

City Council Meeting

Date: Monday, October 30, 2023

Time: 4:00 o'clock p.m.

Location: Council Chambers, 1st Floor, Windsor City Hall

All members will have the option of participating in person in Council Chambers or electronically and will be counted towards quorum in accordance with Procedure By-law 98-2011 as amended, which allows for electronic meetings. The minutes will reflect this accordingly. Any delegations have the option to participate in person or electronically.

MEMBERS:

Mayor Drew Dilkens

Ward 1 – Councillor Fred Francis

Ward 2 – Councillor Fabio Costante

Ward 3 – Councillor Renaldo Agostino

Ward 4 – Councillor Mark McKenzie

Ward 5 – Councillor Ed Sleiman

Ward 6 – Councillor Jo-Anne Gignac

Ward 7 – Councillor Angelo Marignani

Ward 8 – Councillor Gary Kaschak

Ward 9 – Councillor Kieran McKenzie

Ward 10 - Councillor Jim Morrison

ORDER OF BUSINESS

- | Item # | Item Description |
|--------|---|
| 1. | ORDER OF BUSINESS |
| 2. | CALL TO ORDER - Playing of the National Anthem

READING OF LAND ACKNOWLEDGEMENT
We [] would like to begin by acknowledging that the land on which we gather is the traditional territory of the Three Fires Confederacy of First Nations, which includes the Ojibwa, the Odawa, and the Potawatomi. The City of Windsor honours all First Nations, Inuit and Métis peoples and their valuable past and present contributions to this land. |
| 3. | DISCLOSURE OF PECUNIARY INTEREST AND THE GENERAL NATURE THEREOF |
| 4. | ADOPTION OF THE MINUTES |
| 5. | NOTICE OF PROCLAMATIONS

Proclamations

“Project Red Ribbon Day” – November 1, 2023

Flag Raising Ceremony

“Lebanese Heritage Month Canada” – November 1, 2023 – November 10, 2023

Illumination

“Lebanese Heritage Month Canada” – November 1, 2023 – November 7, 2023

“Project Red Ribbon” – November 8, 2023 – November 12, 2023

“Annual Shine the Light Campaign” – November 13, 2023 – November 17, 2023 |
| 6. | COMMITTEE OF THE WHOLE |
| 7. | COMMUNICATIONS INFORMATION PACKAGE (This includes both Correspondence and Communication Reports) |

8. **CONSENT AGENDA**

CONSENT COMMITTEE REPORTS

- 8.1. ProsperUS Advocacy Letter - Primary Care Access **(SCM 241/2023) & (SCM 265/2023)**
- 8.2. Response to Open Streets Petition - City Wide **(SCM 239/2023) & (S 107/2023)**
- 8.3. Response to CQ 20-2023 – Feasibility of Expanding Outreach Services and 311 Operating Hours– City Wide **(SCM 274/2023) & (S 118/2023)**
- 8.4. Official Plan & Zoning Bylaw Amendments Z 024-23 [ZNG-7069] & OPA 175 [OPA-7072] 1027458 Ontario Inc. Multiple Dwelling Development Banwell & McHugh (South) - Ward 7 **(SCM 268/2023) & (S 121/2023)**
- 8.5. Draft Plan of Condominium with Exemption under Section 9(3) of the Condominium Act –CDM 008-23 [CDM-7134] Lankor Horizons Development Inc. 3290, 3320, 3340, 3370 STELLA CRESCENT Ward 7 **(SCM 269/2023) & (S 117/2023)**
- 8.6. Closure of north/south alley located between Joinville Avenue and Haig Avenue, and two intersecting east/west alleys located west of north/south alley, Ward 8, SAA-6822 **(SCM 270/2023) & (S 102/2023)**
- 8.7. Closure of north/south alley located between Taylor Avenue and 1124-1224 Campbell Avenue, Ward 2, SAA-6848 **(SCM 271/2023) & (S 109/2023)**
- 8.8. Additional information regarding Public Communications **(SCM 272/2023) & (SCM 266/2023)**
- 8.9. Official Plan Amendment and Zoning Bylaw Amendment Site specific regulations for Multiple Dwelling – Z 001-22 [ZNG-6653] Riverside Horizons 3251 Riverside Dr. E & 222 Belleview Ave - Ward 5 **(SCM 245/2023) & (S 93/2023)**

