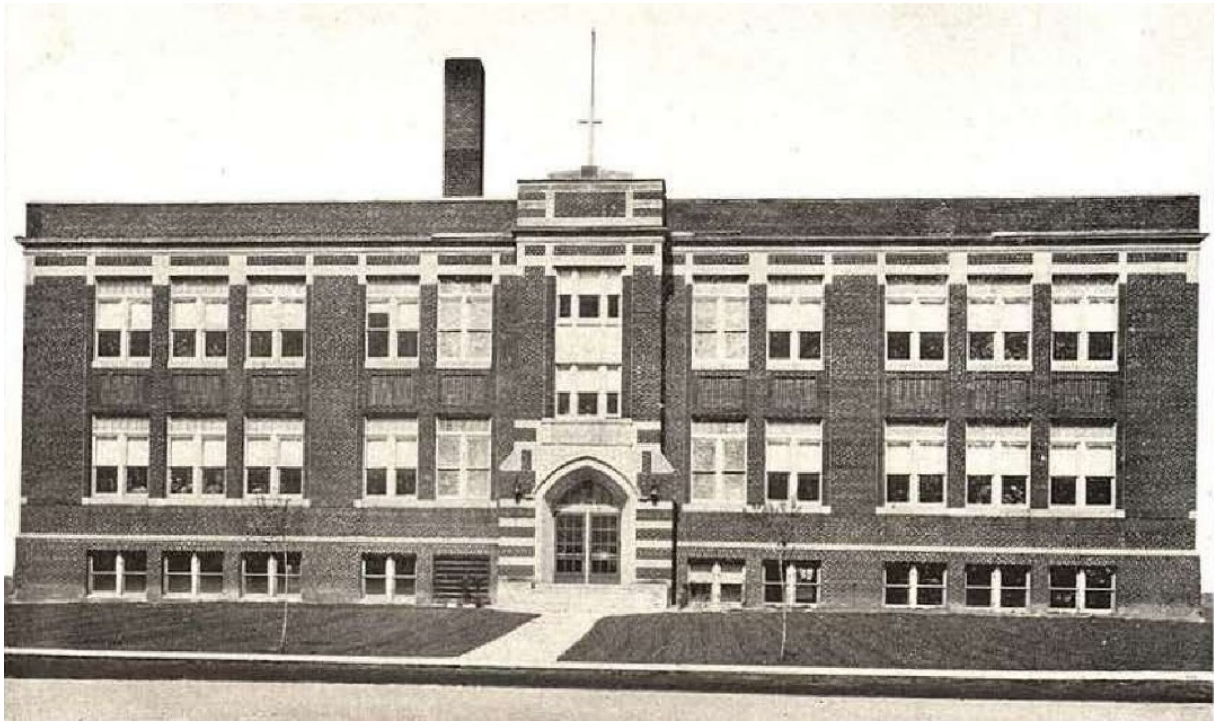
The former St Jules School does not fit exclusively with any distinctive architectural style but exhibits some elements of Edwardian architecture and Classical Revival Styles. Simple, classical and balanced in design ¹, some of these Edwardian design elements include the flat roof, articulated cornices with pronounced horizontal stone banding spanning across the entire building. Whereas some of the Classical Revival design elements are the formal symmetrical design (with center door), front and side entry portico, and rectangular double hung windows. ²

The red-brick, two-storey plus basement school constructed in 1924-1925, was designed by well-known and respected prolific partnership of Pennington & Boyde. ³ and constructed between 1924-1925. The reinforced concrete construction is clad with red-brick and stone accents. The front façade of building has a symmetrical design with pronounced horizontal stone banding at parapet and stone cornice, alternating between stone and brick. A variety of brickwork is used throughout the building, including: American bond brick on the main face of the wall; soldier course below the cornice and along the stone sills of the first floor windows; slightly recessed header brick pattern between the first and second floor windows. There are no pilasters on the building but the design of the stone accents and brick orientation create a visual illusion of some type of shallow pilaster between window bays.

¹ Heritage Resource Center. *Ontario Architectural Style Guide*. Retrieved from <https://www.therealtydeal.com/wp-content/uploads/2018/06/Heritage-Resource-Centre-Achitectural-Styles-Guide.pdf>

² Pennsylvania Historical & Museum Commission. “Classical Revival Style 1895-1950”. Pennsylvania Architectural Field Guide. Retrieved from <http://www.phmc.state.pa.us/portal/communities/architecture/styles/classical-revival.html>

³ Hill, Robert G. “Pennington, James Carlisle”. Biographical Dictionary of Architects in Canada 1800-1950, retrieved from <http://dictionaryofarchitectsincanada.org/node/1385>



Historical photo of the former St Jules Separate School ⁴

Prominent in the appearance of the building is the protruding centered front portico facing Norman Road. Originally topped with a stone cross pediment, the portico is accented with alternating stone and brick banding at top and bottom. The portico had rectangular stone tracery with decorative stone crest/emblem (lost feature), framed by header and stretcher bricks. At the base was a recessed arched entrance with a multipaned glass door with transom. The portico entrance was anchored by concentric rectangular steps, flanked with rectangular pillars.

At each side of the front portico are five window bays (in double and triple sets). The first and second floor windows were double sets of hung windows with transom, separated between floors by slightly recessed header brick pattern. The second floor had separated stone sills, while the first floor windows had adjoined sills between the double and triple set windows. Stone banding above basement windows level visually divided the above ground floors and basement level.

The other sides of the building have similar design, with simplified protruding portico on the north (assumed) and south side entrances (with stone cross design on the parapet). A large square brick chimney is located off center slightly to the north of the building. The building enjoys a generous landscape setback with unhindered views of the building from Norman Street.

⁴ City of East Windsor. "City of East Windsor 1929" (1929). SWODA: Windsor & Region Publications. 71. Retrieved from <https://scholar.uwindsor.ca/swoda-windsor-region/71>

A two storey six classroom addition was constructed in 1960 to the southeast portion of the original building. The south portico was retained with the new construction adjoining behind the portico, using matching red brick wall and mid-century glazing style. Later, the school property was expanded again in 1964 for a gymnasium, this time to the north of the property, disrupting the north side portico.



Google Streetview of property from the southwest corner of the property in May 2014.



Google Streetview photo of the property from the northwest corner in Jan 2021.

Various alterations such as changes to the glazing and parapet have taken place on the property over the past near hundred years. Appropriate glazing could be reintroduced in the future to return the property closer to its original appearance.

Historical/Associative Value

The St. Jules Separate School building was initially constructed on farmland originally owned by Jules Janisse.⁵ The land was part of the village of Ford City at that time (later in 1929 incorporated as the city of East Windsor, then amalgamated into City of Windsor in 1935). Ford City spanned from north at the River to south at Tecumseh Road, and east from Princess Avenue to west along the Chesapeake & Ohio Railway.⁶

The property has direct association to Windsor separate schools. The Windsor's Separate School System was founded in 1901 by Senator Charles E. Casgrain, Michael A. Brian, Gasper Pacaud, Thomas Kinsella, Joseph Neil Reaume, Peter J. Sweeney, J.J. Guitard and Jeremiah (Jerry) G. Gangnier to provide an education setting reflecting

⁵ "Homes Were Not Many 50 Years Back." *The Border Cities' Star*, 31 May 1929, pp. 55 ProQuest

⁶ City of Windsor, Planning Department. "History of Windsor". City of Windsor, retrieved from <https://www.citywindsor.ca/residents/historyofwindsor/pages/default.aspx>

Roman Catholic beliefs.⁷ The first Separate School was built in 1894 called Notre Dame School where it accommodated all the pupils until 1915 when the increase in population made it necessary to expand classes in the Parish-Hall (later Holy Rosary) which accommodated 50 students. Overtime, from September of 1916 it was expanded to a twelve-room structure by the Spring of 1922. Classes became more overcrowded with increased demand for pupils to be taught by the Sisters of St. Joseph. To provide relief, the Trustees of Sandwich East Separate School Board tasked Sisters of St. Joseph to run a two-room school near Pillette and Tecumseh Road in the Summer of 1922. To keep up with demands, the Separate School Board built a six room School named after Jules Janisse, the previous owner of the land.⁴



The building officially opened as St. Jules' Roman Catholic Elementary School with its opening ceremony lead by Father F.X Laurendeau, pastor of Our Lady of the Lake Church on March 30, 1925. The school accommodated 250 pupils in Ford City.⁸

However, in short time, St. Jules experienced overcrowding due to the rapid growth of the Ford City.⁹ It was reported that both public and separate school facilities could not keep up with the rapid pace of growth which resulted in schools boards continually making new plans for construction of new facilities and additions. On June 28, 1928, Contractor Cleveland Mousseau obtained a permit to construct an addition to the school.¹⁰ The addition was reported to be 41x24 feet with a height of 33 feet in dimension, to accommodate three classrooms. The \$28,000 construction value noted

⁷ "7127 Pupils Enrolled in Separate Schools Here" *The Border Cities' Star*, 31 Dec.1930, pp. 41 ProQuest

⁸ "Modern New Separate School to be Opened Sunday" *The Border Cities' Star* 27 Mar. 1925 pp. 3 ProQuest

⁹ "Growth in Population is Steady." *The Border Cities' Star*, 31 December 1926, pp 81 ProQuest

¹⁰ "Permit Taken Out For \$28,000 School" *The Border Cities Star*, 28, June 1928, pp 7 ProQuest

on the permit was not inclusive of the cost of plumbing and other work, amounting to a considerably higher total cost for the addition. The building was reported to be constructed of solid brick walls and brick foundation, with oak, pine, and maple trim and cement floors. In September of 1928, the three rooms were added to serve the 300 pupils enrolled. ⁴

Despite the increasing student population, financing of the school was still a challenge. In 1933, it was reported that two-thirds of the St Jules students were not from East Windsor but were from Sandwich East and unpaid for. Sandwich East Township had already failed to cover the tuition cost of the two years before and tuition payment for 1933. The East Windsor Control Board ruled that the expenses of East Windsor Separate School needed to be reduced from \$46,000 to \$27,990, whereas the 1933 payroll for teachers alone had already exceeded the limit at \$28,000. Closing St Jules School saved \$6000 from six teachers' salaries, and so was contemplated as one of the cost cutting measures. Other budget cuts considered measures such as postponing the reopening of schools by a month after the summer break, reducing teaching staff in other East Windsor schools and salary reduction, and elimination of health, dental and truant officer services.¹¹

The Board ultimately did not close St Jules, but disputes continued as population increased more so. ¹² By 1935, there were five separate schools in East Windsor enrolled with 2,200 children; St Josephs, Holy Rosary, St. Bernard, St Jules, St Francis. St Jules School was overcrowded and had to operate two classes in half-day shifts to accommodate the enrollment of students. As a temporary solution to alleviate overcrowding, the East Windsor Separate School Board rearranged boundary lines to shift students from St. Jules to St. Bernard. However, some parents of the students disagreed with the boundary change and cited that St Jules School was built jointly between the Sandwich East Separate School Board and the East Windsor Separate School Board to ensure accommodation of Sandwich East students. The School Board suggested that after 1935 amalgamation of the Border Cities and the pooling of assessment resources, there would be more financial ability to build new classrooms.¹³ However, it was not until the spring of 1960, that the construction of a two-storey addition began. The addition created six large classrooms to the rear of the building, and was the first major expansion at the school building since its opening in 1924. When completed, the \$137,000 addition was blessed by Very Rev. J. Z. Noel, Pastor of St Theresa parish opposite the School through the sprinkling of holy water in the new rooms and corridors. ¹⁴

The school continued its operation until 2017 when it closed, and students were transferred to the neighbouring St. Teresa School.

Throughout its history from construction in 1924-1925, St Jules School had long served the Ford City and Sandwich East community as a Separate school. It is the oldest remaining Catholic School Building in Ford City, and one of the oldest remaining

¹¹ "Closing of School Seen." *The Border Cities Star*, February 13, 1933, p 2.

¹² "Must Retain Boundaries: Duncan McArthur" *The Border Cities Star*, 15 June, 1935, p 6.

¹³ "11 Children Stay Home: Sandwich East Parents Refuse to Permit School Shift." *The Border Cities Star*, February 9, 1935, p 10.

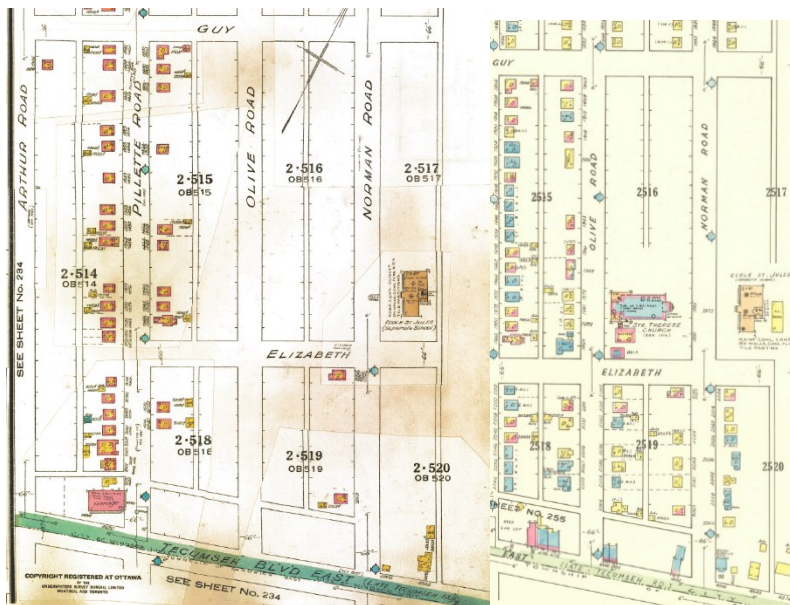
¹⁴ "Structure Blessed: Cleric Consecrates School Addition." *The Windsor Star*, February 15, 1961, p 5.

Separate School buildings in the City (other than the École élémentaire L'Envolée at 1799 Ottawa St, other older Separate School buildings have been demolished). St Jules school has direct association with the rapid growth of the Ford City area, and with the history of Separate Schools in the City. The property is a remnant of the early phases of the former town's development and the growing demands for educational catholic space.

The subject property was designed by locally significant architects John Robert Boyde and James Carlisle Pennington.³ The Pennington and Boyde partnership began from 1920 to 1935, and together, were responsible for many works that are listed and or designated on the Windsor Municipal Heritage Register. Some of their collaborated designated works include the Sacred Heart Church and Rectory, Knights of Columbus/Auditorium Building, and listed works including Pascoe Building, Jackson Park Gateway, David Maxwell School, Hôtel-Dieu five-storey hospital wing, and other residential properties. The majority of Pennington & Boyde's commissions were for public institutions or residential properties.

CONTEXTUAL VALUE

The subject property is located on the northeast corner of Norman Road and Empress Street, in proximity to the major arterial intersection of Tecumseh Road East and Pillette Road. When the school building was first constructed in 1925, the immediate surrounding area seemed to be largely undeveloped. This is reflected in the 1924 Fire Insurance Map, which although showed road alignments down Norman Road to Tecumseh, did not contain information about the development in that area. Lands from Alice Street to Tecumseh Road between Norman Road and Princess Avenue was subdivided only later in Registered Plan 1360, which was approved in 1929. The school site encompassed Lots 35 to 42, Lots 70 to 76, School Block, and parts of alley. Even in 1937, the Fire Insurance Map still only reflected a few houses developed around the area east of Pillette Road with the only buildings depicted being St Jules School and one other house on Norman Road.



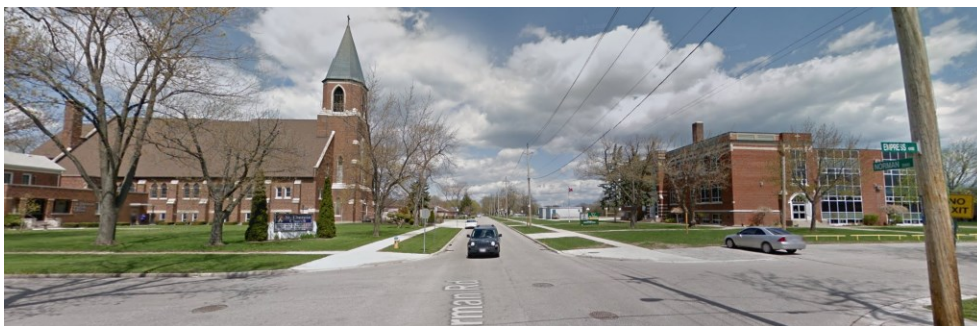
St Jules School is historically linked to its surroundings, particularly with St Theresa Roman Catholic Church at 1991 Normand Road, constructed opposite St Jules School in 1948. The Church was designed by one of the same architect as the School building (J.C. Pennington). Ste. Therese Parish was originally established as a mission of Our Lady of the Rosary Church (which was at Riverside & Drouillard), and located for 23 years, right at the boundary of Ford City & Township of Sandwich East down the street on Tecumseh Road. ¹⁵ Based on the Parish history, it was that location that children in the neighbourhood attended school at a small frame school house, until St Jules School was built in 1924 on Norman Road. Later, the parish continued using the frame building until eventually also moving to Norman Road.

The School and the Church properties are surrounded by low profile residential land uses. The school building stands out visually on its own from its height and generous setback. The Church directly opposite visually corresponds to the school building with similar massing and setback. By 1952, although the surrounding neighbourhood had been developed, the block where the Church and School were located remained undeveloped. The lands on the west Church block was eventually constructed upon but the School block remained as open space to date.

The School property was the first development in the neighbourhood and is visually distinguished from the surroundings. The St. Jules School building is a long-standing and significant landmark in the neighbourhood.



Looking south on Norman Rd (Source: Google streetview April 2012)



¹⁵ St. Thérèse Parish. *St. Thérèse Parish, 1928-1978*. 1978. Retrieved from Windsor Public Library, Local History Center

Looking north on Norman Rd (Source: Google streetview May 2014)

Next Steps:

The heritage evaluation conducted provides Council with knowledge on the cultural heritage value and interest of the subject property and conclusion that the property meets the criteria under the *Ontario Heritage Act* for designation.

If Council chooses for the subject property to remain on the Heritage Register, the property Owner can still at any point choose to redevelop the property and at that time, Council can choose to issue a notice of intention to designate the property or not, within 90 days of the notice of complete *Planning Act* application.

Recent Bill 23 *More Homes Built Faster Act, 2022* has resulted in amendments to the *Ontario Heritage Act*. By January 1, 2025, if Council has not issued a notice of intention to designate any non-designated properties, then those properties will have to be removed from the Heritage Register (including the subject property at 1982 Norman Road). The Planning Department is going through an exercise to review the list of 884 non-designated properties on the Heritage Register to triage and recommend designation of some of the properties. Council may also choose to designate the subject property or not as part of the comprehensive designation review exercise.

Council may also choose to remove the subject property from the Windsor Municipal Heritage Register. This option would not be aligned with the Official Plan policies cited below, and is not recommended by Administration as it may create a precedent of irreversible loss of heritage resources in the City in spite of heritage merits.

Official Plan Policy:

The Windsor Official Plan Volume 1 includes (9.0) “A community’s identity and civic pride is rooted in physical and cultural links to its past. In order to celebrate Windsor’s rich history, Council is committed to recognizing, conserving and enhancing heritage resources.” Goals include (9.1.1) “The identification, recognition, protection, conservation, enhancement and proper management of heritage resources.” Objectives include (9.2.4) “To increase awareness and appreciation of Windsor’s heritage resources and encourage participation by individuals, organizations and other levels of government in heritage conservation.”

Also, (9.3.2.1) “Council will identify Windsor’s heritage resources by: ... (c) Researching and documenting the history and architectural and contextual merit of potential heritage resources on an individual property basis”. (9.3.3.1) “Council will recognize Windsor’s heritage resources by: ... (a) Designating individual buildings, structures, sites and landscapes as heritage properties under the Ontario Heritage Act;” (9.3.4.1) “Council will protect heritage resources by: (g) Encouraging the adaptive reuse of architectural and/or historically significant buildings and structures;”, (9.3.6.1) “Council will manage heritage resources by: ... (e) providing support and encouragement to organizations and individuals who undertake the conservation of heritage resources by private means”

Section 9.3.7.1 “Council will integrate heritage conservation into the development and infrastructure approval process by: (d) Utilizing the planning approval process

(subdivisions / condominiums, official plan amendments, zoning amendments, site plan control, consent, minor variance, demolition control) to facilitate the retention of heritage resources, and to ensure any proposed development is compatible with heritage resources; (e) Having regard to the following factors when assessing applications such as zoning amendments, site plan control applications, demolition control and payment-in-lieu, which may impact heritage resources: (i) Respecting the massing, profile and character of adjacent buildings; (ii) Approximating the width and established setback pattern of nearby heritage buildings; (iii) Respecting the yards, gardens, trees and landscaped grounds associated with the heritage properties and districts which contribute to their integrity, identity, and setting; (iv) Maintaining, enhancing or creating views and vistas of heritage resources; and (v) Minimizing the impact of shadowing on adjacent heritage properties, particularly on landscaped open spaces and outdoor amenity areas. (f) Utilizing the demolition control provisions of the Planning Act and the Heritage Act to assist in the protection of heritage buildings and structures”.

Examples of Adaptive Reuse of Schools in Windsor

The following are some examples of adaptive reuse of school buildings in Windsor.

The property at 5955 Ontario Street, formerly the Edith Cavell School, was constructed in 1919. The property was adaptively reused into its current use as a condominium development with 20 units in 2001. Council approved heritage designation of the property in 2009.



Former Edith Cavell School (Source: Google Streetview)

Ste. Genevieve School at 647 Irvine Avenue is listed on the Heritage Register. The Pennington & Boyde designed school was constructed in 1929, and converted to residential condominiums with 15 units.



Former Ste. Genevieve School (Source: Google Streetview)

Ivor Chandler school at 1095 Bartlet Drive, originally constructed in 1935 (with 1963 addition) was converted in 1994 to residential apartments.



Former Ivor Chandler School (Source: Google Streetview)

Risk Analysis:

If not designated, at status quo, the current issues facing the property may continue. Inappropriate changes could be made to the building and demolition may be more readily allowed. Nevertheless, if the property remains on the Heritage Register, the property Owner would likely still be required to provide a Heritage Impact Study as part of the redevelopment application.

If designated, alterations, modifications or additions that affect the property will require Heritage Permit application and approval by Council, after consultation of the municipal heritage committee. There are also additional Property Standards Bylaw requirements that applies to heritage designated properties, such as the maintenance of specific heritage attributes and maintenance of vacant heritage property.

If Council chooses to remove the property from the Municipal Heritage Register, it might create a precedent of Owners requesting for removals from the Heritage Register despite heritage merits of the property. The end result will be irreversible loss and demolition of heritage resources in the City.

Climate Change Risks

Climate Change Mitigation: N/A

Climate Change Adaptation: N/A

Financial Matters:

If Council chooses to designate the property, the City will pay the costs of legal notices and registering the by-law amendment with the Land Registry Office. The Committee's heritage operating budget will cover the cost of the designation plaque. Once designated, the owner could apply for grants from the City's Community Heritage Fund to repair features identified in the designation, and could request property tax reductions for the costs of eligible conservation works under the City's Heritage Tax Reduction Program.

Consultations:

The Heritage Planner visited the site and planning staff received assistance from Windsor Public Library, Archives, and Local History Centre, and Museum Windsor for research information. Building Department Staff provided input on Property Standards.

Conclusion:

The St. Jules School has sufficient merits to be designated. Council can choose to take no action at this time and allow the property to remain on the heritage register. Alternatively, Council can initiate the designation process for the property under Part IV of the *Ontario Heritage Act* to heighten the protections afforded to a designated heritage property.

Planning Act Matters: N/A

Approvals:

Name	Title
Kristina Tang	Heritage Planner
Neil Robertson for Thom Hunt	City Planner / Executive Director Planning & Building
Wira Vendrasco	Deputy City Solicitor
Jelena Payne	Commissioner of Economic Development & Innovation
Joe Mancina	Chief Administrative Officer

Notifications:

Name	Address	Email
Olivia Construction Homes Inc. Attn: Ashraf Botros		

Appendices:

- 1 Appendix A- Statement of Cultural Heritage Value or Interest
- 2 Appendix B- Photographs of the Property
- 3 Appendix C- Supporting Research Material

Appendix A – Draft Statement of Cultural Heritage Value and Interest

St. Jules School (former)

1982 Norman Road

Description of Historic Place

The former St Jules Catholic Elementary School is located at 1982 Norman Road, within the former boundary of Ford City, (East Windsor). The red-brick, two-storey plus basement school constructed in 1924-1925, was originally designed with a symmetrical facade and a protruding central entrance accented with a stone cross and gothic arch door. Prominent in the design is the horizontal stone banding at the parapet and cornice, and at the base of the center portico. The building is one of the oldest remaining Catholic School buildings. It is a significant neighbourhood landmark at the southeastern boundary of the former Town of Ford City, reflecting the early phases of the town's development and its demands for educational catholic facilities.

Design/Physical value:

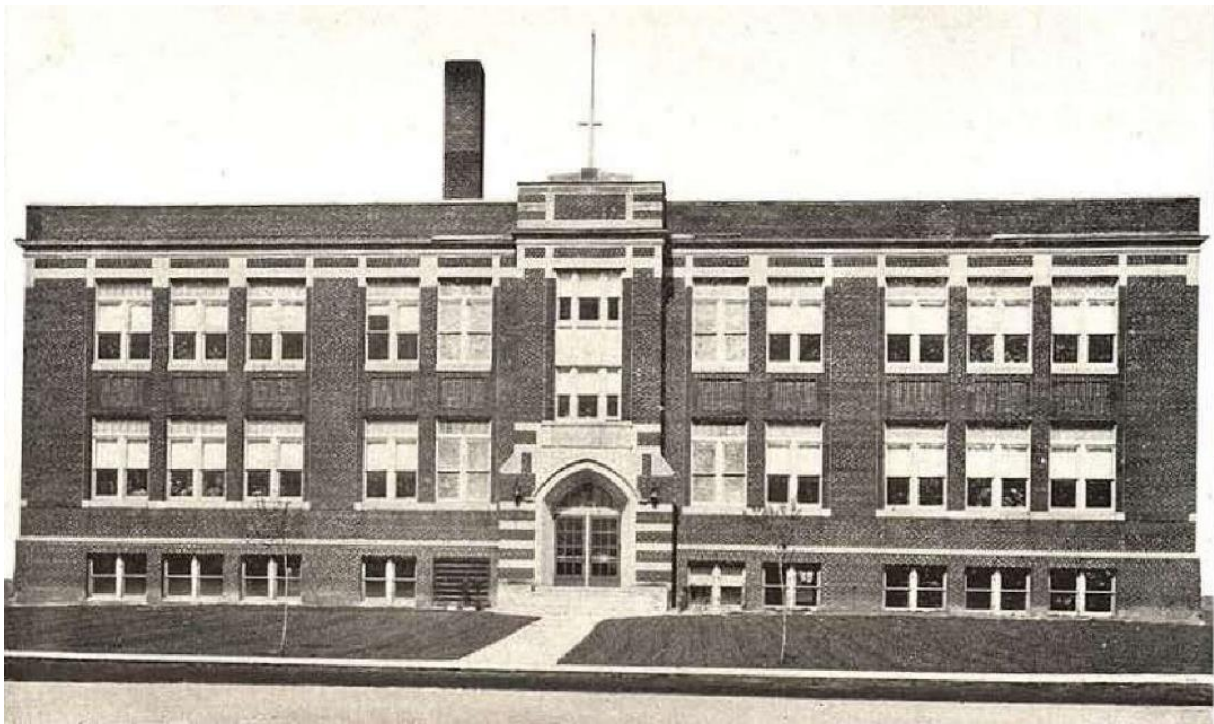
The former St Jules School does not fit exclusively with any distinctive architectural style but exhibits some elements of Edwardian architecture and Classical Revival Styles. Simple, classical and balanced in design ¹, some of these Edwardian design elements include the flat roof, articulated cornices with pronounced horizontal stone banding spanning across the entire building. Whereas some of the Classical Revival design elements are the formal symmetrical design (with center door), front and side entry portico, and rectangular double hung windows. ²

The red-brick, two-storey plus basement school constructed in 1924-1925, was designed by well-known and respected prolific partnership of Pennington & Boyde. ³ and constructed between 1924-1925. The reinforced concrete construction is clad with red-brick and stone accents. The front façade of building has a symmetrical design with pronounced horizontal stone banding at parapet and stone cornice, alternating between stone and brick. A variety of brickwork is used throughout the building, including: American bond brick on the main face of the wall; soldier course below the cornice and along the stone sills of the first floor windows; slightly recessed header brick pattern between the first and second floor windows. There are no pilasters on the building but the design of the stone accents and brick orientation create a visual illusion of some type of shallow pilaster between window bays.

¹ Heritage Resource Center. *Ontario Architectural Style Guide*. Retrieved from <https://www.therealtydeal.com/wp-content/uploads/2018/06/Heritage-Resource-Centre-Achitectural-Styles-Guide.pdf>

² Pennsylvania Historical & Museum Commission. "Classical Revival Style 1895-1950". Pennsylvania Architectural Field Guide. Retrieved from <http://www.phmc.state.pa.us/portal/communities/architecture/styles/classical-revival.html>

³ Hill, Robert G. "Pennington, James Carlisle". Biographical Dictionary of Architects in Canada 1800-1950, retrieved from <http://dictionaryofarchitectsincanada.org/node/1385>



Historical photo of the former St Jules Separate School ⁴

Prominent in the appearance of the building is the protruding centered front portico facing Norman Road. Originally topped with a stone cross pediment, the portico is accented with alternating stone and brick banding at top and bottom. The portico had rectangular stone tracery with decorative stone crest/emblem (lost feature), framed by header and stretcher bricks. At the base was a recessed arched entrance with a multipaned glass door with transom. The portico entrance was anchored by concentric rectangular steps, flanked with rectangular pillars.

At each side of the front portico is five window bays (in double and triple sets). The first and second floor windows were double sets of hung windows with transom, separated between floors by slightly recessed header brick pattern. The second floor had separated stone sills, while the first floor windows had adjoined sills between the double and triple set windows. Stone banding above basement windows level visually divided the above ground floors and basement level.

The other sides of the building have similar design, with simplified protruding portico on the north (assumed) and south side entrances (with stone cross design on the parapet). A large square brick chimney is located off center slightly to the north of the building. The building enjoys a generous landscape setback with unhindered views of the building from Norman Street.

A two storey six classroom addition was constructed in 1960 to the southeast portion of the original building. The south portico was retained with the new construction adjoining

⁴ City of East Windsor. "City of East Windsor 1929" (1929). SWODA: Windsor & Region Publications. 71. Retrieved from <https://scholar.uwindsor.ca/swoda-windsor-region/71>

behind the portico, using matching red brick wall and mid-century glazing style. Later, the school property was expanded again in 1964 for a gymnasium, this time to the north of the property, disrupting the north side portico.

Various alterations such as changes to the glazing and parapet have taken place on the property over the past near hundred years. Appropriate glazing could be reintroduced in the future to return the property closer to its original appearance.

Historical/Associative Value

The St. Jules Separate School building was initially constructed on farmland originally owned by Jules Janisse.⁵ The land was part of the village of Ford City at that time (later in 1929 incorporated as the city of East Windsor, then amalgamated into City of Windsor in 1935). Ford City spanned from north at the River to south at Tecumseh Road, and east from Princess Avenue to west along the Chesapeake & Ohio Railway.⁶

The property has direct association to Windsor separate schools. The Windsor's Separate School System was founded in 1901 by Senator Charles E. Casgrain, Michael A. Brian, Gasper Pacaud, Thomas Kinsella, Joseph Neil Reaume, Peter J. Sweeney, J.J. Guitard and Jeremiah (Jerry) G. Gangnier to provide an education setting reflecting Roman Catholic beliefs.⁷ The first Separate School was built in 1894 called Notre Dame School where it accommodated all the pupils until 1915 when the increase in population made it necessary to expand classes in the Parish-Hall (later Holy Rosary) which accommodated 50 students. Overtime, from September of 1916 it was expanded to a twelve-room structure by the Spring of 1922. Classes became more overcrowded with increased demand for pupils to be taught by the Sisters of St. Joseph. To provide relief, the Trustees of Sandwich East Separate School Board tasked Sisters of St. Joseph to run a two-room school near Pillette and Tecumseh Road in the Summer of 1922. To keep up with demands, the Separate School Board built a six room School named after Jules Janisse, the previous owner of the land.⁴

⁵ "Homes Were Not Many 50 Years Back." *The Border Cities' Star*, 31 May 1929, pp. 55 ProQuest

⁶ City of Windsor, Planning Department. "History of Windsor". City of Windsor, retrieved from <https://www.citywindsor.ca/residents/historyofwindsor/pages/default.aspx>

⁷ "7127 Pupils Enrolled in Separate Schools Here" *The Border Cities' Star*, 31 Dec.1930, pp. 41 ProQuest

Modern New Separate School to be Opened Sunday



THIS modern new separate school, to be known as St. Jules' School, will be opened at Ford City, Sunday, at a ceremony to which the public is invited. The opening ceremonies will be in charge of Rev. Father F. X. Laurendeau, pastor of

Our-Lady of the Lake Church. The school, which is on the corner of Norman and Elizabeth streets, has been built by Strain and Emery, contractors, and will accommodate 250 pupils. Pennington and Boyde, of this city, are the architects.

The building officially opened as St. Jules' Roman Catholic Elementary School with its opening ceremony lead by Father F.X Laurendeau, pastor of Our Lady of the Lake Church on March 30, 1925. The school accommodated 250 pupils in Ford City.⁸

However, in short time, St. Jules experienced overcrowding due to the rapid growth of the Ford City.⁹ It was reported that both public and separate school facilities could not keep up with the rapid pace of growth which resulted in schools boards continually making new plans for construction of new facilities and additions. On June 28, 1928, Contractor Cleveland Mousseau obtained a permit to construct an addition to the school.¹⁰ The addition was reported to be 41x24 feet with a height of 33 feet in dimension, to accommodate three classrooms. The \$28,000 construction value noted on the permit was not inclusive of the cost of plumbing and other work, amounting to a considerably higher total cost for the addition. The building was reported to be constructed of solid brick walls and brick foundation, with oak, pine, and maple trim and cement floors. In September of 1928, the three rooms were added to serve the 300 pupils enrolled.⁴

Despite the increasing student population, financing of the school was still a challenge. In 1933, it was reported that two-thirds of the St Jules students were not from East Windsor but were from Sandwich East and unpaid for. Sandwich East Township had already failed to cover the tuition cost of the two years before and tuition payment for 1933. The East Windsor Control Board ruled that the expenses of East Windsor Separate School needed to be reduced from \$46,000 to \$27,990, whereas the 1933 payroll for teachers alone had already exceeded the limit at \$28,000. Closing St Jules School saved \$6000 from six teachers' salaries, and so was contemplated as one of the cost cutting

⁸ "Modern New Separate School to be Opened Sunday" *The Border Cities' Star* 27 Mar. 1925 pp. 3 ProQuest

⁹ Growth in Population is Steady." *The Border Cities' Star*, 31 December 1926, pp 81 ProQuest

¹⁰ "Permit Taken Out For \$28,000 School" *The Border Cities Star*, 28, June 1928, pp 7 ProQuest

measures. Other budget cuts considered measures such as postponing the reopening of schools by a month after the summer break, reducing teaching staff in other East Windsor schools and salary reduction, and elimination of health, dental and truant officer services.¹¹

The Board ultimately did not close St Jules, but disputes continued as population increased more so.¹² By 1935, there were five separate schools in East Windsor enrolled with 2,200 children; St Josephs, Holy Rosary, St. Bernard, St Jules, St Francis. St Jules School was overcrowded and had to operate two classes in half-day shifts to accommodate the enrollment of students. As a temporary solution to alleviate overcrowding, the East Windsor Separate School Board rearranged boundary lines to shift students from St. Jules to St. Bernard. However, some parents of the students disagreed with the boundary change and cited that St Jules School was built jointly between the Sandwich East Separate School Board and the East Windsor Separate School Board to ensure accommodation of Sandwich East students. The School Board suggested that after 1935 amalgamation of the Border Cities and the pooling of assessment resources, there would be more financial ability to build new classrooms.¹³ However, it was only until spring of 1960, that the construction of a two-storey addition began. The addition created six large classrooms to the rear of the building, and was the first major expansion at the school building since its opening in 1924. When completed, the \$137,000 addition was blessed by Very Rev. J. Z . Noel, Pastor of St Theresa parish opposite the School through the sprinkling of holy water in the new rooms and corridors.¹⁴

The school continued its operation until 2017 when it closed, and students were transferred to the neighbouring St. Teresa School.

Throughout its history from construction in 1924-1925, St Jules School had long served the Ford City and Sandwich East community as a Separate school. It is the oldest remaining Catholic School Building in Ford City, and one of the oldest remaining Separate School buildings in the City (other than the École élémentaire L'Envolée at 1799 Ottawa St, other older Separate School buildings have been demolished). St Jules school has direct association with the rapid growth of the Ford City area, and with the history of Separate Schools in the City. The property is a remnant of the early phases of the former town's development and the growing demands for educational catholic space.

The subject property was designed by locally significant architects John Robert Boyde and James Carlisle Pennington.³ The Pennington and Boyde partnership began from 1920 to 1935, and together, were responsible for many works that are listed and or designated on the Windsor Municipal Heritage Register. Some of their collaborated designated works include the Sacred Heart Church and Rectory, Knights of Columbus/Auditorium Building, and listed works including Pascoe Building, Jackson Park Gateway, David Maxwell School, Hôtel-Dieu five-storey hospital wing, and other

¹¹ "Closing of School Seen." *The Border Cities Star*, February 13, 1933, p 2.

¹² "Must Retain Boundaries: Duncan McArthur" *The Border Cities Star*, 15 June, 1935, p 6.

¹³ "11 Children Stay Home: Sandwich East Parents Refuse to Permit School Shift." *The Border Cities Star*, February 9, 1935, p 10.

¹⁴ "Structure Blessed: Cleric Consecrates School Addition." *The Windsor Star*, February 15, 1961, p 5.

residential properties. The majority of Pennington & Boyde's commissions were for public institutions or residential properties.

CONTEXTUAL VALUE

The subject property is located on the northeast corner of Norman Road and Empress Street, in proximity to the major arterial intersection of Tecumseh Road East and Pillette Road. When the school building was first constructed in 1925, the immediate surrounding area seemed to be largely undeveloped. This is reflected in the 1924 Fire Insurance Map, which although showed road alignments down Norman Road to Tecumseh, did not contain information about the development in that area. Lands from Alice Street to Tecumseh Road between Norman Road and Princess Avenue was subdivided only later in Registered Plan 1360, which was approved in 1929. The school site encompassed Lots 35 to 42, Lots 70 to 76, School Block, and parts of alley. Even in 1937, the Fire Insurance Map still only reflected a few houses developed around the area east of Pillette Road with the only buildings depicted being St Jules School and one other house on Norman Road.

St Jules School is historically linked to its surroundings, particularly with St Theresa Roman Catholic Church at 1991 Normand Road, constructed opposite St Jules School in 1948. The Church was designed by one of the same architect as the School building (J.C. Pennington). Ste. Therese Parish was originally established as a mission of Our Lady of the Rosary Church (which was at Riverside & Drouillard), and located for 23 years, right at the boundary of Ford City & Township of Sandwich East down the street on Tecumseh Road.¹⁵ Based on the Parish history, it was that location that children in the neighbourhood attended school at a small frame school house, until St Jules School was built in 1924 on Norman Road. Later, the parish continued using the frame building until eventually also moving to Norman Road.

The School and the Church properties are surrounded by low profile residential land uses. The school building stands out visually on its own from its height and generous setback. The Church directly opposite visually corresponds to the school building with similar massing and setback. By 1952, although the surrounding neighbourhood had been developed, the block where the Church and School were located remained undeveloped. The lands on the west Church block was eventually constructed upon but the School block remained as open space to date.

The School property was the first development in the neighbourhood and is visually distinguished from the surroundings. The St. Jules School building is a long-standing and significant landmark in the neighbourhood.

¹⁵ St. Thérèse Parish. *St. Thérèse Parish, 1928-1978*. 1978. Retrieved from Windsor Public Library, Local History Center

Heritage Attributes:

Exterior attributes that contribute to the design/physical value of St Jules School:

- Original 1924-1925 construction with elements of Edwardian and Classical Revival architectural styles
- Two Story plus basement reinforced concrete construction clad with red-brick and stone accents
- Front façade of building with symmetrical design
- Pronounced horizontal stone banding at parapet and stone cornice alternating between stone and brick (soldier coursing)
- Variety of bond patterns (American bond of four stretches between every header; soldier and header coursings)
- Prominent protruding centered front portico facing Norman Road
 - o Originally topped with stone cross pediment
 - o Accented with alternating stone and brick banding at top and bottom
 - o Rectangular stone tracery with decorative stone crest/emblem (lost feature), framed by header and stretcher bricks
 - o Recessed arched entrance with multi-paned glass door with transom
 - o Concentric rectangular steps, flanked with rectangular pillars
- Five window bays on each side of center portico (in double and triple sets)
- Double sets of hung windows with transom, separated between floors by slightly recessed header brick pattern (lost feature)
- Separated stone window sills on second floor, and adjoined sills between the double and triple set windows on the first floor windows
- Stone banding above basement windows level
- Protruding portico on north and south side entrances, with stone cross design on parapet
- Large square brick chimney
- Generous landscape setback with unhindered views

Attributes that contribute to the historical or associative value of St Jules School:

- Early (1925) and long-time service to former Ford City (East Windsor) and Sandwich East community as a Separate school
- Oldest remaining former Catholic School building in Ford City and one of the oldest remaining separate school buildings in the City of Windsor
- Direct association with the rapid growth of the Ford City area
- Direct association with the history of Separate Schools in the City
- Remnant of the early phases of the former Ford City's development and the growing demands for educational catholic space
- Its design by locally significant architects Pennington & Boyde

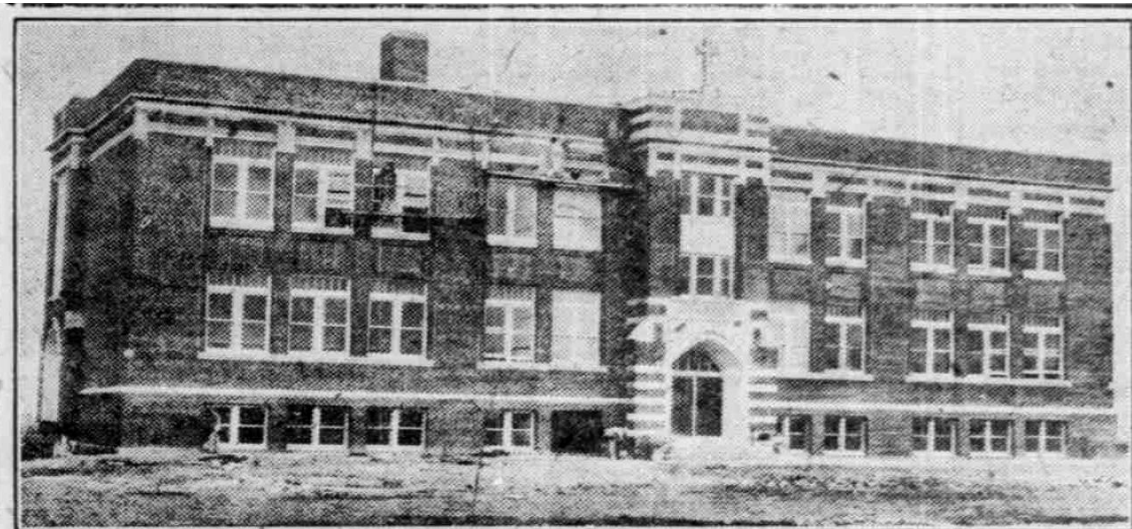
Attributes that contribute to the contextual value of St Jules School:

- Visually and historically linked to St Theresa's Church directly across the street.

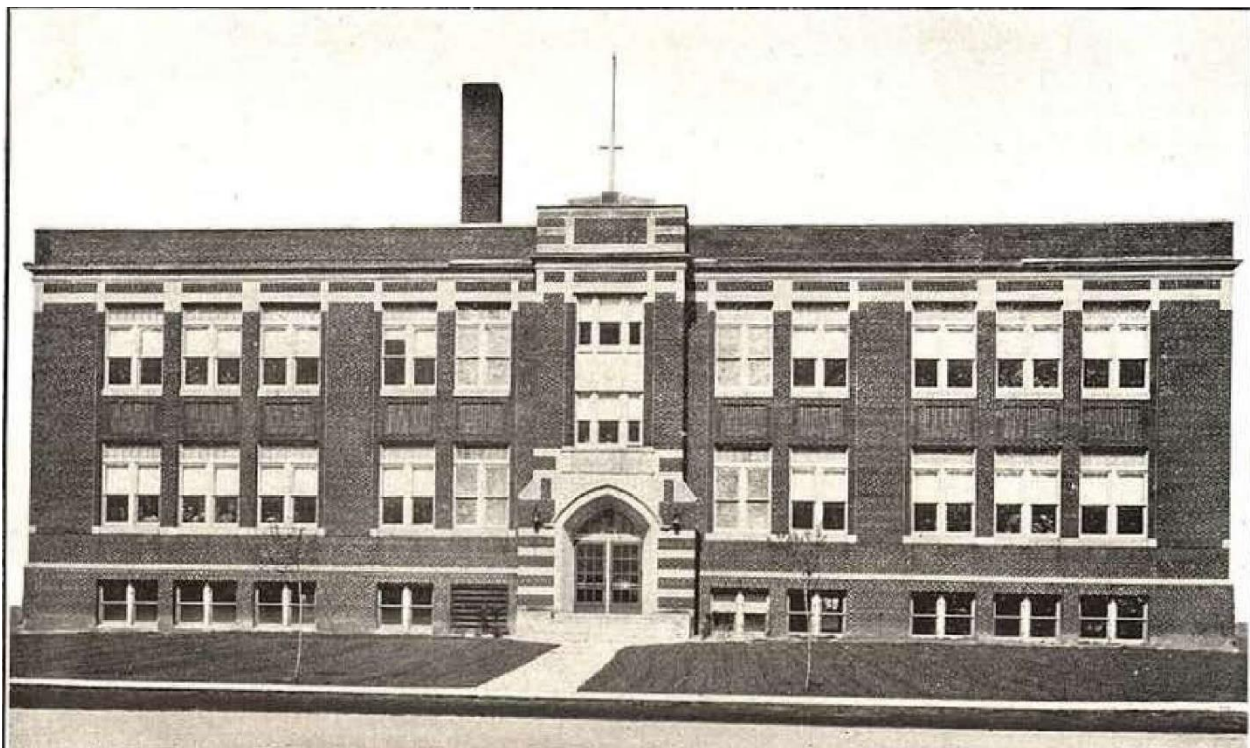
- Longstanding landmark in the neighbourhood of what was the former southeastern Ford City (Eastern Windsor), at the boundary with Township of Sandwich East

APPENDIX B- PHOTOGRAPHS OF THE PROPERTY

Historical photographs of the former St Jules Separate School



Source: "Modern New Separate School to be Opened Sunday" *The Border Cities' Star* 27 Mar. 1925 pp. 3 ProQuest



Source: City of East Windsor. "City of East Windsor 1929" (1929). SWODA: Windsor & Region Publications. 71. Retrieved from <https://scholar.uwindsor.ca/swoda-windsor-region/71>

Site Visit photos from 2021-07-21 (from Building Department)



Front elevation



South Side elevation with 1960 addition

Site Visit photos from 2021-11-19



Front entrance



Front Portico



Stone portion above front entrance



Northwest corner of property showing 1965 Gymnasium addition



Cornice/parapet



Rear of original construction



South side elevation



South side portico

Appendix C- Supporting Research Material

Pennington, James Carlisle

PENNINGTON, James Carlisle (1885-1963), the acknowledged dean of the architectural scene in Windsor and Essex County, and who was active for over fifty years in the following offices:

Crane & Pennington, with dual offices in Windsor, Ont. and Detroit, Mich.
(with **C. Howard Crane**)

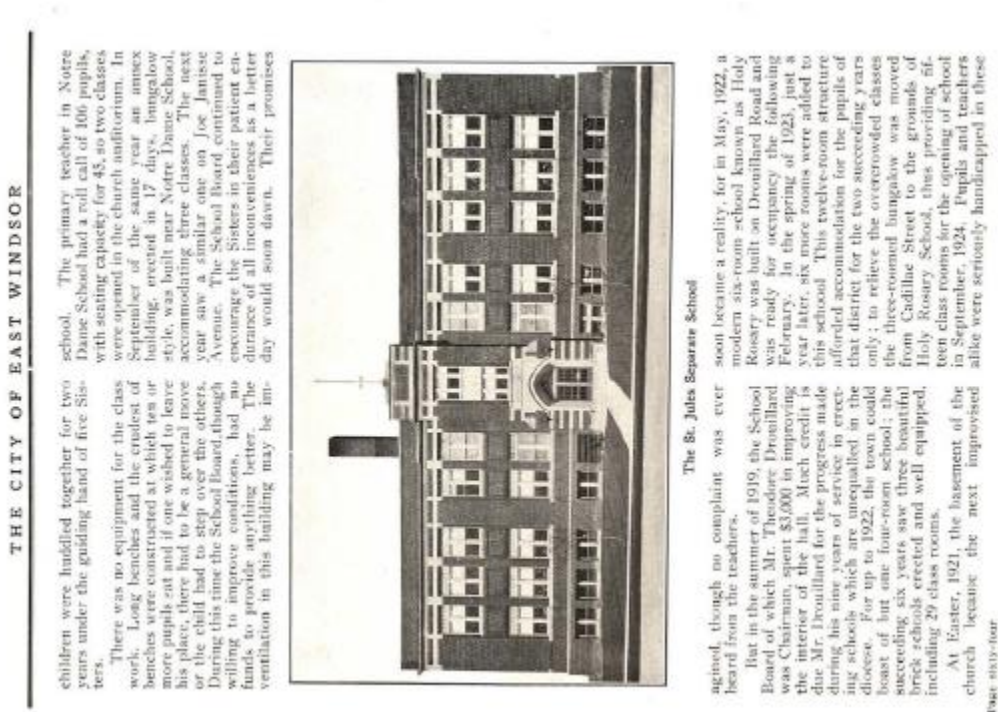
J.C. Pennington, Windsor, Ont. 1913-1920

Pennington & Boyde, Windsor, Ont. 1921-1937 (with **John R. Boyde**)

J.C. Pennington, Windsor, Ont., 1937 to 1950 and after

FORD CITY, ONT., St. Jules Roman Catholic School, Elizabeth Road at Norman Road, 1924 (Border Cities Star [Windsor], 8 July 1924, 9, t.c.; 27 March 1925, 3, illus.)

Resource 3: Hill, Robert G. "Pennington, James Carlisle". Biographical Dictionary of Architects in Canada 1800-1950, retrieved from <http://dictionaryofarchitectsincanada.org/node/1385>



Resource 4: City of East Windsor. "City of East Windsor 1929" (1929). SWODA: Windsor & Region Publications. 71. Retrieved from <https://scholar.uwindsor.ca/swoda-windsor-region/71>

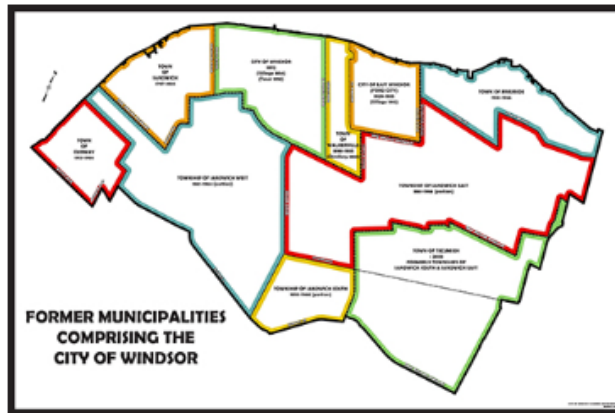


Resource 5: "Homes Were Not Many 50 Years Back." *The Border Cities' Star*, 31 May 1929, pp. 55 ProQuest

History of Windsor



Windsor as we know it today was created from once-separate villages and towns.



(Above: Map showing annexations and amalgamations of Windsor, 1935-2003. Click for [larger map](#).)

In 1935, Windsor, Sandwich, East Windsor and Walkerville united to form the City of Windsor. This amalgamation was intended to address the crushing debt and social demands brought by the Great Depression. In the 1960s, the City of Windsor looked to surrounding communities to expand its land and tax base. The city attempted to annex the towns of Tecumseh, Riverside, Ojibway, all or part of St. Clair Beach and parts of the townships of Sandwich East, Sandwich West, and Sandwich South. It was a topic of heated debate. In the end, it was decided that Riverside, Ojibway and parts of Sandwich townships would become part of Windsor.

Ford City

The rise of the auto industry created another community just outside of Windsor. Ford City was tied to the development of Windsor's Ford Plant. It became a village in 1913 and a town in 1915. As of 1928, its population totalled 16,000. Soon after, it was renamed East Windsor, and incorporated as a city. Learn more through the [Ford City Walking Tour Brochure](#).



(Photo: Looking south on Drouillard Rd. from Edna Street, ca. 1930. Credit: Windsor Community Archives, PC572)

Resource 6 City of Windsor, Planning Department. "History of Windsor". City of Windsor, retrieved from <https://www.citywindsor.ca/residents/historyofwindsor/pages/default.aspx>

THE BORDER CITIES STAR, WINDSOR, ONTARIO, WEDNESDAY, DECEMBER 31, 1930 PAGE NINE

7,127 PUPILS ENROLLED IN SEPARATE SCHOOLS HERE

East Windsor Has New Structure

One of the New Churches in 1930



Founded in 1901

Windsor's separate school system was founded in 1901, the first school being the old St. Alphonsus, since burned, and on the site of the present St. A. school, on the corner of the intersection of Ontario street, just west of the intersection of the street leading to the north of the city, in 1901. The first building was erected in 1901, the same year and occupied in January, 1913.

Members of the Windsor Separate School Board included Senator Charles E. Cawston, Michael A. Bryan, Otagar Peter, Thomas Kinsella, Joseph Neil, Reisman, Peter J. Reisman, J. G. Gattard and Jeremiah (Jerry) G. Gattard. Of the eight founders, the first mentioned are now dead.

The Windsor Board of Trustees of the separate school system of the founding of that body and their terms of office are as follows: Senator Charles E. Cawston, 1903-4, 1912-13 and 1914; Jerry G. Gattard, 1905-6 and 1910; Frank Reisman, 1913-14; Leo Pag, 1913; J. J. O'Connell, 1927; Harry J. March, 1928; P. J. Theoharist, 1921, latter half of 1922; Otagar Peter, 1923; P. V. J. Reisman, 1923; Alex. Latham, 1924; John Wall, 1925; A. J. Reisman, 1926; Wilbur D. Beach, 1927; and Thomas Child, 1930.

Resource 7 "7127 Pupils Enrolled in Separate Schools Here" The Border Cities' Star, 31 Dec.1930, pp. 41 ProQuest

THE STAR'S PHONES
 Price 5 c. a. copy. News 10 c.
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The Third Page

WINDSOR, ONTARIO, FRIDAY, MARCH 27, 1925

HEALY DENIES RUMORS OF POLITICAL RETIREMENT

Ford City Nominations Set for April 6, Election Week Later

RUMORS NOT BASED ON FACT

M. P. DECLARES

Denies California Purchase Has a Significance

Brands Reports as Plain Propaganda; Admits Leaving Home Here

OTAWA, March 27.—"It is nothing more or less than plain propaganda," said A. F. Healy, M. P. for North Essex, when his statements were called today in newspaper columns that he would not seek reelection when the government goes to the people. He denied that purchase of a home in California and leaving of his home in Sandwich had the significance attached to the column by dispatches appearing in the papers here.

Curtis Boys Plead Guilty To Thefts

KINGSTON, JUNE 22.—The Ontario Court, on the 22nd inst., sentenced to the reformatory, for a term of 18 months, two boys who were charged with the theft of a bicycle from the reformatory here.

SALARY SLASH HIT BY MAYOR

Mitchell Points to Living Expenses on Border

Declares School Teachers Not Overpaid; Will Oppose Move

Opposition to any increase in salaries of civic employees, such as has been suggested in some quarters, was voiced today by Mayor Frank J. Mitchell of Windsor.

"I have no objection to the suggestion that the salaries of civic employees be reduced," said Mitchell, "but I do not believe that the suggestion is justified in the case of the city of Windsor. It is not fair to the citizens to reduce the salaries of the employees of the city, and I believe that the suggestion is unjustified."

NEGOTIATIONS PROCEED ON BRIDGE

Modern New Separate School to be Opened Sunday

THIS modern new separate school, to be opened at Ford City, Sunday, at a ceremony to which the public is invited. The opening ceremonies will be in charge of Rev. Father E. X. Laurendeau, pastor of Our-Lady-of-the-Lake Church. The school, which is on the corner of Norman and Elizabeth streets, has been built by Stearns and Emery, contractors, and will accommodate 200 pupils. Farmington and Boyd, of this city, are the architects.

MAYOR SIGNS BALLOT PAPER

Third Deputy Reeve's Seat Held Vacant After Probe

Appeal Against Coughlin's Ruling Waived; Many Rumors Heard

Mayor Cleveland Monument, of Ford City, this morning issued a warrant for the apprehension of the office of third deputy reeve, made vacant by the decision of Judge J. J. Coughlin, pending Alfred Mitchell, following the recent judicial inquiry into the golf irregularities at the last election. The date has been set for April 13. Nominations will be held on April 6. The appeal

TWO HURT IN AUTO CRASH

Two persons were injured in an auto crash here today.

Resource 8 "Modern New Separate School to be Opened Sunday" The Border Cities' Star 27 Mar. 1925 pp. 3 ProQuest

which, there reside today 97,275 people. If one takes in some of the outlying country which might quite properly be called part of the Border Cities, probably the number would reach 100,000, but the count given above is conservative.

This population is divided as follows: Riverside, 4,300; Ford City, 13,046; Walkerville, 9,852; Windsor, 62,000; Sandwich, 8,077. Each one of these municipalities shows an increase over last year, in at least one instance running almost to 20 percent.

The populations last year were as follows: Riverside, 3,612; Ford City, 11,300; Walkerville, 9,071; Windsor, 56,533; Sandwich, 7,448. The respective increases are: Riverside, 688; Ford City, 2,746; Walkerville, 781; Windsor, 5,467; Sandwich, 629.

Riverside shows the greatest percentage of growth, 19.04. Walkerville has the lowest percentage, 8.6, but this is only natural. Confined as it is between Ford City on the east and Windsor on the west, it can expand only toward the south, and even this southern section has practically all been built up and all the houses occupied. The other municipalities show the following percentages of growth within the year: Windsor, 9.85; Ford City, 15.45; Sandwich, 8.89.

A Record of Progress

	1926	1925	1921
Riverside	4,300	3,612	1,155
Ford City	13,046	11,300	5,800
Walkerville	9,852	9,071	7,303
Windsor	62,000	56,533	38,530
Sandwich	8,077	7,448	4,153
Total	97,275	87,964	56,941

Increase in last year, 9,311, or 9.8 percent.
Increase in last five years, 40,334, or 70.82 percent.

doubt that the British preferential tariff had much to do with it.

Due to the terms of this tariff, American industrial firms found it would be to their advantage to establish plants in Canada to care for their export business to Great Britain. Casting about for a suitable location, they found the Border Cities practically ideal. Situated directly on a great waterway, within half a mile of one of the greatest American industrial centres, and well served with rail transportation, the Border loomed up as a veritable "promised land" to the American industrialists.

Naturally, establishment of new industries here brought with it a flood of new residents, for the demand for labor was high. Unfortunately, but withal quite naturally, the Border

Resource 9 "Growth in Population is Steady." *The Border Cities' Star*, 31 December 1926, pp 81 ProQuest

PERMIT TAKEN OUT FOR \$28,000 SCHOOL

Addition For St. Jules to Be Built in Ford City

Ford City building permits were given another substantial boost today in the taking out of a permit by Cleveland Mousseau, contractor, for \$25,000 as part of the cost for an addition to the St. Jules separate school, on the east side of Norman road, between Guy and Elizabeth streets.

Dimensions of the new school addition are given as 41x34 feet in the superstructure, a height of 23 feet, including two-story construction and a large basement under part of the building. The permit amount of \$25,000 does not include plumbing and other work, which will bring the total cost of the addition to a considerable higher figure. The building will have solid brick walls and brick foundation. Trim is to be of oak and pine with maple and cement floors. Pennington and Boyd are named as architects and Cleveland J. Mousseau, ex-mayor, general contractor in charge of construction.

This substantial addition to the St. Jules school is made necessary by the rapid growth of the motor town in its southern part, where the school is situated. Both separate and public school facilities have failed to keep pace in the past with the rapid expansion taking place. School boards everywhere find constructing one school than they have immediately to start plans for another or large additions to existing educational institutions. The recent addition to the Gordon McGregor public school in Windsor is a 12-room school in which it is believed have been necessitated in less than two years, by the rapid growth of the town.

Resource 10 "Permit Taken Out For \$28,000 School" *The Border Cities Star*, 28, June 1928, pp 7 ProQuest

Closing Of School Seen

Appears Only Way To Meet Supervisors' Budget Figure

Most St. Jules Pupils Are From Sandwich East And Not Paid For

The closing of St. Jules School in East Windsor is being considered by the East Windsor Separate School Board as a result of the control board ruling that separate school estimates totalling \$46,000 for 1933 must be reduced to \$27,980. Two-thirds of the students at the St. Jules School come from Sandwich East.

NO FINAL DECISION

The school closing was discussed at a board meeting held yesterday, but no final decision was reached. The board is expected to meet with the Windsor City Council Board this afternoon and will hold another meeting before a final decision is reached. Although it is proposed to reduce the number of pupils at the school, the closing of schools after the summer holidays, was also discussed.

It was pointed out at yesterday's meeting that the limit set by the control board for the school is \$27,980. The board is expected to meet with the Windsor City Council Board this afternoon and will hold another meeting before a final decision is reached. Although it is proposed to reduce the number of pupils at the school, the closing of schools after the summer holidays, was also discussed.

The school board yesterday voted to reduce the number of pupils at the school to \$27,980. The board is expected to meet with the Windsor City Council Board this afternoon and will hold another meeting before a final decision is reached. Although it is proposed to reduce the number of pupils at the school, the closing of schools after the summer holidays, was also discussed.

Resource 11 "Closing of School Seen." *The Border Cities Star*, February 13, 1933, p 2.

A BRIEF HISTORY OF STE THERESE PARISH

A MISSION
1925-1928

The parish of St. Thérèse of the Child Jesus was first established as a mission of Our Lady of the Rosary Church by the Rt. Rev. F.X. Laurendeau on May 27, 1925. There were then 133 families.

A PARISH
1928-1942

The Reverend Father J.I. Ducharme is appointed the first Pastor on December 2, 1928 by His Excellency Bishop Michael F. Fallon of the diocese. An old wooden school house, located on the corner of Westminster Blvd. and Empress St. (Sacred Heart School) was moved to Tecumseh Rd. on the property of the Ursuline Sisters and it served as the parish church for 23 years.

1942-1949

The Reverend Father Isidore J. Poisson is appointed the second pastor on December 18, 1942 by His Excellency Bishop J.T. Kidd of the Diocese. It was during his pastorship that the present Church and Rectory (Norman Rd.) were built.

1949-1969

The Reverend Father Jean Z. Noel becomes the third pastor on September 19, 1949 - appointed by His Excellency Bishop J.T. Kidd of the Diocese. During this period the Church debt is deleted and renovations according to Vatican II norms are implemented in the Church.

1969-

The Reverend Father C.W. Janisse becomes the fourth Pastor on June 11, 1969 - appointed by His Excellency Bishop G. Emmett Carter of the Diocese. Further implementations according to Vatican II norms are effected and a continued enhancing of the interior and exterior of the Church progresses, in preparation for our 50th Anniversary celebration. (1978)

OUR RELIGIOUS VOCATIONS

Over the years our spiritual family has produced a good number of young ladies and young men who have answered the call to the Religious Life.

The parish is justly proud of them and lives in the hope that many more young people will, in the years ahead, follow them to continue their Apostolate in the vineyard of the Lord.

...

OUR SCHOOLS

The first school of the parish was built in 1924 on the corner of Norman Rd. and Empress St. - the property of Mr. Jules Janisse whose Christian name was given to the school. Up to that time, children of the area had been using the little framed school house which was to become the first parish church on Tecumseh Rd.

Since then, two other schools were built within the present boundaries of the parish:

St. Thérèse in 1951
St. Joan of Arc in 1957

...

A Sincere Thank You

To all the ladies of the parish (St. Anne's Altar Society, C.W.L. - Sacristy Committee) who over the years have faithfully devoted themselves to the care of the sanctuaries of our Church and Chapel.

Resource 15: St. Thérèse Parish. St. Thérèse Parish, 1928-1978. Retrieved from Windsor Public Library, Local History Center.



Subject: Municipal Sewer Servicing Charges for Residential Properties - Septic System Changeover - City Wide

Reference:

Date to Council: July 4, 2023
Author: Stacey McGuire
Manager of Development
(519) 255-6100 Ext. 1726
smcguire@citywindsor.ca
Development – Engineering
Report Date: 2023-06-19
Clerk's File #: SL2023

To: Mayor and Members of City Council

Recommendation:

- I. That Council **APPROVE** the extension of the local improvement charges flat rate, private drain connection flat rate and local improvement payment terms for the construction of sanitary sewers and private drain connections to those properties that:
 - A. Are zoned residential for single unit, duplex or semi-detached dwellings only; and,
 - B. Contain an existing dwelling currently on private septic system and not connected to a municipal sanitary sewer; and,
 - C. Have a municipal mainline sanitary sewer within 30m of any property abutting a municipal right-of-way; and,
- II. That Council **APPROVE** the extension of local improvement charges flat rate, private drain connection flat rate and local improvement payment terms for the construction of storm sewers and private drain connections for those properties that qualify under Recommendation I above, when the property owner connects to the municipal storm sewer if one is available plus interest at a rate deemed appropriate by administration applied from the date the storm mainline sewer was substantially completed; and,
- III. That property owners qualifying under Recommendation I above with permits issued by the City for septic systems less than 10 years old **BE GRANTED** either

three (3) years to connect to the available sanitary sewer system once it is made available or the difference between the age of their septic system and ten (10) years, whichever is greater; and,

- IV. That Council **APPROVE** the use of the private local improvement process in Part III of O. Reg. 586/06 under the Municipal Act, for property owners qualifying under Recommendation I above for the cost of the work related to the decommissioning of septic systems and construction of sanitary private drain connections on private property, and that the Chief Administrative Officer and City Clerk **BE AUTHORIZED** to sign agreements, with property owners that consent to their lots being specially charged to raise which agreement shall be satisfactory in form to the City Solicitor, in technical content to the City Engineer and in financial content to the City Treasurer; and,
- V. That for property owners qualifying under Recommendation I above, Council **PASS** by-laws as required for the decommissioning of septic systems and construction of sanitary private drain connections on private property as a local improvement in accordance with Part III of O. Reg. 586/06 under the Municipal Act, for a period of up to 20 years and at an interest rate deemed appropriate by Administration; and,
- VI. That Administration **BE DIRECTED** to prepare the appropriate by-laws and policies to effect the recommendations above.

Executive Summary:

N/A

Background:

At the meeting September 26, 2022 Council received a report on cost sharing and services extension by a developer related to 1500 Northway. Council passed resolution CR424/2022 recommendation III as follows:

That Administration **REPORT BACK** to Council on proposed options to collect costs from the benefiting properties related to servicing charges for municipal services (mainline sewers and private drain connections) that would offer options for payment terms similar to local improvement payments.

At the meeting June 5, 2023 the Development and Heritage Standing Committee received report S 56/2023 being a zoning by-law amendment application for 0 & 1466 St. Patrick's Avenue. This report included, as a condition of approval, a requirement to extend a sanitary sewer along St. Patrick's Avenue to service the subject lands. Under existing by-laws and policies, any property that would be serviced by the resulting sewer extension ("Benefitting Properties") would be required to connect to that sewer and pay

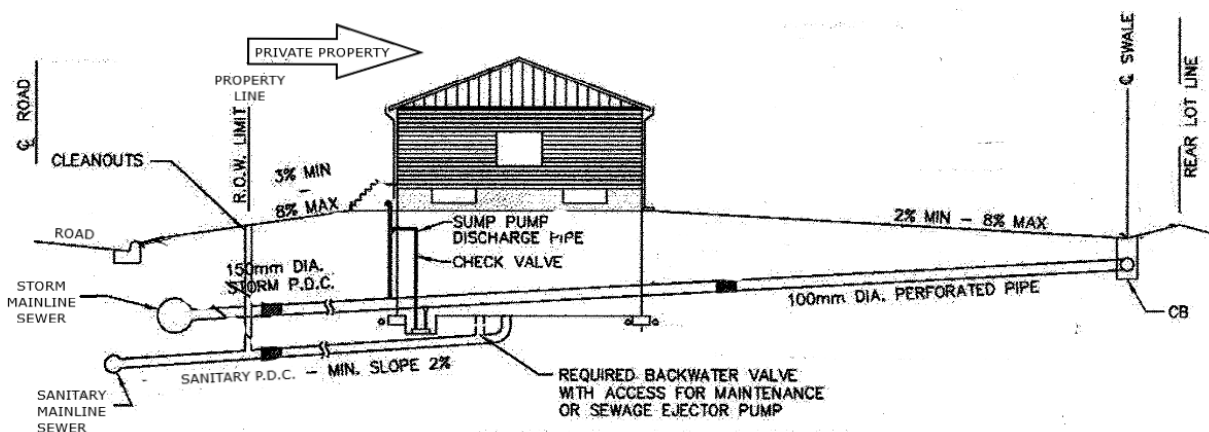
their proportionate share of the actual costs of the mainline sewer and private drain connections in the right-of-way. A number of resident delegations opposed the development due to the costs that they would be required to incur to connect to the sewer and decommission their septic system when this was not their development proposal.

This report provides Administration's recommendations to extend local improvement rates and payment terms to Benefiting Properties in situations similar to the two recent cases above.

Discussion:

When a municipal sanitary sewer is extended to within 30m of a property, the City of Windsor's Property Standards By-law 9-2019 requires said property owner to connect to the available sewer within one (1) year or such other time frame as approved by Council. This allows Council to make exceptions to the connection time frame through a site specific Council Resolution. No such requirement is in place for existing dwellings to connect to a storm sewer when it becomes available, however all new dwellings must connect their rear yard drainage system to the storm sewer if one exists.

For illustrative purposes, the figure below shows a typical lot layout including private drain connections (PDCs) for a new single family dwelling. The mainline sewer is the sewer in the municipal right-of-way that collects sanitary sewage or stormwater runoff from the homes on the block and conveys it downstream to a wastewater treatment facility for processing (sanitary) or to a receiving body of water (storm). The PDC is the individual service connection that conveys sewage or stormwater from the private dwelling to the mainline sewer. Although a portion of the PDC crosses the municipal right-of-way, the entire PDC is the responsibility of the private property owner.



TYPICAL PRIVATE DRAIN CONNECTION (P.D.C.) LAYOUT

When a new mainline sewer is constructed, typically a PDC is extended from the mainline sewer to the property line to avoid the Benefiting Properties having to excavate the roadway at a later date to install a new connection.

To connect to the sewer, the Benefiting Properties must:

- Pay servicing charges equal to the proportionate cost of the mainline sewer based on property frontage – for sanitary and storm as applicable
- Pay the full cost of the PDC within the public right-of-way, including cleanout – for sanitary and storm as applicable
- Pay for the decommissioning of their private septic system in accordance with City standards and install a PDC to connect the house’s plumbing to the PDC at the property line – for sanitary only
- Obtain permits from the City to connect to the PDC for sanitary and storm as applicable – for sanitary, this also includes modifying plumbing and decommissioning the septic system

Construction of sanitary sewers on roadways where residents and businesses are utilizing septic tanks has been a long-standing goal of the province and the City. Aging and substandard septic tanks and failed leaching beds cause environmental contamination to watercourses by delivering health-endangering contaminants, including bacteria, viruses, parasites and nitrate, through overflows to storm systems.

On August 24, 2020, Council approved a report regarding the Baseline Road Local Improvement (Baseline LIP) by Council Resolution CR432/2020 which allowed property owners with septic systems less than ten (10) years old to be given either three (3) years to connect to the available sewer system once it is made available or the difference between the age of their septic system and ten (10) years, whichever is greater. This acknowledges the costs incurred by the home owner for a new septic system and weighs it against the lesser risk of contamination afforded by newer septic systems.

Administration recommends the same extension be granted to property owners with septic systems less than ten (10) years old as was established for the Baseline LIP, as long as the property owner obtained a permit from the City for the septic system. This will provide assurance to the City that proper installation and inspection practices were followed. No such extension is necessary for storm sewers as there is no existing mechanism to require the existing dwelling to connect to a new storm sewer when it becomes available.

Septic System Changeover

Decommissioning of the private septic system to connect to the newly available sanitary sewer (“Septic Changeover”) is work that occurs on private property and the cost is the full responsibility of the property owner. Septic Changeover involves the removal and/or backfilling of the existing septic tank system, plumbing modifications in the home if required and construction of a PDC from the home to the property line.

Acknowledging the cost of a Septic Changeover, for the Baseline LIP, Council approved through CR476/2020 an option for a private local improvement under the provisions of Part III of O. Reg. 586/06 related to the decommissioning of the septic systems. This council resolution allows property owners in the Baseline LIP assessment area to pay 100% of the cost of undertaking the septic system changeover on private property

through assessment of special charges on the lots with the costs to be paid over a repayment period of up to twenty (20) years.

This is essentially a loan option where 100% of the cost of the Septic Changeover is placed on the property owner's taxes for the loan period with applicable interest at a rate deemed appropriate by administration. The loan balance can be paid by lump sum at any point during the loan.

Although it is recommended that this loan option be extended to property owners qualifying under recommendation I of this report, it should be noted that it is not mandatory for property owners to take advantage of this loan option. This process would be at the sole request of the property owner. Alternately, the property owner can hire a contractor and pay directly for the work on private property.

Local Improvement

The local improvement process allows properties (typically residential) to receive new services (e.g. sanitary or storm sewer, curb & gutter, streetlighting, sidewalks) where those services don't currently exist. The local improvement policy is administered in accordance with O. Reg. 586/06 and can be initiated in one of two ways:

1. By the Benefiting Properties through a sufficiently signed petition
2. By the City through notice to the Benefiting Properties

For sewer local improvements, following construction of the sewer, the Benefiting Properties are charged a flat rate per meter of frontage of their property for the cost of the mainline sewer plus a flat rate cost per PDC. For the purposes of this section, the mainline sewer and PDC flat rates refer to the sewers located on the public right-of-way only. Any work on the private property is completed and paid for directly by the property owner at their entire cost.

Local improvement flat rates are generally less than the actual full costs of construction of mainline sewers and private drain connections to encourage existing neighbourhoods to upgrade their services to municipal standards, including the elimination of septic systems where sanitary sewers don't exist. The local improvement rates represent a savings for a number of reasons:

- The rate excludes a portion of the cost related to hard surface restoration, roadway drainage and City owned frontage,
- The rates are based on competitive bids and savings based on magnitudes of scale, and
- The rates are based on installation costs for the minimum size sewer used by the City and avoids overcharging residents at the downstream end of a system where sewers must be larger to accept flows from upstream of the property in question.

Payment for local improvement charges may be submitted in one lump sum or through annual payments applied to the property tax bill. On April 27, 2020, Council received a

report from the City Engineer in response to CQ3-2020 “Extended Payment Terms for Drainage/Local Improvement Assessments”. As a result, Council passed CR136/2020 that states, “That a 20-year payment option for property owners to repay special charges for drainage and local improvements be implemented at interest rates deemed appropriate by administration.”

Administration recommends that the local improvement rates (sanitary, storm and PDC) and payment options noted above be extended to the properties that meet the criteria in Recommendations I & II of this report. The local improvement rates are established annually by Council during budget sessions when reviewing the Fees and Charges By-law.

Should the recommendations of this report be approved, Administration will make the necessary housekeeping amendments.

The private local improvement process under the provisions of Part III of O. Reg. 586/06 relating to Septic Changeover is less onerous in part due to the voluntary nature of the process. As noted in the Risk Analysis section below, local improvement charges administered under O. Reg. 586/06 have priority lien status which reduce risk to the Corporation. As such, recommendations IV & V of this report recommend administering this optional private Septic Changeover loan program under the provisions of Part III of O. Reg. 586/06.

Risk Analysis:

There are financial risks associated with the application of the local improvement flat rates. The portion of the cost not recoverable from the property owner would have to be funded through the general tax levy.

The local improvement process set out in O. Reg. 586/06 provides that local improvement charges can be added to taxes and these charges have priority lien status. This means that such charges are treated exactly the same way as property taxes. The resulting risk to the municipality of not collecting these funds is low. The process outlined in this report relating to the payment of servicing charges for the mainline sewer and PDC on public right-of-way can be collected using the property tax system. However, these charges do not have priority lien status and additional effort will be needed to collect defaulting payments. The overall risk is lower as the private Septic Changeover work is proposed to be administered under Part III of O. Reg. 586/06 and would have priority lien status.

Any extension of timelines to connect beyond the one (1) year requirement under By-law 9-2019 carries an increased risk that the owners of Benefitting Properties may change prior to the servicing fees being paid in full. The City is not involved in purchase and sale transactions during the sale of a property. If the seller does not notify the buyer that Septic Changeover is required and that sanitary servicing charges are pending, the buyer may not be aware of the significant costs they must incur. This risk should be considered when granting exceptions to By-law 9-2019.

Climate Change Risks

Climate Change Mitigation:

The recommendations of this report do not directly contribute to Greenhouse gas (GHG) emissions, however, extending local improvement charge rates and payment options to property owners may increase the likelihood of Septic Changeover in areas of the City currently on septic systems, thus reducing trucking of septic waste for wastewater treatment. Private septic systems are also a known minor source of fugitive GHG emissions including methane, carbon dioxide, and nitrous oxide which have not been estimated nor included in the Community GHG inventory.

Climate Change Adaptation:

In order for septic systems to properly filter wastewater, there must be enough unsaturated soil between the leachfield and the groundwater table. Changes in rainfall volumes and higher ground water levels can impact the effectiveness of leachfields increasing the risk of contamination of surface water.

Financial Matters:

As noted in the Risk Analysis section, the portion of the mainline sewer and private drain connection cost not recoverable from the property owner would have to be funded through the general tax levy. This would be accounted for as part of the Capital budget process on an annual basis.

Recommendations IV & V of this report is essentially the creation of a long-term loan program for Septic Changeovers and depending upon the economic conditions, largely interest rates at the time of changeover, may or may not be attractive to property owners. The property owner pays 100% of these costs. If the property owner chooses to receive financial assistance from the City by entering into this program, at the time the homeowner completes the required works the City would undertake to pay the contractor and a corresponding long-term loan would be set up from the property owner, which would decline each year until such time as the full cost, inclusive of interest is repaid. As indicated in the report, the interest rate to be applied is one that is deemed reasonable by Administration and is currently tied to municipal borrowing rates for capital projects plus 2 to 4% for Administrative costs incurred to manage and monitor the program.

Consultations:

Legal - Wira Vendrasco

Finance - Tony Ardovini, Linda Mancina

Building - Rob Vani

Right-of-Way - Adam Pillon, Amy Olsen

Engineering – Robert Perissinotti

Planning – Greg Atkinson, Neil Robertson

Conclusion:

The extension of local improvement rates for servicing of properties in the cases noted in this report provide a fair and equitable assessment of costs to the Benefiting Properties. Additionally, Administration recommends extending the private local improvement process in Part III of O.Reg. 586/06 for the decommissioning and changeover of septic systems when a sanitary sewer becomes available. This is consistent with the precedent set for the Baseline Road sanitary local improvement project. This means that residents paying for the construction of sanitary sewers by way of local improvement charges or as outlined in this report will be essentially paying the same costs. If approved, Administration will prepare the appropriate by-law and policies as necessary to effect these recommendations.

PLANNING ACT MATTERS:

N/A

Approvals:

Name	Title
Stacey McGuire	Manager of Development
France Isabelle-Tunks	Executive Director, Engineering / Deputy City Engineer
Chris Nepszy	Commissioner, Infrastructure Services
Shelby Askin Hager	Commissioner, Legal and Legislative Services
Janice Guthrie	Commissioner, Corporate Services CFO/City Treasurer
Joe Mancina	Chief Administrative Officer

Notification:

Name	Address	Email

Name	Address	Email

Appendices:

Subject: Closure of east/west alley between closed Fifth Street R.O.W. and E. C. Row Expressway westbound onramp; Closure of Hudson Avenue R.O.W. between 4505 Fourth Street and E. C. Row Expressway westbound onramp; Amend Alley Closing By-law 10354, Ward 2, SAA-6688

Reference:

Date to Council: July 4, 2023
Author: Brian Nagata, MCIP, RPP
Planner II - Development Review
(519) 255-6543 ext. 6181

Planning & Building Services
Report Date: May 18, 2023
Clerk's File #: SAA2023

To: Mayor and Members of City Council

Recommendation:

- I. THAT the 4.57-metre-wide east/west alley located between the closed Fifth Street right-of-way and E. C. Row Expressway westbound onramp at Huron Church Road, shown on Drawing No. CC-1831 (*attached* hereto as Appendix "A"), and hereinafter referred to as the "subject alley", **BE ASSUMED** for subsequent closure;
- II. THAT the subject alley **BE CLOSED AND CONVEYED** to the owner of the property known municipally as 0 Hudson Avenue (legally described as Lots 94 & 95, Plan 1154) and as necessary, in a manner deemed appropriate by the City Planner, subject to the following;
 - a. Location survey to determine if existing City of Windsor underground street light conductor is situated within the subject alley; and
 - b. Relocation of City of Windsor underground street light conductor from the subject alley if deemed necessary by EnWin Utilities Ltd.
- III. THAT the 20.12-metre-wide Hudson Avenue right-of-way located between the property known municipally as 4505 Fourth Street (P.I.N. No. 01262-1533) and E. C. Row Expressway westbound onramp at Huron Church Road, shown on Drawing No. CC-1831 (*attached* hereto as Appendix "A"), and hereinafter referred to as the "subject right-of-way", **BE ASSUMED** for subsequent closure;

- IV. THAT the subject right-of-way **BE CLOSED AND CONVEYED** to the owner of the property known municipally as 0 Hudson Avenue (legally described as Lots 94 & 95, Plan 1154) and as necessary, in a manner deemed appropriate by the City Planner;
- V. THAT Conveyance Cost **BE SET** as follows:
- a. For right-of-way and alley abutting lands zoned MD1.4, \$7.00 per square foot without easements plus HST (if applicable), and \$3.50 per square foot with easements plus HST (if applicable). Survey cost and deed preparation cost included.

- VI. THAT Alley Closing By-law 10354, adopted on July 16, 1990, and registered on title on July 24, 1990 as Registration No. R1135300 **BE AMENDED** as follows:

By **DELETING** the following wording under section 2. to the By-law:

That each of the owners whose lands abut upon lands described herein shall have the right to purchase, at a price of \$1.00 per square foot, that part thereof upon which his land abuts to the middle line of such closed up and stopped up part; provided, however, that any such owner shall notify the Clerk of The Corporation of the City of Windsor, in writing, of his intention to exercise his right to purchase by not later than July 27, 1990, provided that, if such owner does not exercise his right to purchase on or before such date, the Council may sell the part that he has the right to purchase to any other person at the same or a greater price, as the Council shall see fit.

And **INSERTING**:

THAT Conveyance Cost **BE SET** as follows:

- a. For alley abutting lands zoned MD1.4, \$7.00 per square foot without easements plus HST (if applicable), and \$3.50 per square foot with easements plus HST (if applicable). Survey cost and deed preparation cost included.
- VII. THAT The City Planner **BE REQUESTED** to supply the appropriate legal description, in accordance with Drawing No. CC-1831, *attached* hereto as Appendix "A".
- VIII. THAT The City Solicitor **BE REQUESTED** to prepare the necessary by-law(s).
- IX. THAT The Chief Administrative Officer and City Clerk **BE AUTHORIZED** to sign all necessary documents approved as to form and content satisfactory to the City Solicitor.

Executive Summary:

N/A



STREET & ALLEY CLOSING (SAA/6688)

1:750

APPLICANT : HALLER ENTERPRISES INC.

 SUBJECT LANDS

PLANNING DEPARTMENT - DEVELOPMENT DIVISION

DATE: DECEMBER, 2022



Background:

The applicant, Haller Enterprises Inc. (Richard Haller), owner of the properties known municipally as 0 Continental Avenue (Roll No. 080-660-09900), 0 Hudson Avenue (Roll No. 080-680-01300) and 0 Hudson Avenue (Roll No. 080-680-02200) (the subject property) (Refer to Figure 4 below), applied to close the 4.57-metre-wide east/west alley located between the closed Fifth Street right-of-way and E. C. Row Expressway westbound onramp at Huron Church Road (the alley), and the 20.12-metre-wide Hudson Avenue right-of-way located between 4505 Fourth Street and E. C. Row Expressway westbound onramp at Huron Church Road (the right-of-way), and shown on Drawing No. CC-1831 *attached* hereto as Appendix “A”, and also shown on the aerial photo *attached* hereto as Appendix “B”.

The subject property, the alley and the right-of-way, together with the City owned properties known municipally as 0 Coney Street (Roll No. 080-680-04600), 0 Continental Avenue (Roll No. 080-660-09800) and 0 Hudson Avenue (Roll No. 080-680-02100) (Refer to Figure 5 below) have been used as one contiguous parcel since at least 1996 (Refer to Figure 3 below). 0 Continental Avenue (Roll No. 080-660-09800) is part of the alley closed by By-law 10354 on July 16, 1990.



Figure 1 - 1978 Aerial Photo



Figure 2 - 1996 Aerial Photo

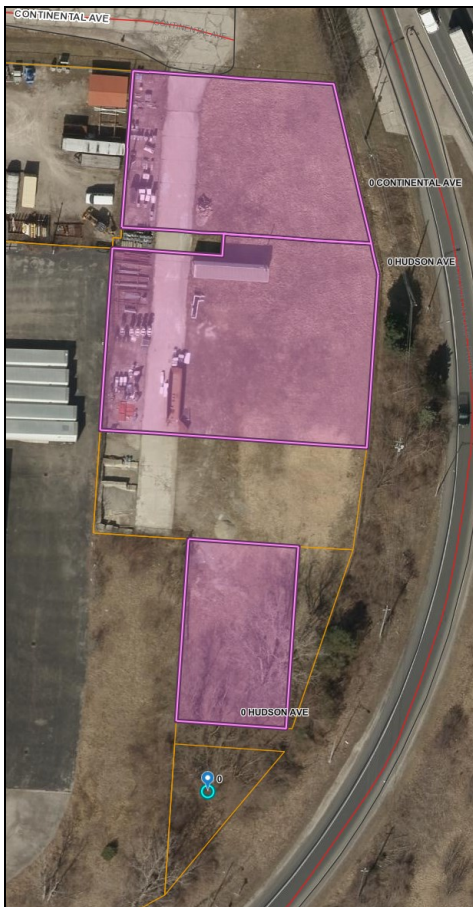


Figure 4 - Subject Property



Figure 5 - City Owned Properties

The alley is unmaintained and composed of heavy vegetation in a natural state. The alley was established by Registered Plan of Subdivision 1154, registered on December 2, 1924, and used for agricultural purposes until being allowed to naturalize. There are no encroachment agreements in place for use of the alley.

The right-of-way is unmaintained and composed primarily of landscaped open space yard (grass), save and except for a concrete driveway and gravel area at its westerly

end. The concrete driveway and gravel area have been in place since at least 1978 (Refer to Figure 2 above). The concrete driveway extends north to Continental Avenue. The gravel area is part of a linear outdoor storage yard that borders the concrete driveway to the west. The right-of-way was established by Registered Plan of Subdivision 1154, registered on December 2, 1924, and used for agricultural purposes until the concrete driveway and gravel area were established. There are no encroachment agreements in place for use of the right-of-way.

The applicant wishes to close the alley and right-of-way, and purchase the City owned lands for the purpose of consolidating them with the subject property to create one large contiguous parcel of land.

Discussion:

The decision to recommend closure of an alley or right-of-way is derived from the City's *Classification of Alleys and Suitability for Closure* guideline document (the document), **attached** hereto as **Appendix "E"**. The document details four classifications of alleys/right-of-ways based on their usefulness, and provides corresponding criteria for determining suitability for closure.

Classification of Public Right-of-Ways

The initial step is to determine if the alley is indispensable. This is achieved through the evaluation of the following criteria set forth in Section 1 of the document.

1. *Does the subject alley and right-of-way serve commercial properties?*
 - a. The alley and right-of-way do not serve any commercial properties.
2. *Does the subject alley and right-of-way serve properties fronting on heavily traveled streets i.e. major arterial routes?*
 - a. The alley and right-of-way do not serve properties fronting on heavily travelled streets.
3. *Does the subject alley and right-of-way contain sewers, and must the alley remain accessible for servicing?*
 - a. The alley and right-of-way do not contain any sewers.
4. *Does the subject alley and right-of-way serve as the only vehicular means of access to rear parking areas and garages where the property has insufficient lot width for a side drive?*
 - a. The alley and right-of-way do not serve as the only vehicular means of access to any rear parking areas and garages.