9. **REQUEST FOR DEFERRALS, REFERRALS AND/OR WITHDRAWALS**

10. **PRESENTATIONS AND DELEGATIONS**

PRESENTATIONS

- 10.1. Windsor Firefighters – Sparky’s 2023 Toy Drive **(5 minutes)**
- 10.2. Reverend Gene Lotz, Military Historian/Author – Book Presentation – “Canadians in a Forgotten War” to Mayor & Members of Council **(5 minutes)**
- 10.3. Tourism Windsor Essex Pelee Island – presentation and video **(10 minutes)**

11. **REGULAR BUSINESS ITEMS** (Non-Consent Items)
 - 11.1. Gouin Drain Provisional By-Law for Repair and Improvement - Ward 9 (**C 151/2023**)
 - 11.2. Update Regarding The Divestment of the East Riverside Shorewall; Furthering Report C77/2023 - Ward 7 & Update Regarding Council Decision B14/2019 (**C 149/2023**)
and Proposed Shoreline Structures Local Improvement Policy Amendment - City-Wide (**C 77/2023**)
Clerk's Note: P & C memo dated May 16, 2023 submitted to Mayor & members of Council only.

12. **CONSIDERATION OF COMMITTEE REPORTS**
 - 12.1. (i) Report of the Special In-Camera meeting or other Committee as may be held prior to Council (if scheduled)

13. **BY-LAWS** (First and Second Reading)

14. **MOVE BACK INTO FORMAL SESSION**

15. **NOTICES OF MOTION**

16. **THIRD AND FINAL READING OF THE BY-LAWS**

17. **PETITIONS**

18. **QUESTION PERIOD**

19. **STATEMENTS BY MEMBERS**

20. **UPCOMING MEETINGS**

Community Services Standing Committee
Wednesday, November 1, 2023
9:00 a.m., Council Chambers

Development & Heritage Standing Committee
Monday, November 6, 2023
4:30 p.m., Council Chambers

City Council Meeting
Tuesday, November 14, 2023
4:00 p.m., Council Chambers

Committee of Management for Huron Lodge
Monday, November 20, 2023
9:00 a.m., via Zoom video conference

21. **ADJOURNMENT**

Item No. 8.1



Committee Matters: SCM 241/2023

Subject: ProsperUS Advocacy Letter - Primary Care Access

Moved by: Councillor Fabio Costante
Seconded by: Councillor Renaldo Agostino

That city administration REPORT BACK to council on striking a regional steering committee in collaboration with key stakeholders to explore the need for Primary Care Provider recruitment and retention; and

That the Ontario Health Team BE REQUESTED to report to Council on what they are doing to address Physician Recruitment and how the City can work with them for Council's consideration.

The motion is **put** and **lost**.

Councillors Jo-Anne Gignac and Mark McKenzie voting nay.

Moved by: Jo-Anne Gignac
Seconded by: Mark McKenzie

Decision Number: **CSSC 207**

That the presentation provided by United Way – ProsperUs Collective Impact Initiative – Primary Care Access **BE RECEIVED** for information
Carried.

Councillors Fabio Costante and Renaldo Agostino voting nay.