5. *Does the subject alley and right-of-way contain Fire Department connections that are deemed to be necessary for firefighting access?*
 - a. The alley and right-of-way do not contain any Fire Department connections.

6. *Does the subject alley and right-of-way lie within a Holding zone or other similar undeveloped areas where the right-of-way system is clearly obsolete and has never been developed, but where the City needs to keep its options open until new area plans are prepared and development is imminent?*
 - a. The alley and right-of-way do not lie within a Holding zone or other similar undeveloped area.

Based on the above, the Planning Department deems the alley and right-of-way “dispensable” and supports the requested closure.

It is our recommendation that, upon closure, the owner of the subject property be given a chance to acquire the alley and right-of-way. Hence the recommendation is to close and convey the alley to the owner of the subject property.

This recommendation is contrary to the standard manner of conveyance offering abutting property owners first right to purchase their half of the alley or right-of-way. The rationale for this recommendation is as follows:

1. Alley
 - a. The other two properties abutting the alley are City owned
 - b. The City has no use for the alley

2. Right-of-Way
 - a. One of the two other properties abutting the right-of-way is City owned
 - b. The City has no use for the right-of-way
 - c. The owner of the other property abutting the right-of-way (4505 Fourth Street) has expressed no interest in purchasing it
 - d. The right-of-way has been encroached on by the subject property since at least 1978 (Refer to Figure 2 above)

Risk Analysis:

The recommended closures will divest the City of associated liability risks and maintenance costs. The recommended closure poses no known risk to City.

Climate Change Risks

Climate Change Mitigation:

N/A

Climate Change Adaptation:

N/A

Financial Matters:

The conveyance price for a closed alley and right-of-way abutting lands zoned MD1.4 is assessed at \$7.00 per square foot without easements plus HST (if applicable), and \$3.50 per square foot with easements plus HST (if applicable). This includes the survey cost and deed preparation costs. This conveyance price also applies to the part of the alley closed by By-law 10354. This consequently means that By-law 10354 must be amended in order to update the conveyance price from the original rate assessed at \$1.00 per square foot, based on current market values in 1990.

Consultations:

Consultations were held with Municipal Departments and Utility Companies, which resulted in the information found in *attached* hereto as Appendix "C".

Notice of this application was issued to property owners abutting the alley and right-of-way by regular mail, with no objections being received as of the date of writing this report.

Notice of Development & Heritage Standing Committee meeting and Council meeting are published in the Windsor Star prior to each of the meetings. In addition, notice of each of the public meetings will be mailed to the abutting/affected property owners prior to the meetings.

Conclusion:

The Planning Department recommends closure of the alley and right-of-way shown on attached Appendix "A".

The closed alley and right-of-way are to be conveyed to the owner of the subject property as in Recommendations II and IV of this report respectively.

The Planning Department recommends that By-law 10354 be amended to allow for 0 Continental Avenue (P.I.N. No. 01262-1548) to be conveyed to the owner of the subject property as in Recommendation VI of this report.

Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Greg Atkinson, MCIP, RPP
Acting Manager of Policy Planning

Thom Hunt, MCIP, RPP
City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP

JM

Approvals:

Name	Title
Greg Atkinson	Acting Manager of Planning Policy/Deputy City Planner
Neil Robertson for Thom Hunt	City Planner / Executive Director, Planning & Development Services
Stephanie Santos	Coordinator of Real Estate Services
Kate Tracey	Senior Legal Council, Legal Services & Real Estate
Jelena Payne	Commissioner, Economic Development & Innovation
Joe Mancina	Chief Administration Officer

Notifications:

Name	Address	Email
Ward 2 Councillor Fabio Costante	350 City Hall Square West, Suite 220 Windsor, ON N9A 6S1	fcostante@citywindsor.ca
List of mailing labels for property owners abutting alley issued to Clerks office		

Appendices:

- 1 Appendix A - Drawing No. CC-1831
- 2 Appendix B - EIS Drawing - Aerial Photo
- 3 Appendix C - Consultations with Municipal Departments and Utility Companies
- 4 Appendix D - Site Photos
- 5 Appendix E - Classification of Alleys and Suitability for Closure

APPENDIX "B"

EIS Drawing - Aerial Photo



STREET & ALLEY CLOSING (SAA/6688)

1:750

APPLICANT : HALLER ENTERPRISES INC.



 SUBJECT LANDS

PLANNING DEPARTMENT - DEVELOPMENT DIVISION

DATE: DECEMBER, 2022

APPENDIX “C”

Consultations with Municipal Departments and Utility Companies

BELL CANADA WSP

March 7, 2022 (Alley)

Bell has no concerns with the proposed lane closure.

January 10, 2023 (Right-of-Way)

Bell has no requirements

[Charleyne Hall, Bell Canada External Liaison - Right-of-Way]

COGECO CABLE SYSTEMS INC.

No comments provided

ENBRIDGE GAS

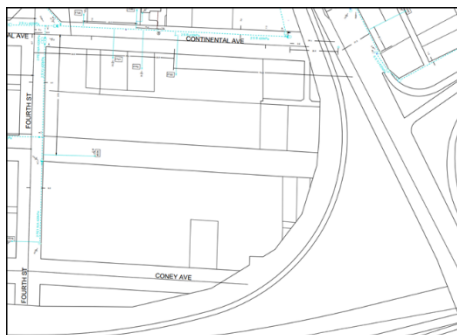
April 1, 2022 (Alley)

After reviewing the provided drawing and consulting our mapping system, please note that Enbridge Gas has no active infrastructure in the proposed area. A PDF drawing has been attached for reference.

Also, please note the following should you find any abandoned infrastructure in the area:

- Any pipe that is excavated, please assume that it is live
- If during the course of any job, any pipe is found that is not on the locate sheet and is in conflict with your work, please call our emergency number (1-877-969-0999), and one of our Union Gas representatives will respond to determine if that plant is in fact live or dead
- Please note that our Enbridge Gas representative will respond to the live or dead call within 1-4 hours, so please plan your work accordingly

[Jose Delloso, Drafter Estimator]

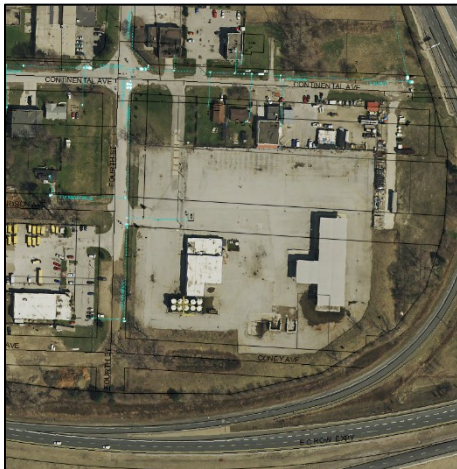


January 16, 2023 (Right-of-Way)

After reviewing the provided drawing for 0 Hudson and consulting our mapping system, please note that Enbridge Gas has no active infrastructure in the proposed area. A PDF drawing has been attached for reference.

Also, please note the following should you find any abandoned infrastructure in the area:

- Any pipe that is excavated, please assume that it is live
- If during the course of any job, any pipe is found that is not on the locate sheet and is in conflict with your work, please call our emergency number (1-877-969-0999), and one of our Union Gas representatives will respond to determine if that plant is in fact live or dead
- Please note that our Enbridge Gas representative will respond to the live or dead call within 1-4 hours, so please plan your work accordingly



[Gord Joynson, Drafter Estimator]

ENVIRONMENTAL SERVICES

No concerns from Environmental Services.

[Anne-Marie Albidone, Manager, Environmental Services]

ENWIN UTILITIES - HYDRO

April 4, 2022 (Alley)

No objection to alley closing.

[Anwar Nagar, Senior Hydro Engineering Technologist]

January 18, 2023 (Right-of-Way)

No objection, however, it appears that existing City of Windsor street light conductor may be present underground beneath the southernmost section of land to be closed. Location

surveys will be required to determine the location of the underground conductor, which may require relocation to accommodate the proposed alley closing.



[Jeremy Allossery, Hydro Engineering Technologist]

ENWIN UTILITIES - WATER

Water Engineering has no objections.

[Bruce Ogg, Water Project Review Officer]

LEGAL DEPARTMENT

For right-of-way and alley lands abutting lands zoned MD 1.4, \$7.00/square foot without easements and \$3.50/square foot with easements. Survey cost and deed preparation cost included.

For alley lands abutting City of Windsor owned lands, retain ½ of alley for municipal purposes. No charge.

[Stephanie Santos, Coordinator of Real Estate Services]

MNSi

MNSi does not require an easement through the subject properties.

[Dave Hartleib, Outside Plant Manager]

MTO

MTO has no concerns with the proposed closing.

[Ryan Mentley, Corridor Management Planner]

PARKS & FACILITIES

Please note that the Parks Design & Development has no comments pertaining this SAA/6688 LIAISON.

[Sherif Barsom, Landscape Architect]

PLANNING DEPARTMENT

No comments provided

PLANNING DEPARTMENT - LANDSCAPE ARCHITECT

As per Park Design and Development comments, there are no objections to the proposed closure from a landscape architectural perspective.

[Stefan Fediuk - Landscape Architect]

PUBLIC WORKS - ENGINEERING

The subject alley closure is approximately 24 meters long by 5.5 meters wide, and composed of grass and trees. There are no hydro poles, overhead wires, municipal sewers or manholes located in the alley. This alley appears to serve no useful purpose by CR146/2005; therefore, we have no objections to the closure.

[Adam Pillon - Manager Right-of-Way]

PUBLIC WORKS - TRAFFIC

March 10, 2022

The alley shown is located between private property and MTO owned ROW. They are not required for vehicular or pedestrian access. They alley may be closed as proposed.

Also suggest closing the adjacent City owned parcels to the south and to the north.

January 5, 2023

The alley shown is located between private property and MTO owned ROW. They are not required for vehicular or pedestrian access. They alley may be closed as proposed.

Recommend closing the adjacent City owned parcels to the south and to the north to reduce maintenance.

[Mike Spagnuolo, Signal Systems Analyst]

ROGERS COMMUNICATIONS

No comments provided

TELUS COMMUNICATIONS

March 7, 2022 (Alley)

TELUS has no underground infrastructure in the area of your proposed work.

[Meghna Patel, Permit Coordinator]

January 6, 2023 (Right-of-Way)

TELUS has no underground infrastructure in the area of your proposed work.

[Derek Dukhu, CAD Technician]

TRANSPORTATION PLANNING

No concerns with the proposed closure.

[Rania Toufelli, Policy Analyst]

TRANSIT WINDSOR

No comments provided

WINDSOR FIRE

No objections

[Michael Coste, Chief Fire Prevention Officer]

WINDSOR POLICE**March 16, 2022**

The Windsor Police Service has no concerns or objections with the closure of this section of alley. Its location carries no impact to police service delivery capability in any way.

January 18, 2023

The Windsor Police Service has no concerns or objections with the closure of these sections of alley and road rights-of-way. The end result from this will not carry any negative impact to police incident response or service delivery capacity for the affected properties.

[Barry Horrobin, Director of Planning & Physical Resources]

APPENDIX "D"
Site Photos (June 28, 2022)



Figure 1 - Looking east towards alley from outdoor storage yard at Penske Truck Rental



Figure 2 - Looking east towards alley from edge of outdoor storage yard at Penske Truck Rental



Figure 3 - Outdoor storage yard at Penske Truck Rental

APPENDIX “E”

Classification of Alleys and Suitability for Closure

Classification of Public Rights-of-Ways:

Currently streets and alleys fall into four classifications on the basis of their usefulness:

- 1) Alleys that are **indispensable**. These would be alleys serving commercial properties and properties fronting on heavily traveled streets i.e. major arterial routes and alleys which contain sewers and must remain accessible for servicing; alleys or streets which serve as the only vehicular means of access to rear parking areas and garages where the property has insufficient lot width for a side drive; and, alleys which contain Fire Department connections that are deemed to be necessary for firefighting access.
- 2) Alleys that, **have some usefulness**, are nevertheless dispensable and may or may not be a complete liability.
- 3) Alleys that appear to serve **no useful purpose**, either now, or anticipated. Such alleys are in residential areas and locations where generally the lots are wide enough for side drives, or those alleys abutting parks and other parcels of land that do not require any servicing from the alley. Remnant or stub-end streets which are dead-ended and do not serve as access to other streets.
- 4) Alleys lying in Holding zones and other similar undeveloped areas where the alley system is **clearly obsolete** and has never been developed, but where the City needs to keep its options open until new area plans are prepared and development is imminent.

Suitability for Closing:

Following are the criteria and suitability for closing alleys in each of the above classifications.

- 1) Indispensable alleys should **not be closed**, conveyed, reduced or otherwise jeopardized through minority interests unless a suitable substitute alley is opened in lieu thereof. They are essential from the viewpoint of fire protection, police protection, emergency services (i.e. ambulance) and loading or unloading of goods, refuse collection, servicing of blocked sewers and utility services. Without such alleys, the above noted services would at least be more costly if not impossible to complete or adequately access; and would noticeably interfere with street traffic, thereby reducing the access capacity of the adjacent arterial, collector, or street for business.
- 2) Alleys having some usefulness should **be considered for closing** only upon request of abutting owners rather than by encouragement of the City.
- 3) Alleys that serve no useful purpose should **be closed** if at all possible, and in fact the owners abutting thereon should be encouraged to accept conveyance.
- 4) Alleys that are clearly obsolete should **not be closed** unless there is a municipal need or specific development proposals acceptable to the City are submitted.

Subject: Closure of east/west alley between Rockwell Avenue and Closed Woodland Avenue; east/west alley between Closed Woodland Avenue and Granada Avenue West, Ward 1, SAA-6921

Reference:

Date to Council: July 4, 2023
Author: Brian Nagata, MCIP, RPP
Planner II - Development Review
(519) 255-6543 ext. 6181

Planning & Building Services
Report Date: May 25, 2023
Clerk's File #: SAA2023

To: Mayor and Members of City Council

Recommendation:

- I. THAT the 4.27-metre-wide east/west alley located between Rockwell Avenue and Closed Woodland Avenue, and shown on Drawing No. CC-1825 (*attached* hereto as Appendix "A"), and hereinafter referred to as the "subject west alley", **BE ASSUMED** for subsequent closure;
- II. THAT the subject west alley **BE CLOSED AND CONVEYED** to the abutting property owners and as necessary, in a manner deemed appropriate by the City Planner, subject to the following:
 - a. 3.0-metre-wide easement (measured 1.50 metres from either side of utility infrastructure), subject to there being accepted in the City's standard form and in accordance with the City's standard practice, be granted to:
 - i. Bell Canada to accommodate existing aerial facilities;
 - ii. ENWIN Utilities Ltd. to accommodate existing overhead 120/240V hydro pole distribution with guy and anchor; and
 - iii. MNSi to accommodate existing aerial facilities.
- III. THAT the 4.88-metre-wide east/west alley located between Closed Woodland Avenue and Granada Avenue West, and shown on Drawing No. CC-1825 (*attached* hereto as Appendix "A"), and hereinafter referred to as the "subject east alley", **BE ASSUMED** for subsequent closure;
- IV. THAT the subject east alley **BE CLOSED AND CONVEYED** to the abutting property owners and as necessary, in a manner deemed appropriate by the City Planner, subject to the following:

- a. 3.0-metre-wide easement (measured 1.50 metres from either side of utility infrastructure), subject to there being accepted in the City's standard form and in accordance with the City's standard practice, be granted to:
 - i. Bell Canada to accommodate existing aerial facilities;
 - ii. ENWIN Utilities Ltd. to accommodate existing overhead 120/240V hydro pole distribution with guy and anchor; and
 - iii. MNSi to accommodate existing aerial facilities.
 - b. Ontario Land Surveyor be directed to use existing encroachments for determining the boundaries of the lands to be conveyed to each abutting property owner.
- V. THAT Conveyance Cost **BE SET** as follows:
- c. For alley conveyed to abutting lands zoned RD1.4, \$1.00 plus HST (if applicable), deed preparation fee and proportionate share of the survey costs as invoiced to The Corporation of the City of Windsor by an Ontario Land Surveyor.
- VI. THAT The City Planner **BE REQUESTED** to supply the appropriate legal description, in accordance with Drawing No. CC-1825, *attached* hereto as Appendix "A".
- VII. THAT The City Solicitor **BE REQUESTED** to prepare the necessary by-law(s).
- VIII. THAT The Chief Administrative Officer and City Clerk **BE AUTHORIZED** to sign all necessary documents approved as to form and content satisfactory to the City Solicitor.

Executive Summary:

N/A



STREET & ALLEY CLOSING (SAA/6921)

1:1,500

APPLICANT : MUNTADHER TUAMA & ABBAS TUAMA

 REQUESTED & TO BE RECOMMENDED FOR CLOSURE  TO BE RECOMMENDED FOR CLOSURE

PLANNING DEPARTMENT - PLANNING POLICY

DATE: NOVEMBER, 2022



Background:

The applicant, Muntadher Tuama, owner of the property known municipally as 760 Cabana Road West (the subject property), applied to close the portion of east/west alley located between Rockwell Avenue and Closed Woodland Avenue, abutting the subject property.

The Planning Department is recommending closure of the full east/west alley located between Rockwell Avenue and Closed Woodland Avenue (the west alley), as well as the east/west alley located between Closed Woodland Avenue and Granada Avenue West (the east alley), and shown on Drawing No. CC-1825 *attached* hereto as Appendix “A”, and also shown on the aerial photo *attached* hereto as Appendix “B”.

The east alley was established by Registered Plan of Subdivision 1305, registered on May 14, 1928, and used for agricultural purposes until approximately 1953 when the abutting lands began to be developed as single family dwelling lots. Over the years

abutting properties encroached into the alley with accessory buildings, structures and fences. Each lot roughly encroached into its abutting half of the alley. The alley also contains utility poles with guy wires and anchors, as well as several mature trees. There are no Encroachment Agreements on record for the use of the alley.

The west alley was established by Registered Plan of Subdivision 1345, registered on January 30, 1929, and used for agricultural purposes until approximately 1950 when the abutting lands began to be developed as single family dwelling lots. Over the years abutting properties encroached into the alley with structures and fences. The subject property encroached into the abutting portion of the alley, which nearly divides the lot into two separate parcels. 780 Cabana Road West encroached into almost all of the remainder of the alley, save and except a small strip encroached into by 3790 Rockwell Avenue. The alley also contains utility poles with guy wires and anchors, as well as some mature trees. There are no Encroachment Agreements on record for the use of the alley.

Woodland Avenue was established by Registered Plan of Subdivision 1305, registered on May 14, 1928, and used for agricultural purposes until approximately 1950 when the abutting lands began to be developed as single family dwelling lots. Woodland Avenue was officially closed by Judges Order on October 25, 1963.

The applicant wishes to close the abutting portion of the west alley for the purpose of rounding out the irregular shaped subject property, thereafter making it more functional for future development and/or use.

Discussion:

The decision to recommend closure of an alley is derived from the City's *Classification of Alleys and Suitability for Closure* guideline document (the document), *attached* hereto as Appendix "E". The document details four classifications of alleys based on their usefulness, and provides corresponding criteria for determining suitability for closure.

Classification of Public Right-of-Ways

The initial step is to determine if the alley is indispensable. This is achieved through the evaluation of the following criteria set forth in Section 1 of the document.

1. *Does the subject alley serve commercial properties?*
 - a. The east alley and west alley do not serve any commercial properties.
2. *Does the subject alley serve properties fronting on heavily traveled streets i.e. major arterial routes?*
 - a. The east alley and west alley do not serve properties on heavily traveled streets.
3. *Does the subject alley contain sewers, and must the alley remain accessible for servicing?*

- a. The east alley and west alley do not contain any sewers.
4. *Does the subject alley serve as the only vehicular means of access to rear parking areas and garages where the property has insufficient lot width for a side drive?*
- a. The east alley and west alley do not serve as a means of vehicular access to any rear garages or parking areas.
5. *Does the subject alley contain Fire Department connections that are deemed to be necessary for firefighting access?*
- a. The east alley and west alley do not contain any Fire Department connections.
6. *Does the subject alley lie within a Holding zone or other similar undeveloped areas where the right-of-way system is clearly obsolete and has never been developed, but where the City needs to keep its options open until new area plans are prepared and development is imminent?*
- a. The east alley and west alley do not lie within a Holding zone or other similar undeveloped areas.

Based on the above, the Planning Department deems the east alley and west alley “dispensable”, and supports the requested closure.

It is our recommendation that, upon closure, the abutting property owners be given the chance to acquire the east alley and west alley in the manner described in the Recommendation section herein.

Hence the recommendation is to close and convey the east alley and west alley to the abutting property owners, which is the standard manner of conveyance. This includes 780 Cabana Road West in order to be as fair as possible in conveying the alley which no abutting property owners have permission to encroach upon. This also includes direction to the Ontario Land Surveyor to use existing encroachments for determining the boundaries of the lands to be conveyed to each abutting property owner.

Risk Analysis:

The recommended closure will divest the City of associated liability risks and maintenance costs. The recommended closure poses no known risk to City.

Climate Change Risks

Climate Change Mitigation:

N/A

Climate Change Adaptation:

N/A

Financial Matters:

The rate for an alley conveyed to abutting lands zoned RD1.4 is assessed at \$1.00 plus HST (if applicable), deed preparation fee and proportionate share of the survey costs as invoiced to the City by an Ontario Land Surveyor.

Consultations:

Consultations were held with Municipal Departments and Utility Companies, which resulted in the information found in *attached* hereto as Appendix “C”.

Notice of this application was issued to property owners abutting the east alley and west alley by regular mail, with no objections being received as of the date of writing this report.

Notice of Development & Heritage Standing Committee meeting and Council meeting are published in the Windsor Star prior to each of the meetings. In addition, notice of each of the public meetings will be mailed to the abutting/affected property owners prior to the meetings.

Conclusion:

The Planning Department recommends closure of the east alley and west alley shown on attached Appendix “A”, subject to easements in favour of Bell Canada, ENWIN Utilities Ltd. and MNSi as in Recommendations II and IV of this report respectively.

The closed east alley and west alley are to be conveyed to the abutting property owners as in Recommendations II and IV of this report, respectively.

Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Greg Atkinson, MCIP, RPP *Thom Hunt, MCIP, RPP*
Acting Manager of Policy Planning *City Planner*

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP *JM*

Approvals:

Name	Title
Greg Atkinson	Acting Manager of Planning Policy/Deputy City Planner
Neil Robertson for Thom Hunt	City Planner / Executive Director, Planning & Development Services
Stephanie Santos	Coordinator of Real Estate Services

Name	Title
Kate Tracey	Senior Legal Council, Legal Services & Real Estate
Jelena Payne	Commissioner, Economic Development & Innovation
Joe Mancina	Chief Administration Officer

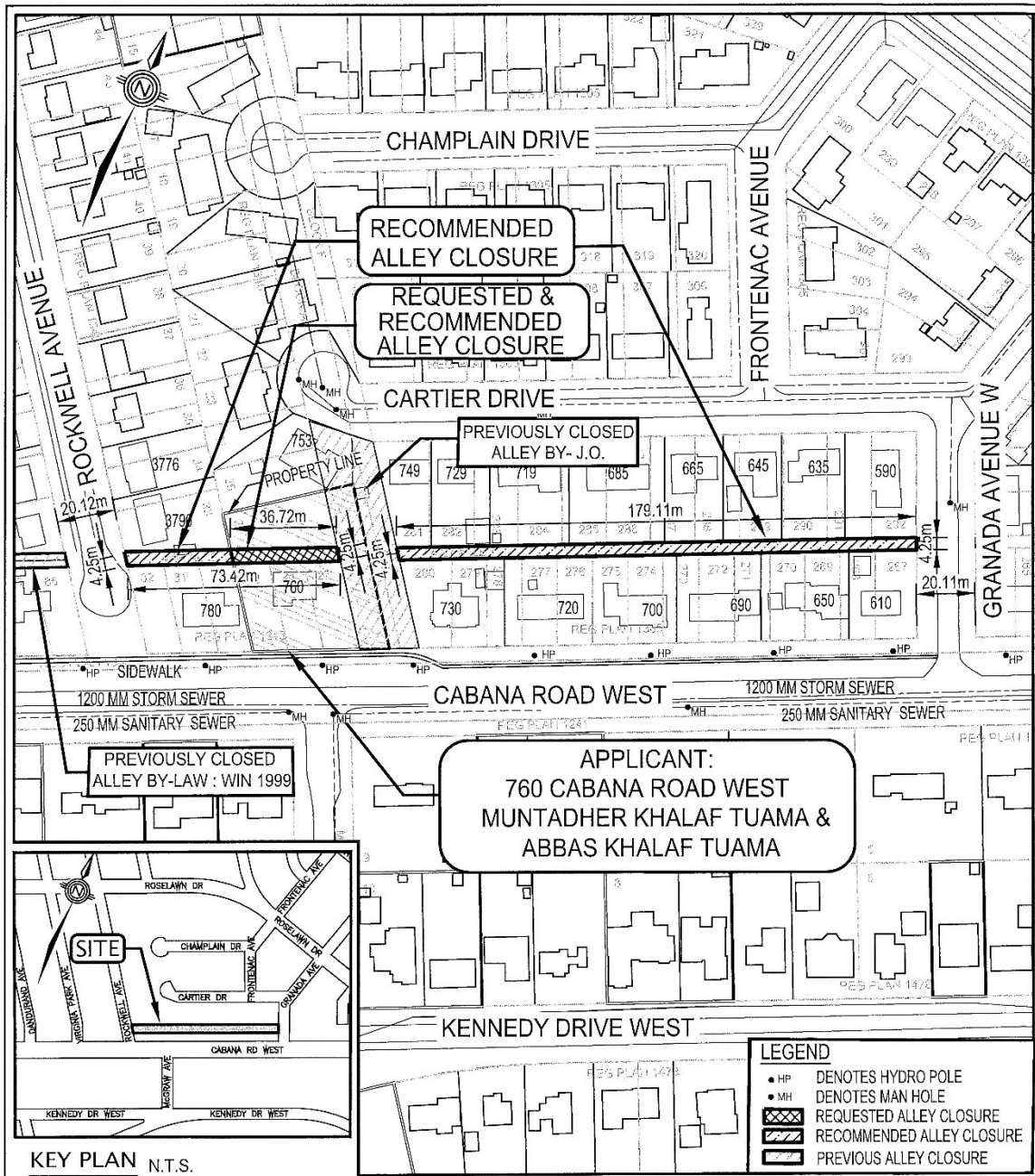
Notifications:

Name	Address	Email
Ward 1 Councillor Fred Francis	350 City Hall Square West, Suite 220 Windsor, Ontario N9A 6S1	ffrancis@citywindsor.ca
List of mailing labels for property owners abutting alley issued to Clerks office		

Appendices:

- 1 Appendix A - Drawing No. CC-1825
- 2 Appendix B - EIS Drawing - Aerial Photo
- 3 Appendix C - Consultations with Municipal Departments and Utility Companies
- 4 Appendix D - Site Photos
- 5 Appendix E - Classification of Alleys and Suitability for Closure

APPENDIX "A" Drawing No. CC-1825



THE CORPORATION OF THE CITY OF WINDSOR - ENGINEERING DEPARTMENT
**Proposed East/West Alley closure in between Cabana Road West and Cartier Dr,
 from Granada Avenue West to Rockwell Avenue**

 Kirk Tamm, Manager of Geomatics	SCALE: 1:1750	DATE: DEC 2022	REVISED: —	DWG. NO. CC-1825
	DWN BY: UT	CHKD BY: PJU / MM	REVISION NO.: —	

APPENDIX "B" EIS Drawing - Aerial Photo



STREET & ALLEY CLOSING (SAA/6921)

1:1,500

APPLICANT : MUNTADHER TUAMA & ABBAS TUAMA

 REQUESTED & TO BE RECOMMENDED FOR CLOSURE
  TO BE RECOMMENDED FOR CLOSURE

PLANNING DEPARTMENT - PLANNING POLICY

DATE: NOVEMBER, 2022



APPENDIX “C”
Consultations with Municipal Departments and Utility Companies

BELL CANADA WSP

Bell Canada requests a 3.0 m wide easement, 1.5 m on either side of the existing aerial facilities for the length of the proposed closure area. If the recommended area is also closed, we'd like to extend our request over that portion as well.

[Charleyne Hall - Bell Canada External Liaison, Right of Way & Indigenous Relation]

COGECO CABLE SYSTEMS INC.

No comments provided

ENBRIDGE GAS

After reviewing the provided drawing for the Alley closing north of Cabana Rd, and consulting our mapping system, please note that Enbridge Gas has active infrastructure outside the alleyway. A PDF drawing has been attached for reference.

Please Note:

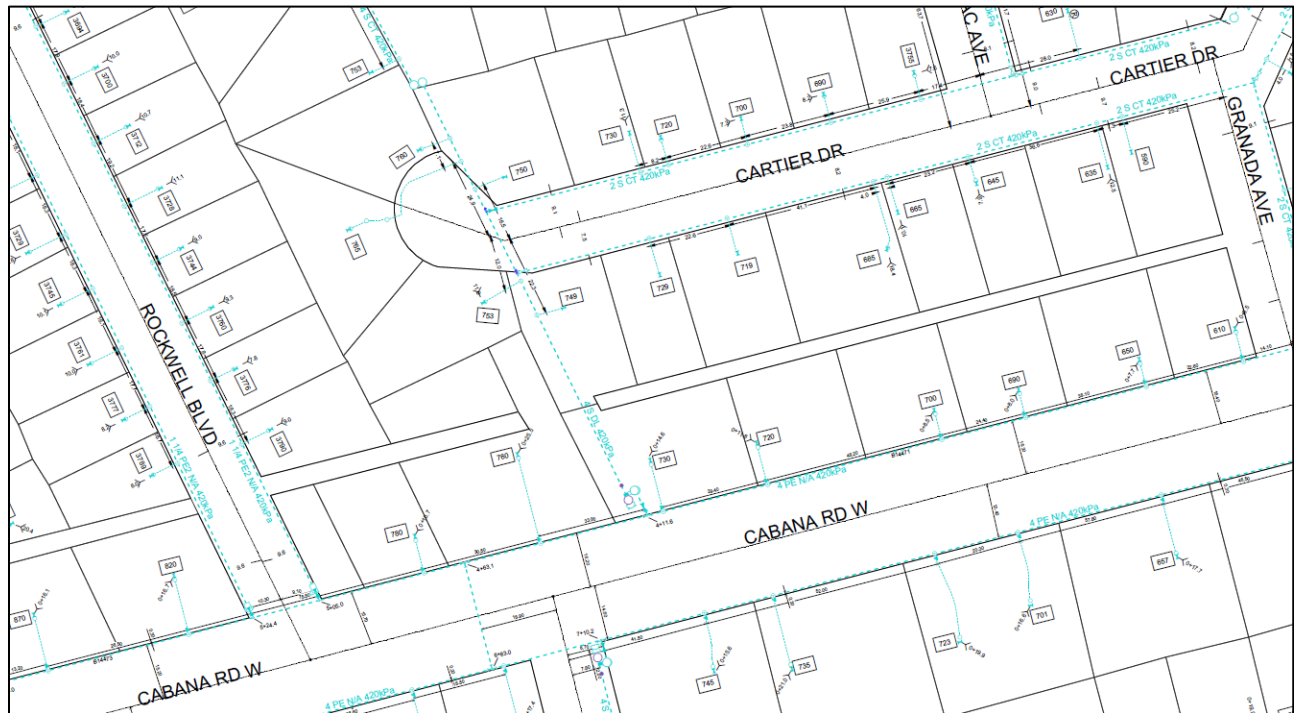
1. The shown piping locations are approximate and for information purposes only
2. The drawings are not to scale
3. This drawing does not replace field locates. Please contact Ontario One Call for onsite locates prior to excavating, digging, etc.

Enbridge Gas requires a minimum separation of 0.6 m horizontal and 0.3 m vertical from all of our plant less than NPS 16 and a minimum separation 1.0 m horizontal and 0.6 m vertical between any CER-regulated and vital pipelines. For all pipelines (including vital pipelines), when drilling parallel to the pipeline, a minimum horizontal clearance measured from the edge of the pipeline to the edge of the final bore hole of 1 m (3.3 ft.) is required. Please ensure that this minimum separation requirement is maintained, and that the contractor obtains locates prior to performing any work and utilizes safe excavation practices while performing any work in the vicinity.

Also, please note the following should you find any abandoned infrastructure in the area:

- Any pipe that is excavated, please assume that it is live
- If during the course of any job, any pipe is found that is not on the locate sheet and is in conflict with your work, please call our emergency number (1-877-969-0999), and one of our Union Gas representatives will respond to determine if that plant is in fact live or dead
- Please note that our Enbridge Gas representative will respond to the live or dead call within 1-4 hours, so please plan your work accordingly

[Sandro Aversa - Drafter / Estimator, Construction & Growth]



ENVIRONMENTAL SERVICES

No concerns from Environmental Services.

[Anne-Marie Albidone - Manager, Environmental Services]

ENWIN UTILITIES - HYDRO

No objection provided clearances are maintained from our distribution plant. However, an easement named to ENWIN Utilities Ltd., will be required for East to the West limit of the property to accommodate for the existing overhead 120/240V hydro pole distribution.

Please see attached for the highlighted area of easement needed.

Please note the following distribution and services:

- Overhead 120/240V triplex secondary distribution pole line and associated down guy wires/ anchors servicing multiple residential houses along the alleyway.
- Overhead 16kV primary distribution pole line adjacent to the west limit of the 730 Cabana Rd W. and 749 Cartier Dr.

Proposed buildings and/or building additions must have adequate clearance requirements from all hydro distribution and services.

We recommend referring to the Occupational Health and Safety Act for minimum safe limits of approach during construction and the Ontario Building Code for adequate clearance requirements for New Buildings and/or Building Additions.

ENWINs easement guidelines:

Overhead

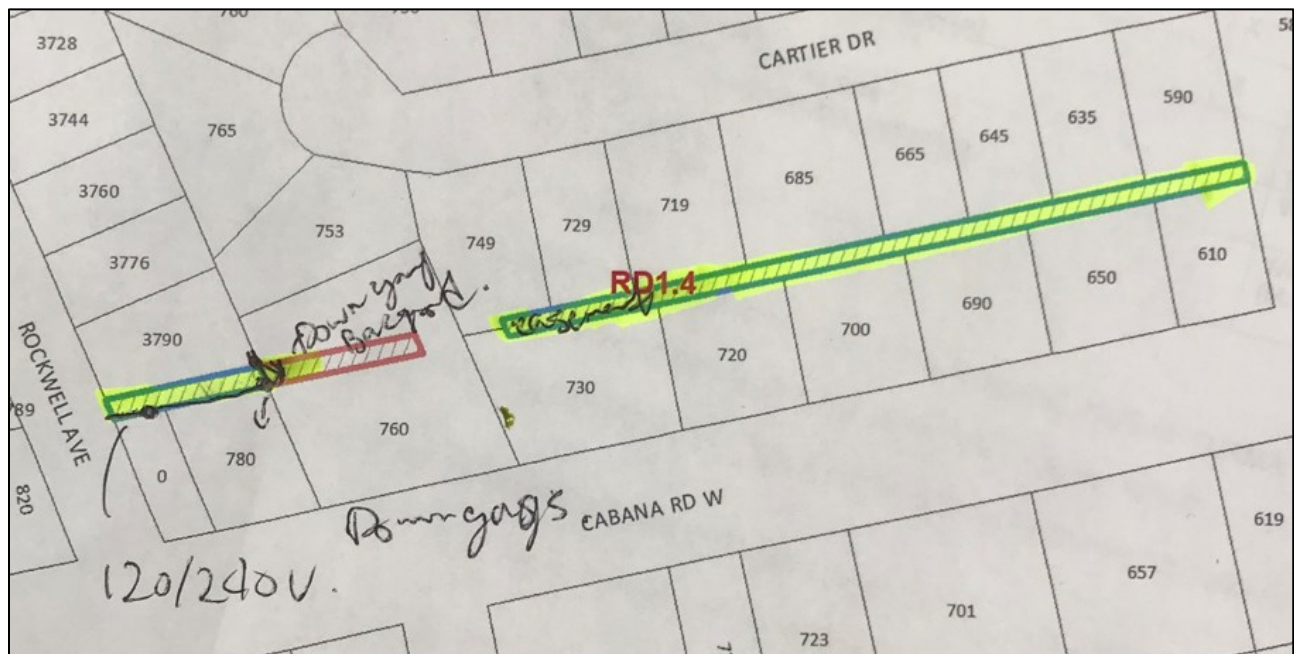
A 3 m (10 ft.) Easement is required for a straight pole line 1.5 m (5 ft.) (on each side). This takes into consideration a 0.3 m (1 ft.) pole diameter, 0.46 m (1.5 ft.) primary insulator, and 0.9 m (3 ft.) clearance from any nearby structure.

Guy and Anchor

The easement for the guy and anchor is 3 m (10 ft.) wide easement is required into private property. This easement should extend 1 m (3.3 ft.) into the property.

Underground Cable

A 0.6 m (2 ft.) easement on either side of a duct bank is required for underground installations.

[Tia McCloskey - Hydro Engineering Technologist]**ENWIN UTILITIES - WATER**

Water Engineering has no objections.

[Bruce Ogg - Water Project Review Officer]**LEGAL DEPARTMENT**

For lands abutting RD1.4, \$1.00 plus deed preparation fee and proportionate share of the survey cost as invoiced to The Corporation of the City of Windsor by an Ontario Land Surveyor.

[Stephanie Santos - Coordinator of Real Estate Services]

MNSi

MNSi will require an aerial easement through the proposed alley closing as we have existing aerial plant on the poles.

[Dave Hartleib - Outside Plant Manager]

PARKS & FACILITIES

Parks D&D has no objection pertaining this LIAISON

[Sherif Barsom - Landscape Architect]

PLANNING DEPARTMENT

No comments provided

[Jim Abbs - Senior Planner, Subdivisions]

PLANNING DEPARTMENT - LANDSCAPE ARCHITECT

No objections from a landscape architectural or urban design perspective.

[Stefan Fediuk - Landscape Architect]

PUBLIC WORKS - ENGINEERING

The subject alley closure is approximately 4.8 m (16.0 ft.) wide, appears to be untraveled and composed of grass. There are no municipal sewers or manholes located in the closure. There are hydro poles, guy wires and overhead wires. An easement will be required for the utilities. There are encroaching objects within the closure. This alley appears to serve no useful purpose by CR146/2005; therefore, we have no objections to the closure.

[Adam Pillon - Manager of Right-of-Way]

PUBLIC WORKS - TRAFFIC

The alleyway shown is not required for vehicles or pedestrian access. The alley system is cut off by a missing link in the center and would not be useful to the general public if left open. No objections with closing the entire alley system as shown.

[Mike Spagnuolo - Signal Systems Analyst]

ROGERS COMMUNICATIONS

No comments provided

TELUS COMMUNICATIONS

TELUS has no underground infrastructure in the area of your proposed work.

[Meghna Patel - Permit Coordinator]

TRANSPORTATION PLANNING

The alley does not seem to be used by pedestrians or vehicles. Transportation Planning has no objections to the proposed alley closure and no objections to the closure of the alley portions that are “to be recommended for closure.”

[Clare Amicarelli - Transportation Planner I]

TRANSIT WINDSOR

Transit Windsor has no objections.

[Jason Scott - Supervisor, Planning]

WINDSOR FIRE

No comments provided

[Mike Coste - Chief Fire Prevention Officer]

WINDSOR POLICE

The Windsor Police Service has no concerns or objections with the closure of this section opposite 760 Cabana Road West. Closure will not carry any negative impact to police response or service delivery capacity for the affected properties.

[Barry Horrobin - Director of Planning & Physical Resources]

APPENDIX "D"
Site Photos (December 5, 2022)



Figure 1 - Looking east towards alley from Rockwell Avenue (3790 Rockwell Avenue - left)



Figure 2 - Looking east towards alley from Rockwell Avenue (760 Cabana Road West - right)



Figure 3 - Looking west towards alley from Granada Avenue West (610 Cabana Road West - left)



Figure 4 - Looking west towards alley from Granada Avenue West (590 Granada Avenue West - right)

APPENDIX “E”

Classification of Alleys and Suitability for Closure

Classification of Public Rights-of-Ways:

Currently streets and alleys fall into four classifications on the basis of their usefulness:

- 1) Alleys that are **indispensable**. These would be alleys serving commercial properties and properties fronting on heavily traveled streets i.e. major arterial routes and alleys which contain sewers and must remain accessible for servicing; alleys or streets which serve as the only vehicular means of access to rear parking areas and garages where the property has insufficient lot width for a side drive; and, alleys which contain Fire Department connections that are deemed to be necessary for firefighting access.
- 2) Alleys that, **have some usefulness**, are nevertheless dispensable and may or may not be a complete liability.
- 3) Alleys that appear to serve **no useful purpose**, either now, or anticipated. Such alleys are in residential areas and locations where generally the lots are wide enough for side drives, or those alleys abutting parks and other parcels of land that do not require any servicing from the alley. Remnant or stub-end streets which are dead-ended and do not serve as access to other streets.
- 4) Alleys lying in Holding zones and other similar undeveloped areas where the alley system is **clearly obsolete** and has never been developed, but where the City needs to keep its options open until new area plans are prepared and development is imminent.

Suitability for Closing:

Following are the criteria and suitability for closing alleys in each of the above classifications:

- 1) Indispensable alleys should **not be closed**, conveyed, reduced or otherwise jeopardized through minority interests unless a suitable substitute alley is opened in lieu thereof. They are essential from the viewpoint of fire protection, police protection, emergency services (i.e. ambulance) and loading or unloading of goods, refuse collection, servicing of blocked sewers and utility services. Without such alleys, the above noted services would at least be more costly if not impossible to complete or adequately access; and would noticeably interfere with street traffic, thereby reducing the access capacity of the adjacent arterial, collector, or street for business.
- 2) Alleys having some usefulness should **be considered for closing** only upon request of abutting owners rather than by encouragement of the City.
- 3) Alleys that serve no useful purpose should **be closed** if at all possible, and in fact the owners abutting thereon should be encouraged to accept conveyance.
- 4) Alleys that are clearly obsolete should **not be closed** unless there is a municipal need or specific development proposals acceptable to the City are submitted.

Subject: Riverside Drive Streetscape Standards Manual, Spans Wards 2 through 7.

Reference:

Date to Council: July 4, 2023

Author: Kevin Alexander, MCIP RPP
Senior Planner - Special Projects
519-255-6543 ext. 6732
kalexander@citywindsor.ca
Planning & Building Services
Report Date: May 18, 2023
Clerk's File #: SR2023

To: Mayor and Members of City Council

Recommendation:

- I. **THAT** the Riverside Drive Streetscape Standards Manual identified in Appendix 'A' **BE ADOPTED** as the minimum design standard to consistently implement the *Riverside Drive Vista Improvement Streetscape Guidelines* identified through the Riverside Drive Vista Improvement Project Environmental Assessment (E.A.) and future phases of the Riverside Drive Vista Improvements.

Executive Summary:

N/A

Background:

Historically known as the St. Clair Trail by early settlers to this region, Riverside Drive spans the entire city from east to west and connects with the Towns of Tecumseh and LaSalle. Within the city it links with Windsor's City Centre and former Border City towns such as Sandwich, Walkerville, and Riverside. There are also several important parks including Windsor's Central Riverfront Park and several residential properties along Riverside Drive where north to south running streets and trails intersect, linking neighbourhoods to the south of Riverside Drive.

Windsor City Council recognized Riverside Drive's importance by designating Riverside Drive a Scenic Drive and Civic Way in Windsor's Official Plan. Special polices aimed at enhancing the public right-of-way, creating both an attractive and distinctive image by using unifying elements such as landscaping, fixtures, boulevard/median treatment,

protecting and enhancing views and vistas, public spaces and heritage resources along Riverside Drive are also identified in the Official Plan.

In December 2006, Windsor City Council approved the Riverside Drive Vista Improvement Project Environmental Assessment (E.A.). Through extensive public consultation, the following objectives were identified:

1. Reduce Traffic Speed
2. Reduce Traffic Volume
3. Make Riverside Drive Safer for all users; and
4. Make Riverside Drive look like a Scenic Drive

An important element of the E.A. was the development of Streetscape Guidelines. The Guidelines provide a general guide to the types of streetscape elements that can be used along Riverside Drive and within the five Special Streetscape Improvement Areas (SSIA) identified in the E.A., to “make Riverside Drive look like a Scenic Drive”. The document also provides design guidance for the many different nodes identified through the E.A. process aimed at reducing traffic speeds and volumes and to “make Riverside Drive safer for all users.”

Realizing that the Streetscape Guidelines only provide a guide to what the streetscape elements should look like along Riverside Drive, Windsor City Council passed the following resolution (CR364/2009) in 2009:

“.....That Administration BE DIRECTED to report back on developing the Standards Manual required to consistently implement the Riverside Drive Vista Improvement Streetscape Guidelines”

In addition, through CR59/2017 Council directed Administration to prepare a report for Council consideration that institutes a by-law standard for decorative fencing and parking areas along Riverside Drive. Fence design is currently addressed by the Fence By-law and Site Plan Review. Site Plan Review also provides guidelines and requirements that bring parking areas above four (4) spaces to compliance with municipal standards for public parking areas. However, section 7.12 of the Riverside Streetscape Standards Manual also provides guidelines for screening devices such as low walls, fences and landscape elements.

Past Phases and Timing:

Over the last few years, the Riverside Drive Vista Improvement Project E.A. Streetscape Guidelines were used to guide the implementation of the following phases of the Riverside Drive Vista Improvement Project:

Phase 1 (Stage 1) -- Riverside Drive East from Solidarity Towers east to the Little River Bridge.

Phase 1 (Stage 2) -- Riverside Drive East from Solidarity Towers west to Lauzon Road.

There were also intersection improvements at the locations of Riverside Drive West at Crawford Avenue, and Riverside Drive East at Devonshire Road as part of the Walkerville Theming and Districting Plan.

The Riverside Drive Vista Improvement Project E.A. Streetscape Guidelines, Phase 1 (Stage 1 and 2), and intersection improvements were used to inform the development of the Riverside Drive Streetscape Standards Manual included in Appendix 'A'.

The timing of the adoption of the Riverside Drive Streetscape Standards Manual coincides with the planning and implementation, and construction of Phase 2 (Riverside Drive East, from Ford Boulevard to St. Rose Avenue) and (Riverside Drive East, from Strabane Avenue to Ford Boulevard) and future phases of the Riverside Drive Vista Improvements.

The purpose of this report is to highlight some of the design elements in the Standards Manual and recommend the adoption of the Riverside Drive Streetscape Standards Manual identified in Appendix 'A' as the minimum design standard for Riverside Drive.

Discussion:

The Riverside Streetscape Standards Manual is divided into 13 sections and provides guidelines for the location and design of new streetscape elements that address the objectives of the *Riverside Drive Vista Improvement Project E.A.* and *Riverside Drive Vista Improvement Streetscape Guidelines*. In a general sense, the guidelines and standards are provided for the following areas:

Special Streetscape Improvement Areas (SSIA) and Nodes





Through the Riverside Drive Vista Improvement Project E.A., five Special Streetscape Improvement Areas emerged with unique characteristics based on existing conditions, building styles, land uses, and relationship to surrounding context. These areas of interest provide opportunities to incorporate and celebrate local character and create variety within the streetscape treatment while still creating a rhythm and distinct aesthetic that is identifiable to Riverside Drive.

The five Special Streetscape Improvement Areas (SSIA) include:



SSIA.1 West End

The Western gateway to both Riverside Drive and downtown that includes the Canada / US Ambassador Bridge with an established open space network and formal parks with views and access to the Detroit River.

	<p>SSIA.2 Downtown</p> <p>An established high profile shopping and tourist area that contains the Casino and two specific areas of interest. The first, Ouellette Avenue, has a north/ south visual access from the Detroit River to the Chrysler Building and second, the Civic Esplanade that has a north/ south visual access from the Detroit River to City Hall</p>
	<p>SSIA.3 Walker Road</p> <p>Also known as Walkerville, is an historic area that was the original home to Hiram Walker & Sons Distillery, that produced Canadian Club Whiskey. The distillery remains today through new ownership and produces J.P. Wisers Whiskey ad also supplies other brands.</p>
	<p>SSIA.4 Lakeview Park Marina</p> <p>An established marina and residential area with existing multi-use recreational trails and views, but limited access to the Detroit River.</p>
	<p>SSIA.5 East End</p> <p>The Eastern gateway to Riverside Drive and established residential district adjacent to the neighbouring Town of Tecumseh.</p>

The Riverside Drive Streetscape Standards Manual articulates the desired character within the five Special Streetscape Improvement Areas and along the entire stretch of Riverside Drive through the following streetscape elements:

Pavement Standards - through the use of materials, textures, colour, and patterns the paving elements proposed help to unify Riverside Drive as a continuous and cohesive corridor. As a cohesive design, these paving elements also reinforce public safety, accessibility and articulate the character established at the different nodes and Special Streetscape Improvement Areas identified along the “Scenic Drive”. Section 4.0 identifies the criteria and placement of materials, textures, colour, and patterns at each node and along each Special Streetscape Improvement Areas providing unifying elements that furthers helps to articulate Riverside Drive as a “Scenic Drive”

Lighting Standards - Section 5.0 identifies proposed light standards that serve to unify the Riverside Drive Corridor and play an important role in creating a vertical element that establishes Riverside Drive's significance as a "Scenic Drive." The Riverside Drive Streetscape Standards Manual proposes a new light standard based off the City Centre-"Windsor Style" light standard found in Windsor's Downtown identified as "Windsor Vista". In areas along Riverside Drive where it is cost prohibitive to bury utilities and utility poles remain, a decorative sidewalk-oriented arm is proposed that is consistent with the new "Windsor Vista" standard.

Landscape Standards - the establishment of a continuous tree canopy of diverse, drought resistant and climate appropriate trees is integral to the redevelopment along the Riverside Drive Scenic Vista, and is fundamental to the image of a quality liveable city. Section 6.0 identifies the criteria and placement of not only street trees, but also other plantings such as shrubs, perennials and ornamental grasses. Section 6.0 also identifies the criteria and placement of plantings in subsurface pits or in elevated planters, movable planters and hanging baskets for seasonal displays. The combined impact of a large tree canopy and others plantings help to establish Riverside Drives unique character as a "Scenic Drive".

Street Furniture - identified in Section 7.0 include benches, bicycle parking, bollards, trash receptacles, transit shelters, sidewalk cafe enclosures, screening/fencing, banners, and other street element provide important functional amenities that contribute to creating a liveable pedestrian environment along Riverside Drive.

Orientation Signage - this section identifies the types of signage that can be used along Riverside Drive providing an organized signage system that fits within the streetscape, is compatible with other streetscape elements, and minimizes visual clutter contributing to one's wayfinding ability and experience while walking, biking, or driving along the Riverside Drive "Scenic Drive". Section 8.0 provides criteria and placement guidelines for signage, regarding orientation and wayfinding, parking, active transportation, street signs, gateway markers, and heritage interpretation.

Other Streetscape Features - other streetscape elements may include Open Space, Public Art, Water Features, Gateway Markers, Riverfront Beacons, Landscape Elements (as described in Section 6 of this document) and should connect with the multi-use recreational trails that run the full length of the riverfront, depending on the land use types surrounding that node.

To emphasize the function and role of Riverside Drive as a "Scenic Drive", improvements can be viewed as a string of gateways (into SSIA and neighbourhoods) at key nodes along the waterfront connecting and interfacing with the many trails, parks and open space, and land uses along Riverside Drive.

Risk Analysis:

Approval of the attached Riverside Drive Streetscape Standards Manual will have no immediate financial risk given that the budget for each phase of construction for the implementation of the Riverside Drive Vista Improvements is sent to City Council for approval.

There is no real risk in approving the document from a public perception viewpoint. However, when implementing the Riverside Drive Vista Improvements there could be some negative public feed back as a result of construction and road closures.

Climate Change Risks

Climate Change Mitigation:

Approval of the Riverside Drive Streetscape Standards Manual will have no direct impact on climate change. However, materials used and construction required to implement design standards identified in the manual will have an indirect impact on Carbon dioxide emissions (CO2).

Climate Change Adaptation:

As part of the implementation of the Riverside Drive Vista Improvements, there could be an increase in the use of concrete and asphalt, which can contribute to the Urban Heat Island effect for the City of Windsor. However, improvements also include bike lanes and improved sidewalk areas and pedestrian crossings, which promotes walkability and safety making it easier for residents to access parks located along Riverside Drive.

Financial Matters:

There are no financial considerations with regards to the adoption of the Manual. The Implementation of the Riverside Drive Vista Improvements are presented to City Council for approval in phases and includes a budget request for road improvements and other elements including the streetscape elements identified in the Riverside Drive Streetscape Standards Manual.

Below are general costs related to some of the elements identified in the manual identified in Appendix 'A' and in this report which will be factored into the overall project budget request.

Streetscape Elements	Costs (per unit)
Windsor Vista Streetlight Assembly with pedestrian light	\$12,970
Windsor Vista Streetlight Assembly (no pedestrian light)	\$10,045
Windsor Vista Pedestrian Streetlight Assembly	\$8,270
Windsor Vista Pedestrian Fixture and clamp on Pipe arm	\$3,197
Standard Downtown Streetlight Assembly	\$14,020

Black Texture coated Traffic Intersection Pole, Arms, signal lights (MTO Standard) (3.7m Arm, 4 light signal, 23 foot pole)	\$4,217
Benches	\$1,960
Backless Wall Mount Bench	\$1,475
Trash Receptacle	\$1,590
Trash/Recycle Side Opening	\$4,025
Side Ash Receptacle	\$325
Post and Ring Bike Rack	\$445
4 Ring Bike Rack	\$1,260
Table/Game Board	\$3,605
Bollard	\$675
Bollard Swing Gate (per standard 28" swing gate)	\$1,110
Street Trees in pits with Modular Suspended Pavement System (per tree)	\$5,000
Street Trees in Large Planters	\$2,000 to \$5,000
Stamped concrete (per metre square)	\$257

All price estimates are subject to change
Street lights prices are based on a quantity of 10

Maintenance:

Given the improved design standard recommended for Riverside Drive for upgraded surface treatments, landscaping, and maintain and supply amenities (e.g. decorative light standards, benches, planters) there may be the need to establish an ongoing maintenance budget. Any ongoing maintenance budget would also be presented as part of each new phase of implementation for the Riverside Drive Vista Improvement to maintain and supply the upgraded streetscape elements recommended.

Consultations:

The following Departments and staff have been consulted regarding the development of the Riverside Drive Streetscape Standards Manual:

Public Works:

Shawna Boakes, Executive Director, Operations/Deputy City Engineer
Ian Day, Manager Traffic Operations
Roberta Harrison, Coordinator Maintenance Operations
Jeff Hagen, Transportation Planner-Senior Engineer
Anne Marie Albidone, Manager Environmental Services

Adam Mourad, Engineer II, Infrastructure Services

Transit Windsor:

Jason Scott, Supervisor, Planning

Parks & Facilities:

Yemi Adeyeye, City Forester/Manager Forest & Natural Areas

James Scott, Manager Parks Operations

Recreation & Culture:

Michelle Staaedegaard, Manager Culture & Events

ENWIN Utilities Ltd.:

Marvio Vinhaes, Director Engineering (Hydro)

Human Resources:

Gayle Jones, Diversity/Access Officer

Planning & Building:

Stefan Fediuk, Senior Urban Designer/Landscape Architect

Kristina Tang, Senior Planner-Heritage Planner

Conclusion:

Historically Riverside Drive has always played an important role as a road linking Windsor and its former Border City's (Sandwich Town, Downtown, Walkerville, Ford City, Olde Riverside) to the Towns of LaSalle to the west and Tecumseh to the east. It also plays an important role linking and interfacing with several important parks and trails including Windsor's Central Riverfront and several residential properties and neighbourhoods south of Riverside Drive. Windsor City Council recognized Riverside Drive's importance by designating it a Scenic Drive and Civic Way in Windsor's Official Plan.

Approval of the attached Riverside Drive Streetscape Standards Manual is consistent with the policies developed in the City's Official Plan regarding Riverside Drives importance as a "Scenic Drive" and "Civic Way". The attached Standards Manual also provides the minimum design standard "*....required to consistently implement the Riverside Drive Vista Improvement Streetscape Guidelines*" as directed through CR364/2009.

The timing of the adoption of the Riverside Drive Streetscape Standards Manual coincides with the planning and implementation, and construction of Phase 2 (Riverside Drive East, from Ford Boulevard to St. Rose Avenue) and (Riverside Drive East, from Strabane Avenue to Ford Boulevard) and future phases of the Riverside Drive Vista Improvements.

Administration supports the adoption of the Riverside Drive Streetscape Standards Manual as the minimum design standard to appropriately address Riverside Drives function and role as Windsor's only "Scenic Drive".

Planning Act Matters:

N/A

Approvals:

Name	Title
Kevin Alexander	Planner III – Special Projects
Neil Robertson	Manager of Urban Design / Deputy City Planner
Neil Robertson for Thom Hunt	City Planner / Executive Director, Planning & Development Services
Wira Vendrasco	Deputy City Solicitor, Legal Services & Real Estate
Shelby Askin Hager	Commissioner, Legal & Legislative Services
Jelena Payne	Commissioner, Economic Development & Innovation
Chris Nepszy	Commissioner, Infrastructure Services
Janice Guthrie	Commissioner, Corporate Services Chief Financial Officer / City Treasurer
Joe Mancina	Chief Administrative Officer

Notifications:

Name	Address	Email

Appendices:

Appendix 'A' -Riverside Drive Streetscape Standards Manual



RIVERSIDE DRIVE STREETSCAPE STANDARDS MANUAL



City of Windsor
RIVERSIDE DRIVE VISTA IMPROVEMENT PROJECT
Class Environmental Assessment



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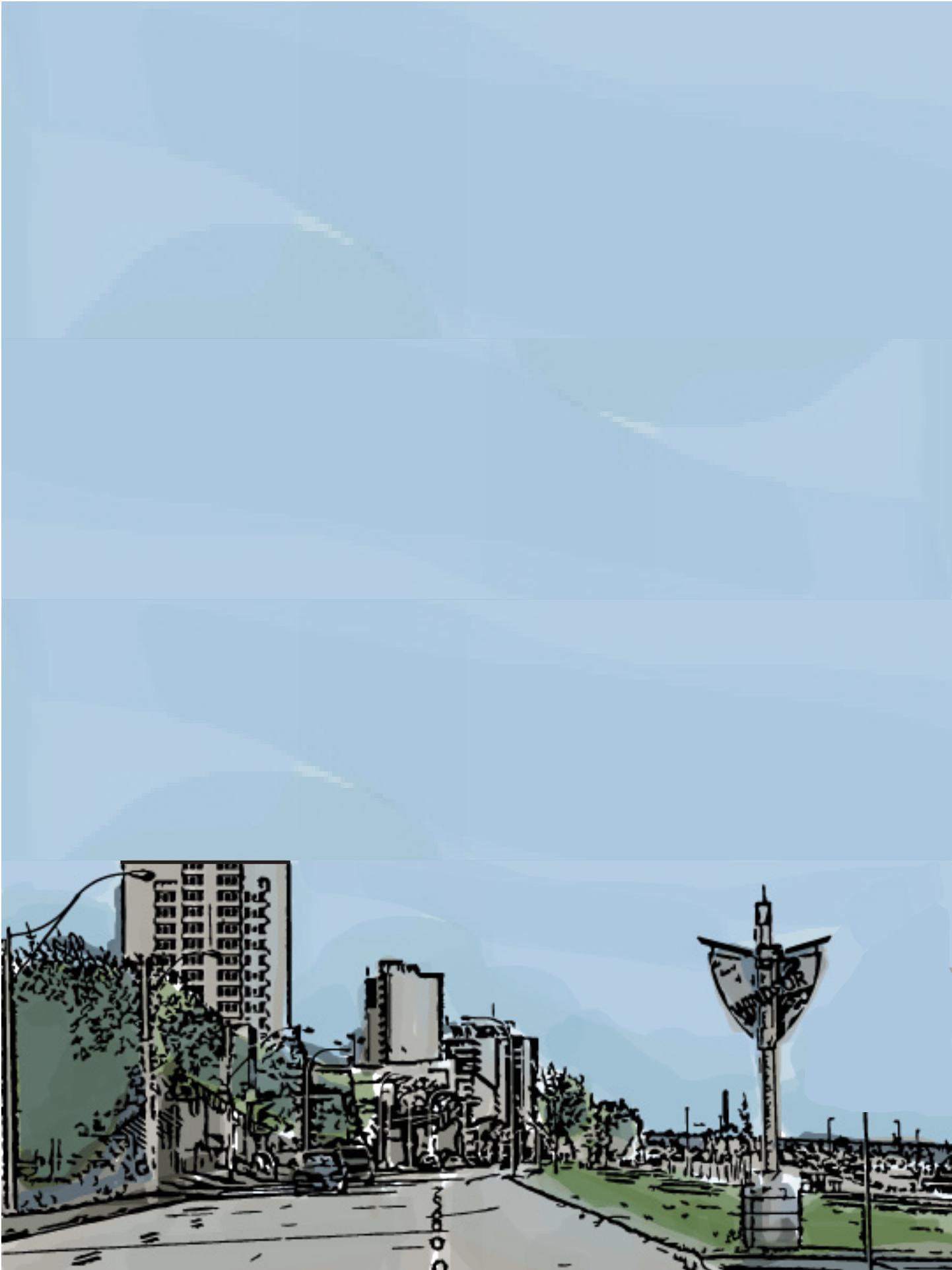


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1.0 Purpose of Riverside Drive Streetscape Improvements

1.0 Purpose of Riverside Drive Streetscape Improvements

Historically known as the St. Clair Trail by early settlers to this region, Riverside Drive spans east to west and connects with the Town of Tecumseh and LaSalle. Within the City it links with Windsor’s City Centre and former Border City towns such as Sandwich, Walkerville, and Riverside. There are also several important parks including Windsor’s Central River-front and several residential properties along Riverside Drive where north to south running streets intersect linking neighbourhoods to the south.

Windsor City Council recognized Riverside Drive’s importance by designating Riverside Drive a Scenic Drive and Civic Way in Windsor’s Official Plan. Special polices aimed at enhancing the public right-of-way and creating a highly attractive and distinctive image by using unifying elements such as landscaping, fixtures, boulevard and median treatment, and protecting and enhancing views and vistas, public spaces and heritage resources along Riverside Drive are also identified in the Official Plan.

In December 2006 Windsor City Council approved the Riverside Drive Vista Improvement Project E.A. Through extensive public consultation the following objectives were identified:

1. Reduce Traffic Speed
2. Reduce Traffic Volume
3. Make Riverside Drive Safer for all users; and
4. Make Riverside Drive look like a Scenic Drive

What is a Scenic Drive?

Merriam-Websters dictionary identifies “scenic drive” with “scenic route” and as “ a way that is not the fastest way but that has beautiful scenery”. Other sources such as Collins dictionary provide adjectives such as having to do with natural and beautiful scenery or affording beautiful or pleasing views. A scenic route is often described as a road or path designed to take one past a pleasant view or nice scenery, the long way round; and a deliberately slow path (source: Wikipedia). Scenic Drives in Ontario often pass through wild life corridors with views of lakes, rivers, and streams and connect with historic towns and places. Riverside Drive is no different and provides views of the Detroit River and skyline, parks and also connects with historic neighbourhoods such as Sandwich Town, Walkerville and Riverside.



An important element of the E.A. was the development of Streetscape Guidelines, which provide a general guide to the types of streetscape elements that can be used along Riverside Drive and within the five Special Streetscape Improvement Areas (SSIA), to “Make Riverside Drive look like a Scenic Drive”. However, the document also provides guidelines for the many different nodes identified through the E.A. process aimed at reducing traffic speeds and volumes and to “Make Riverside Drive safer for all users.” Realizing that the Streetscape Guidelines only provided a guide to what the streetscape elements should look like along Riverside Drive, in 2009 Windsor City Council passed the following resolution (CR364/2009):

“.....That Administration BE DIRECTED to report back on developing the Standards Manual required to consistently implement the Riverside Drive Vista Improvement Streetscape Guidelines”

For this purpose, the Riverside Drive Standards Manual was developed to provide a consistent approach to the selection of materials and streetscape elements such as decorative street lights, crosswalks, benches, trash receptacles, etc., that will be consistently applied along Riverside as each Phase of the E.A. is implemented.



1.1 Defining the Scope of Streetscape Improvements



1.1 - Defining the Scope of the Streetscape Improvements

Streetscaping improvements will include:

- Roadway crosswalks;
- Corners of intersections;
- Sidewalks and curbs- pedestrian access and driveway curb cuts;
- Pedestrian paths;
- Decorative roadway and pedestrian lighting;
- Pathway lighting;
- Landscape standards for streets trees, standards for trees in subsurface pits, trees in grates, trees in fixed planter beds;
- Landscape standards for vegetation in moveable planters and seasonal decorations and hanging baskets;
- Street furniture, such as benches, trash and recycling receptacles, bicycle parking, bollards, transit shelters, telephone booths, newspaper vending racks, mail collection and storage boxes, banner poles and decorative banners, kiosks, and screening devices;
- Signage standards for trailblazing, way finding, parking, orientation, street names, gateway markers, and heritage interpretation;
- Special streetscaping features such as monuments, sculptures, fountains, parks, plazas, and water related features;
- Fence/screening and low wall standards;
- Street Nodes and Special Streetscape Improvement Areas (SSIA);
- Building side standards;
- Parking and loading standards for on-street personal and commercial vehicles; and
- Parking and loading standards for on-street passengers and bus passengers.



Standards for these streetscape elements will help to satisfy the objectives of the E.A. and define the Streetscape Guidelines by creating an attractive and consistent approach when implementing these improvements throughout each phase in consideration of the different contexts identified along Riverside Drive.

1.2 Recommendations for Streetscape Improvements

Section 8.3 of the E.A. identifies these streetscape improvements as an integral part of the improvement program for Riverside Drive. The E.A. describes these improvements as having both a functional transportation and visual quality in helping to improve Riverside Drive as a Scenic Drive and Civic Way. The EA identifies the 38 nodes along the corridor as providing an opportunity for urban design enhancements in the form of visual treatments, streetscaping with vegetation, signage, public art, vista enhancements, and through the built form of development and redevelopment projects along the Drive. Section 7 of the Riverside Drive Vista Improvement Project Class EA includes a list of node locations along Riverside Drive. However some of the locations can change based on detailed design of Riverside Drive when considering the implementation of each phase. See Schedule B for Nodes identified along Riverside Drive. Council Resolution 364/2009 also directs Administration to report back on developing the Standards Manual required to consistently implement the Riverside Drive Vista Improvement Streetscape Guidelines.

1.3 Where Streetscape Improvements Will Occur

The streetscape improvements will occur generally within the Riverside Drive right-of-way from the City limits, bordering the Town of Tecumseh just past Lakeview Drive on the east to the intersection of Rosedale Avenue and Sandwich Street on the west. However, improvements may also take place at the entrances of parks and plaza areas as well as on private property. Along the Riverside Drive right-of-way both functional and urban design improvements will occur within each node and at the five SSIA's where a higher level of design detail and standards will be implemented. Additional information regarding the location of these improvements can be found in Section 2.0.



1.4 Need for Streetscape Improvements

The Streetscape Improvements identified in the E.A. are an integral part of the functional and visual elements required to implement the E.A. objectives. These improvements include the necessary design elements required to meet the following identified E.A. objectives:

1. Reduce Traffic Speed;
2. Reduce traffic Volume;
3. Make Riverside Drive safer for all users; and
4. Make Riverside Drive look like a Scenic Drive.

1.5 How The Streetscape Standards Manual Will Be Used

This manual establishes the minimum expectations and design performances for all development and streetscape improvement initiatives proposed within the Riverside Drive right-of-way and adjacent lands.

In some cases, due to the fact that privately owned land may be within (what appears to be) the public right-of-way or within a publicly owned park, the privately owned land within these areas will also be required to conform to the standards of this manual.

The Riverside Drive Streetscaping Standards Manual will be used by:

- The City of Windsor and their contractors;
- developers;
- designers;
- business owners;
- property owners; and
- Others involved in the rehabilitation and reconstruction of Riverside Drive as a Scenic Way as identified in The City of Windsor Official Plan.



Before any reconstruction activity can be undertaken on the public rights-of-way of Riverside Drive all work must be reviewed and approved by City of Windsor Administration to ensure consistency with the Council approved Riverside Drive Vista Improvement Project E.A., Streetscape Guidelines and this Streetscape Standards Manual. The following Departments are responsible for reviewing and recommending to Council Capital Works and plans of redevelopment regarding Riverside Drive:

- Public Works Department (Infrastructure Services);
- Transportation Planning;
- Traffic Operations ;
- Planning and Building Department (Urban Design Section and Policy Section);
- Parks Department (Parks Development, Parks and Facilities);
- Recreation and Culture (Cultural and Events);
- Transit Windsor; and
- Utility Companies

All works and redevelopment will be reviewed by City Administration through Capital Works proposals on and adjacent to Riverside Drive, such as for the direct implementation of the Riverside Drive Vista Improvement E.A. or any Parks redevelopment or master plans, and through the development review process (Zoning, Site Plan Control, plans of subdivision, plans of condominium, Committee of Adjustment, etc.).

The Manual outlines the following:

- Function and Application of Streetscaping Standards for the City Centre;
- Identification of Individual Streetscape Elements for Installation on Theme Streets;
- Standards for Individual Streetscape Elements;
 - Design Criteria – “How it Functions”
 - Placement – “Where it Goes”
- Specifications and Details for selected Streetscape Elements; and
- Installation and Maintenance for each Streetscape Element.



1.6 Criteria Used in the Selection of Streetscape Elements

The following criteria were considered when selecting and designing the various streetscaping elements to be implemented as public improvements on Theme Streets in the City Centre:

Appearance:

- What does the element look like?
- Does the design of the element have an enduring appeal?
- Does the overall character of the element reinforce the district's image?
- Is the element in scale with its surroundings?

Function and Placement:

- Will the element be located in the right place?
- Can this element do its job?
- Are trash containers large enough, easily emptied and conveniently placed?
- Are benches comfortable for sitting or will the material be too hot in the summer or too cold in the winter?
- Is the lighting adequate to give a sense of security at night without creating excessive glare or light pollution?
- Is the element compatible with the function of the facility for all intended users and modes, including drivers, pedestrians, cyclists, and transit?
- Does the element allow safety-related requirements (e.g. visibility, clear zone) to be met?

Durability and Maintenance:

- Is the element designed to last a long time?
- Will the materials of the element withstand changing seasons in this climate?
- What are the maintenance needs of the element?



Cost:

- What will the improvements cost be?
- Is the improvement worth the price?
- Is it better to select a less expensive element?
- How much will it cost to maintain the element?

Although it may be tempting to select an item or material because of its low initial cost, often this is a false economy as elements of a lesser quality are more prone to show signs of wear and tear, be vulnerable to vandalism or in need of constant repair. It is generally better to purchase a few elements of good quality rather than a large number of lesser quality elements.

Maintenance:

Given the need to maintain upgraded surface treatments, landscaping, and maintain and supply amenities (e.g. decorative light standards, benches, planters) identified in this Standards Manual there will be the need for an ongoing maintenance budget that should be identified through each phase of the Riverside Drive Vista Improvement Project.



2.0 Locations for Streetscaping on Riverside Drive

The Riverside Drive Vista Improvement Project (Class Environmental Assessment) identified Special Streetscape Implementation Areas (SSIAs) and Nodes at key intersections to prioritize important design areas and to help articulate the Riverside Corridor as a Scenic Drive. The figures below identify these SSIAs and Nodes from Rosedale Avenue on the west to the boundary of the City of Windsor on the east side.

2.1 Special Streetscape Improvement Areas (SSIA)



Riverside Dr. looking West- SSIA.1



Ouellette & Riverside - SSIA.2



Walker & Riverside - SSIA.3



Riverdale & Riverside - SSIA.4



East Boundary of Riverside - SSIA.5

Within the Riverside Drive Corridor, five Special Streetscape Improvement Areas have emerged, each with its own unique characteristics based on existing conditions, building styles, land uses, and relationship to surrounding context.

By identifying areas of interest within the Riverside Drive corridor, it provides opportunities to incorporate and celebrate local character and create variety within the streetscape treatment while still creating a rhythm and distinct aesthetic that is identifiable as Riverside Drive.

The five Special Streetscape Improvement Areas (SSIA) include:

SSIA.1 West End – The Western gateway to both Riverside Drive and downtown that includes the Canada / US Ambassador Bridge with an established open space network and formal parks with views and access to the Detroit River.

SSIA.2 Downtown - An established high profile shopping and tourist area that contains the Casino and two specific areas of interest. The first, Ouellette Avenue, has a north/south visual access from the Detroit River to the Chrysler Building and second, the Civic Esplanade that has a north/south visual access from the Detroit River to City Hall.

SSIA.3 Walker Road – Also forms part of the area designated through CR 330/2022 as the Walkerville Theming and Districting Plan.

SSIA.4 Lakeview Park Marina - An established marina and residential area with existing multi-use recreational trails and views, but limited access to the Detroit River.

SSIA.5 East End – The Eastern gateway to Riverside Drive and established residential district adjacent to the neighbouring Town of Tecumseh.

(See Schedule B where SSIAs have been identified)

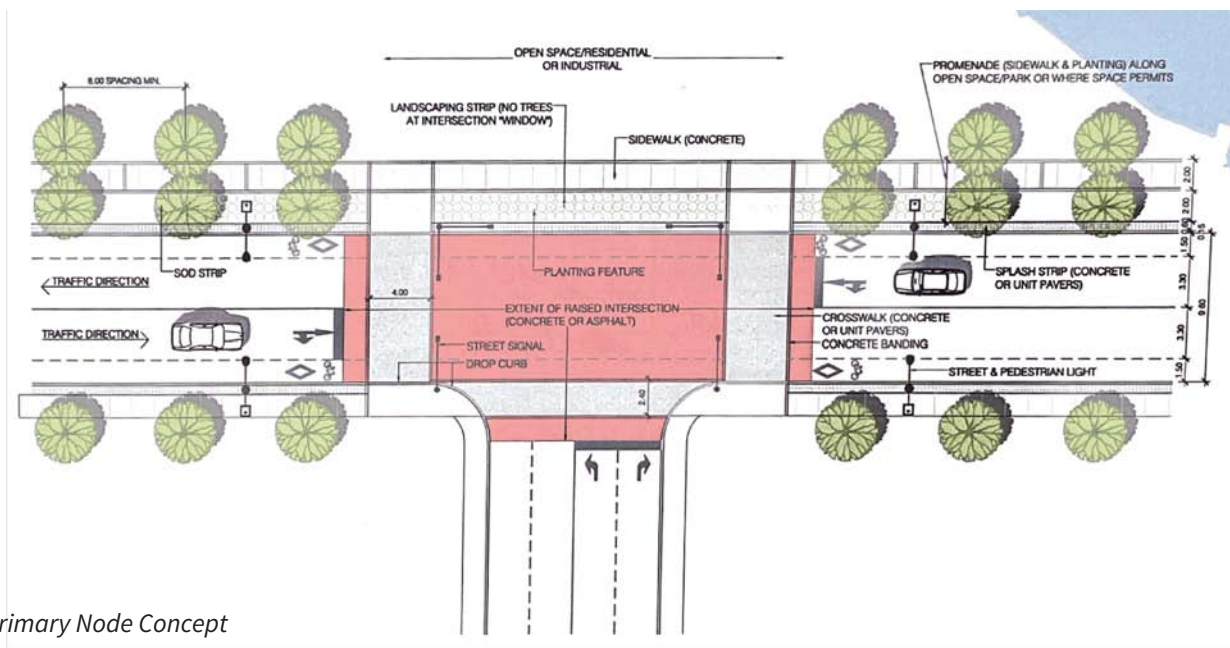


RIVERSIDE DRIVE STREETSCAPE
STANDARDS MANUAL
CITY OF WINDSOR

2.2 Nodes (Primary/Secondary/Tertiary)

A node is identified as a point along Riverside Drive where there is a significant opportunity for activity and some form of special design treatment. These have been categorized as being primary, secondary, or tertiary as outlined in the Riverside Drive Vista Improvement Project-Streetscape Improvement Guidelines.

N1 Primary Node: Are located at major signalized intersections or mid-block connections that have direct waterfront access and no physical or visual barriers separating Riverside Drive from the water's edge. Pedestrian crossings are defined with a combination of raised surfaces where traffic calming is required and improved material selections such as impressed colour concrete or unit pavers.



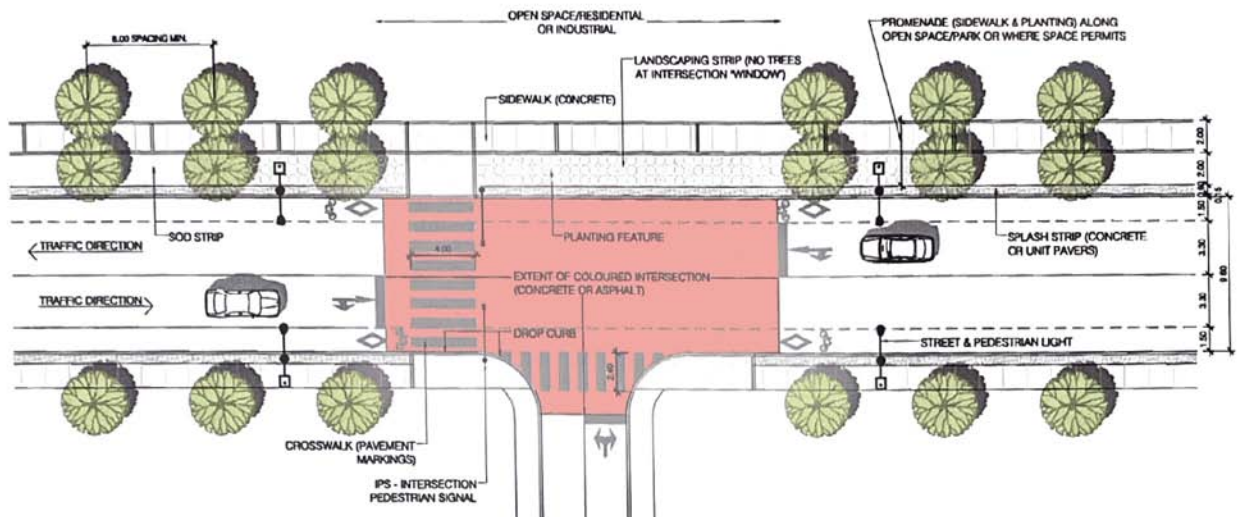
N1 Primary Node Concept

Note: Node design to comply with current OTM and AODA standards.

Source: Riverside Dr. Vista Improvement ESR- IBI Group

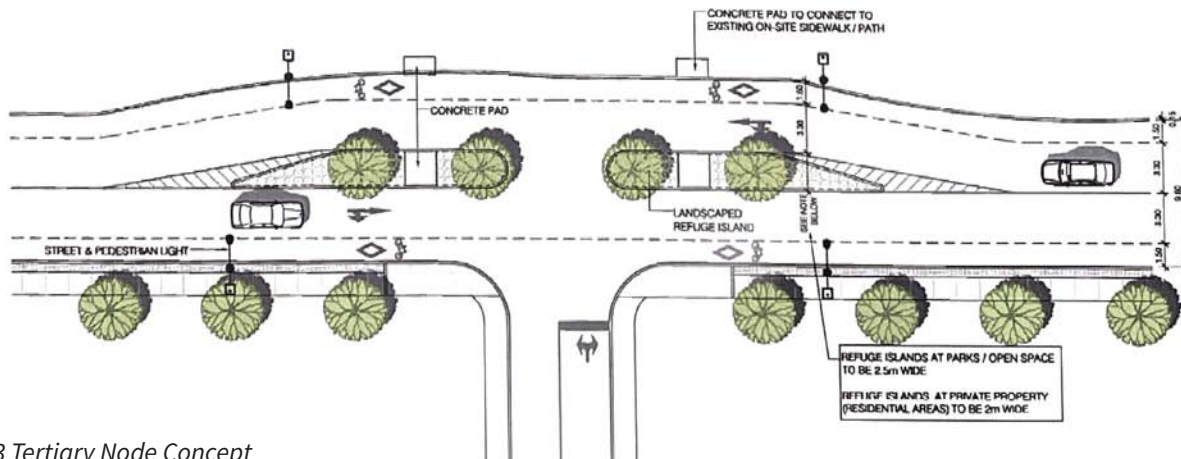


N2 Secondary Node: Provide pedestrian crossings at key locations along Riverside Drive, predominantly at locations associated with riverfront parkland or locations that mark the boundary or change from one character district to the next. These are sometimes referred to as edge conditions since there is a transition from one type of land use or character to another. The nodes are marked by a colour surface treatment and marked cross walk with an intersection pedestrian signal (IPS) that can be activated by the pedestrian. Planting along the north side of the node adjacent to the parkland are low in height to maintain the visual connection to the river.



N2 Secondary Node Concept

N3 Tertiary Node: Are designed to function as a visual and physical traffic-calming feature with a centre median refuge island and pavement markings to facilitate safer crossing and increase accessibility versus a typical stop control. Tertiary nodes are primarily located adjacent to residential neighbourhoods to service local demand and access to riverfront parkland.



N3 Tertiary Node Concept

Note: Node design to comply with current OTM and AODA standards.

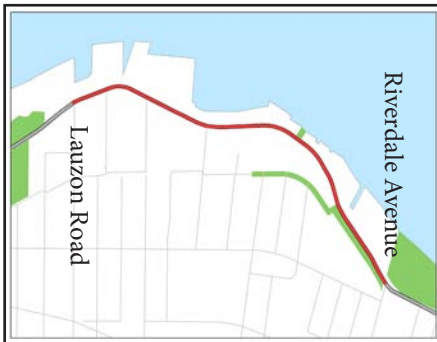


3.0 Streetscaping Implementation Priorities

3.1 Implementation Priorities for Locations on Riverside Drive

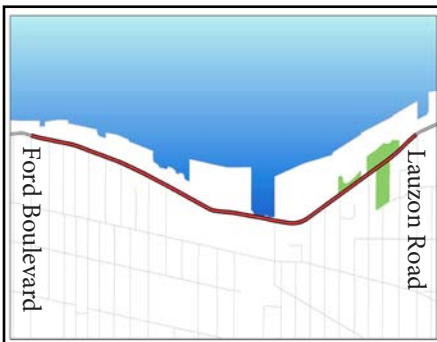
Generally Streetscaping improvements will coincide with the approved Improvement Program Staging identified in Section 7.0 of the Riverside Drive Vista Improvement Project E.A. and the Streetscape Guidelines. The E.A. identifies the following stages:

Stage 1: Riverdale Avenue to Lauzon Road



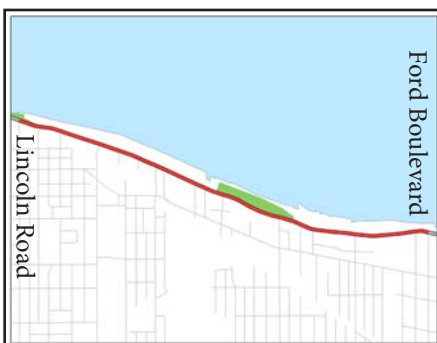
Streetscape improvements will coincide with improving poor roadway surface conditions, reducing high collision incidents, and making the roadway safer for pedestrians. Pedestrian safety will be improved with pedestrian crossings and the enhancement of nodes via the implementation of the Lakeview Park Marina SSIA. New bike lanes will make the roadway safer for cyclists and will also link with the existing Ganachio Trail, extending the bikeway to Lauzon Road.

Stage 2: Lauzon Road to Coventry Gardens at Ford Boulevard



Streetscape improvements will coincide with improving roadway surface conditions, improve pedestrian crossing safety through the development of nodes and crosswalks, and the introduction of north side sidewalks and extend the Riverside bikeway further west to the attractions along the riverfront.

Stage 3: Ford Boulevard to Lincoln Road



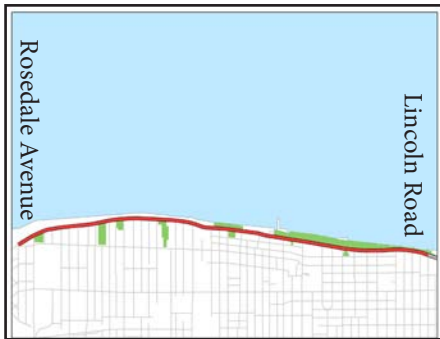
Streetscape improvements will coincide with improving roadway surface conditions and intersection improvements to encourage traffic diversion off of Riverside Drive to Wyandotte Street. Pedestrian safety will be improved through the development of nodes and the Walker Road SSIA. Cyclist safety will be improved by extending bicycle lanes to link with the Central Riverfront parkland recreationway.





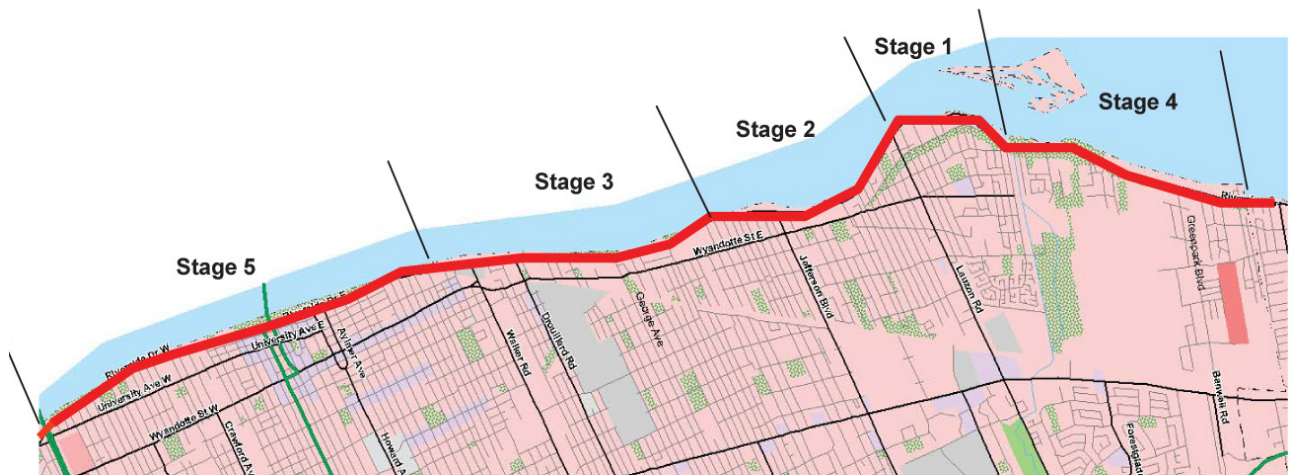
Stage 4: Riverdale Avenue to the eastern City limit

Streetscape improvements will coincide with improving roadway surface conditions and intersection improvements through the development of nodes which will also promote pedestrian safety. Cyclist safety will be improved with the introduction of bike lanes.



Stage 5: Lincoln Road to Rosedale Avenue

Streetscape improvements will coincide with improving roadway surface conditions and intersection improvements through the development of nodes and a new pedestrian promenade adjacent the north side of Riverside Drive along the Central Riverfront Parkland which will also promote pedestrian safety. Pedestrian safety will also be improved with the introduction of three SSIsAs spanning along the roadway. Cyclist safety will be enhanced with the establishment of bike lanes leading into Sandwich Town.



3.2 Implementation Priorities for Individual Streetscape Elements



The following primary elements will be prioritized within each stage of implementation as identified in Section 3.1 of the Riverside Drive Streetscape Standards Manual.

1. PRIMARY ELEMENTS – Streetscape Infrastructure

a) Hardscaping

- All Pavement Elements for raised Nodes, crosswalks, sidewalks pathways and splash strips;
- All Parking and Loading Elements;
- All conduits for Roadway, Pedestrian, Bollard and Pathway Lighting; and
- Light Standards.
 - All Combined Roadway and Pedestrian Lighting
 - Supplemental Pedestrian-only Lighting
 - Special Area Lighting

b) Landscaping

- Street Trees; and
- Fixed Planter Beds.

2. SECONDARY ELEMENTS – Amenities

a) Furnishings

- Benches;
- Waste Receptacles;
- Bicycle Parking;
- Other furnishings as needed and as resources allow; and
- Elements placed by others.

b) Landscaping

- Moveable Planters.

c) Orientation Signage

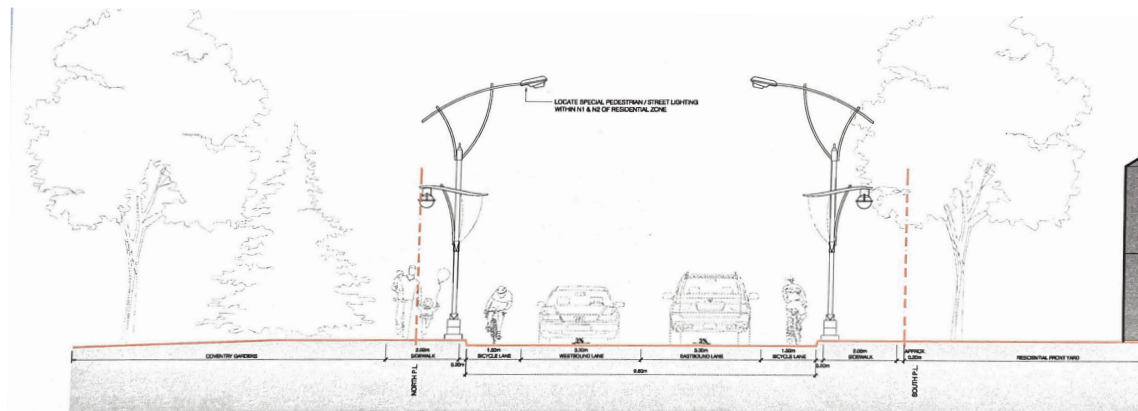
- Street Name Signs;
- Decorative Banners;
- Gateway Markers;
- Heritage Interpretive Signs; and
- Other Orientation Signage as needed.