Clerk's File: ACO2023

Clerk's Note:

1. Please refer to Item 6.1 from the Community Services Standing Committee held on September 6, 2023.
2. To view the stream of this Standing Committee meeting, please refer to:
<http://csg001-harmony.sliq.net/00310/Harmony/en/PowerBrowser/PowerBrowserV2/20230906/-1/9431>



Committee Matters: SCM 265/2023

Subject: United Way – ProsperUs Collective Impact Initiative – Primary Care Access

August 2, 2023

Dear Windsor City Council,

ProsperUs is a Collective Impact partnership comprised of non-profit, government, healthcare, education, labour, business, and inspired community members including parents and youth. We have a shared vision of a prosperous Windsor-Essex County where all children and youth have the support they need to succeed from cradle to career. You can learn more about our work at weunlockpotential.com.

Our Cradle to Career approach is a proven strategy that focuses on systems change to ensure young people have wrap-around supports from the time they are born until they enter the workforce. This work is focused on priority neighbourhoods where data tell us families face the greatest barriers: Downtown Windsor, West Windsor, and Leamington.

In May 2022, ProsperUs formed a resident-led Advocacy Working Group for West Windsor and Downtown Windsor. Advocacy Working Group members identified that access to healthcare services is a significant barrier they regularly face. Our research demonstrates a need to focus on increasing access to primary healthcare services:

- According to the Primary Care Data Reports for Ontario Health Teams (2022), 32,118 people in Windsor-Essex County are not attached to a Primary Care Provider. This includes 6,998 children between the ages of 0-14.ⁱ
- Among people not currently attached to a Primary Care Provider in our region, 45% live in neighbourhoods with the lowest average incomes.ⁱⁱ
- The need for physician recruitment is growing as Windsor-Essex County has an aging Physician population. More than 1 in 5 physicians in Windsor-Essex County were over the age of 65 and approaching retirement in 2019.ⁱⁱⁱ
- According to the Windsor Essex Ontario Health Team's Comprehensive Primary Care Report (2022), Physicians over the age of 65 in Windsor have a total of 28,986 patients. In Leamington, Physicians over the age of 65 have a total of 4992 patients.
- Furthermore, there are only 31 pediatricians^{iv} currently working in our region serving a population of 36,640 children ages 0-14.^v
- The pressure on our healthcare system was reflected in 2022, when Windsor Regional Hospital had the longest Emergency Room wait times in Ontario as too many people used hospitals for primary care.^{vi}

This is important because in Ontario, people living with low incomes are more likely to experience health risks in their daily life such as having less access to nutritious food. They are also more likely to live with multiple chronic conditions which can lead to further health problems, yet they are less likely to receive quality healthcare.^{vii} For

example, children living in low-income households are more likely to experience health issues such as asthma and obesity and face barriers to accessing preventive and primary care services.^{viii}

As of January 2023, the Regional Physician Recruiter Office Windsor/Essex (RPROWE) is no longer funded by any municipality in Windsor-Essex County. RPROWE supported all hospitals, community clinics, agencies, and private offices in our region, as well as the College of Physicians and Surgeons of Ontario. This initiative recruited an average of 34 physicians annually and recruited 569 physicians between 2003 and 2019.^{ix} 1 in 6 physicians required help with immigration which this initiative provided.^x

The ProsperUs partnership, including resident members of the ProsperUs Advocacy Working Group, recommends that the City of Windsor collaborate with other municipalities and healthcare leaders in Windsor-Essex County to establish a strategy for physician recruitment and retention in our region. This strategy could include:

- 1) Conducting a comprehensive review of the unmet healthcare needs of residents in Windsor-Essex County and how municipalities can collaborate with local healthcare leaders and the Government of Ontario to increase our region's capacity to meet the needs of residents.
- 2) Reconvening the Regional Physician Recruitment Steering Committee to increase data sharing, collaboration, and strategic action among healthcare providers. This could include informing and implementing a comprehensive strategy for physician recruitment and retention and working with the [Canadian Society of Physician Recruitment](#) to ensure more local residents have access to a Primary Care Provider.
- 3) Increasing investment in physician recruitment and retention efforts for local healthcare services based on the needs of residents, including specializations and languages spoken.
- 4) Coordinating regional advocacy efforts to the Government of Ontario regarding increased access to Primary Care Providers, including increased funding for local healthcare services and expanding the number of residency spaces at schools of medicine.