Note: An ongoing maintenance budget will need to be established through each new phase of the Riverside Drive Vista Improvement Project for the ongoing maintenance of upgraded surface treatments, landscaping, and to maintain and supply the new amenities (e.g. decorative light standards benches, and planters).



4.0 Pavement Standards

Paving is a significant unifying element in streetscape design. The materials, textures, colours, and patterns that are utilized should unify Riverside Drive as a continuous and well thought out “Scenic Drive.” Aside from helping to create a unified appearance, the paving elements can also help reinforce: Public Safety, Accessibility Standards, Sustainable Design Practices and Planning Objectives.



The objectives that are to be achieved include:

- Paving patterns, colours and textures should create a “rhythm and distinct aesthetic that are identifiable as Riverside Drive”;
- In the “Special Streetscape Improvement Areas” continue this “rhythm and distinct aesthetic” while “incorporating and celebrating local characteristics”;
- Establish consistency in pavement materials and treatments so an obvious and unobstructed pedestrian route is clearly distinguished;
- Establish paving patterns that provide the organizational framework for the placement of fixtures, furnishings, decorations and vegetation within the streetscape design;
- Establish and clearly differentiate between the three zones that form the “Boulevard”. Consisting of the: Curb Side, Pedestrian Path and the Building Side; and
- All work will meet Accessibility for Ontarians with Disabilities Act (AODA) Standards.

References

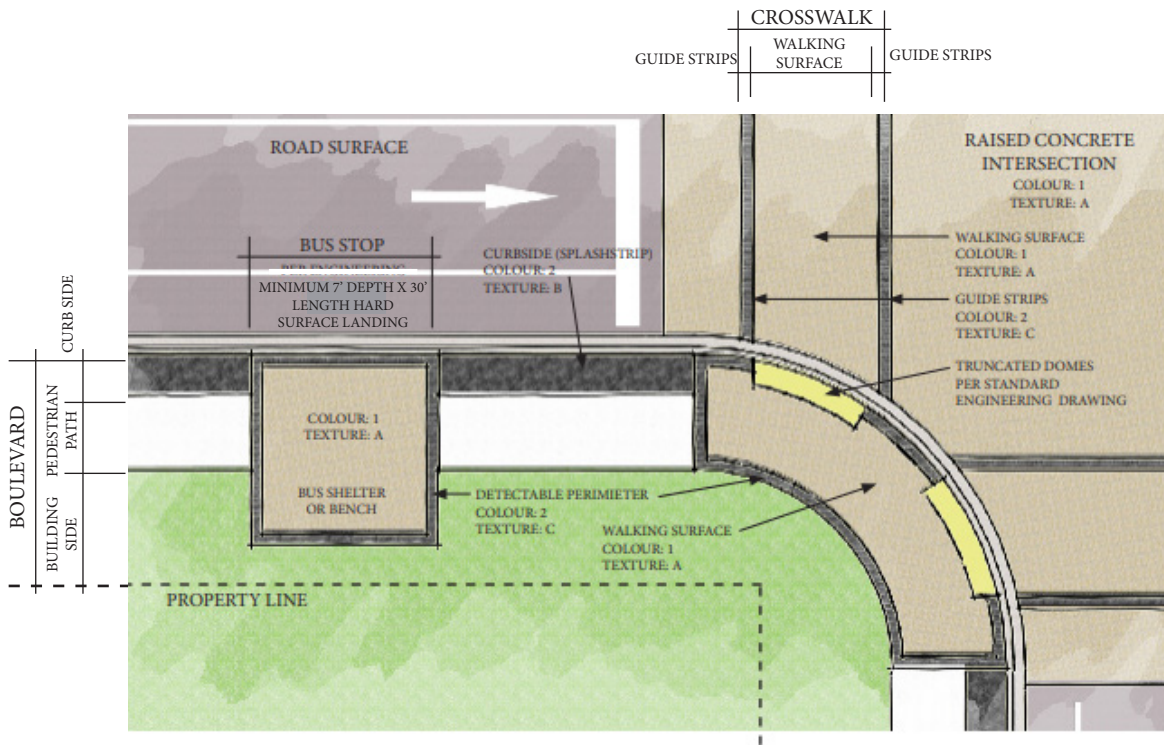
Riverside Drive Vista Improvement Project – Environmental Study Report
Riverside Drive Vista Improvement Project – Streetscape Guidelines
“FADS” – Facility Accessible Design Standards - City of Windsor
City of Windsor – Development Manual

These standards are to be applied in conjunction with the Riverside Drive Vista Improvement Project – Streetscape Guidelines Section 4.1.1 – Surface Materials.



RIVERSIDE DRIVE STREETScape
STANDARDS MANUAL
CITY OF WINDSOR

Pavement Design Framework - N1 Node (Figure 4.0-A)



LEGEND

- Colour 1: "Adobe" (61078)
- Colour 2: "Dark Grey" (860)

- Texture A: Broom Finish
- Texture B: "Riverstone" Seamless
- Texture C: "Cobblestone Running Bond"

Note:
The concrete colours that have been selected are from "Davis Colors" Inc. (1.800.356.4848, www.daviscolors.com) Davis Colors was only selected as a guide. Colours from other manufacturers may be used and are subject to approval.

The stamping patterns have been selected from "Scofield Inc." (1.800.800.9900, www.scofield.com) These patterns have been selected as a guide and similar patterns from other manufacturers may be used and are subject to approval.

N2 Nodes

- At N2 intersections painted markings delineate the crosswalk in lieu of coloured concrete walking surface and guide strips.

N3 Nodes

- At N3 intersections there is no coloured or textured concrete within the traffic lanes; and
- Traffic islands will be treated the same as the corner of the intersection

Downtown Riverside Drive SSIA - Downtown

- Refer to City Centre Design Manual (located at the planning department) for the Downtown Special Streetscape Improvement Area.



4.1 Crosswalks

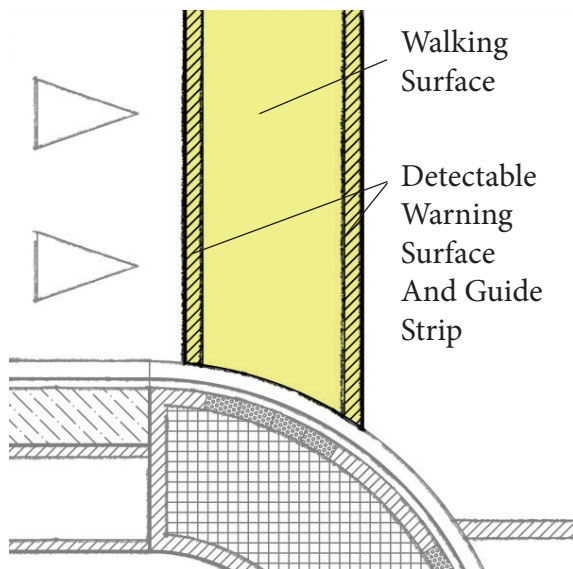
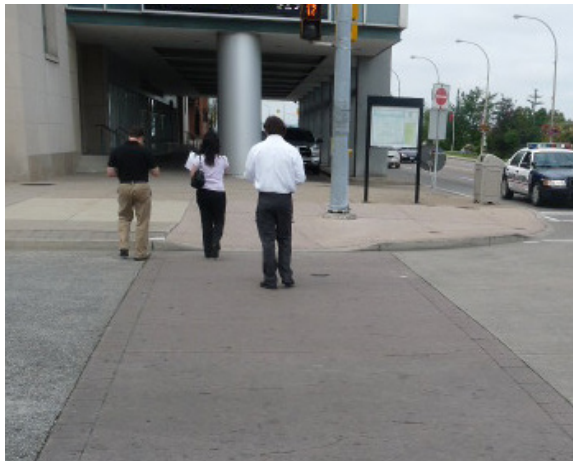


Figure 4.1-A, Crosswalk



A Crosswalk that Continues the Pedestrian Route

Placement

- The concrete crosswalk stretches across the roadway portion of the public right-of-way and is in alignment between the pedestrian access curb cuts of the corners of intersections.

Crosswalks are considered a continuation of the pedestrian route. A crosswalk provides for the safe movement and integration of pedestrians within the traveled right-of-way.

Design Criteria

- The crosswalk pavement must contrast with the adjacent surface to indicate where the pedestrians can cross safely;
- The material shall be concrete or asphalt depending on the type of “Node” the intersection is classified as. Refer to Section 2.0 “Locations for Streetscaping on Riverside Drive” ;
- The “N1 Node” crosswalk is made up of two distinct parts;
 - (See Figure 4.1-A, Crosswalk)
 - Part 1 – The “Walking Surface”
 - Part 2 – The “Detectable Warning Surface and Guide Strip”
 - The “Walking Surface” of the crosswalk will be a slip-resistant surface with a broom finish and not constructed with large paving joints, corrugated textures, or loose gravel.
 - The “Detectable Warning Surface and Guide Strip” within the crosswalk will reinforce the separation of vehicle and pedestrian paths of travel through the use of coloured and textured concrete.
- Coloured and textured concrete will act as visual and tactile cues for motorist and pedestrians;
- The “N2 Node” crosswalk consists of painted markings that delineate the crosswalk in lieu of the coloured concrete “Walking Surface” and “Guide Strips”; and
- The “N3 Node” does not have delineated crosswalks.
- All work will meet Accessibility for Ontarians with Disabilities Act (AODA) Standards.



4.2 Corners of Intersections

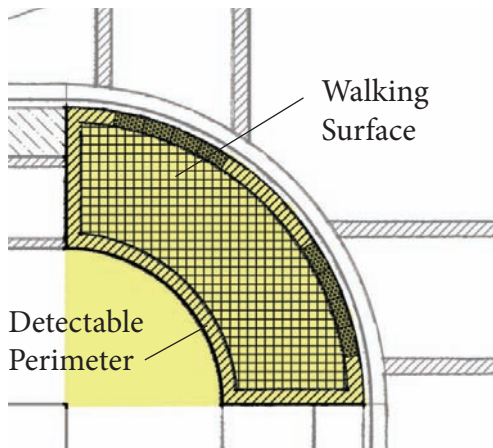


Figure 4.2-A, Corners of Intersections



Intersection Corner

The corners of intersections are an integral part of the pedestrian route. They should reinforce a unified appearance along Riverside Drive. In the “Special Streetscape Improvement Areas” the corners of intersections should continue this “rhythm and distinct aesthetic” while “incorporating and celebrating local characteristics.” The corners of intersections define the area of transition for the pedestrian between the sidewalk and the crosswalk.

Design Criteria

- The corners of intersections will be defined by two distinct Parts:
 - Part 1 – The “Walking Surface”
 - Part 2 – The “Detectable Perimeter”
- The “Walking Surface” at the corners of intersections will not be constructed with large paving joints, corrugated textures, or loose gravel;
- The “Walking Surface” shall be coloured concrete (or Unit Pavers at N1 nodes);
- The “Detectable Perimeter” will be coloured concrete with a stamped pattern. This will act as a visual and tactile cue to inform pedestrians;
- The width, colour and texture of the “Detectable Perimeter” will be consistent within a “Special Streetscape Implementation Area”; and
- The corners of intersections may contain a blend of grade transitions; the “Walking Surface” and the “Detectable Perimeter” will slope together as to not create a tripping hazard.
- All work will meet Accessibility for Ontarians with Disabilities Act (AODA) Standards.

Placement

- The shape of corners of intersections represent a $\frac{1}{4}$ circle with the radius in alignment with a line projecting out from the property line between public and private properties, with the arc of the circle following the curb between the two streets. The width of the “radius” should be consistent with the width of the pedestrian path and the curb side areas combined.



4.3 Curb Side

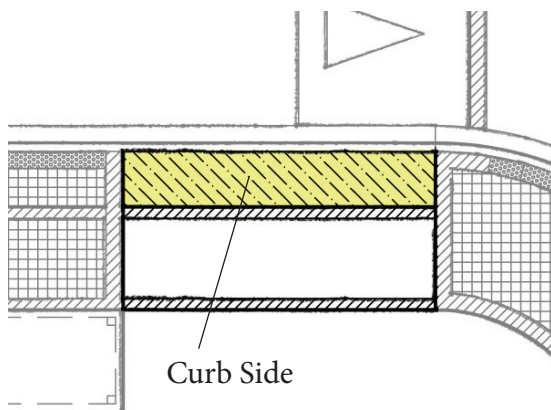


Figure 4.3-A, Curb Side



Curb Side Splash Strip

The curb side zone is the area of pavement or vegetation located directly along the back of the curb and serves as a location for the installation of lighting fixtures, utilities, furnishings, parking meters, trash receptacles and street trees.

Design Criteria

- The curb side width in residential areas should be a minimum of 1.0m from the back of curb to the pedestrian path. per (City of Windsor – Development Manual - Section 7.1.1);
- Some areas along Riverside Drive have a limited R.O.W width. Where space is limited, the width of the curbside zone may be reduced;
- The curb side width in commercial areas should be a minimum of 1.5m from the back of curb to the pedestrian path. per (City of Windsor – Development Manual - Section 7.1.1); and
- The curb side zone may consist of vegetation, textured and coloured concrete, or unit pavers depending where it is located along Riverside Drive.
- The curb side will have a considerable contrast, through colour and texture compared with the adjacent pedestrian path and roadway curbing.
- If unit pavers are utilized in a SSIA, then they will compliment the concrete colour and texture.
- In non-SSIA, the curbside zone shall have a consistent design.
- Vegetation & tree plans to be consistent with Section 6.0 of this document
- All work will meet Accessibility for Ontarians with Disabilities Act (AODA) Standards.

Placement

- The curb side paving pattern is ‘paused’ where the curb is cut by mid-block pedestrian access points, at corners of intersections, driveway access cuts, and bus stop loading areas.



4.4 Pedestrian Path

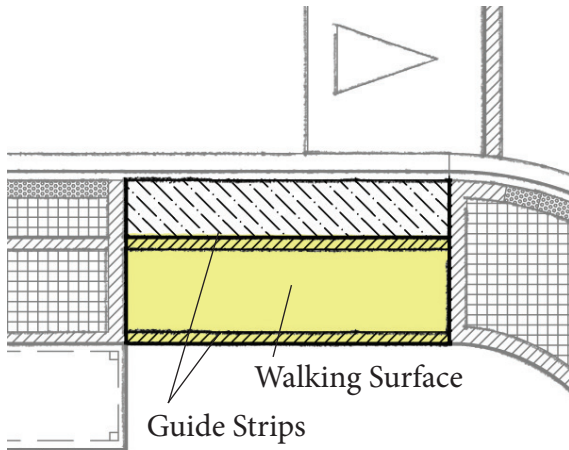


Figure 4.4-A, Pedestrian Path



Pedestrian Path

The pedestrian path zone is the pavement area located between the curb side and the building side zones and is the clear route designated for pedestrian circulation. (See Figure 4.0-A, Pavement Design Framework)

Design Criteria

- The Pedestrian Path should be designed to reflect the amount of pedestrian traffic that is expected;
 - The minimum width of the Pedestrian Path is 1.5m.
 - The preferred minimum width is 1.83m
- It is important to ensure that the Pedestrian Path is not reduced by obstacles, such as garbage receptacles, mail boxes, planters, lighting standards, grates and other street furniture;
 - This includes a minimum vertical clearance of 2.1m from the ground to any overhanging objects.
- The “Walking Surface” of the pedestrian path will be a slip-resistant concrete surfaces should have broom finish or an integral abrasive grain;
- When applicable, “Sidewalks should have linear detectable strips that define the edges of the walkway as a warning and orientation system.” (FADS – 6.6.1); and
- Sidewalks will be constructed per City of Windsor Standards:
 - Residential Concrete Sidewalks AS-401
 - Commercial Concrete Sidewalks AS-403

Placement

- The pedestrian path is the pavement located between the Curb Side zone and the Building Side zone on the Boulevard.



4.5 Building Side

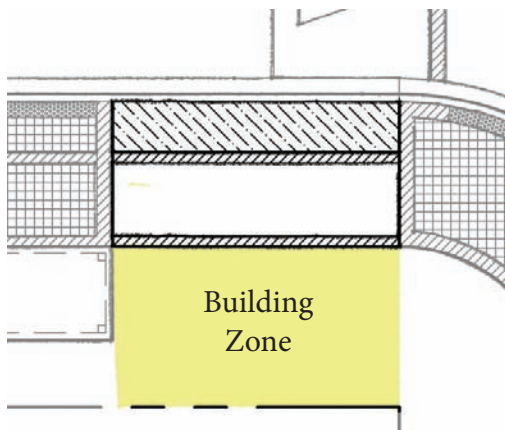


Figure 4.5-A, Building Side



Pedestrian Path and Building Zone



The Building Side zone is the area located between the Pedestrian Path and the property line. In commercial areas this is the “encroachment zone” on the boulevard for sidewalk cafes, merchandise displays, public art, public open space and sidewalk signs in order to keep the pedestrian path unobstructed.

Design Criteria

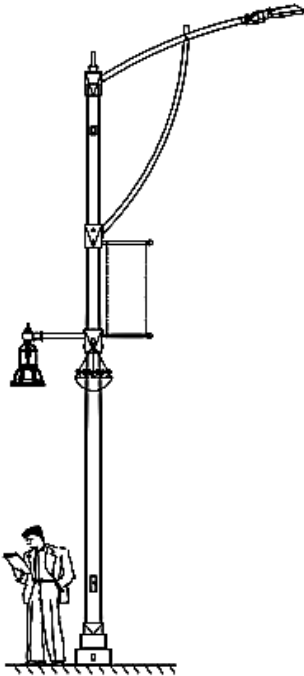
- The width of the building side zone will vary as it is comprised of the remaining space of the boulevard after the curb side and pedestrian path have been established. The width may vary from no space available to several metres – dependent on the setback of the private property line;
- The Building Side zone along Riverside Drive will largely consist of landscaping and vegetation opportunities; and
- In commercial areas along Riverside Drive, where buildings are at the property line, the Building Side zone may be comprised of hard scape
- The Building Side zone is the appropriate location for individual businesses to accommodate limited outdoor uses (sidewalk cafes and merchandise displays/outdoor plazas with public art)

Placement

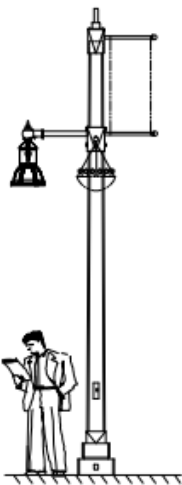
- The building side zone is the area located between the pedestrian path and the property line.



5.0 Lighting Standards



Street and Pedestrian Combined
Lighting Standard



Pedestrian Lighting Standard

One of the purposes of the Riverside Drive Vista Improvement E.A. was to showcase the importance of the scenic drive to both the residents who live in Windsor and people who visit the area. Street lighting plays an important role in establishing Riverside Drive's significance as a Scenic Drive. It also helps to establish the character, function, quality and security of a streetscape, especially at night. The scale, style, lighting effect, existing infrastructure and contexts, cost and maintenance of lighting standards are criteria that have influenced the selection of the standard posts, armatures and luminaires.

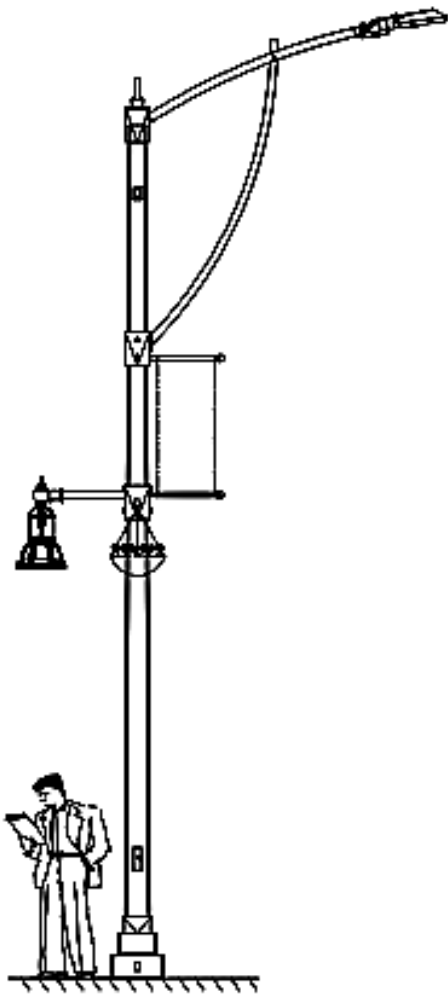
To be consistent with the "Windsor Style" light standard which will remain the standard for the stretch of the Downtown along Riverside Drive, the City worked with King Luminaire to design the new "Windsor Vista" family of street light standards based on the "Windsor Style". Ensure all light standards are full-cut off as per CR 228/2005. The following section identifies the type of standard that will be used when considering the different contexts along the drive.

Lighting Standards

- 5.1 "Windsor Vista"--Street and Pedestrian Combined Lighting Standards
- 5.2 "Windsor Vista"--Pedestrian Lighting standards
- 5.3 Street Lights for Utility Poles
- 5.4 City Centre-"Windsor Style"



5.1 “Windsor Vista” - Street and Pedestrian Combined Lighting Standards



Street and Pedestrian Lighting Standard

The Street and Pedestrian Combined Lighting Standard provides direct and adequate illumination to the roadway for enhanced vehicular circulation and to the sidewalk for pedestrian activities. The street light standard selected for this area is the King Luminaire in black concrete with an acrylic finish (see appendix for details).

Ensure all light standards are full-cut off as per CR 228/2005.

DESIGN CRITERIA

The lighting standard is designed with:

- Two luminaires:
- ‘cobra-head’ (high-pressure sodium-vapour) fixture suspended over the roadway.
- pendent (metal halide) fixture suspended over the boulevard.
- the luminaires and components will be an aluminum metal in a textured black finish
- Two mounting arms:
- roadway-oriented arm suspending a luminaire over the roadway and attached to the decorative concrete pole mounted approximately 7.62m (17.6’) above grade.
- sidewalk-oriented arm suspending a pendent luminaire over the pedestrian path and suspending a banner over the curb side, and attached to the decorative concrete pole approximately 4.27m (14’) above grade.



PLACEMENT

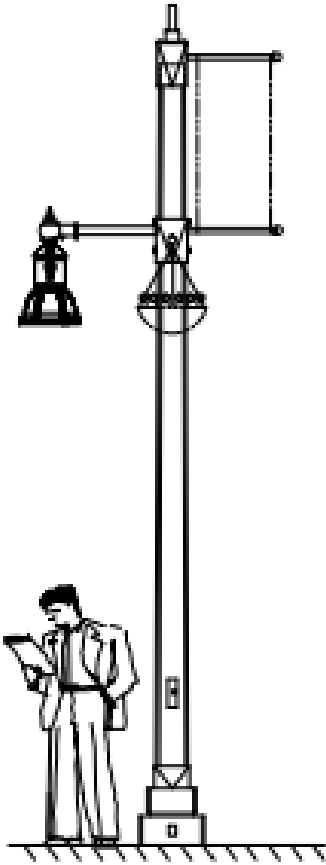
The street/pedestrian combined lighting standards are installed within the curb side service zone which conforms to the overall streetscape organizational system.

- The spacing of street/pedestrian combined lighting standards along the streetscape is dependent on: the physical attributes of the corridor (width of overall public right-of-way from building wall to building wall, the width of the boulevard and the adjacent land uses);
- to meet the requirements as per the City of Windsor Street Lighting Design and Installation Guidelines
- The street/pedestrian combined lighting standards are typically spaced at 15m (49') apart.
- Ensure all light standards are full-cut off as per CR 228/2005.



5.2 “Windsor Vista” - Pedestrian Lighting Standards

The Pedestrian Lighting Standards provides direct illumination to enhance sidewalk and walkway areas for pedestrian activities.



Pedestrian Lighting Standard

DESIGN CRITERIA

The lighting standard is designed with one mounting arm containing a decorative pendant luminaire (LED) suspended over the sidewalk and attached to a decorative concrete pole approximately 4.27m (14') above grade. The lighting standard will be full cut-off to comply with CR 228/2005.

The pedestrian light standard selected for this area will be black concrete with an acrylic finish.

The luminaire and components, banner arms, capping and base are aluminum with a textured black finish.

PLACEMENT

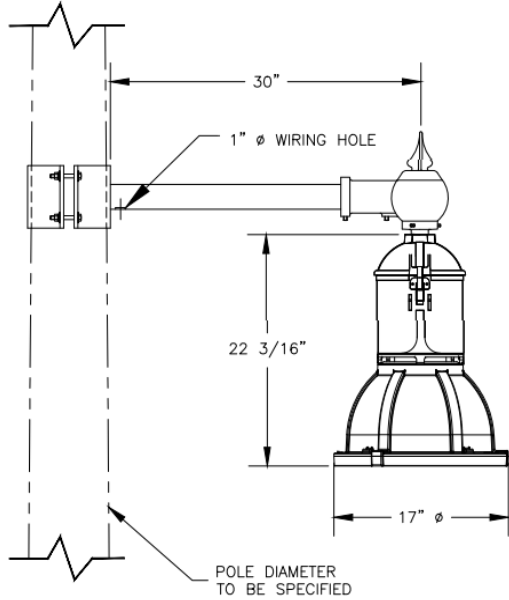
The pedestrian lighting standards are installed within the curb side service zone which conforms to the overall streetscape organizational system as the location for fixed and moveable fixtures.

The placement and spacing of pedestrian lighting standards along the streetscape is dependent on:

- Increasing or supplementing illumination levels greater than the illumination output from the street/pedestrian combined lighting standard;
- providing greater illuminations levels in areas of intense pedestrian traffic;
- providing illumination in areas where the combined lighting standard is not deemed to be appropriate;
- providing a greater sense of security at night through enhanced illumination; and
- enhancing the unique pedestrian-oriented character of the downtown.



5.3 Street Lights for Utility Poles



Pedestrian Light

There are several utility lines than span along Riverside Drive that provide hydro, telephone, cable, and other important services. These lines are supplied through a standard concrete utility pole that often includes a cobra light arm and has the dual function of acting as a street light. The preferred solution is to bury these services to reduce their negative visual impact and improve safety and maintenance associated with servicing these lines. However, often, because of cost, they cannot be buried. Where these hydro lines still exist, the existing cobra arm will be replaced, in textured black. The elliptical arm is available in 4', 6' or 8' lengths. See Schedule A specifications for details.

If utility poles cannot be buried and they are being replaced, the "black" composite poles supplied through ENWIN should be used.



5.4 City Centre - “Windsor Style”



Street and Pedestrian Lighting Standard



Pedestrian Lighting Standard

Riverside Drive spans through a number of different contexts including Downtown Windsor. In 2005 Windsor City Council adopted the City Centre Planning District City Centre Streetscape Standards Manual (CR48/2005). However by 1996, prior to City Centre Planning District City Centre Streetscape Standards Manual adoption the existing “shoe box” light standards were being decommissioned in favour of a new “Windsor Style” light standard specially designed for Windsor’s Downtown through King Luminaire.

The “Windsor Style” light standard will remain the standard for the stretch of the Downtown along Riverside Drive between Salter Avenue on the West (As if Salter Avenue continued north to intersect with Riverside Drive) and Marentette Avenue on the East. The “Windsor Style” includes a combined street and pedestrian light standard and a pole designed just for pedestrian lighting . Refer to the City Centre Planning District: City Centre Streetscape Standards Manual for specifications. Ensure all light standards are full-cut off as per CR 228/2005.



5.5 Traffic Signal Lights



For the purpose of being consistent with other streetscape elements such as the new street lights selected for Riverside Drive (see Section 5.1) all traffic signal light poles, traffic signal light arms and standard elliptical arms and cobra heads will be textured black.

5.5 Signal Light Boxes



All traffic signal boxes and utility boxes should be wrapped with print designs that further illustrate the cities history and brings some colour and art to Riverside Drive.



6.0 Landscape Standards

Plantings will consist of boulevard trees in either subsurface pits or in elevated planters, as well as moveable planters and hanging baskets for seasonal display, planted with a colourful range of plant materials. Permanent plantings along Riverside Drive to give priority to indigenous plant material where appropriate natural settings are desired. Plant material should also be chosen for its ability to withstand urban conditions and the climate of Windsor, for its visual interest throughout the year, and for ease of maintenance.

A continuous tree canopy of diverse, drought resistant and climate appropriate trees is integral to the redevelopment along the Riverside Drive Scenic Vista, and is fundamental to the image of a quality liveable city. While trees provide positive modification to our climate, help to diminish water and air pollution, calm traffic and to some extent mitigate sound, it is fundamental that the tree canopy should be high-branching in order maintain vistas from the street level to the Detroit Skyline while fundamentally establishing a friendlier and healthier environment desirable for a pedestrian streetscape.

The following standards are to be read in conjunction with section 4.1.4 Street Trees and Horticultural Plantings of the Class Environmental Assessment for Riverside Drive Vista Improvement Project of the City of Windsor.

DESIGN CRITERIA

- Existing mature trees and woodlots should be preserved and made features of the community/neighbourhood;
- Street trees should be planted throughout the roadway, especially along both edges of the Riverside Drive Scenic Vista to encourage pedestrian activity;
- Tree pits (where required) should be constructed using a connected trench method to provide optimal growing conditions, ensuring regulation of moisture levels, and maximum room for root growth;
- Only non-invasive and preferably native species that are tolerant of urban conditions, salt, poor soil, vehicle pollutants and uneven irrigation, with high branching characteristics should be planted (See recommended Tree Species List);
- Riparian area along the Detroit River is to be planted with native species only;
- It is highly encouraged that “no mow” areas are installed with native plants and grasses within the Riparian areas along the rivers edge.
- To ensure that trees do not suffer from soil compaction that restricts water and air around their roots, the bases of trees should be planted with groundcover or shrubs and mulch. Metal tree grates should only be used in intensely used areas such as pedestrian plazas and combined vehicle/pedestrian accesses;
- Consider the proximity to hydro lines and the height of tree growth when selecting trees in open areas. Refer to recommended street tree list for small trees or height restrictions under Section 6.3.



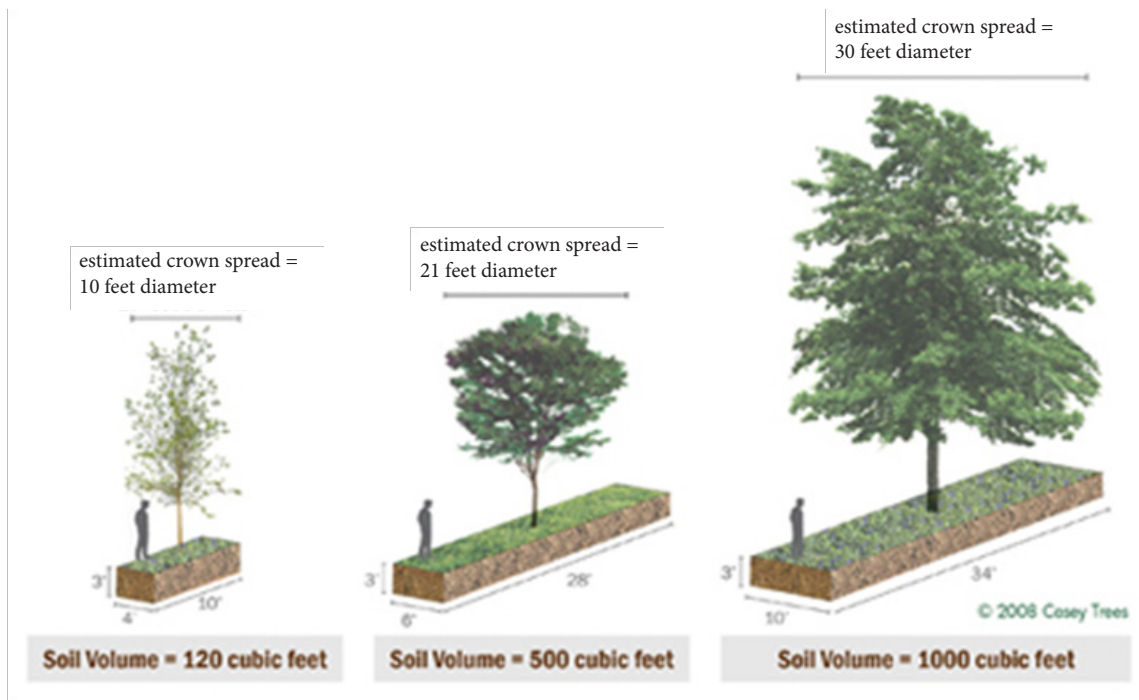
- Where medians are proposed, raised planted areas with trees are encouraged, but with a simplified planting pattern that eliminates the need for constant maintenance; and
- It is desirable to use local native species to provide the most benefit to local wildlife, and climate change adaptation, and reduce the spread by local wildlife of aggressive species, which may cause harm to natural heritage.

6.1 Soil Volumes for Trees

Planting trees with proper soil volumes is imperative to the longevity of the planting. Using correct soil volumes ensures that a tree is more than an amenity but a vibrant component in the urban environment to assist in; air filtration, storm water run-off, heat island effect reduction, and cooling.

DESIGN CRITERIA

- Recommendations ranges for soil volume are:
 - 11.3 cm (400 cu.ft.) min. for a tree with a crown of 6m (20ft) wide; to
 - 28cm (1000 cu.ft.) for a tree with a crown spread of 9m (30 ft) wide.



6.2 Trees in Subsurface Pits

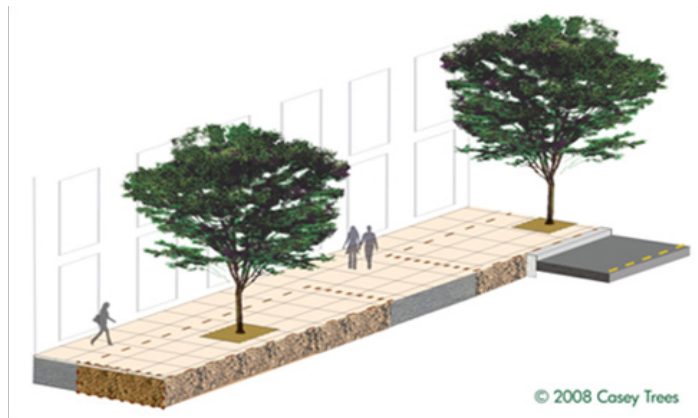
Subsurface pits are only to be used where there is a need or desire for trees when there is no place but to incorporate tree planting with paved areas. Often these areas may experience high pedestrian volumes which require modest landscape engineering.

DESIGN CRITERIA

- The installation method consists of a large subsurface pit for the establishment of good roots systems;
- A support, consisting of either a prefabricated metal grate (i.e. DURA-Plate) or a Modular Suspended Pavement System (i.e. Strata Cell or Silva Cell) designed for planting trees in suspended pavement with a minimum 90% soil capacity;
- Size of trees in subsurface pits to be a minimum of 70mm (3in) caliper ABH to provide immediate impact and more resistance to vandalism; and
- Surfaces over the planting pit may consist of solid or permeable paving that allows water to penetrate in to the roots.

PLACEMENT

- Trees planted in subsurface pits to be between 8 m to 12 m (26 to 40ft) apart, depending on the species to achieve proper root volumes;
- Where placement of trees are closer than what an appropriate root volume would dictate for a particular tree species, then continuous interconnection of subsurface pits, such as vaults, will be utilized;



- Where prefabricated metal grates are necessary, they should only cover the soil not immediately adjacent to tree stem; and
- Adjustable tree grates that allow for growth of the tree should be used. Gravel should be filled under the tree grate to prevent debris from accumulating between it and the finished planted grade.



6.3 Trees in Open Areas or Fixed Planters

Trees planted in open areas or provide the most ideal condition for growing when proper soil volumes are respected. Areas of open tree planting provide a park-like setting and offer greater pedestrian aesthetics.



DESIGN CRITERIA

- The installation method consists of a large subsurface pit for the establishment of good roots systems but open to allow for a vegetative porous surface treatment at grade level;
- Turf grass may be used where modest pedestrian activity may be encountered over the root zone;
- Vegetative plantings of low shrubs and herbaceous perennial plants or low profile ornamental fencing not exceeding 70mm (27in) may used to restrict pedestrian access over root zones;
- Walls of planter beds should be raised to a height between 40cm and 50cm (16in and 20in) so that enclosure does not create a trip hazard, while offering an opportunity for pedestrians to sit; and
- Selected plant material to be tolerant of salt sprays and snow loading.

PLACEMENT

- Predominantly to be located along residential properties and passive park settings along Riverside Drive where traffic volumes are low and speed is below 50km/hr;
- These types of planting areas are not suitable for high pedestrian areas unless there is a desire to control the pedestrian flow to areas (i.e. mid-block street crossings or unprotected grade changes);
- Raised planters may be desired where salt spray might become an issue to the health of the plantings or pedestrian control is required; Where existing trees have already been established and require maintaining the current growing conditions;
- These types of planting areas are not suitable for high pedestrian areas unless there is a desire to control the pedestrian flow to areas (i.e. mid-block street crossings or unprotected grade changes);



- Raised planters may be desired where salt spray might become an issue to the health of the plantings or pedestrian control is required; and
- Where existing trees have already been established and require maintaining the current growing conditions.
- The full list of trees and more details can be found on the City of Windsor Parks Department for the Common Tree Species Planting List: [<https://www.citywindsor.ca/residents/parksandforestry/Urban-Forest/Pages/Tree-Guide.aspx>]

The following are recommended street trees:

Acceptable Native Trees of Windsor/Essex	
NAME	COMMON NAME
Acer x freemanii	Autumn Blaze Maple
Acer saccharinum	Silver Maple
Acer saccharum	Sugar Maple
Acer rubrum	Red Maple
Amelanchier spp.	Serviceberry (Downy, Canadian, Smooth)
Betula nigra	River Birch
Celtis occidentalis	Hackberry
Gleditsia triacanthos	Honey locust (thorn-less varieties only)
Gymnocladus dioicus	Kentucky Coffee Tree
Liriodendron tulipifera	Tulip Tree (Liriodendron tulipifera)
Liquidambar styraciflua	Sweetgum (Liquidambar styraciflua)
Platanus occidentalis	Sycamore – Spring only
Quercus alba	White Oak – Spring only
Quercus rubra	Red Oak – Spring only
Quercus macrocarpa	Burr Oak – Spring only
Quercus bicolor	Swamp White Oak – Spring only
Quercus palustris	Pin Oak – Spring only
Tilia americana	American Basswood
Ulmus americana	American Elm (DED Resistant varieties)
Acceptable Non-Native Trees for Windsor	
Tillia cordata	Little Leaf Linden
Platanus acerifolia	London Plane Tree 1 – Spring only
Syringa reticulata	Japanese Tree Lilac
Ginkgo biloba	Ginkgo (Maidenhair) Tree (Male Plants Only)
Cercidiphyllum japonicum	Katsura Tree^ 1
Small Trees for Height Restricted Areas or Confined Spaces. (Acceptable as Small trees & Tree-form Shrubs)	
Serviceberry	Serviceberry
Japanese Lilac Tree *	Japanese Lilac Tree *
Flowering Pear	Flowering Pear
Ironwood	Ironwood
Redbud +	Redbud +
Mountain Ash +	Mountain Ash +
Mimosa Silk Tree *1	Mimosa Silk Tree *1
Blue Beech	Blue Beech
Magnolia*^ 1	Magnolia*^ 1
Columnar English Oak 1	Columnar English Oak 1



Recommended street trees continued:

SMALL CANOPY TREES (Under 7.6m (25ft) high) (Ideal for raised planters)	
NAME	COMMON NAME
Large	
Abies concolor	Columnar Fir
Chamaecyparis nootkatensis	False Cypress
Metasequoia glyptostroboides	Dawn Redwood
Picea gloucu	White Spruce
Picea pcengens	Columnar Spruce
Tsuya canadensis	Easton Hemlock
Small/Narrow	
Juniperus virginiana	Easton Red Cedar
Thuja occidentalis	Easton White Cedar

NOTES:

* Non-native

+ Both Native & non-native species/varieties are available

^ Special soil considerations required

1 Limited to small numbers and use for special projects

1. Acceptable ‘varieties’ or ‘cultivars’ of the above listed trees may be available from local nurseries. i.e. Honey Locust “Bloodgood”. Availability of specific varieties may change from year to year.
2. Other suitable species or varieties may be available. Please contact City of Windsor, Forestry, to confirm acceptable selections and for possible special requirements or planting conditions.
3. In general, we wish to limit the use of exotic species in street tree plantings across Windsor and focus on locally native and North American native species with a select few naturalized or commonly used exotics. Anything with the potential to become invasive should be avoided. Anything with disease, insect pest or structural issues should be avoided or in limited use only.



6.4 Shrub, Perennials and Ornamental Grass Plantings

The preferred method is simplicity. Massed plantings with a diversity of colours will create a unified composition, appropriately scaled to the width and proposed character of the street.

DESIGN CRITERIA

- All plant material should be chosen for its ability to withstand Windsor's climate and the vehicular environment along Riverside Drive;
- Simple planting patterns should be employed for ease of maintenance and visual impact. Strong consideration for the use of blue flowering or blue foliage plant material to connect the plantings with the river side location is recommended;
- Plant material used should be chosen for its low maintenance requirements, year round durability, and seasonal colours;
- Irrigation of planting areas should be used where necessary to ensure plant health and attractiveness; and
- Irrigation of any median plantings including trees is required.

PLACEMENT

- Use masses of low-maintenance plants placed at key locations to direct pedestrian traffic, screen parking lots and provide visual interest;
- Low maintenance planters and planting areas should be used at the street edge to soften hard surfaces, i.e. parking lots. Plantings should be used to announce entrances, accent open space areas and define walkways; and
- These elements should not cause vehicular or pedestrian siteline issues.



6.5 Vegetation in Moveable Planters

Vegetation in moveable planters provides an added dimension and additional colour to the overall landscaping scheme for the streetscape. Planters should contain ornamental grasses, evergreen shrubs, seasonal plant materials, annual flowers and various groundcover plants.

DESIGN CRITERIA

- All plant material for moveable planters should be chosen for its ability to withstand Windsor's climate especially during the summer months;
- Simple planting patterns should be employed for ease of maintenance and visual impact;
- Plant material used should be chosen for its low maintenance requirements, and seasonal colours including fall colour. Alternately, vegetation should be replaced in September for a seasonal effect lasting through the autumn months;
- Moveable planters are to be of a high quality, light weight materials (i.e. Metal (with polyvinyl liner), Fibreglass or Glass Fibre Reinforced Concrete) easy to be relocated when required;
- Good planter design incorporates the following:
 - The container size and design reflects the type of greenery to be planted (root growth and spread) and weighted to prevent easy movement;
 - The materials are coordinated with other elements of street furniture and/or adjacent buildings;
 - Provides adequate watering and drainage, with consideration for reducing instances of staining on the nearby paving surface from planter drains.
- Colour ranges of moveable planters material will be black, to maintain a complementary and unified character with other street furnishings along Riverside Drive, however other colour ranges may be selected for SSIA, and important nodes based on administration's approval;
- Depth of planters to be adequate enough to accommodate the root systems of plant material being supported for that growing season;
- Moveable planters should occupy a surface area of at least 0.37m² (4 square feet); and
- Where planters are to be used as additional seating areas, they should be between 0.4m and 0.5m (16 in. and 20 in.) in height with a rim of at least 0.4m (16 in.) in width.



PLACEMENT

- Moveable planters should be restricted to areas of higher importance along Riverside Drive (i.e. Primary Nodes) where larger numbers of public may be found;
- Moveable planters can be located to direct pedestrian traffic, create focal points and provide resting areas; and
- Moveable planters must not block other streetscaping elements and pedestrian mobility. Moveable planters are restricted to the curb side zone and or building side zone – not within the pedestrian path and corners of intersections.



6.6 Seasonal/Festival Displays and Hanging Baskets

Vegetation for seasonal displays such as hanging baskets and holiday/festival decoration, can provide a sense of place as well vibrancy to the experience of travelling along the drive or by attending functions at the various nodes. Such displays should contain non-permanent plant material such as ornamental grasses, seasonal plant materials, annual flowers and cuttings (i.e twigs and fresh cut holiday evergreens).