Delayed access to healthcare services has life-long consequences and increases costs to the healthcare system.^{xi} Increasing early prevention, and easing the pressure on healthcare workers and services requires the involvement of all levels of government. ProsperUs is presenting this letter to the City of Windsor and the County of Essex to raise awareness of the need for increased regional dialogue on access to primary care providers.

Through continued advocacy, as well as endorsement of a regional strategy for physician recruitment and retention, we believe that municipalities in Windsor-Essex County can make a significant impact on this key issue.

Thank you for your consideration of our suggested regional approach to this issue.

Sincerely,



Jessica Sartori
Owner/Operator, Parallel 42 Systems
Co-Chair ProsperUs



Shelley Fellows
Chair of the Board of Directors,
Automate Canada
Co-Chair ProsperUs

On behalf of the ProsperUs Collective Impact Partnership and Caroline Taylor, West Windsor Resident, Co-Chair of the ProsperUs Windsor Advocacy Working Group and Rose Hayes, Downtown Resident, Co-Chair of the ProsperUs Windsor Advocacy Working Group.

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- ⁱ INSPIRE-PHC, Primary Care Data Working Group. (2023) *Primary Care Data Reports for Ontario Health Teams (OHTs)*. <https://www.ontariohealthprofiles.ca/>
- ⁱⁱ INSPIRE-PHC, Primary Care Data Working Group. (2023) *Primary Care Data Reports for Ontario Health Teams (OHTs)*. <https://www.ontariohealthprofiles.ca/>
- ⁱⁱⁱ Mavrinac, J. (2019, October 16). *Regional Physician Recruitment* [PowerPoint slides]. <https://coe.pub.escribemeetings.com/filestream.ashx?DocumentId=12817>
- ^{iv} College of Physicians and Surgeons of Ontario (2023). *Advanced Search for Registered Doctors in Ontario*. [Data Set]. CPSO. <https://doctors.cpso.on.ca/?search=general> (accessed February 6, 2023)
- ^v Statistics Canada (2022). *Census Profile, 2021 Census of Population*. (Statistics Canada Catalogue no. 98-316-X2021001.[Data Set]. Statistics Canada. <https://www.statcan.gc.ca/census-recensement/2021/dp-pd/prof/index.cfm?Lang=E>
- ^{vi} Wilhelm, T. (2022, July). *Windsor Regional Hospital grapples with highest ER wait times in Ontario*. CBC News. <https://windsorstar.com/news/local-news/windsor-regional-hospital-grapples-with-highest-er-wait-times-in-ontario>
- ^{vii} Health Quality Ontario (2016). *Income and Health: Opportunities to achieve health equity in Ontario*. <http://www.hqontario.ca/portals/0/documents/system-performance/health-equity-report-en.pdf>
- ^{viii} Children First Canada. (2023). *Pedonomics: the Social Return on Investment in Improving the Health and Wellbeing of Children and Adolescents*. <https://childrenfirstcanada.org/wp-content/uploads/2023/05/Pedonomics-Raising-Canada-2023-Children-First-Canada.pdf>
- ^{ix} Mavrinac, J. (2019, October 16). *Regional Physician Recruitment* [PowerPoint slides]. <https://coe.pub.escribemeetings.com/filestream.ashx?DocumentId=12817>
- ^x Mavrinac, J. (2019, October 16). *Regional Physician Recruitment* [PowerPoint slides]. <https://coe.pub.escribemeetings.com/filestream.ashx?DocumentId=12817>
- ^{xi} Children's Health Coalition. (2021). *Make Kids Count: A Children's health action plan to ensure hospital capacity and provide timely access to care both in the immediate pandemic response and through recovery*. <https://www.chcontario.ca/wp-content/uploads/2022/04/Make-Kids-Count-Action-Plan-from-the-Childrens-Health-Coalition-2.pdf>