DESIGN CRITERIA

- Simple planting patterns should be employed for ease of maintenance and visual impact;
- Plant material used should be chosen for its low maintenance requirements, and coordinated with seasonal colours themes for lights and special events;
- Vegetation in hanging baskets should last into the cooler autumn months, or alternately replaced in September for a seasonal effect lasting through the season; and
- Hanging basket design incorporates the following:
 - The container size and design reflects the character of the space in which it is being installed;
 - The materials are coordinated with the of other elements of street furniture and/or adjacent buildings;
 - Provides adequate watering and drainage, with consideration for reducing instances of staining on the nearby paving surface from planter drains;
 - Seasonal/Festival Displays should be complementary to Riverside Drive Scenic Vista and not overpower the views to the Detroit Skyline; and
 - Illumination of/within seasonal displays to add to the positive character of Riverside Drive. Use of LED lights is highly recommended (Required).

PLACEMENT

- Hanging baskets can be associated with any street lamp or specifically designated pole for such installations; and
- Seasonal display such as holiday decoration or temporary festival plantings should be associated with areas where larger public gatherings may occur during the time that they are installed. Such displays may not be appropriate along the areas of Riverside Drive where the primary use is residential.



7.0 Street Furnishing Standards



Bench



Waste Receptacle



Café Enclosure

Street furnishings such as benches, bicycle parking, and trash receptacles are important functional amenities along the streetscape that create a liveable pedestrian environment.

All furnishings placed within the boulevard should be of high quality, designed for outdoor use and require minimum maintenance. To be consistent with the street furnishings in the downtown, refer to the *City Centre Streetscape Standards Manual* (CR 48/2005) for street furnishings standards for the stretch of the downtown along Riverside Drive between Salter Avenue on the west and Marentette Avenue on the east. To be consistent with design elements within the Central Riverfront Parkland area, street furnishings will be those selected as the standard for the Central Riverfront Parkland area.

- **Comfort:** Furnishings are designed for ease of use and are easily accessible from the pedestrian path;
- **Safety:** The consistency of the placement of street furniture adjacent to the pedestrian path establishes a sense of predictability. Consider any moving parts on the furniture that can make the streetscaping element unsafe or hazardous
- **Maintenance:** Street furniture should be designed to be durable, easy to maintain with standardized parts and easy to remove for maintenance or replacement; and
- **Crime Prevention:** Street furniture should be constructed with materials and finishes that resist and discourage vandalism and do not attract excessive loitering.

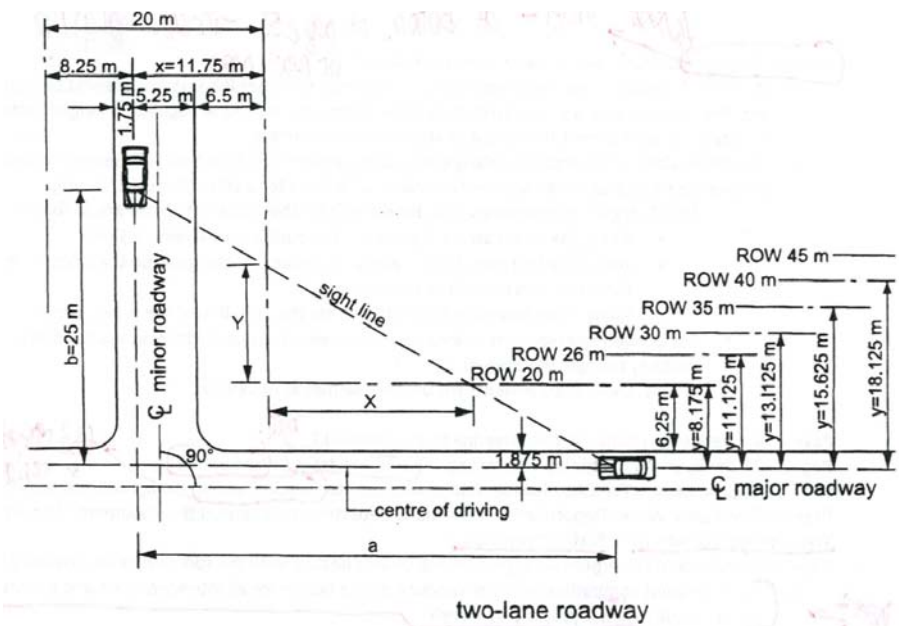
Street Furnishings

7.1	Benches	7.8	Newspaper Vending Racks
7.2	Waste Receptacles	7.9	Sidewalk Cafe Enclosures
7.3	Bicycle Parking	7.10	Sidewalk Signs
7.4	Dog Waste System	7.11	Information Kiosks
7.5	Bollards	7.12	Banner Poles
7.6	Transit Shelters	7.13	Decorative Banners
7.7	Telephone Booths	7.14	Screening Devices



Visibility Triangles

- Within the visibility triangle at intersections, all street furniture, pedestrian orientation signs, plantings, etc. that could create a view obstruction (other than official signs at regulation height) should be avoided in a zone from 1.0 m to 2.4 m above the road surface;
- The dimensions of the visibility triangle should be determined based on TAC Geometric Design Guide requirements (see attached scan - dimension “a” is 50 m for a 60 km/h design speed:
 - For 90 degree intersections, the dimensions for the visibility triangle are as follows:
 - Along the minor street: 23 m from the curb line of Riverside Drive
 - Left along Riverside Drive (relative to an approaching car on the minor street): 45 m from the curb line of the minor street
 - Right along Riverside Drive: 48 m from the curb line of the minor street
 - For skewed intersections or locations on curves, a site-specific review is needed to determine visibility triangle requirements
 - Ideally, these requirements should be met at driveways



7.1 Benches



Regular Bench with Back
Maglin MBE-0870-00025 in Black/Onyx Colour



Backless Bench
MBE-09700008 in Black/Onyx Colour



Backless Legless Bench
MBE-0720-0005 in Black/Onyx Colour



Table Set with Chess Board
Maglin MTB-1100-00002 in Black/Onyx Colour

BENCHES provide opportunities for seating along the streetscape. The bench selected for the area of Riverside Drive outside of the Downtown and Central Riverfront Parkland is the Maglin MLB870W (bench with back) and the MLB970W (backless bench) in the Black/Onyx Colour.

DESIGN CRITERIA “How It Functions”

- The benches selected are:
 - Designed to be durable and comfortable;
 - Complementary to the style of the surroundings and other selected street furnishings; and
 - Permanently secured to the ground (paving) for stabilized sitting and to avoid theft and/or vandalism.

PLACEMENT “Where It Goes”

- Benches will be provided along the streetscape in the curb side zone and the building side zone where space allows and that the clear pedestrian path remains unobstructed by providing a separate area for seating.



7.2 Waste and Recycle Receptacles



Maglin Waste Receptacle With Bonnet
MTR-0200-00020 , in Black/Onyx



Maglin Waste Receptacle
MRR-0200-00005 , in Black/Onyx



Side-Open Recycle Unit
Maglin- MAR-0100-00001 Black/Onyx Colour

WASTE RECEPTACLES are designed and positioned so they are unobtrusive and easily accessible for use by pedestrians and for trash collection.

The waste receptacle to be used along Riverside Drive except in the Downtown or Central Riverfront Parkland area will be the Maglin side-open waste receptacle, product number MLW200S-32 with top-shield side mounted ash receptacle. Product number MLAU101. If recycling is required use the side-open unit from Maglin, product number MLAU101. All Receptacles will be black/onyx in colour.

DESIGN CRITERIA – “How It Functions”

- Waste receptacles are designed with the following features:
 - Two separate pieces – an inner container to contain the trash and ensure easy removal and an outer shell that blends aesthetically with other streetscape elements;
 - Covered tops and sealed bottom to keep the contents dry, contained and out of sight at all times; and
 - Firmly attached to the paving surface to avoid vandalism.

PLACEMENT – “Where It Goes”

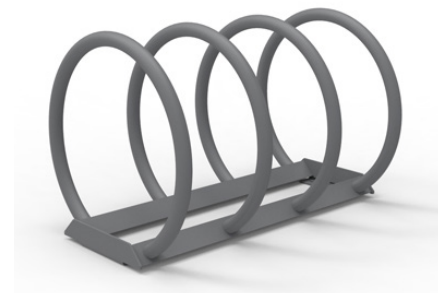
- Waste receptacles will be positioned within the curb side zone of the boulevard where they are easily accessible for use and collection. Receptacles will be strategically arranged with other streetscape elements into functional clusters.
- Waste receptacles will be placed where trash originates or is likely to be deposited. Typical locations include bus stops, parking lots, major pedestrian street crossings, restaurants and places where people lunch outdoors.



7.3 Bicycle Parking



Single Bicycle Parking Maglin MBR-0200-00005 (double sided)



Multi Bicycle Parking Maglin MBR-0350-00001

Bicycle Parking are provided as on-street parking spaces for bicycles to encourage bicycle transportation use to and through the district. The installation of all bicycle parking must comply with the Policy for Bicycle Parking on Public Parking (CR724/2004). The standard bicycle rack to be used along Riverside Drive will be the Maglin MBR200 (double sided) with lettering that states “*City of Windsor Riverside Drive*” in black/onyx colour and lock to the post. In some locations, such as the entrances of parks, a multiple-use bicycle rack may be required. The selected model is the Maglin CMBR 91270 in black/onyx colour (see schedule A for details).

DESIGN CRITERIA – “How It Functions”

- The bicycle rack selected consists of a single post and ring design;
- Key features in the bicycle rack design include:
 - Simplicity in the style;
 - Slim design to prevent intrusions onto the pedestrian path; and
 - Ease of use in providing a temporary place to securely lock the frame and at least one wheel of a bicycle.
- The bicycle rack can be ‘personalized’ for the district though raised lettering on the ring and various finish colours to complement the other streetscape elements and urban surroundings; and
- All bicycle parking will be black/onyx colour.

PLACEMENT – “Where It Goes”

- Bicycle parking will be placed in the curb side of the boulevard where they will not present a tripping hazard on the pedestrian path; and
- Bicycle parking will be distributed in small numbers widely throughout the City Centre district where parked bicycles will be visible and can be easily monitored, and in the following general locations:
 - Near public building entrances;
 - Near formal and informal gathering places;
 - Designated and protected spaces within a vehicle surface parking lot and/or adjacent to a parking garage.



7.4 Bollards



650 Bollard from Maglin (or similar) Fixed or Removable Black/Onyx



650 Bollard from Maglin (or similar) Removable swing gate

BOLLARDS create low, semi-transparent barriers where it is desirable to separate vehicular and pedestrian traffic and to direct circulation patterns. The bollard to be used along Riverside Drive except in the downtown and Central Riverfront Parkland will be the Maglin Fixed or removable 650 Bollard in black/onyx colour. Consideration may be given to other bollard types based on use and context.

DESIGN CRITERIA – “How It Functions”

- The bollard style selected is of the same material and finishes as the decorative pre-cast concrete lighting standards and complements the other streetscaping elements.
- Depending on the location and the desired effect, a bollard can contain a luminaire to provide low-level illumination to highlight or limit access to a special feature, such as a pathway or monument.

PLACEMENT – “Where It Goes”

- Bollards are permanently installed elements. Bollards will not be installed within the pedestrian path;
- Removable bollards will be used in locations where service and emergency vehicles require periodic access;
- Clearances between bollards, or between a bollard and any other structure or pole, must be 1.0m (39”) at a minimum, but not wide enough to permit the passage of a motorized vehicle; and
- Clearances to permit pedestrians to pass between bollards or a bollard and a structure must be 1.5m (60”) at a minimum.



7.5 Transit Shelters



Example of a proposed Daytech Avanti bus shelter

TRANSIT SHELTERS are provided for transit riders at major transfer points along Riverside Drive. Shelter structures may be provided at transit stops where extended waiting times are experienced or where the stop is not adequately shielded from prevailing weather conditions.

DESIGN CRITERIA “How It Functions”

- The size of the transit shelter installed should reflect the number of boarding passengers expected for that stop and the space available on the boulevard;
- Transit shelters are enclosed in a transparent material (usually glass) so transit riders can clearly see arriving busses and the sightlines of drivers are not obstructed;
- Transit shelters are oriented so that the doorways’ are facing away from the roadway to prevent waiting patrons from being trapped;
- Must comply with Transit Windsor Standards;
- Transit shelters will be powder coated in black/onyx to match other streetscape elements.

SIGNAGE

- Signage required for transit stops consists of:
 - “No Stopping” signs at “Bus Stop” clearly printed, which are visible to oncoming vehicular traffic.
 - Transit Route information displayed on or within the shelter structure detailing the bus departure times and contact information for schedules.

PLACEMENT “Where It Goes”

- Transit shelters are positioned between the sidewalk and abutting the property line;
- Transit shelters are positioned so they do not interfere with the surrounding pedestrian path; and
- Where possible, position transit shelters so access to underground utilities can be easily achieved.



7.6 EMERGENCY CALL STATIONS



Emergency Pylon

DESCRIPTION

Emergency voice communication call box or phone stations provide dedicated instant one-button, hands-free contact with a 911 operator to offer assistance in situations such as fire, accidents, criminal activity and injury or medical emergencies.

DESIGN CRITERIA

- Tower/Pylon/column styles are ideal as they are readily visible from a distance when necessary.
- Units are to be equipped with a blue emergency light as the wavelength is longer than any other colour in the visible spectrum, so as to be readily seen further even in inclement weather. The emergency light is to be able to flash/pulse when in use to help attract and identify to emergency services the exact location of the caller.
- Single One-Button operation, or optional easy connection to a personal cellphone should the incident requiring assistance be out of arm's reach.
- Provision of video cameras for 911 operator to monitor situations when in use.

- Two-way, hands-free speaker phone to allow for user to offer emergency medical assistance to injured person under direction of the emergency professional on the other end of the call.
- Ideally, solar powered or hybrid electrical/solar powered especially in remote locations where electrical services cannot be provided.
- All housing and equipment to be weatherproof, capable of withstanding extreme weather condition experienced in Windsor including but limited to; precipitation, humidity, heat and cold.
- Ideally coloured blue and contain clear lettering indicating EMERGENCY visible from all directions and compliant with AODA standards.
- Tower to include the City of Windsor logo and an emergency call number as an alternate means of contact for users.
- Tower to be numbered or provide an address should the Blue Light malfunction so that the user can provide information to the 911 operator

PLACEMENT “Where they go”

- Primary locations in visible areas such as parking lots or plazas.
- At trail entrance or crossing points.
- Playgrounds.
- Near built structures where no telephone services are available.
- Unit to be visible from Riverside Drive to allow for emergency services to locate the flashing blue light of an active/in-use system.
- Placement to be non-intrusive of popular vistas and views of the Detroit skyline, or popular tourist views along the riverfront trails system.



7.7 Sidewalk Cafe Enclosure



Example of Sidewalk Café Enclosure

The extent of a **SIDEWALK CAFÉ ENCLOSURES** should be defined by enclosures that clearly distinguish the private use from the pedestrian path

DESIGN CRITERIA – “How It Functions”

- All sidewalk cafes must have perimeter enclosure, which should appear ‘open’ to give the impression that it is part of the streetscape and contributes to the overall image of the area.
- Sidewalk café enclosures must be consistent with the *Sidewalk Café Handbook (2004)*

- The width of any access opening in the enclosure shall be no less than 1.5m (5’) and no greater than 2.0m (6.5’)
- Enclosures are constructed a sturdy material and are easily removed during the off-season.
- Dark painted, ornamental iron or aluminum enclosures are preferred. Wood enclosures are not permitted.
- Enclosures shall not penetrate the surface of the sidewalk (footings), although bolt attachments are acceptable.

PLACEMENT “Where It Goes”

- Sidewalks are first and foremost the public’s space and cafes cannot interfere with their use. Sidewalk cafes are located within the building side ‘encroachment’ zone of the boulevard and must not block or obstruct the free flow of pedestrian traffic.
- Additional sidewalk clearance width (greater than 2.4m or 8’) may be required near intersections to allow ample space for pedestrians to congregate.



7.8 Sidewalk Signs



Example of Sidewalk Signs



SIDEWALK SIGNS are temporary signs placed directly in front of commercial premises to identify the business and advertise the goods and services available inside. Sidewalk signs are also commonly known as “A-Frame Signs” and “Sandwich Board Signs”.

DESIGN CRITERIA – “How It Functions”

- Sidewalk signs cannot be permanently anchored to the ground and must be constructed in a manner and of materials to permit easy repositioning and/or removal;
- The regulations to display sidewalk signs on private and public property are outlined in Section 7.2 and Section 8.3.3 of By-Law 250-2004; and
- Regulations for sidewalk signs on the public right-of-way include:
 - illumination, animation and rotation of the sign are not permitted;
 - 1 sign is permitted per occupancy with a maximum sign face area of 1.0m² (10.7 square feet) and a maximum sign height of 1.2m (4.0’);
 - sign can only be displayed during hours of business operation and removed at all other times;
 - sign cannot be attached to any fence, tree or street furniture; and
 - sign must be displayed according to the provisions of the required Encroachment Agreement.

PLACEMENT “Where It Goes”

- Sidewalk signs will be permitted where there is adequate space within the building side encroachment zone of the boulevard to accommodate the sign and maintain a clear, unobstructed pedestrian path.
- The provisions for locating and placing each sidewalk sign on the public right-of-way will be outlined and enforced through the Encroachment Agreement for that sign.



7.9 Information Kiosks



Existing Information Kiosk with
Proposed Signage and Decoration

INFORMATION KIOSKS are intended to serve as collection and reference points for information on upcoming events, to direct and orient pedestrian traffic and to organize the outdoor space along the streetscape. They may only be used at key gateway areas, nodes and parks.

DESIGN CRITERIA – “How It Functions”

- The information kiosks are designed and positioned within the streetscape environment to meet the following criteria:
 - compatible with and complementary to the surrounding architecture and other streetscape furnishings;
 - become the focal point in an open area and may be combined with other streetscape elements;
 - facilitate the posting and easy removal of notices;
 - easily accessible from all sides; and
 - adequately illuminated.

PLACEMENT “Where It Goes”

- Information kiosks will be used sparingly where they are needed at specific locations to impart community information.
- Locating information kiosks is dependent on available spaces in the streetscape and at natural places of gathering.



7.10 Banner Poles



Example of existing Banner Poles



BANNER POLES are used as a mounting mechanism to display community and special event banner signs and other streetscape decorations that are suspended for a temporary period of time across the public right of way.

DESIGN CRITERIA “How It Functions”

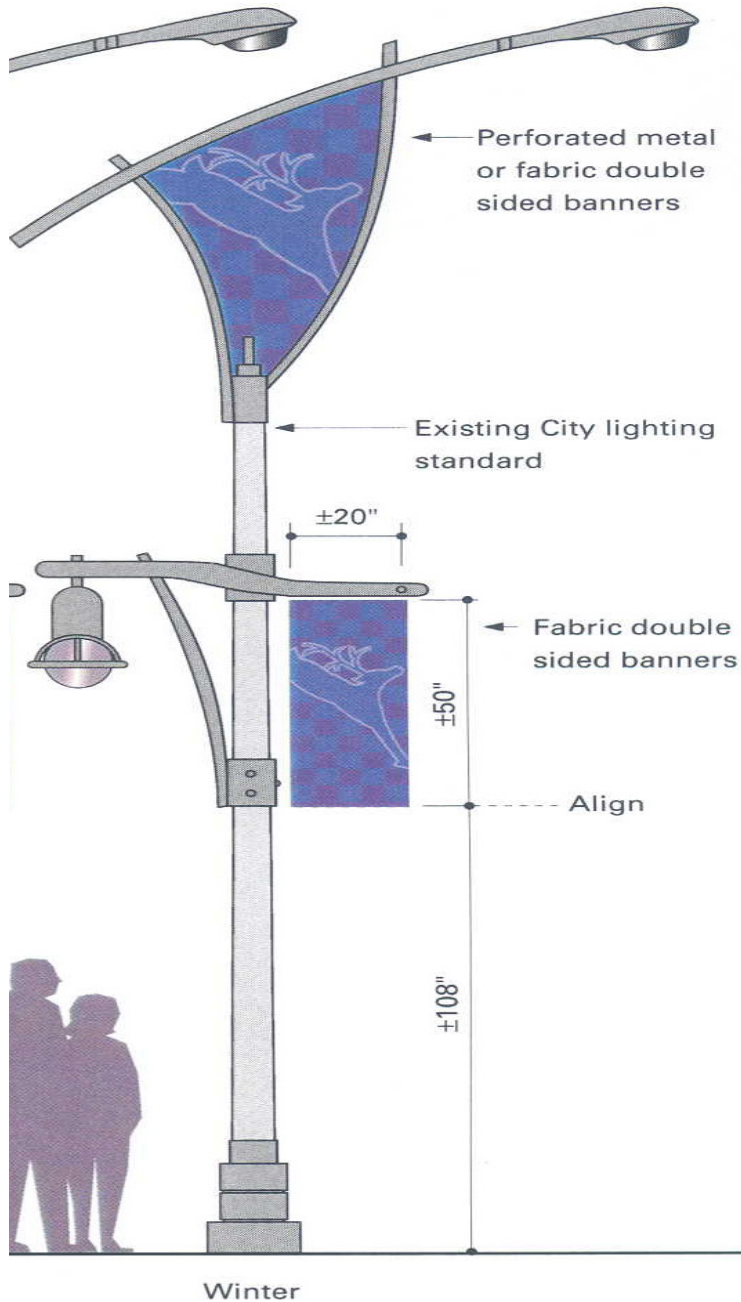
- Two existing banner poles within the district will be painted to match the other streetscaping elements until they are replaced;
- All new banner pole will be constructed of the same material and finishes as the decorative pre-cast concrete lighting standards and will complement the other streetscaping elements;
- Banner poles are engineered to support the live loading of the banner sign (and other attached decorative elements) at its maximum allowable size and through various environmental conditions;
- Banner poles are designed to be functional and attractive streetscape elements, which can also display various permanent and temporary decorative and artistic attachments to reflect the character of the district; and
- The new banner poles can contain electrical outlets to allow for the display of illuminated decorations.

PLACEMENT “Where It Goes”

- Banner poles are installed within the curb side zone of the boulevard in parallel positions to each other on opposite sides of the roadway and at the same setback from the curb as the lighting standards.
- Banner poles are typically placed at gateway areas into the City Centre to signify special areas or corridors within the larger district.



7.11 Decorative Banners



Example of Proposed Decorative Banners on Lighting Standards

DECORATIVE BANNERS provide colour, character and impact throughout the streetscape on a programmed or seasonal basis.

DESIGN CRITERIA “How It Functions”

- The installation of decorative banners can be used to:
 - Define the edges, roadways and pedestrian routes within the district;
 - Advertise or promote the district using the Business Association’s colours and logos;
 - Illustrate themes for current or upcoming events and civic programs; and
 - Provide historical information or references through logos and images.
- Decorative banners should be positioned to prevent the reduction of illumination output from the lighting standard to which it is attached or cast an undue degree of shadow.
- Banners to adhere to current Banner Policy

PLACEMENT “Where It Goes”

- Decorative banners are attached to the lighting standards in two (2) possible locations:
 - Displayed on the pedestrian lighting fixture’s mounting arm and suspended over the roadway;
 - Displayed within the open space created by the structure of the roadway lighting fixture’s mounting arm.



7.12 Screening Devices



Example of Existing Screening Device



Example of Proposed Screening Device

SCREENING DEVICES are used to shield an undesirable view, such as a surface parking lot, and to create a continuous building edge along the sidewalk that defines and unifies the streetscape.

DESIGN CRITERIA “How It Functions”

- Screening devices can be low walls, fences and landscaping elements. The use of solid screening devices should be discouraged as they generally restrict visibility for security purposes.
- Screening devices are designed based on the following criteria:
 - Screening devices should have an ornamental character as well as utilitarian function.
 - The materials selected for screening devices must relate to the surrounding building architecture through the use of same or similar design details and reinforce the character of the overall district.
 - The height of screening devices should be 0.8m to 1.1m (2.7’ to 3.5’) to allow for viewing into the adjacent space as a security measure without creating an area of entrapment
 - Where necessary, the screening device should have openings measuring 1.5m to 2.0m (5.0’ to 6.5’) in width to provide for pedestrian access to the sidewalk and that is barrier free.

PLACEMENT “Where It Goes”

- Screening devices are located on the property line between the public right-of-way and the abutting private property containing a use that must be screened (typically a surface parking lot). Corner cuts-offs at intersections may be required to enhance driver and pedestrian visibility.



8.0 Orientation Signage Standards



Proposed Street Name Signs

Orientation can be accomplished with an organized signage system that fits within the streetscape, minimizes visual clutter and provides understandable guidance that contributes to one's wayfinding ability. Signage shall conform to the Ontario Traffic manual(s) where applicable.

'Efficiency of information' is the ultimate goal in the establishment of the following design attributes for developing orientation signage:

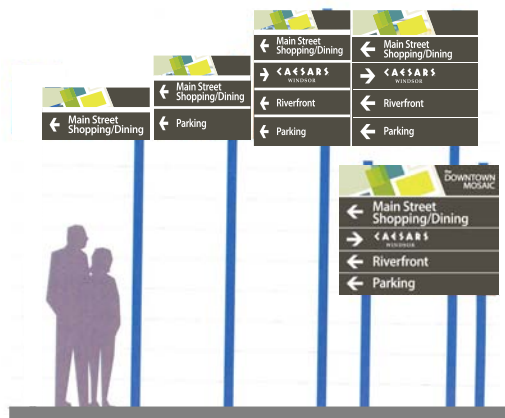
- Strength of Identity – consistency + uniformity [public space with a strong identity – know where you are]
- Consideration of Environment and Land Use [install signs only where necessary]
- Sign Hierarchy [distinction between signs geared to different users]
- Consistent Appearance [signs easy to find and reduces search time]
- Contrast with the Environment [signs stand out from their surroundings – yet unobtrusive]
- Consistent Placement Standards [reduce frustration in searching for sign and information]
- Differentiate between User Groups [geared to pedestrians versus drivers]
- Attain Maximum Legibility [a great amount of information easily understood]

Orientation Signage Standards

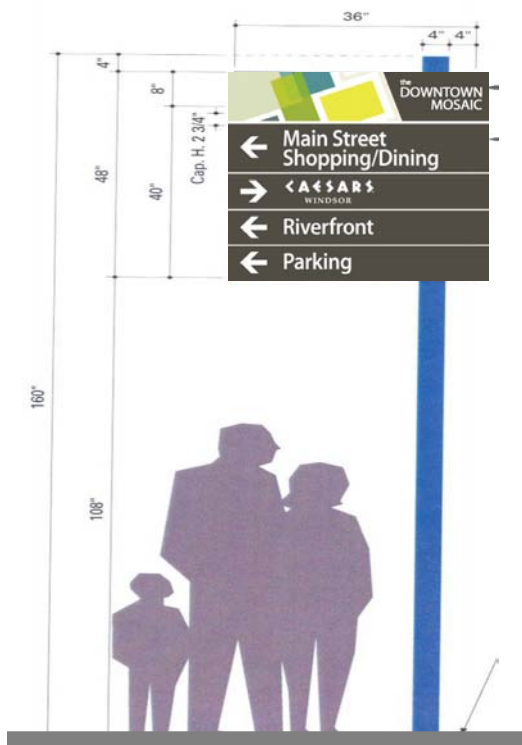
- 8.1 Trailblazing and Wayfinding Signs
- 8.2 Parking Area Identification Signs
- 8.3 Active Transportation Facilities Orientation Signs
- 8.4 Street Name Signs
- 8.5 Gateway Markers
- 8.6 Heritage Interpretation Signs



8.1 Trailblazing and Wayfinding Signs



Proposed Trailblazing and Wayfinding Signs



TRAILBLAZING SIGNS are intended to direct vehicular traffic towards major destinations inside and outside of the Windsor area.

WAYFINDING SIGNS are the basic type of orientation sign for directing users to significant visitor/tourist attractions, services and destinations within the core area.

DESIGN CRITERIA “How It Functions”

- Trailblazing and Wayfinding Signs contain a highly readable font and are characterized by an attractive, distinctive and readily discerned colour palette based on the Riverside Drive District logo.
- All signs must meet Accessibility for Ontarians with Disabilities Act (AODA) Standards.
- A range of Wayfinding sign sizes will be utilized, determined by the number of messages required at each location to a maximum of four (4) destinations identified per sign.
- Must utilize a UV resistance colour palette
- Refer to Trailblazing Policy

PLACEMENT “Where It Goes”

- Trailblazing and Wayfinding signs will be located at significant decision-making points within the Riverside Drive corridor, where they will be mounted as stand-alone elements;
- Trailblazing and Wayfinding signs are not to be attached to light standards
- The highest visibility for the pedestrian and cyclists is essential in positioning Wayfinding signs;
- Wayfinding signs may be placed on building walls at certain locations with the owner’s approval; and
- Should not block driver, pedestrian, or cyclist sightlines.
- Refer to Trailblazing Policy



8.2 Parking Area Identification Signs



Proposed Parking Area Identification Sign

PARKING AREA IDENTIFICATION SIGNS clearly identify and provide directions to parking lots and parking garages within the commercial district.

DESIGN CRITERIA “How It Functions”

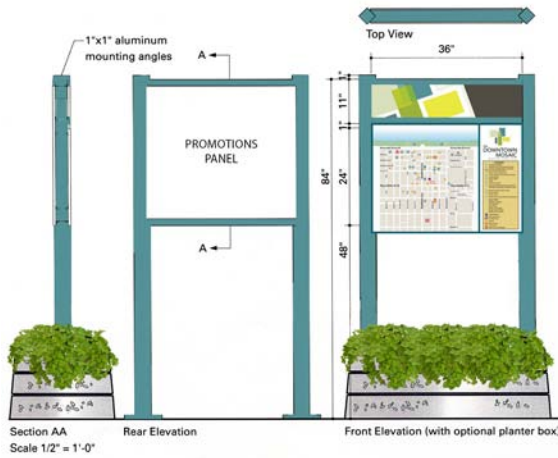
- The circular-shape containing the “P” is a distinguishable and easily understood symbol. The sign must be distinctive and stand out in the urban environment with colours consistent with the orientation signage program.
- The sign is mounted on a black aluminium pole with white lettering spelling out “parking”.

PLACEMENT “Where It Goes”

- To be positioned as stand-alone markers to attract the attention of oncoming vehicular traffic or to be placed at or near parking area entries to attract oncoming vehicles.



8.3 Active Transportation Facilities Orientation Signs



Proposed Active Transportation Facilities Orientation Sign



Example of Active Transportation Facilities Orientation Sign

ACTIVE TRANSPORTATION FACILITIES

ORIENTATION SIGNS are designed to assist in directing pedestrian movement to major public and tourist-type destinations along the Riverside Drive district. These signs form a critical component in the comprehensive wayfinding system as they visually display the key pedestrian and bicycle routes.

DESIGN CRITERIA “How It Functions”

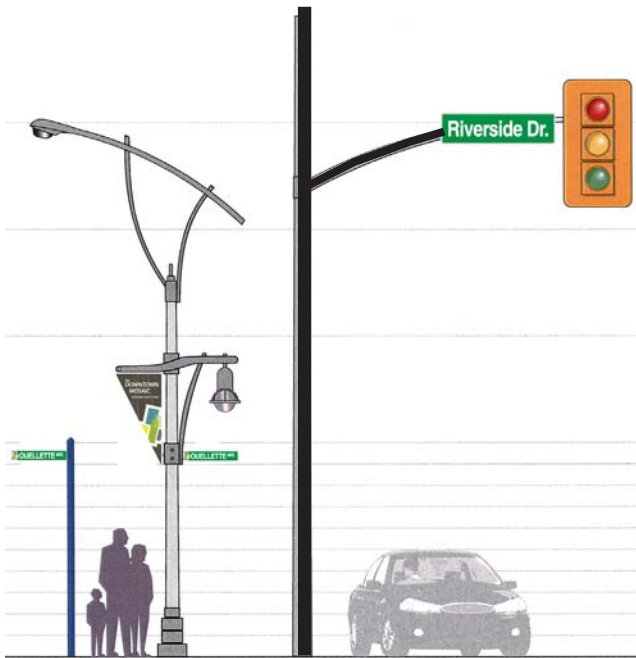
- The primary face of the sign will display a map graphic that is designed to be user-friendly with highest visibility and visually appealing;
- The graphic panel must be easily edited, removed or replaced as new features and facilities are introduced in the district; and
- The secondary face of the sign could contain a directory of destinations (attractions, shopping, eating, entertainment facilities, etc.) or serve as promotional space for events within the district.

PLACEMENT “Where It Goes”

- Orientation signs should be placed along key pedestrian routes and in gathering places throughout the Riverside Drive area;
- Orientation signs can be placed on the back of wayfinding signs at appropriate locations (for example, the Festival Plaza on Riverside Drive).;
- Pedestrian Orientation Signs can be incorporated into the leaning rail’s panel.



8.4 Street Name Signs



Proposed Street Name Signs



Example of Themed Street Name Signs

STREET NAME SIGNS identify the streets and public right-of-way of the Riverside Drive district.

DESIGN CRITERIA – “How It Functions”

- Street Name Signs, incorporating the colours and logo of the City of Windsor or Riverside Drive, will assist in highlighting the unique character and reinforce the boundaries of the area.

PLACEMENT “Where It Goes”

- Street Name Signs will be mounted in locations that are visible to both drivers and pedestrians.
- Street Name Signs have three (3) mounting possibilities depending on the sign location and signage situation:
 - as a stand-alone element on matching specifically designed poles.
 - incorporated onto street and pedestrian combined lighting fixtures, which will also take advantage of the existing illumination levels for the signs.
 - on upgraded City traffic signalization standards at intersection within and along the boundary of the Riverside Drive district. (Note, the background colour used on these signs is green).



8.5 Gateway Markers



Proposed Gateway Markers

PLACEMENT “Where It Goes”

Type B: To be located in dense urban areas where limited space is available.

Type A: To be located at major/prominent nodes and prominent nodes where open space is available, along viewing distances and at significant areas of attraction or areas consisting of a theme.

GATEWAY MARKERS are landmark elements located at the entry points that introduce visitors to the Riverside Drive area. Markers must be attractive and interesting from both the street and the sidewalk to be effective at both the pedestrian and vehicular scales. Downtown Windsor already includes gateway markers letting people know that they are in the downtown. See the *City Centre Streetscape Standards Manual* for details of this gateway marker.

DESIGN CRITERIA “How It Functions”

- Gateway markers will be implemented on an individual basis where site conditions are deemed feasible or appropriate, and under the following design criteria:
 - the scale and proportion of the gateway markers should relate to the width of the street, nearby buildings, the significance of the location and any additional physical constraints or opportunities.
 - the materials and detailing of gateway markers should be coordinated with the other streetscape elements within the district and embody the characteristics that identify the area.
 - the site and surrounding elements of gateway markers should be carefully designed to reinforce the gateway and role in the wayfinding system, enhanced through the appropriate use of landscaping, lighting and signage.
 - there is great potential for creative illumination techniques to enhance the uniqueness, impact and visibility of gateway markers during the night.



8.6 Heritage Interpretation Features



Existing Heritage Interpretation Sign Structure



Existing Heritage Feature

HERITAGE INTERPRETATION SIGNS are installed adjacent to or on sites of historic and cultural value and contain information that provides interpretation by “telling the story” of the former and/or existing property and associated events, by emphasizing their contribution and role in the development of the City Centre.

DESIGN CRITERIA “How It Functions”

- The existing heritage interpretation signs are designed with the single-sided sign face contained in a plexi-glass frame supported by a two-post system.

PLACEMENT “Where It Goes”

- Heritage interpretative features (ie. interpretive signs, walls from demolished historic buildings, landscapes and land forms) will be installed adjacent to or on sites of historic and cultural value;
- Heritage interpretative signs will be placed in highly visible locations to discourage vandalism; and
- Locating heritage interpretative signs is dependent on the availability of space on the streetscape for gathering around the sign and visibility – but not located within the pedestrian path.



9.0 Streetscape Features



In order to emphasize the function and role of Riverside Drive as a Scenic Drive, the improvements can be viewed as a string of gateways (into SSIA and neighbourhoods) at key nodes along the waterfront as established in the Design Framework (Section 3.0) of the Class Environmental Assessment for Riverside Drive Vista Improvement Project. Streetscape elements and features are required to establish a unique sense of place that is desirable for pedestrians and vehicles travelling along the drive.

Elements of the streetscape at these various nodes may include Open Space, Public Art or Water related features, Gateway Markers, Riverfront Beacons, Landscape Elements (as described in Section 6 of this document) and should all connect with the multi-use recreational trails that run the full length of the riverfront, depending on the land uses types surrounding that node.

DESIGN CRITERIA

- Each type of Node defined by the Design Framework has a particular character related to the importance of that node:
 - N1 Primary Nodes – Unique Civic/Ceremonial Node;
 - N2 Secondary Nodes – Transitional Use Nodes; and
 - N3 Tertiary Nodes – Residential Traffic Calming Nodes
- A variety of possible design themes may be adopted for right-of-way installations at the various nodes. Themes can be integral to the gateway, incorporating both fixed and changeable displays that welcome and celebrate entrance and arrival. The following design themes have been considered in the creation of the Master Plan:
 - Awareness of Environmental Sustainability
 - Canadian Identity/History and Culture
 - Public Art
 - Local Industry especially the cross-border trade relationship between Windsor & Detroit
 - Technology and Innovation including green energies
 - Tourism
 - Education



9.1 Open Spaces



With an abundance of open space areas and the already existing parks on the north side of Riverside Drive, the foundation to implement extraordinary and memorable landscapes and vistas already exists. Continuation of the greenway along with continuous multi-use recreational trails to connect these spaces is fundamental to creating a sustainable and liveable community. The enhancement of green open spaces at key nodes is especially important in residential areas to facilitate connections that are contextually related to urban residential life along Riverside Drive, while providing strategic vantage points from which to observe the Detroit Skyline and Belle Isle Park.

Open spaces in the downtown may take on a more urban plaza environment. An effort to provide greenery in these plazas is important to mitigate the extreme environmental and climatic conditions that result from hard surface paving and proximity to the river.

9.1.1 Urban Plaza Open Spaces

Urban open spaces such as plazas and large congregation areas including: amphitheatres, fountains, festival plazas and outdoor cafés, provide people places and generate vitality for the urban environment. Several spaces have been established in the downtown along the riverfront, especially near the casino, Dieppe Park and the Art Gallery. A variety of urban spaces is necessary along Riverside Drive to help generate the pedestrian activity that fosters popular urban places throughout the world. Primary and Secondary nodes identified in the Class Environmental Assessment for Riverside Drive Vista Improvement Project may provide the opportunity to develop such people generating places.

DESIGN CRITERIA

- Hard surfaces to be of high-quality materials with visually pleasing patterns, and colours that complement the surrounding built environment;
- Paved areas and primary nodes (N1) will be complementary to the paving patterns found along the sidewalks and intersection treatments on Riverside Drive, but different enough to convey the notion of a Unique Civic/Ceremonial Node;
- Provide shade through the use of tree canopies or landscape architectural elements such as pergolas, trellises, or high quality UV-resistant material shade structures;
- Preserve views from the street level by allowing visually permeable views to the Detroit Skyline. Any structure erected on the north side of Riverside Drive should be open enough to allow for views from the street level.



RIVERSIDE DRIVE STREETScape
STANDARDS MANUAL
CITY OF WINDSOR

- Shade structures will provide a relationship to the riverfront and contribute to the character of the site as a focal point;
- Provide suitable site furnishings, seating, landscaping, lighting and gateway signage as detailed in other sections of this document;
- Site furnishings associated with public open spaces on the north side of Riverside Drive will be black in colour except in the downtown which has an established palette;
- Site furnishings associated with urban open spaces on the south side of Riverside Drive are to be dark coloured or metallic and complementary to the architectural details of the buildings with which the plaza is associated; and
- Transition from the surrounding areas to an Urban Plaza Open Space, should be easily identifiable with traditional procession of arrival as found in successful urban open spaces through-out the world (*example, pedestrian collection area -> transition-> rest area -> transition -> primary open space*).

PLACEMENT

- Though not intended to straddle the intersection, urban plazas along Riverside Drive may be on either side of the road with visual connections for pedestrians crossing Riverside Drive;
- Areas for congregation of larger groups of people can be built as part of other structures, such as: edifice office towers, higher density residential properties, larger collection of commercial properties, art gallery, hotels, casino, or historic building or spaces;
- Urban Plazas will be located at, but not limited to, all N1 Primary Nodes – Unique Civic/ Ceremonial Nodes as identified in Class Environmental Assessment for Riverside Drive Vista Improvement Project:
 - Church Street - Art Gallery/Aquatic Centre/Riverfront Hotels. This location has been redefined with the new aquatic centre and it may be effective to extend this node to include the Bruce Avenue intersection as this area may become more of an urban plaza for large gatherings.
 - Ouellette Avenue – Dieppe Gardens. A small urban plaza exists with #1 Riverside Drive on the southwest corner, as well as two plazas in Dieppe Gardens which includes the Peace Beacon on the northwest corner and Udine Fountain on the northeast corner.
 - Mid-block between Goyeau and McDougall – Civic Esplanade. A small plaza exists on the south side of the drive, however it has no formal visual or physical connection to the north side.



- Glengarry Avenue - Casino. Extensive work has been completed on both the north and south sides of this node; though it still operates as a vehicular crossing point. The recent addition of the civic stage has provided an urban plaza further into the northwest at the McDougall Avenue intersection, but no physical or visual connectivity to the south side of Riverside Drive. A small urban plaza with a pergola has also been established on the north side of Riverside Drive at Aylmer Avenue, but there is currently no continuity to the development on the south side of Riverside Drive. Should the lands on the south side of Riverside Drive be developed as proposed by the Glengarry Marentette C.I.P., then an urban plaza with a commercial connection is strongly recommended to connect the plazas on the north side.
- Walker Rd – Hiram Walker Distillery and Train Station. Predominantly historic built environment with the distillery building on both sides of the drive leaving little to no space for an urban plaza of any significance. The recent relocation of the train station further east to Montreuil Avenue and the potential for future development along the north and south sides of Riverside Drive at Drouillard Road, may provide better opportunity for establishing an Urban Plaza related to this historic district in that vicinity. The foot of Drouillard Road also offers the best vantage point of the James Scott Memorial Fountain on Belle Isle (Detroit) from Windsor.
- Pillette N1 – Reaume Park, Coventry Gardens and Riverside Drive Residential Towers. Both the north and south sides of this node primarily operate as a vehicular crossing point. The recent improvements around the Coventry Park Peace Fountain has improved the urban plaza in that area, but the N1 intersection is significantly removed from the plaza. It may be effective to re-evaluate the location with that of the proposed N2 node associated with the Fountain Plaza.
- Riverdale – Lakeview Marina and Riverside Drive Residential Towers. Currently under construction, but no designated plaza or festival space has been proposed.
- Urban Plazas may suitable for N2 Secondary Nodes – Transitional Use Nodes located in the Downtown between Crawford Avenue and Drouillard Road as identified in Class Environmental Assessment for Riverside Drive Vista Improvement Project:
 - Crawford Avenue – CBC and the eastern end of the Odette Sculpture Garden
 - Caron Avenue – Downtown Residential Towers
 - Ferry Street – St. Clair Centre for the Arts and Dieppe Park Pedestrian Entrance
 - Parent Avenue – Bert Weeks Memorial Fountain (north side only)
 - Pierre Avenue – Great Western Park (north side only)
 - Lincoln Road – Eastern limit of the downtown riverfront parkway system
- The intersections of Goyeau Avenue, McDougall Avenue, and Aylmer Avenue have the components of both N1 and N2 nodes including traffic signals and pedestrian crossings, established pedestrian generators are not yet identified as significant nodes in the Class Environmental Assessment for Riverside Drive Vista Improvement Project. Further





consideration should be given before development of these areas, as the potential for an urban plaza at these locations may be suitable.

9.1.2 Urban Green (passive) Open spaces

Passive green spaces along the north side of Riverside Drive are treasured resources of the City’s green infrastructure. Such spaces enhance the City’s sustainability as well as the health and well being of residents and visitors. Currently, the City boasts over 8 km of public open space along Riverside Drive including Ambassador Park, Centennial Park, Dieppe Gardens, Great Western Park, Alexander Park, Goose Bay, Coventry Gardens (Reaume Park), Bridges Bay Park, and Sandpoint Beach. These riverside green spaces are also complemented by community and neighbourhood oriented parks on the south side of Riverside Drive. Connectivity to and between these parks through a multi-use recreational trails provide passive recreation and informal places for people and urban oases from the traditional hardscaping found in the urban environment.



Studies also show that the establishment or preservation of Urban Green Open Spaces can provide for sustainable, healthier environments and contribute to traffic calming and lower vehicular accidents. Secondary and some Tertiary nodes identified in the *Class Environmental Assessment for Riverside Drive Vista Improvement Project* may provide opportunity to further develop such urban green oases along the waterfront.



PLACEMENT

- Development of additional Urban Green Open Spaces may be suitable at N2 Secondary Nodes
- Transitional Use Nodes that do not offer a suitable location for an Urban Plaza Open Space;
- Development of additional Urban Green Open Spaces may be suitable at some N3; and
- Tertiary Nodes – Residential Traffic Calming Nodes, as identified in *Class Environmental Assessment for Riverside Drive Vista Improvement Project*, which have yet to be developed other than at a pedestrian crossing point to provide a central focus for the residential neighbourhood or community around that pedestrian crossing point.

DESIGN CRITERIA

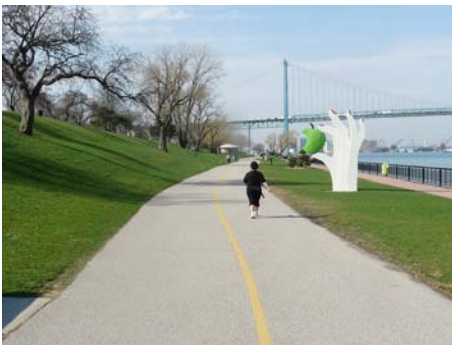
- Each urban green open space is to have its own individual character to establish a strong “sense of place”;
- All new landscaping is encouraged to utilize the planting palette or suggest equivalent to maintain consistency with the streetscape design, especially at the interface of the open space and the right-of-way;
- Separation of the park from the right-of-way should be in the form of soft landscaping with vertical elements to act as an implied wall to deter pedestrian crossings at inappropriate locations;
- Street furnishings should be included as part of any outdoor hardscape area to provide rest areas for users especially those with limited mobility and where children’s playspaces may be located;
- Re-naturalized landscapes along the north side of Riverside drive are encouraged but should be of a low to medium grassed prairie or riparian environment to avoid disruption of views to the Detroit Skyline;
- With the exception of benches, and trash receptacles, green open spaces established on the south side of Riverside Drive should provide passive park amenities for the neighbourhood or community which are not already provided within an 800m distance;
- N3 Tertiary nodes might provide simple pocket park-type settings with minimal maintenance to establish a focal point for that crossing point; and
- Provision of suitable site furnishings, seating, landscaping, lighting and gateway signage as detailed in other sections of this document are required as appropriate.



9.2 Multi-Use Recreational Trails



Urban Green Open spaces may also exist simply as part of a linear Multi-use Recreational Trails. Pedestrian friendly areas with pathways lined with trees are integral to the success of establishing a walkable urban environment. In these situations, effort must be taken to ensure that the experience of travelling either by foot, cycle, skate or other (non-motorized) recreational means of travel, is enjoyable and physically separated and protected from the vehicular traffic along Riverside Drive to the greatest distance possible.



A continuous multi-use recreational trail is proposed for the entire length of Riverside Drive, connecting McKee Park in Sandwich Town, west of the Ambassador Bridge to the existing Ganatcho Trail, located on the east end of Riverside Drive. There are multi-use recreational trails along the central riverfront parks however, connection to the eastern and western limits of the City along Riverside require a better connection, especially in primarily residential areas.

DESIGN CRITERIA

- Continuous multi-use recreational trails to be constructed of smooth asphalt with appropriate markings;
- Where sidewalks adjacent to buildings (i.e. commercial, institutional, and medium to high density residential, etc.) are used as multi-use recreational trails an increased width to 2m minimum (plus any splash strip) should be considered, except in the downtown area where development is a more urban cross section and development will be implemented through the City Centre Streetscape Standards manual;
- Trail markers and trail heads to be consistent with those erected along the Roy A. Battagello River Walk Bike Trail;
- Pathways are to be buffered from vehicular lanes by a 3m soft landscape separation where possible;
- Pathways and sidewalks should be continuous across driveways. Where crossings over driveways and intersections occur, sidewalks should be marked through a material other than the driveway or roadbed paving;



- Any separation should be in the form of soft landscaping with vertical elements in the landscape to act as implied walls as well as to deter pedestrian crossings at inappropriate locations. Planting of trees between pedestrian paths and vehicular traffic provides pedestrians with an added sense of security without the use of fencing;
- Provide rest areas at approximately 100m apart for person with reduced mobility along the length of the multi-use recreational trails. Rest areas should be situated to not create conflicts with residential properties; and
- Rest areas to be clearly defined with visually appealing and textured hardscaping, bench/benches and trash/recycle receptacle.

PLACEMENT

- Multi-use recreational trails are proposed for the entire length of Riverside Drive with priority to be given to open spaces on the north side of the right-of-way; and
- Where space is not provided, the multi-use recreational trails shall incorporate with the municipal sidewalk with a minimum 0.6m splash strip from the edge of the curb as per section 4.0 (Pavement Standards); and
- Trailhead markers to be established at key locations, especially where the multi-use recreational trail intersects Primary (N1) and Secondary (N2) Nodes.



9.3 Public Art

Pedestrian improvements create a unique opportunity where people can interact with and be positively impacted by public art as part of their everyday activities; they also help create more walkable communities. Public art enhances the quality and ambiance of the municipality’s built environment. Public art not only invests a space with “place making” qualities and culture, it has the capacity to interpret the historic, social, cultural and narrative nuances that distinguish our neighbourhoods. Successful public art programs help communities develop and express their unique sense of place and character, which in turn, draws tourism and investment activity to the region.

Public art refers to original works of art in any medium for temporary or permanent placement in outdoor or indoor settings that are accessible to the public for their enjoyment. Public art can take a variety of forms including traditional permanent installations such as statues, murals on building facades, ornamental parks, pathway and plaza treatments at special locations and special site furnishing, as well as temporary art installations such annual art pieces, performances and festivals. Encouraging the use of streets for festivals, parades, and other cultural events that promote pride helps to further reinforce a sense of place at given areas along the Riverfront and Riverside Drive. When appropriate, consideration should be given to commissioning artists to create art pieces at specific locations along Riverside Drive, if considered appropriate.

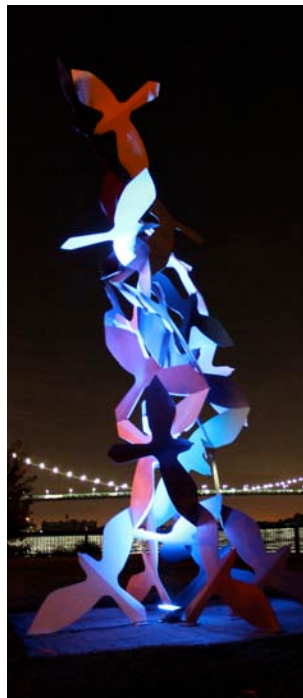
Public art has the ability to unify the special areas with a theme, and at a pedestrian/street level it can provide visual interest for the passerby. Public art is an effective means of creating a sense of place through presence of a monumental icon in the public realm and ideally should reflect the character and/or history of the space in which it is installed.

DESIGN CRITERIA

All Public Art installations, including temporary installations must go to the Public Art Advisory Committee (PAAC), then to the Standing Committee, and finally to Council for approval if the piece is to be installed on public property.

- **Artistic Excellence:** Public Art of the highest standards, that is deemed relevant to the City/ neighbourhood, and that allows the public to be inspired, make connections and understand relationships among the works in the collection and the community. All permanent and temporary Public art must meet the following artistic criteria:
 - The work of art shall merit placement in a City public place.
 - The artist demonstrates the ability and potential to execute the proposed piece, based on previous artistic achievement and experience.
 - In the case of a donation of an existing work of art, the quality of the work(s) must be supported by documentation from an independent professional evaluator.





- The artwork must enhance the City’s public art collection.
 - Each art installation is to be unique to reflect a wide variety of creative expression, art practice and art work.
 - Intention of all public art installations is to reflect a community’s diversity, values, and history so as to foster a sense of belonging, identity, and place.
 - Presents a positive image to visitors and potential investors.
 - Increases public understanding, awareness and enjoyment of the arts in everyday life.
- **Theme:** A stated theme of the artwork, which is consistent with the goals, objectives and mandate of the City and/or Riverside Drive, must be identified.
 - **Relevance:** Works of art must be appropriate for the proposed site and its surroundings by complementing nearby architecture, topography, history and the social dynamic of the particular community in which the art will be placed. Public art responds to and enhances the natural, social and built environment of Windsor and/or Riverside Drive and/or the neighbourhood in which it is being installed.
 - **Physical Durability:** Consideration of the artwork's long-term durability against theft, vandalism, and weather must be addressed.
 - **Public Safety:** All proposals must address public safety concerns.
 - **Environmental Sustainability:** Consideration will be given to the environmental impact and sustainability of the proposed artwork.
 - **Costs:** A work of art must have a sponsor or co-sponsors, who will present the proposal and, when necessary, be responsible for raising or providing the necessary funds in accordance with the budget that they present to the City.
 - The City will consider the following types of proposals for works of art intended for placement along Riverside drive in the public realm:
 - A sponsor's offer of an already completed work of art.
 - A sponsor's offer to commission an artwork by a specific artist or artists.
 - A sponsor's offer to commission an artwork by means of a public competition as described in the Public Art Program.
 - The City will consider gifts of works of art for placement at City owned sites along Riverside Drive with the understanding that no City funds will be required for production, siting, installation or ongoing maintenance of the work without prior approval of City Council.
 - The City will accept no gift of an artwork until all funds for its development and siting have been secured.

PLACEMENT

- Permanent public art installations and potential water elements (such as fountains or decorative irrigation) shall be interspersed along the pathway creating visual points of interests and destination. Public Art sites that can be viewed by both pedestrians and vehicles should be strategically situated along the roadway;



- Artwork must be located in areas offering the public a free and unobstructed experience of the work, with preference given to areas providing the greatest opportunities for interaction and accessibility;
- Locations designated for the installation (current or future) of public art projects should meet the following criteria:
 - Visibility for pedestrians and/or motorists (including public transit);
 - Proximity to recognized pedestrian routes and high pedestrian activity areas (e.g. active retail areas), transit stops (especially those serving high ridership routes), places of public gathering, public open spaces;
 - Opportunities to expand on existing or future public artworks as part of an existing or proposed multi-artwork concept; and
 - Public art should be located so as to be a pedestrian amenity without compromising safety.
- Places of special heritage, community or environmental significance may be considered for locating public art;
- The designation of public art siting locations (as required for current and/or future installations) should be indicated on the appropriate document (e.g., Development Permit and Building Permit drawings) to the satisfaction of City staff, with regard to urban design and technical considerations (e.g., utilities, pedestrian and vehicle safety, maintenance access);
- Temporary public art locations are recommended in the Downtown along the Riverside Drive Promenade to provide an ever-changing, original, and engaging landscape of interest for local residents and visitors; and
- Public Art on municipal property shall be chosen and sited in conjunction with the Windsor Public Art Advisory Committee.



9.4 Water Related Features

Water related features are a form of public art that can express the special association and natural connection between Windsor’s Riverfront and Riverside Drive. Incorporation of water in any public feature presents an opportunity to highlight riverfront heritage including Aboriginal culture, natural ecosystems, early European settlement, international trade, and Windsor’s close relationship with Detroit.

DESIGN CRITERIA

- Water related features may be used in place of permanent public art installations and would be subject to the same conditions of design criteria.
- The use of water as a feature in public art or as a landscape element requires serious consideration and for site location, visibility, and long-term maintenance.



10.0 Utilities

10.0 Utilities along Riverside Drive

Given the importance of Riverside Drive as a “Scenic Drive” in the City’s Official Plan it is important to reduce visual clutter. Although the many overhead utilities and transformer boxes on both the north and south sides of Riverside Drive provide important services to businesses and residents, they have also become a sources of visual clutter. In Windsor’s Downtown utilities are buried or often hidden within alleys behind development and transformers are often located within developments or screened to reduce their visual impact. Section 20 (1) 3 of Zoning By-law 8600 already includes a provision limiting the height of buildings, structures, and utilities on the north side of Riverside Drive to not exceed the crown of the pavement.

Although costly, given Riverside Drive’s designation as a “Scenic Drive”, it is important to reduce visual clutter created by overhead utilities whenever possible. Therefore any new utilities or those being replaced will be located underground on both the north and south side of the entire length of Riverside Drive, when it is feasible.



11.0 Low Impact Development

11.0 Low Impact Development Considerations

Low Impact Development (LID) is an approach to development that works with nature to manage stormwater as close to the source as possible. LID uses stormwater as a resource rather than a waste product through recreated natural landscape features and reduced impervious surfaces to form site drainage features that are functional and aesthetically pleasing¹. Examples of LID features include, but are not limited to:

- Bioretention facilities;
- Bioswales;
- Perforated stormwater pipes;
- Prefabricated modules; and
- Permeable pavers².

Incorporation of Low Impact Development features into Riverside Drive's streetscape, plantings, and plazas should be considered as a way to mitigate stormwater flooding and improve the Detroit River's water quality while also providing attractive elements that can be used to educate the public about hydrology and the ecological integrity of the River and the larger Great Lakes basin.

Consideration of implementing LID must take into account the costs and staff time required for regular maintenance associated with some LID facilities. Without proper maintenance, benefits from LID facilities can decrease substantially².

¹ Environmental Protection Agency. "Low Impact Development (LID)." October 3, 2014. Web.

² Credit Valley Conservation. "Grey to Green Road Retrofits: Optimizing your Infrastructure Assets through Low Impact Development." 2013. Web.



12.0 Streetscape Element Maintenance Standards

With the following and not exclusive to these streetscape elements there must be maintenance for and associated budget funding established for:

12.1 Street Furniture (Benched, Bollards, Trash Recptacles, etc)

- Paint peeling and welding issues
- Replacement
- Tamperproof fasteners
- Commercial grade metal
- UV-proof colour selection
- Powder coating thickness and warranty
- Graffiti proof material and/or cleaners

12.2 Concrete Issues

- Upheaval and spalling
- UV-proof colour selection
- Colour matching for replacement

12.3 Street Light Pole

- Paint deterioration issues and replacement
- Banner fasteners

12.4 Tree Replacement

- Appropriate species for soil volumes and location with respect to use
- Size to be large enough to not be damaged through vandalism

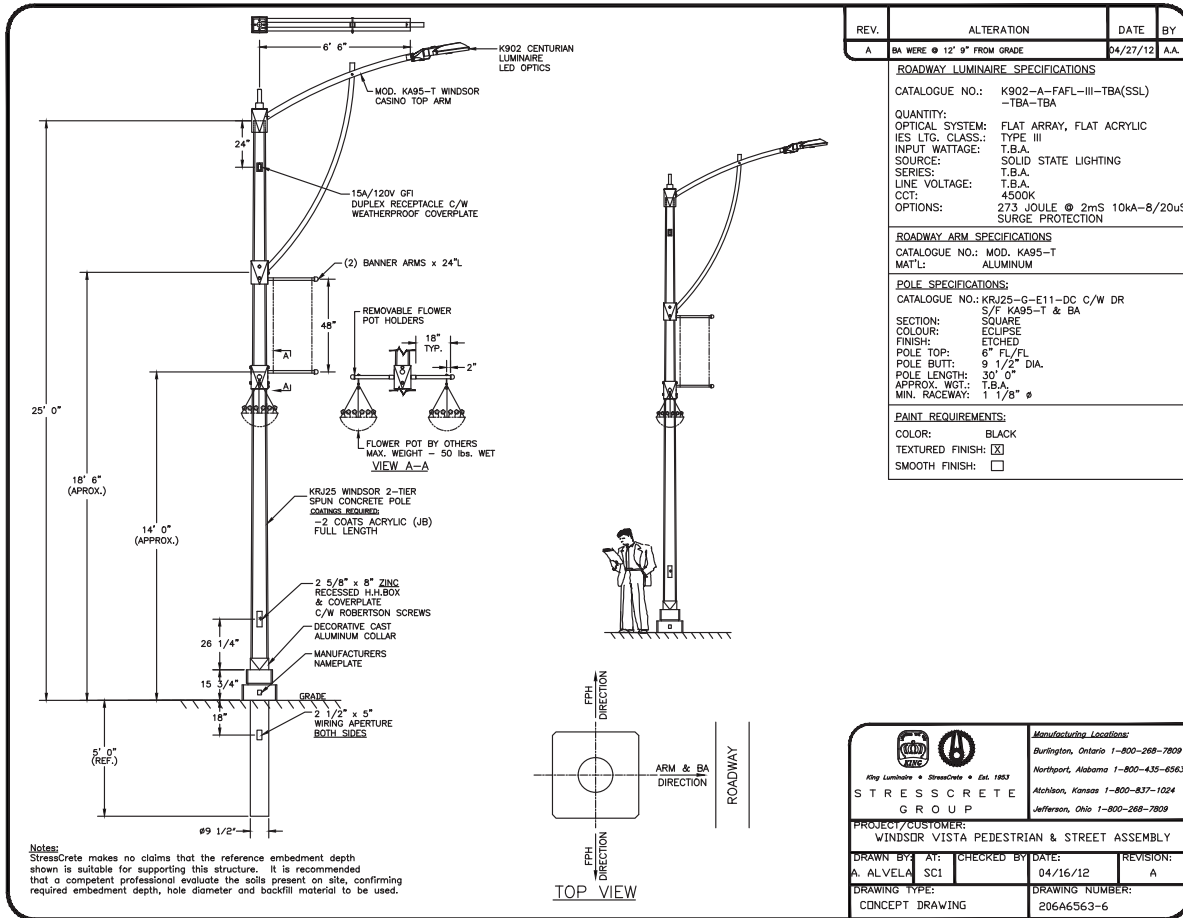


13.0 Recommended Streetscape Design and Specifications

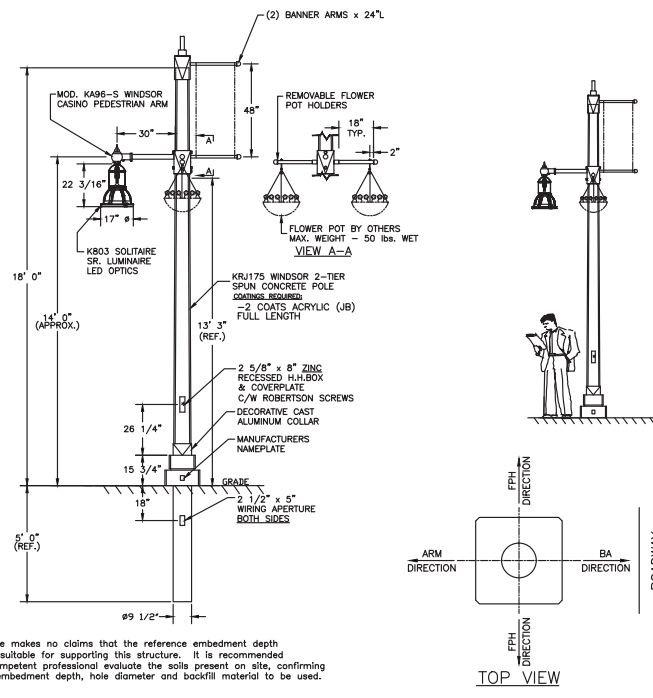


A Schedule A: Specifications

Note: The following specifications are examples of streetscape elements for the Riverside Drive Vista Improvement Project. The selection of these elements will adhere to the City's Purchasing By-Law Number 93-2012



REV.	ALTERATION	DATE	BY
A	BA WERE @ 12' 9" FROM GRADE; POLE WAS KRJ175	04/27/12	AA



Notes:
 StressCrete makes no claims that the reference embedment depth shown is suitable for supporting this structure. It is recommended that a competent professional evaluate the soils present on site, confirming required embedment depth, hole diameter and backfill material to be used.

PEDESTRIAN LUMINAIRE SPECIFICATIONS	
CATALOGUE NO.:	K803-FAFL-III-TBA(SSL) -TBA-TBA S/F KPL-20
QUANTITY:	
OPTICAL SYSTEM:	FLAT ARRAY
IES LIT. CLASS:	TYPE III
INPUT WATTAGE:	T.B.A.
SOURCE:	SOLID STATE LIGHTING
SERIES:	T.B.A.
CCT:	4500K
LINE VOLTAGE:	T.B.A.
OPTIONS:	273 JOULE @ 2ms 10ka-B/20us SURGE PROTECTION
PEDESTRIAN ARM SPECIFICATIONS	
CATALOGUE NO.:	MOD. KA96-S-KPL-20
MAT'L:	ALUMINUM
OPTIONS:	KPL-20 LEVELING DEVICE
POLE SPECIFICATIONS:	
CATALOGUE NO.:	KR118-G-E11-DC S/F KA96-S & KA96-S & BA
SECTION:	SQUARE
COLOR:	ECLIPSE
FINISH:	ETCHED
POLE TOP:	6 7/8" FL/FL
POLE BUTT:	9 1/2" DIA.
POLE LENGTH:	23' 0"
APPROX. WGT.:	T.B.A.
MIN. RACEWAY:	1 1/8" #
PAINT REQUIREMENTS:	
COLOR:	BLACK
TEXTURED FINISH:	<input checked="" type="checkbox"/>
SMOOTH FINISH:	<input type="checkbox"/>

<p>STRESSCRETE GROUP</p>	<p>Manufacturing Locations:</p> <p>Burlington, Ontario 1-800-268-7809</p> <p>Northport, Alabama 1-800-435-6563</p> <p>Alhambra, Kansas 1-800-837-1024</p> <p>Jefferson, Ohio 1-800-268-7809</p>																			
	<p>PROJECT/CUSTOMER:</p> <p>WINDSOR VISTA PEDESTRIAN & STREET ASSEMBLY</p>																			
	<table border="1"> <tr> <td>DRAWN BY:</td> <td>AI:</td> <td>CHECKED BY:</td> <td>DAE:</td> <td>REVISION:</td> </tr> <tr> <td>A. ALVELLA</td> <td>SC1</td> <td></td> <td>04/16/12</td> <td>A</td> </tr> <tr> <td colspan="2">DRAWING TYPE:</td> <td colspan="3">DRAWING NUMBER:</td> </tr> <tr> <td colspan="2">CONCEPT DRAWING</td> <td colspan="3">206A6563-7</td> </tr> </table>	DRAWN BY:	AI:	CHECKED BY:	DAE:	REVISION:	A. ALVELLA	SC1		04/16/12	A	DRAWING TYPE:		DRAWING NUMBER:			CONCEPT DRAWING		206A6563-7	
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A. ALVELLA	SC1		04/16/12	A																
DRAWING TYPE:		DRAWING NUMBER:																		
CONCEPT DRAWING		206A6563-7																		



RIVERSIDE DRIVE STREETScape STANDARDS MANUAL
 CITY OF WINDSOR

720 SERIES

MBE-0720-00005

Legacy # *MLB720BL-W*



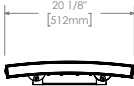
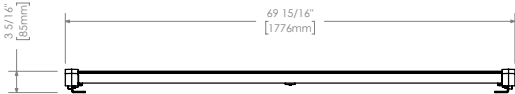
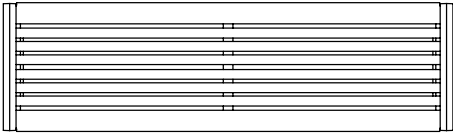
MATERIALS: The legs are aluminum castings. The boards are made from Ipe wood. All brackets are made of steel.

FINISH: All aluminum castings are powder coated. The wood boards are treated with penetrating sealers. The steel brackets are e-coated and powder coated.

INSTALLATION: The bench comes pre-assembled. The legs have 1/2" holes for anchoring.

TO SPECIFY: Select MBE-0720-00005
Choose: Powdercoat Color

HEIGHT: 3 5/16" (85mm) **LENGTH:** 69 15/16" (1776mm) **DEPTH:** 20 1/8" (512mm) **WEIGHT:** 66.8 lbs (30.31kg)



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REV-01
SCALE (A) 1:20



800 SERIES

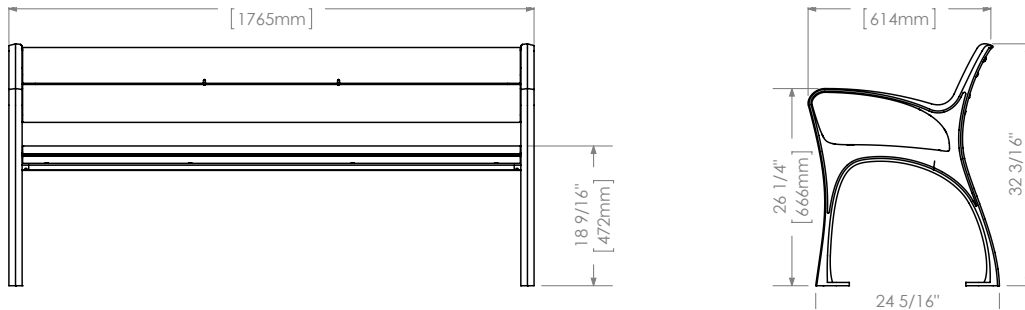
MBE-0870-00025

Legacy # MLB870-W



- MATERIALS:** Bench ends are made from solid cast aluminum. The seat employs Ipe wood slats.
- FINISH:** All steel components are protected with E-Coat rust proofing. The Maglin Powdercoat System provides a durable finish on all metal surfaces.
- INSTALLATION:** The bench is delivered pre-assembled. Holes (0.5") are provided in each foot for securing to base.
- TO SPECIFY:** Select MBE-0870-00025
Choose:
- Powdercoat Color

HEIGHT: 32.2" (81.7cm) LENGTH: 69.5" (176.5cm) DEPTH: 24.2" (61.4cm) WEIGHT: 121.62lbs (55.1kg)



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RIVERSIDE DRIVE STREETScape
STANDARDS MANUAL
CITY OF WINDSOR

900 SERIES

MBE-0970-00008

Legacy # MLB970B-W



- MATERIALS:** Bench ends are made from solid cast aluminum. The seat employs 5.5" x 1" (actual) Ipe slats.
- FINISH:** The Maglin Powdercoat System provides a durable finish on all metal surfaces. Wood slats are treated with penetrating sealers.
- INSTALLATION:** The bench is delivered pre-assembled. Holes (0.5") are provided in each foot for securing to base.
- TO SPECIFY:** Select MBE-0970-00008
Choose:
- Powdercoat Color

HEIGHT: 17.31" (44.0cm) LENGTH: 70" (177.8cm) DEPTH: 18.69" (47.5cm) WEIGHT: 72.1lbs (32.7kg)



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600 SERIES

MBO-0650-00001

Legacy # MTB650-B1

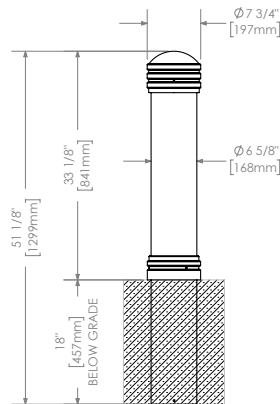


- MATERIALS:** The bollard post is constructed of H.S. steel tube. The top and bottom are capped with cast aluminum parts.
- FINISH:** All steel components are protected with E-Coat rust proofing. The Maglin Powdercoat System provides a durable finish on all metal surfaces.
- INSTALLATION:** Base Type - B1 bollard is supplied with additional 18" of tubing to be set in concrete. Bollard is permanently fixed in place.
- TO SPECIFY:** Select MBO-0650-00001
Choose:
- Powdercoat Color

HEIGHT: 33.13" (84.1cm)

DIAMETER: 6.625" (16.8cm)

WEIGHT: 85lbs (38.6kg)



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RIVERSIDE DRIVE STREETSCAPE
STANDARDS MANUAL
CITY OF WINDSOR

200 SERIES

MBR-0200-00005

Legacy # MBR200-S



Sustainability Facts

Unit Size	One (1) 200 Bike Rack	
	MIN.	MAX.
Carbon footprint (GWP)	113	147
<i>Measured in kilograms of carbon dioxide equivalent</i>		
Total energy use (TPE)	1900	2470
<i>Measured in megajoules of energy equivalent</i>		
Water use (WDP)	1.27	1.52
<i>Measured in cubic metres of water</i>		
Material recyclability	100%	
LEED v4.1 Credits		
Type III Environmental Product Declaration	<input checked="" type="checkbox"/>	
Material Inventory	<input checked="" type="checkbox"/>	
Low VOC finishes	<input checked="" type="checkbox"/>	
Free of Red List substances	<input checked="" type="checkbox"/>	
<small>*Full EPD can be referenced for more information: https://www.epdregistration.com/maglin/</small>		

DESCRIPTION: 200 Series - 200 Bike Rack: H.S. Steel Tube, Aluminum Top Casting, Surface Mount, 2 Bike Configuration

FINISH: All steel components are protected with E-Coat Rust Proofing. The Maglin Powdercoat System provides a durable finish on all metal surfaces.

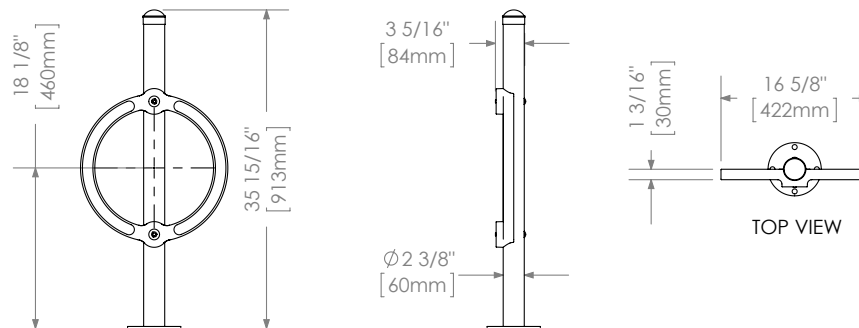
INSTALLATION: The bike rack is delivered pre-assembled.

TO SPECIFY: Select MBE-0200-00005
Choose:
- Powdercoat Color

HEIGHT: 35.94" (91.3cm)

DIAMETER: 16.63" (42.2cm)

WEIGHT: 18.45lbs (8.37kg)



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RIVERSIDE DRIVE STREETSCAPE
STANDARDS MANUAL
CITY OF WINDSOR



300 SERIES

MBR-0350-00001

Legacy # MBR350-4-S



Sustainability Facts

Unit Size	One (1) MBR-0350-00001 Bike Rack
Carbon footprint (GWP)	178 kg CO ₂ -Eq <i>Measured in kilograms of carbon dioxide equivalent</i>
Total energy use (TPE)	3050 Mj-Eq <i>Measured in megajoules of energy equivalent</i>
Water use (WDP)	1.4 m ³ water <i>Measured in cubic metres of water</i>
Material recyclability	100%
LEED v4.1 Credits	
Type III Environmental Product Declaration	<input checked="" type="checkbox"/>
Material Inventory	<input checked="" type="checkbox"/>
Low VOC finishes	<input checked="" type="checkbox"/>
Free of Red List substances	<input checked="" type="checkbox"/>
<small>*Full EPD can be referenced for more information: https://www.epdregistration.com/maglin/</small>	

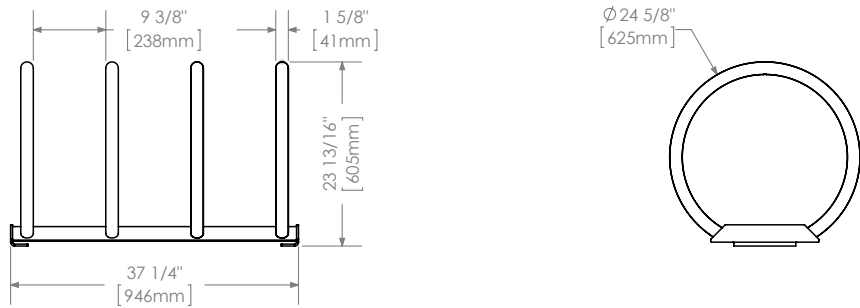
DESCRIPTION: 300 Series - 350 Bike Rack: H.S. Steel Tube, Formed Steel and Solid Steel Angle, Surface Mount, 4 Loops, 3 Bike Configuration

FINISH: All steel components are protected with E-Coat rust proofing. The Maglin Powdercoat System provides a durable finish on all metal surfaces.

INSTALLATION: The bike rack is delivered pre-assembled. It is available with a surface mount installation.

TO SPECIFY: Select MBR-0350-00001
Choose:
- Powdercoat Color

HEIGHT: 23.81" (60.48cm) LENGTH: 37.25" (94.62cm) DIAMETER: 24.63" (62.56cm) WEIGHT: 79.4 lbs (36kg)



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RIVERSIDE DRIVE STREETSCAPE
STANDARDS MANUAL
CITY OF WINDSOR

200 SERIES

MRR-0200-00005

Legacy # MRC202-ST



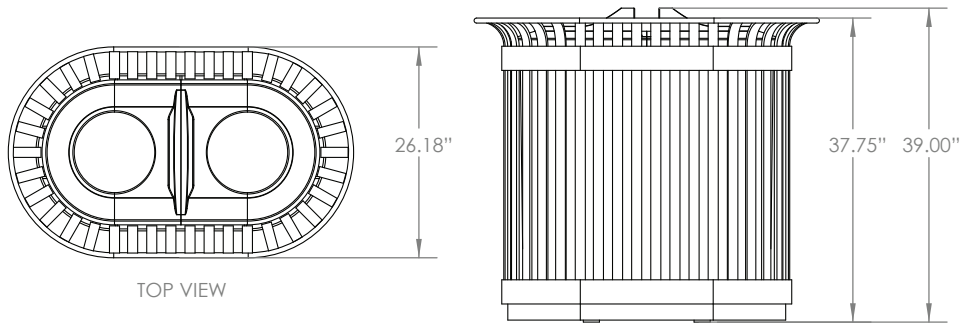
DESCRIPTION: 200 Series - 200 Recycle Receptacle: Heavy Duty Steel Flat Bar Frame, Black ABS Plastic Lid, 2 Stream, 2 x 20 Gallon Polyethylene Liners, Lamacoid Labels

FINISH: All steel components are protected with E-Coat rust proofing. The Maglin Powdercoat System provides a durable finish on all metal surfaces.

INSTALLATION: The recycling station is delivered pre-assembled. Holes (0.5") are provided in each mounting foot for securing to base.

TO SPECIFY: Select MRR-0200-00005
Choose:
- Powdercoat Color
- Lamacoid Labels

HEIGHT: 37.75" (96cm) WIDTH: 42.88" (109cm) DEPTH: 26.18" (66.5cm) WEIGHT: 250lbs (113kg)



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RIVERSIDE DRIVE STREETSCAPE
STANDARDS MANUAL
CITY OF WINDSOR



1100 SERIES

MTB-1100-00002

Legacy # *MLPT1100B-M-GB*



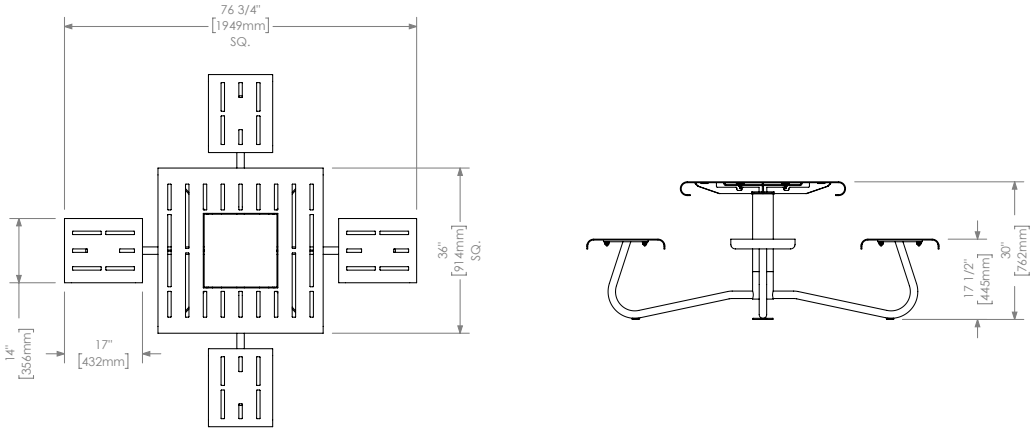
MATERIALS: Table top and Seats are manufactured using lasered steel panels. The frame is HSS steel tube and is available as surface mount only.

FINISH: The Maglin Powdercoat System provides a durable finish on all metal surfaces.

INSTALLATION: The table is delivered pre-assembled. The legs have 1/2" holes for anchoring.

TO SPECIFY: Select MTB-1100-00002
Choose: Powdercoat Color

HEIGHT: 30" (762.0mm)	LENGTH: 76 3/4" (1949.4mm)	DEPTH: 76 3/4" (1949.4mm)	WEIGHT: 199.81lbs (90.63kg)
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REV-01
SCALE (A) 1:32



**RIVERSIDE DRIVE STREETSCAPE
STANDARDS MANUAL**
CITY OF WINDSOR

200 SERIES

MTR-0200-00020

Legacy # MLWR200S-32-ST



DESCRIPTION: 200 Series - 200 Trash Receptacle: Heavy Duty Steel Flat Bar Frame, Metal Lid, Side Opening, 1 x 32 Gallon Polyethylene Liner

FINISH: All steel components are protected with E-Coat rust proofing. The Maglin Powdercoat System provides a durable finish on all metal surfaces.

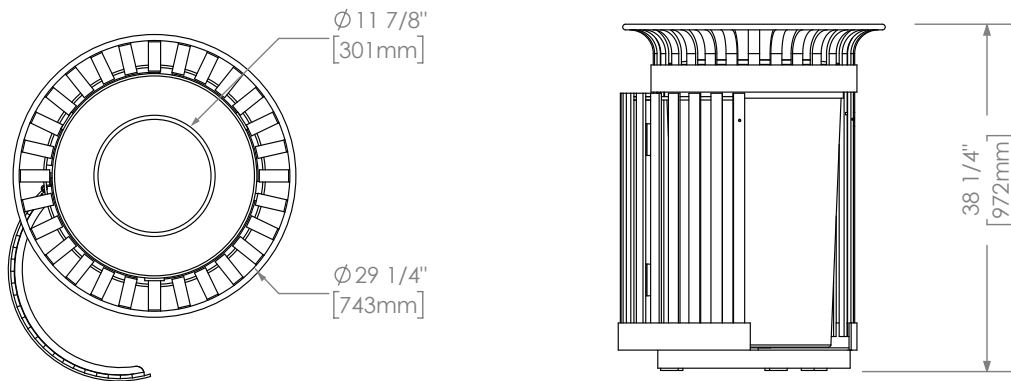
INSTALLATION: The trash container is delivered pre-assembled. Holes (0.5") are provided in each foot for securing to base.

TO SPECIFY: Select MTR-0200-00020
Choose:
- Powdercoat Color

HEIGHT: 38 1/4" (97.2cm)

DIAMETER: 29 1/4" (74.3cm)

WEIGHT: 201lbs (91.2kg)



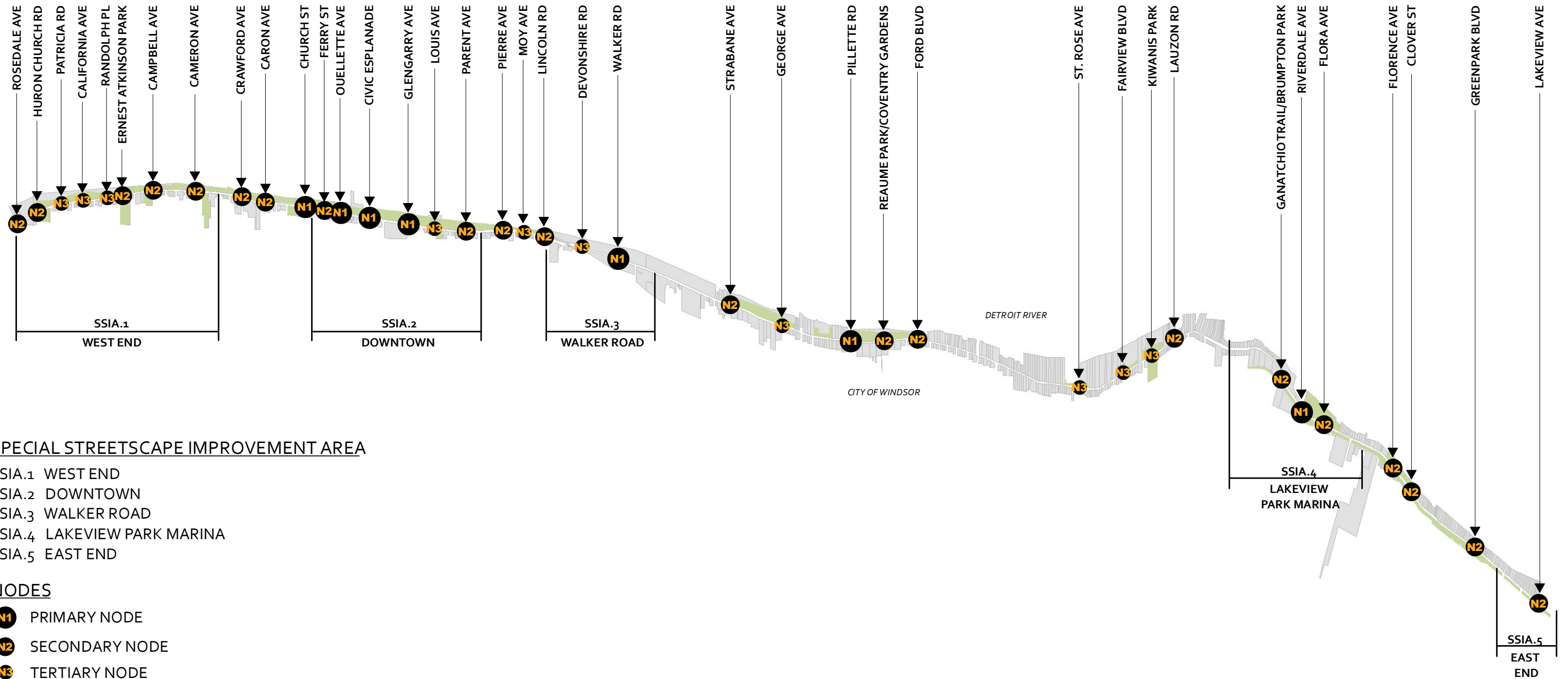
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SCHEDULE B: SPECIAL STREETSCAPE IMPROVEMENT AREAS AND NODES

The Riverside Drive Vista Improvement Project (Class Environmental Assessment) identified Special Streetscape Implementation Areas (S.S.I.A) and Nodes at key intersections to prioritize important design and to help articulate the Riverside Drive Corridor as a Scenic Drive. The figure below identifies these S.S.I.A and Nodes from Rosedale Avenue (in the west) to the east boundary of the City of Windsor.





Subject: Main Street CIP/Ford City CIP Application, 1000 Drouillard Road, Owner: SPOTVIN INC. (c/o: Shane Potvin)

Reference:

Date to Council: July 4, 2023
Author: Kevin Alexander, MCIP RPP
Senior Planner-Special Projects
519-255-6543 x6732
kalexander@citywindsor.ca

Author: Cherilynne Chau
Planning Assistant
519-255-6543 x6438
cchau@citywindsor.ca

Planning & Building Services
Report Date: June 15, 2023
Clerk's File #: Z/13002

To: Mayor and Members of City Council

Recommendation:

- I. **THAT** the request for incentives through the *Main Streets Community Improvement Plan (CIP), Building Facade Improvement Program* made by Spotvin Inc. (c/o: Shane Potvin) , owner of the property located at 1000 Drouillard Road **BE APPROVED** for grants totalling +/- \$5,367.50 in principle for the supply and installation of a projecting wall sign (see Appendix 'B') subject to completion and review satisfactory to the City Planner, Chief Building Official, and Manager of Right-of-Way;
- II. **THAT** request for incentives under the *Ford City Community Improvement Plan (CIP), Municipal Development Fees Grant Program* made by Spotvin Inc., (c/o Shane Potvin), owner of the property located at 1000 Drouillard Road **BE APPROVED**, for grants totalling +/- \$3,247.85 for costs related to the installation of the a projecting wall sign, subject to completion and review satisfactory to the City Planner, Chief Building Official, and Manager of Right-of-Way;
- III. **THAT** funds in the amount of +/- \$5,367.50 under the *Main Streets CIP* **BE TRANSFERRED** from the CIP Reserve Fund to the *Main Streets CIP* Fund (Project #7219018) upon completion of the work;

- IV. **THAT** funds in the amount of +/- \$3,247.85 for the *Municipal Development Fees Grant Program* **BE TRANSFERRED** from the CIP Reserve Fund to the *Ford City CIP* Fund (Project #7181046) upon completion of the work;
- V. **THAT** grants **BE PAID** to Spotvin Inc., (c/o Shane Potvin), upon completion and installation of a projecting sign located at 1000 Drouillard Road from the *Main Streets CIP* Fund (Project #7219018) and *Ford City CIP* Fund (Project #7181046) to the satisfaction of the City Planner, Chief Building Official and Manager of Right-of-Way;
- VI. **THAT** the annual Encroachment Fee of \$25.00 and Annual Inspection Fee of \$68.00 identified in Encroachment Policy M67-2015 **BE WAIVED** for the projecting wall sign located at the corner of 1000 Drouillard Road; and
- VII. **THAT** grants approved **SHALL LAPSE** and be **UNCOMMITTED** if the applicant has not completed the work and fulfilled the conditions within 2 years of the approval date. Extensions **SHALL BE** given at the discretion of the City Planner.

Executive Summary:

N/A

Background:

On January 8th, 2018, City Council approved the Building Facade Improvement Program and Urban Design Guidelines for Main Streets Community Improvement Plan (CIP) (CR9/2018 PHED 533) adopted through By-law 26-2018.

On November 19, 2018, City Council approved the Ford City Community Improvement Area and Ford City Community Improvement Plan (CIP) (CR625/2018 PHED 603) adopted through by-laws 171-2018 and 172-2018. These By-laws came into effect in January of 2019. In addition, on November 19, 2018, City Council activated the following financial incentive programs from the Ford City CIP:

1. Municipal Development Fees Grant Program
2. Retail Investment Grant Program
3. Building/Property Improvement Tax Increment Grant Program

Through CR383/2022 DHSC 414 the *Building Facade Improvement Program and Urban Design Guidelines for Main Streets CIP* was amended and renamed *Main Streets CIP*. The new *Main Streets CIP* includes the following programs:

1. Building Facade Improvement Program
2. Building Property Improvement Tax Increment Grant Program
3. New Residential Development Grant Program (Creation of residential units)

On February 28th, 2023, Spotvin Inc., (c/o Shane Potvin), owner of the property located at 1000 Drouillard Road, submitted an application for grants under the *Main Streets CIP* and *Ford City CIP* Financial Incentive Programs. The property is located in the Ford City Business Improvement Area (BIA) and CIP Area.

Discussion:

Building Facade Improvement Grant Program (now *Main Streets CIP*)

The *Main Street CIP* offers financial incentives to encourage property owners and businesses to make investments to improve the exterior appearance of their buildings and storefronts along Main Streets. Such improvements provide a benefit to the community as a whole, by preserving heritage features, protecting Main Streets, and reconnecting storefronts with the public realm. The CIP is applicable to the Ford City BIA Main Street and all other BIAs in the City of Windsor, except for the Sandwich Town and Downtown Windsor BIAs, which are under separate CIPs. Funding for the Building

Facade Improvement Grant Program is broken down into three categories:



Category A (Beautification) —aesthetic and minor functional improvements aimed at making the building facade and storefront more attractive and welcoming to tenants and customers



Category B (Restoration)—aesthetic, functional, and restoration improvements made to restore key features of the building facade



Category C (Replacement)— encourage work that will replace or reinstate key features that have been lost or deteriorated beyond repair or are of a style that is no longer consistent with the building design.

Applicants can receive a grant for 50% of the costs for eligible building facade and storefront improvements up to a maximum of \$30,000 per project. The amount can be increased up to \$60,000 per project for larger buildings with multiple storefronts. The grant also applies to the side(s) and rear of buildings provided the building facade is visible from an adjacent street or public right-of-way or park, and as long as the storefront/facade facing the main street is improved at the same time.

The applicant proposes to install a four (4') feet by four (4') feet double-sided aluminium metal neon projecting sign as part of beautification improvements to the exterior of the building located at 1000 Drouillard Road. The owner suggests that the new sign will be similar to the signs once found along the Drouillard Road Main Street in the 1920's to circa 1950s.



Example of the projecting wall sign located at 1000 Drouillard Road in the 1930's



Example of projecting wall signs on Buildings at the intersection of Whelpton Street and Drouillard Road in the 1950s.

The applicant provided two quotes and selected the lowest estimate of \$10,735.00 to have the proposed projecting wall sign engineered, supplied, and installed. This quote was also selected because this this particular company is familiar and has a track record of restoring and supplying this type of projecting wall sign that were supplied between 1920 to 1950. The project is eligible for a grant of \$5,367.50 (50% of the costs) for the supply and installation of the proposed projecting wall sign.

Ford City CIP Financial Incentive Programs

The applicant is eligible for the *Municipal Development Fees Grant Program* under the Ford City CIP Financial Incentive Programs. The intent of the program is to encourage development within Ford City by providing an incentive to offset the costs associated with seeking the appropriate planning approvals and building permits for a project. Property owners will be eligible to receive a grant for 100% of the specified *Municipal Development Fees*, up to a maximum of \$50,000 per property.

A preliminary review of the information and drawings provided determined that the owner would have to apply for the following applications to be permitted to install the proposed projecting wall sign located on the north-west facing side of the building at 1000 Drouillard Road.

- A Sign Permit in compliance with the submission requirements of Sub-Sections 4.2.3 and 4.2.7.
- An Encroachment Agreement with the City of Windsor to allow for the sign to be suspended within the right-of-way,
- A Sign Bylaw Variance through Committee of Adjustment to allow for an encroachment into the ROW of 0.6m, which is greater than the allowed 0.3m (Ss4.7.1).

The applicant is eligible for +/- \$3,247.85 under this program for the following fees:

Encroachment Application Fee	\$229.25
Encroachment Agreement Fee	\$336.60
Public Works Permit Fee	\$212.00
COA Fee for Sign By-law Variance	\$2,395.00
Sign By-law Permit Application Fee	\$75.00
Total: \$3,247.85	

An annual Encroachment Agreement Fee of \$25.00 and Inspection Fee of \$68.00 is also required to be paid for an initial period of up to 20 years. Thereafter, continuation of the fees for another 20 year period is dependant upon whether the encroachment still remains. This type of projecting wall sign is supported by Administration through the CIP incentive programs for the purpose of enhancing the unique identity at this key intersection (Drouillard Road and Whelpton Street) that is identified as a Neighbourhood Gateway in the Ford City CIP. However, owners are reluctant to pay for the installation of this type of sign, which encroaches over the public Rights-of-way if they have to pay an annual fee. Therefore, Recommendation VI of the Report requests that Council waive the annual fees for this particular project.

In future, this type of waiver would only be supported on a case-by-case basis based on the neighbourhood context and history of the area.

Risk Analysis:

The approval of these grants does not carry significant risk, as there are sufficient funds within the new CIP reserve fund approved by Council on February 22, 2021. The applicant will not receive any grants until all work is completed and inspected to the satisfaction of the Planning and Building Department. As a requirement of Section 28 (7.3) of the Planning Act, Administration has confirmed that the grant amount does not exceed the total cost of the project.

The installation of the projecting wall sign encroaches onto the public Right-of-Way on Drouillard Road. As discussed above the applicant is required to apply for a number of applications including an Encroachment Agreement to be permitted to install the proposed projecting wall sign located on the north-west facing side of the building at 1000 Drouillard Road.

Climate Change Risks

Climate Change Mitigation:

The beautification of the existing building will have a low impact on how the project affects climate change, because improvements are largely contained to the existing building footprint while also limiting use to existing infrastructure.

The beautification of the existing building contributes to the revitalization of an existing Main Street that services the surrounding Ford City. The installation of a new projecting wall sign that is proposed to be similar to those found along the Drouillard Road Main Street between 1920 to 1950 will help to draw interest to the intersection of Whelpton Street and Drouillard Road which is identified as a Neighbourhood Gateway area in the Ford City CIP. The new sign will also draw attention to the new restaurant that will be located in the building.

Utilizing an existing building and infrastructure in an existing built-up area of the City also promotes energy efficiency on the existing infrastructure network by not promoting development on greenfield land.

Climate Change Adaptation:

N/A

Financial Matters:

On February 22, 2021, Council approved the 2021 budget, which included a new reserve fund for all active CIPs in the City. As CIP grant applications are approved, the *approved grant amount will be transferred to the capital project account to be kept as committed funds, until the grant is ready to be paid out.* The current uncommitted balance in the CIP reserve fund is \$741,710.01 however this balance does not account for other CIP grant requests that are currently being considered by the standing committee or have been endorsed by the standing committee and are not yet approved by City Council.

If approved, funds will be transferred from the CIP reserve fund to the *Main Street CIP Capital Project Fund (Project #7219018)* to disperse the maximum amount of +/- \$5,367.50 for the *Facade Improvement Grant Program* identified in this report when all work is completed.

Also, if approved funds will be transferred from the CIP reserve fund to the Ford City CIP Fund (Project #7181046) in the maximum amount of +/- \$3,247.85 for the *Municipal Development Fees Grant Program* when all work is completed.

If this report is approved the applicant will receive +/- \$8,615.35 in grants.

With regards to the waiver of ongoing encroachment fees, the impact on revenues over the first 20 years is \$1,860.

The applicant had previously received grants for the property through the City's Brownfield CIP for a Phase II Environmental Site Assessment (ESA) in the amount of \$6,550.

Consultations:

The owner for the property located at 1000 Drouillard Road has been consulted regarding grants and application fees related to the improvements outlined in this report.

The following members of Administration have also been consulted regarding this Report:

- Josie Gualtieri, Financial Planning Administrator regarding the *Main Streets CIP* and *Ford City CIP* grants, and related capital project/reserve fund balances.
- Stefan Fediuk, Landscape Architect regarding the compliance of the Permanent Projecting Wall Sign according to regulations in the Sign By-law 250-2004; and
- Sandy Mio, Technologist II, Engineering regarding Encroachment Agreements costs and requirements.

Conclusion:

The improvements to 1000 Drouillard Road meet all eligibility criteria identified in this report, concerning signage as part of the *Building Facade Improvement Program* through the *Main Streets CIP*, and the *Municipal Development Fees Grant Program* through the *Ford City CIP*. The suspension of the projecting wall sign will encroach approximately 0.6 metres into the ROW on Drouillard Road. A Sign By-law Permit, Encroachment Agreement and a Sign Bylaw Variance through the Committee of Adjustment will be required.

The improvement will contribute to the Revitalization of Ford City and a vibrant main street through the installation of the projecting wall sign to enhance the unique identity of the intersection of Whelpton Street and Drouillard Road, which is identified as a Neighbourhood Gateway area in the Ford City CIP.

There are sufficient funds in the CIP reserve fund to provide grants for the proposed improvements. Administration recommends approval of the grants identified in this report.

Planning Act Matters:

N/A

Approvals:

Name	Title
Kevin Alexander	Senior Planner-Special Projects
Josie Gualtieri	Financial Planning Administrator

Neil Robertson	Manager of Urban Design/ Deputy City Planner
John Revell	Chief Building Official
Neil Robertson for Thom Hunt	City Planner/Executive Director of Planning & Development
Wira Vendrasco	Deputy City Solicitor, Legal Services & Real Estate
Adam Pillon	Manager of Right-of-Way
Jelena Payne	Commissioner, Economic Development & Innovation
Janice Guthrie	Commissioner, Corporate Services Chief Financial Officer / City Treasurer
Joe Mancina	Chief Administrative Officer

Notifications:

Name	Address	Email
Spotvin Inc. (c/o Shane Potvin)	2141 Niagara Street, Windsor, ON, N8Y 1K4	shane@spotvin.com

Appendices:

- 1 Appendix 'A' - Location and Existing Condition
- 2 Appendix 'B' - Proposed Projecting Wall Sign

APPENDIX 'A'

1000 DROUILLARD ROAD – LOCATION MAP



LOCATION MAP

4



SUBJECT PROPERTY : 1000 DROUILLARD RD

APPENDIX 'A'

1000 DROUILLARD ROAD – EXISTING CONDITION



Image from Google Maps, taken November 2020



Image from Google Maps, taken November 2020

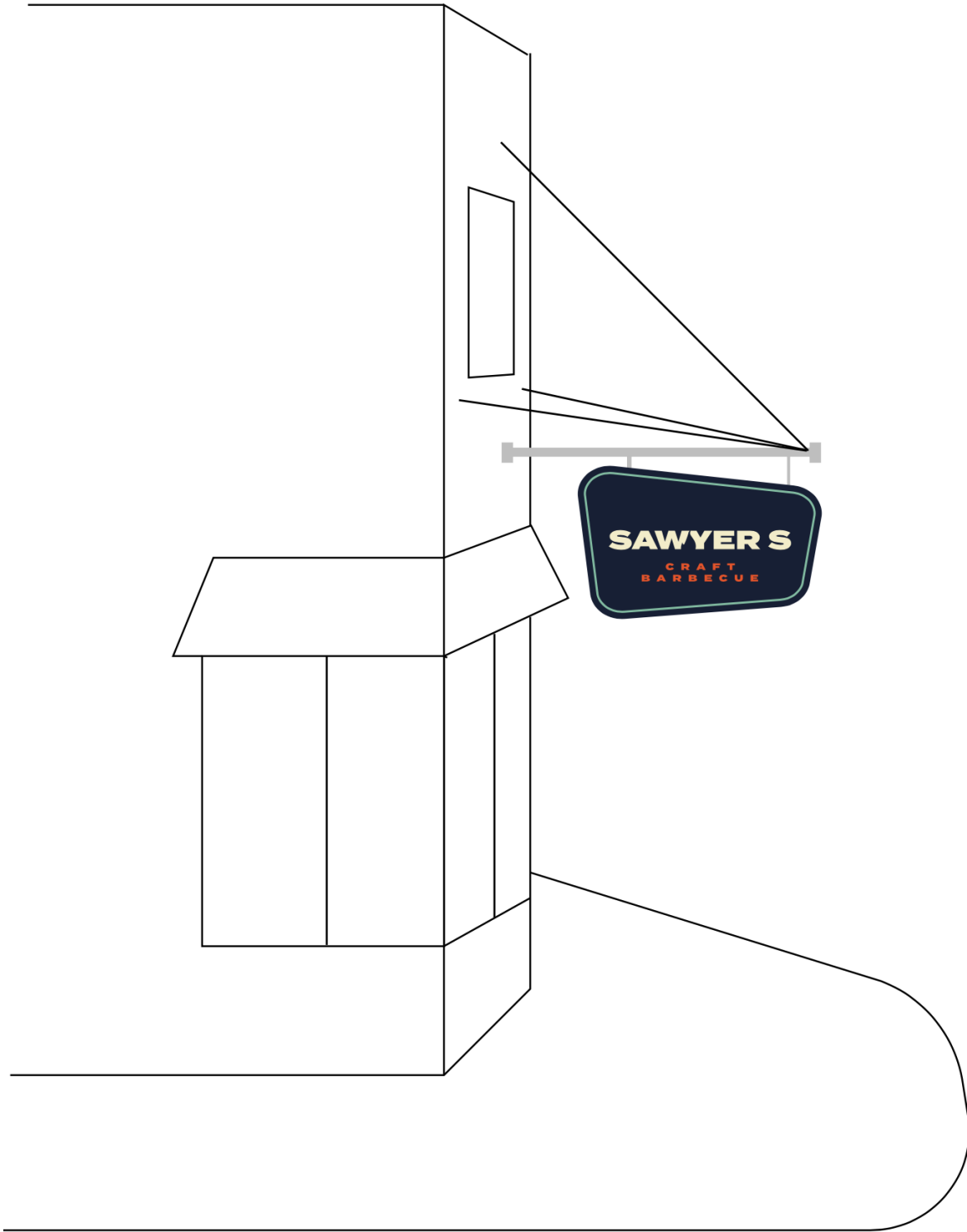
APPENDIX 'B'

1000 DROUILLARD ROAD – PROPOSED PROJECTING WALL SIGN



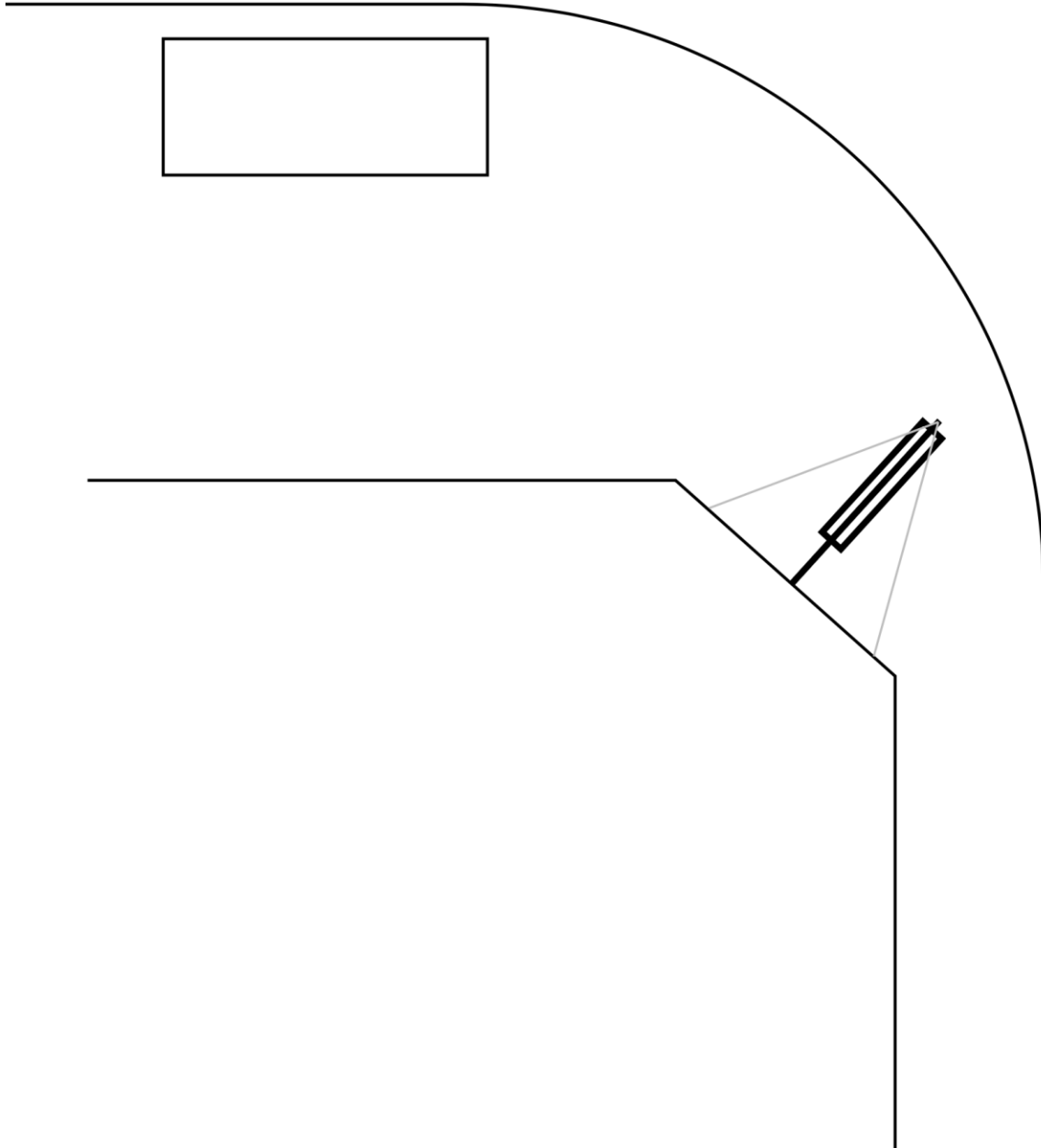
APPENDIX 'B'

1000 DROUILLARD ROAD – PROPOSED PROJECTING WALL SIGN



APPENDIX 'B'

1000 DROUILLARD ROAD – PROPOSED PROJECTING WALL SIGN



APPENDIX 'B'

1000 DROUILLARD ROAD – PROPOSED PROJECTING WALL SIGN



APPENDIX 'B'

1000 DROUILLARD ROAD – PROPOSED PROJECTING WALL SIGN

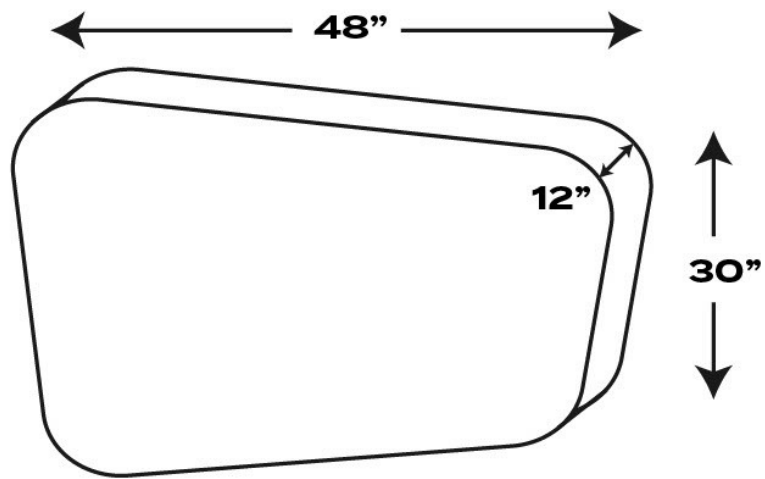
NEON (ON BOTH SIDES)



APPENDIX 'B'

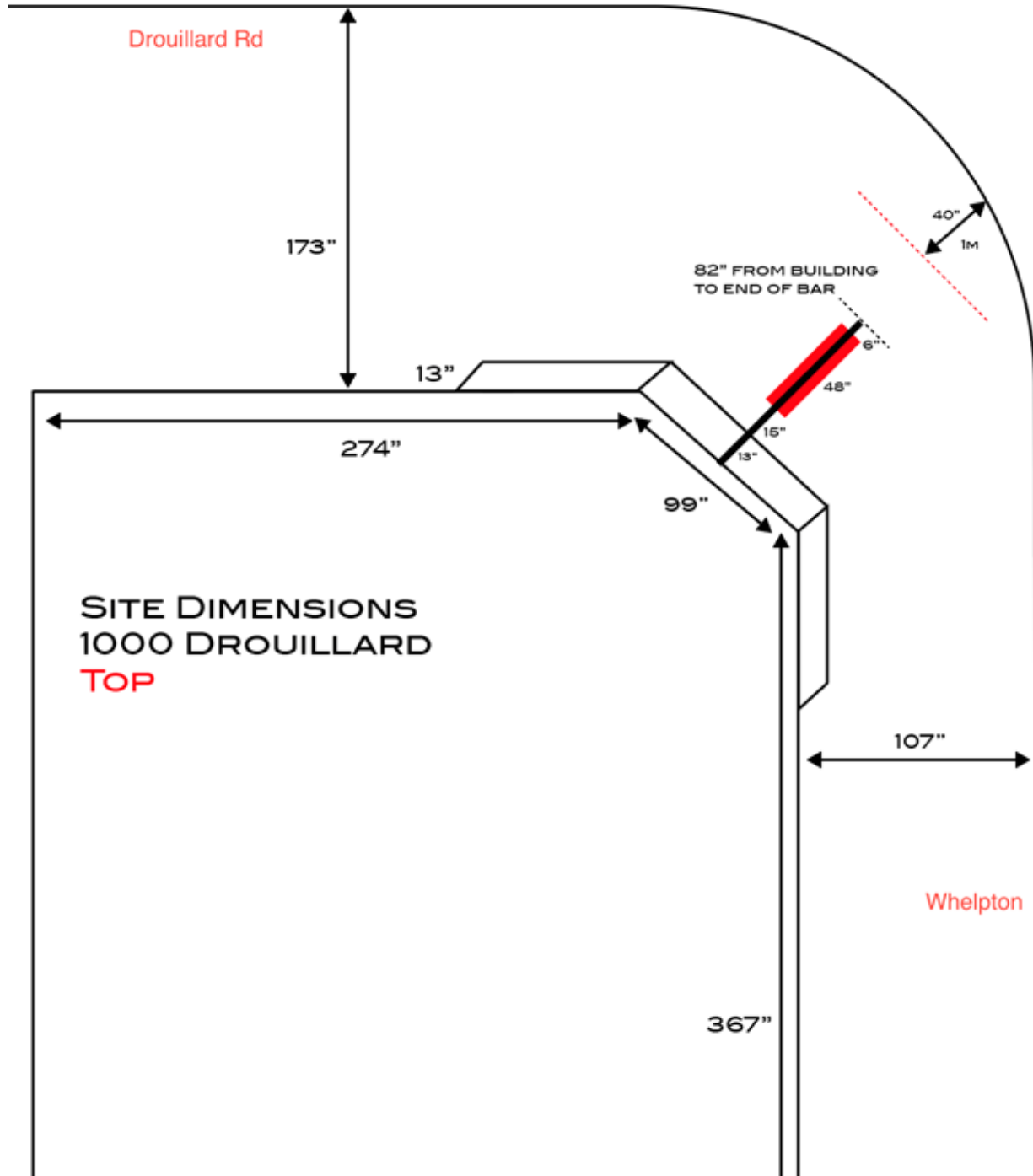
1000 DROUILLARD ROAD – PROPOSED PROJECTING WALL SIGN

DIMENSIONS



APPENDIX 'B'

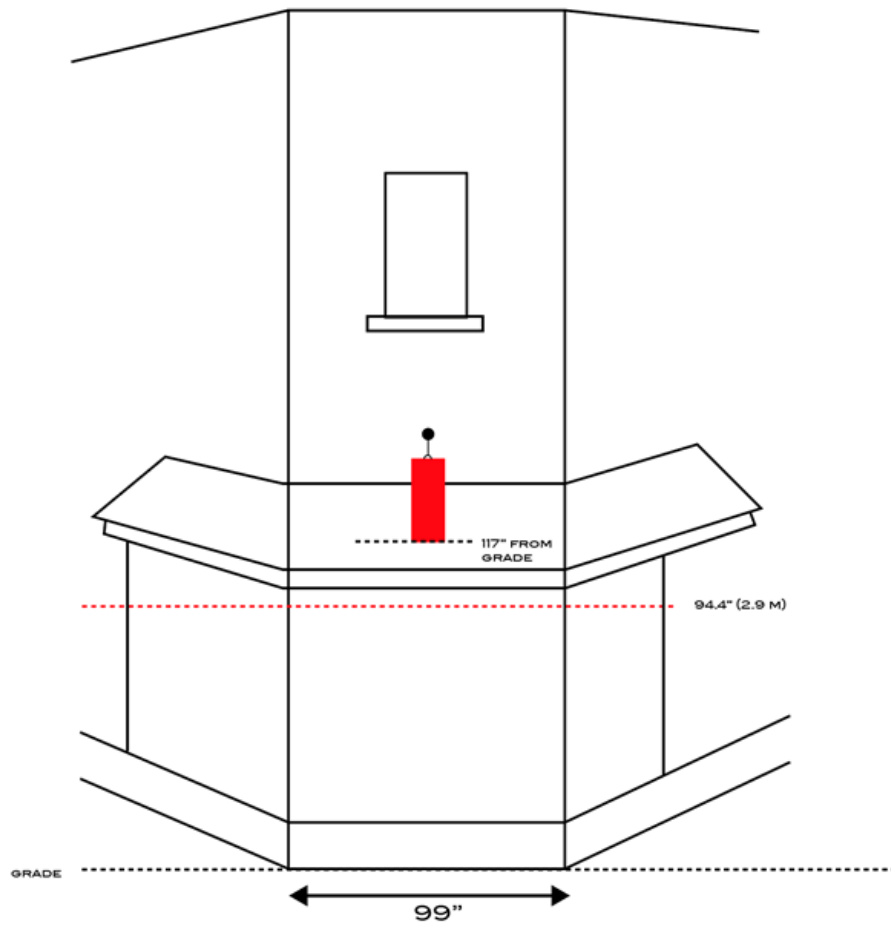
1000 DROUILLARD ROAD – PROPOSED PROJECTING WALL SIGN



APPENDIX 'B'

1000 DROUILLARD ROAD – PROPOSED PROJECTING WALL SIGN

SITE DIMENSIONS
1000 DROUILLARD
NORTH WEST SIDE



Item No. 12.1



Committee Matters: SCM 161/2023

**Subject: Minutes of the Property Standards Committee of its meeting held May 2,
2023**

Property Standards Committee

Meeting held May 2, 2023

A meeting of the Property Standards Committee is held this day commencing at 10:00 o'clock a.m. in Room 140, 350 City Hall Square West, there being present the following members:

Councillor Ed Sleiman, Chair
Councillor Fabio Costante
Councillor Mark McKenzie
Michael George
Dan Lunardi

Delegate in attendance:

Louis Vaupotic, regarding *Item 6.2*

Also present are the following resource personnel:

Rob Vani, Manager of Inspections, Deputy Chief Building Official
Wira Vendrasco, Deputy City Solicitor
Kevin Alexander, Planner III
Brandon Calleja, Manager Permits, Deputy Chief Building Official
Armando Cala, Building By-law Enforcement Officer
Jay McGuire, Manager, Inspections
Peter Quaglia, Building By-law Enforcement Officer
Karen Kadour, Committee Coordinator

1. Call to Order

The Committee Coordinator calls the meeting to order at 10:02 o'clock a.m. and the Committee considers the Agenda being Schedule A attached hereto, matters which are dealt with as follows:

2. Election of Chair

The Committee Coordinator calls for nominations from the floor for the position of Chair. Councillor Mark McKenzie nominates Councillor Ed Sleiman, seconded by Councillor Fabio Costante. The Committee Coordinator asks if there are further nominations from the floor. Seeing none, the Committee Coordinator asks Councillor Ed Sleiman if he accepts. Councillor Ed Sleiman accepts and assumes the Chair.

Moved by Councillor Mark McKenzie, seconded by Councillor Fabio Costante,
That Councillor Sleiman **BE ELECTED** Chair of the Property Standards
Committee.

Carried.

3. Disclosure of Interest

None disclosed.

4. Adoption of the Minutes

Moved by Councillor Fabio Costante, seconded by Councillor Mark McKenzie,
That the minutes of the Property Standards Committee of its meeting held June
14, 2022 **BE ADOPTED** as presented.

Carried.

5. Request for Deferral, Referral or Withdrawal

None.

Opening Remarks

Rob Vani, Manager of Inspectors and Deputy Chief Building Official welcomes the
members to the Property Standards Committee. He provides an overview of the Property
Standards Committee as follows:

- The Property Standards Committee is a quasi-judicial body and not an advisory committee of Council. Decisions are made by this Committee and is somewhat governed by the *Ontario Building Code Act*.
- The Property Standards By-law in the City of Windsor derives its authority under the *Ontario Building Code Act* which mandates a property standards committee to hear any appeals.
- In rendering its decision, the Committee may:
 - Confirm the Order as issued.
 - Modify the terms of the Order
 - Quash the Order
 - Extend the time limit for compliance
- When discussing deadlines, it helps from an enforcement perspective to choose a date to comply with the decision of the Committee.
- The decision of the Committee may not be final. There is an opportunity under the legislation to appeal this decision to the Superior Court of Ontario.

6. Appeal

Louis Vaupotic appears before the Property Standards Committee against an Order to Repair VY 23-5067 issued January 17, 2023 regarding property at 575 Riverside Drive East, Plan 129, Lot 2 Blk 2. The Notice of Appeal dated February 6, 2023 was received within the 14 day time frame.

Peter Quaglia, Building By-law Officer provides a presentation entitled “575 Riverside Drive East – OTR 23-5067”, **attached** as Appendix “A”, that includes the Summary of Events – site inspection held on January 17, 2023, various photos of the building and the current compliance status. He advises to date there is no significant compliance on any of the defects outlined in the Order to Repair.

The Chair asks if the Property Standards By-law applies to this residence whether it is inhabited or not. Rob Vani responds that the Property Standards By-law applies to all property whether it is vacant or not. The building still needs to be maintained, protected for repurpose, renovated and reused as residential stock in our municipality. He adds that most of the items identified in the presentation would be violations under the By-law.

Louis Vaupotic, appellant provides a summary of events regarding the property at 575 Riverside Drive East including legal issues.

Councillor Fabio Costante asks for a primer on the history of this file. Rob Vani responds that enforcement dates back two years (original Orders were issued to the previous owner). Once the title on the property was resolved, the Building Department reissued new Orders to the current owners of the property.

Councillor Fabio Costante states that the appellant has been aware of the Property Standards Orders for two years. He asks the appellant if there are any plans to remediate the defects outlined in the Order to Repair.

Dan Lunardi asks the appellant if there are any elements of the Order that are not valid and inquires if he is seeking more time. Louis Vaupotic responds the Order is valid.

Peter Quaglia, remarks that the deadline to comply with the Order was February 16, 2023.

Moved by Councillor Fabio Costante, seconded by Councillor Mark McKenzie,
UPON THE APPLICATION of the Appellant by way of Appeal from the Order to Repair VY 23-5067 issued by the Building By-law Enforcement Officer on the **17th of January 2023** respecting the property at 575 Riverside Drive East, and upon reading of the said Order:

IT IS ORDERED that the said Order to Repair **BE APPROVED** as follows:

That the Order to Repair VY 23-5067 regarding property at 575 Riverside Drive East **BE CONFIRMED.**

Carried.

7. Adjournment

There being no further business, the meeting is adjourned at 10:30 o'clock a.m.



575 RIVERSIDE DR E – OTR 23-5067

Building By-law Officer: Peter Quaglia

Inspection Completed: January 17, 2023

OTR Issued: January 17, 2023

Property Standards Committee Meeting: May 2, 2023

SUMMARY OF EVENTS

January 17, 2023 – Site inspection at 9:00 AM.

Attended property to complete follow up inspection in continued effort to gain compliance at historically problematic property. Observed the multi-unit residential building continues to present as vacant and derelict / neglected for a period of more than 120 days.

I observed the following property standard defects pursuant to By-Law 9-2019:

Sections 1.6 & 1.7

1. Defective concrete block / missing mortar at foundations walls of main building and exterior walls of accessory structure.
2. Defective / missing siding at main building and accessory structure

Sections 1.8 & 3.4

1. Several windows and exterior doors beyond life span and in need of repair / replacement.
2. Defective overhead door to accessory structure.

Sections 1.9, 1.10, 1.11, 1.12

1. Missing handrail at front and rear doors
2. Missing rear exterior stairs; rear 2nd floor deck / balcony / canopy poorly repaired containing defective / deteriorated materials. Permit with engineer's report / scope of work required for required repairs / replacement.

Section 1.14

1. Defective asphalt roof, soffits and fascia.

Sections 1.15 & 1.32

1. Missing / defective / deteriorated eaves troughs and down spouts throughout.

Order to Repair issued and sent registered mail same day as inspection with compliance date of **February 16, 2023.**



1. Rear accessory structure.



2. Front of building (North elevation).



3. Rear of building (South elevation).



4. Side of building (East elevation).



5. Side of building (West elevation).



6. Roof line close up at side of building (West elevation).



7. Side of building (West elevation).



8. Foundation wall close up at side of building (West elevation).

CURRENT COMPLIANCE STATUS

Status of defects:

Sections 1.6 & 1.7

1. Defective concrete block / missing mortar at foundations walls of main building and exterior walls of accessory structure. **NO ACTION TAKEN – NOT IN COMPLIANCE.**
2. Defective missing siding at main building and accessory structure. **NO ACTION TAKEN – NOT IN COMPLIANCE.**

Sections 1.8 & 3.4

1. Several windows and exterior doors beyond life span and in need of repair / replacement. **NO ACTION TAKEN – NOT IN COMPLIANCE.**
2. Defective overhead door to accessory structure. **NO ACTION TAKEN – NOT IN COMPLIANCE.**

Sections 1.9, 1.10, 1.11, 1.12

1. Missing handrail at front and rear doors. **NO ACTION TAKEN – NOT IN COMPLIANCE.**
2. Missing rear exterior stairs; rear 2nd floor deck / balcony / canopy poorly repaired containing defective / deteriorated materials. Permit with engineer's report / scope of work required for required repairs / replacement. **NO ACTION TAKEN; NO RECORD OF PERMIT APPLICATION IN CLOUDPERMIT OR AMANDA – NOT IN COMPLIANCE.**

Section 1.14

1. Defective asphalt roof, soffits and fascia. **NO ACTION TAKEN – NOT IN COMPLIANCE.**

Sections 1.15 & 1.32

1. Missing / defective / deteriorated eaves troughs and down spouts throughout. **NO ACTION TAKEN – NOT IN COMPLIANCE.**



Item No. 12.2

Committee Matters: SCM 169/2023

**Subject: Minutes of the International Relations Committee of its meeting held
May 10, 2023**

International Relations Committee

Meeting held May 10, 2023

A meeting of the International Relations Committee is held this day commencing at 3:30 o'clock p.m. in Room 522a, there being present the following members:

Councillor Angelo Marignani, Chair
Councillor Renaldo Agostino
Councillor Fred Francis
Councillor Ed Sleiman
Jerry Barycki
Ronnie Haidar
William Ma

Absent:

Lubna Barakat
Saiful Bhuiyan
L.T. Zhao

Also present are the following resource personnel:

Milan Vujanovic, Senior Economic Development Officer
Sandra Gebauer, Council Assistant
Karen Kadour, Committee Coordinator

1. Call to Order

The Chair calls the meeting to order at 3:30 o'clock p.m. and the Committee considers the Agenda being Schedule A attached hereto, matters which are dealt with as follows:

2. Declaration of Conflict

None disclosed.

3. Adoption of the Minutes

Moved by Councillor Fred Francis, seconded by Councillor Renaldo Agostino,
That the minutes of the International Relations Committee of its meeting held
March 8, 2023 **BE ADOPTED** as presented.
Carried.

4. Presentation – Request for a Sister City Agreement with the City of Arlington, Texas

Sheri Capehart, Program Director for Arlington, Texas Sister Cities, Bruce Payne, Manager of Economic Development for Arlington, Texas and Michael Jacobson, President and CEO of the Greater Arlington, Chamber of Commerce are present via Zoom video conference. The Committee members view two videos highlighting the educational and medical institutions, arts, entertainment, businesses, GM Plant, sports facilities and attractions in Arlington, Texas. Ms. Capehart refers to the many synergies with the City of Windsor.

In response to a question asked by the Chair regarding the main industries in Arlington, Michael Jacobson responds the main industries are automotive and aerospace.

Councillor Francis refers to the GM Plant in Arlington, and asks how many employees work at the Plant. The delegations respond there are 4,600 employees working at the Plant (three shifts a day, 6 days a week).

In terms of diversity and the demographics in Arlington, Sheri Capehart responds that there are a large number of folks that are Hispanic, African American, Vietnamese and Asian. She adds that they have one Sister City in Bad Konigshofen, Germany and advises they celebrated their 70th Anniversary in 2021.

Ronnie Haidar refers to the EV Battery Plant happening in Windsor and asks about the Wallbox EV Charging Plant in Arlington and what the response has been.

Bruce Payne states the Wallbox EV Charging Plant is designed so everything happens in one area (130,000 square foot factory) and is integrated with computer software. The plant is designed to produce over one million units by 2030 and will serve multiple segments of EV charging.

Councillor Ed Sleiman asks the delegations to expand on the reasons for twinning with the City of Windsor. Sheri Capehart responds because they have only one sister city which was born out of humanitarian relief, the City of Arlington is interested in twinning with a city with like interests, such as educational components, economic development, and to allow for student exchanges to experience cultural differences outside of their country.

The Chair invites the delegation to ask questions of the Committee.

Bruce Payne asks for an overview of the International Relations Committee including the role of the committee and information relating to the number of Sister Cities.

Councillor Fred Francis responds that the City of Windsor has 12 Sister Cities and the Committee consists of City Councillors and members of the public. The relationship with the Sister Cities has always been “people to people” and because Windsor is so

ethnically diverse, an ethnic community would reach back home to their families, resulting in relationships with China, Japan, Poland, England, France, Italy, Macedonia, El Salvador, Mexico, Germany and South Korea. The relationship with our Sister Cities is based on aspects such as “people to people”, humanitarian, cultural, sports, educational, economic and adds that once the people are bonded together, the relationship tends to last decades.

Bruce Payne asks why the City of Windsor does not have a Sister City in the United States.

Councillor Fred Francis states there was never a need to formalize that relationship as there is an interconnectedness between Canadians and Americans, i.e. crossing the Bridge and the Tunnel on a daily basis. Councillor Renaldo Agostino adds that the City of Detroit is like our next door neighbours.

Ronnie Haidar remarks that the City of Windsor is somewhat linked with Detroit as we watch their television programs, follow their sports teams, and go shopping. What is good about having a border community is that there are many educational transnational opportunities.

The Chair thanks the delegation from Arlington, Texas for their presentation.

Councillor Fred Francis expresses concern that there is no “people to people connection”. He suggests starting with a “friendship city”, build a relationship and then consider a Sister City connection. He adds exchanges with Arlington could include culture, sports, tourism, business, music and education.

Sandra Gebauer remarks that Arlington does have an alliance with the City of Toronto for music. She suggests reaching out to Toronto to determine if there is an agreement with Arlington.

Councillor Renaldo Agostino asks if discussions on homelessness or other topics take place between the Sister Cities and if ideas are shared. The Chair responds that oftentimes, we can learn by seeing the methods of other Sister Cities.

Councillor Fred Francis responds that the relationship with the City of Detroit is about the people; the relationship between government to government is not as close as one would think. He adds there is no relationship with Detroit City Council.

Milan Vujanovic remarks that the Economic Development Office has been in touch with the Consulate General in Detroit and is working on a bi-national project between Windsor and Detroit basically trying to communicate what is the value proposition of this region collectively.

Councillor Fred Francis requests that the City of Arlington, Texas provide a formal proposal.

Sandra Gebauer advises that as per the City of Windsor's Twin City Policy, the City of Arlington, Texas must submit a formal proposal and adds that it is the IRC Committee who will make a recommendation to the Mayor's Office. The Mayor in turn will bring the recommendation to Council for a decision.

5. Business Items

5.1 Letter of Invitation from Changchun, China for a delegation from the City of Windsor to visit

Moved by Councillor Fred Francis, seconded by Councillor Renaldo Agostino,
That the invitation from Changchun, China for a delegation from the City of Windsor to visit Changchun, China **BE RECEIVED** and further, that correspondence will be sent acknowledging when and if a delegation from the City of Windsor will visit in 2023.
Carried.

5.2 Children's Art Exhibition

Sandra Gebauer reports that since 2017, the IRC determines every year if there is interest in participating in the Children's Art Exhibition and if so, when and where.

Councillor Fred Francis suggests status quo, i.e. holding the Children's Art Exhibition in City Hall.

Councillor Renaldo Agostino proposes holding the Children's Art Exhibition at the Aquatic Centre or taking the Children's Art Exhibition on tour to different venues.

Councillor Fred Francis suggests reaching out to Devonshire Mall as a venue as there is more foot traffic.

It is generally agreed that the Children's Art Exhibition will be held in November 2023. Sandra Gebauer will research to determine if the venues at the Aquatic Centre and Devonshire Mall are available. She will report back at the next meeting.

6. Business Arising from the Minutes

International Conference of Twin Cities & Ceremonial Opening of the Garden of the Twin Cities in Mannheim, Germany

The Chair provides the highlights of the International Conference of Twin Cities and ceremonial opening of the Garden of the Twin Cities in Mannheim, Germany as follows:

- The delegation to Mannheim, Germany consisted of the Chair, William Ma and Wanda Letourneau, Horticulturalist
- Upon arrival, there was a casual meet and greet
- Provided gifts to the dignitaries that were present
- The event included a three kilometer gondola ride
- Environmental concerns and options were discussed at the conference.
- Attended a dinner with Dr. Peter Kurz, Mayor of Mannheim
- Toured the Garden of the Twin Cities
- The display by Wanda Letourneau was outstanding.

William Ma adds upon arrival in Mannheim, there was an information exchange and a discussion regarding best practices relating to the twin cities and suggests incorporating best practices into the IRC Mandate.

Sandra Gebauer reports that approval was given at the IRC meeting held March 8, 2023 to an expenditure in the amount of \$10,000 for the Mannheim, Germany delegation to attend the Ceremonial Opening of the Garden of Twin Cities and the International Conference of Twin Cities. The actual amount spent was approximately \$4,707.

Ronnie Haidar asks when delegations from the IRC visit various Sister Cities, do they wear anything that is representative of the City of Windsor. The Chair responded that the delegations to Mannheim wore the City of Windsor Pin at every event.

7. Communications

7.1 Nicolaus Copernicus Pictorial Report

Jerry Barycki provides an update of the Pictorial Report – The National Inauguration of the Copernicus Jubilee held on February 17, 2023 in Windsor. He adds that The Copernicus Jubilee was also held in Poland from February – March 2023.

Business Arising from the Minutes

Jerry Barycki suggests a brainstorming discussion be held regarding “where does the IRC want to be in two to five years”.

Jerry Barycki inquires about the status of the City Post. Sandra Gebauer responds that the location near the parking lot in the City Esplanade is no longer feasible. She

suggests inviting the Manager, Parks Development to the next meeting to discuss an alternate location.

8. Date of Next Meeting

The next meeting will be held at the call of the Chair.

9. Adjournment

There being no further business, the meeting is adjourned at 5:10 o'clock p.m.