ProsperUs is a collective of non-profit, government, health care, education, labour, business, and inspired residents with a shared vision of a prosperous Windsor-Essex County where every young person has the opportunity and support they need to thrive. ProsperUs is a partnership between:

BMO Financial Group | City of Windsor | Collège Boréal | Connecting Windsor-Essex | Conseil scolaire catholique Providence | County of Essex | CUPE Local 543 | Erie Shores Healthcare | FCA Greater Essex County District School Board | Green Shield Canada | Hôtel-Dieu Grace Healthcare | Invest WindsorEssex | John McGarvey Children's Centre | Leamington & District Chamber of Commerce | Libro Credit Union | Ministry of Children, Community & Social Services | Ministry of Citizenship & Immigration | Ministry of Community Safety & Correctional Services | Ministry of Education | Ministry of Training, Colleges & Universities | Municipality of Leamington | New Canadians' Centre of Excellence | Ontario Health Steering Committee | Ontario Provincial Police | RBC Royal Bank | South Essex Community Council | South Essex Community Council | St. Clair College | TD Canada Trust | Toppenman's | The O'Neil Foundation | Toldo Foundation | Unifor Local 444 | United Way/Centraide Windsor-Essex County | University of Windsor | Windsor-Essex Labour Council | Windsor-Essex Catholic District School Board | Windsor-Essex Children's Aid Society | WindsorEssex Community Foundation | Windsor-Essex Community Housing Corporation | Windsor-Essex County Health Unit | Windsor-Essex Regional Chamber of Commerce | Windsor Police Service | Windsor Regional Hospital | Workforce WindsorEssex



University
of Windsor

Dr. Lawrence Jacobs, MD, FRCPC, FACP
Associate Dean, Windsor Campus
Assistant Professor, Schulich School of Medicine and Dentistry
Western University
Division of General Internal Medicine
Windsor Regional Hospital – Ouellette Campus
Windsor, Ontario, Canada

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August 31, 2023

RE: Physician Recruitment in Windsor-Essex

I write today as I understand that strategies to best support recruitment and retention of physicians are being discussed. In my role as Associate Dean – Windsor Campus, I feel that my perspective may assist in this discussion.

Medical students began training in Windsor in 2002 when several clinical clerks rotated for a few weeks at a time. Over the next few years, the numbers of learners and duration of training increased substantially. This led to the Windsor Campus' opening as a formal Regional Medical Campus in 2008. Initially there were 24 Year-1 medical students in 2008. This year, we welcomed our largest class of 42 first year students with plans for a phased expansion to 50 students over the next few years. In all, there are 156 students across four years of undergraduate medical education (UME) in this community and 421 students have graduated from this campus alone. The Windsor Campus celebrated its 15th anniversary in 2023 and proudly functions with stable operations. As such, our focus has increasingly been on the impact the school has had on the community and certainly towards physician recruitment.

To support and further enhance physician recruitment to our region, the Family Medicine and Psychiatry Windsor Programs, in conjunction with the home departments in London, have formulated a comprehensive program to further enhance the experience of our existing residents. The goal of this is to encourage the recruitment of future residents to the city. Currently, the Windsor Campus supports a range of postgraduate training across a variety of specialties including full-time trainees in Psychiatry (4 residents per year – 5-year training) and Family Medicine (10 residents per year – two-year training). The Campus also supports upwards of 230 visiting residents a year, from London and other communities throughout the country and on an international scale. Previous efforts with the family medicine training have been incredibly effective in supporting retention of these trainees (77% of Family Medicine residents who have graduated from our program have chosen to set up practice in this region). Additionally, since its inception, the psychiatry program continues to see graduating residents remain in Windsor (4/6 so far). Retention of a graduating resident is significant for any community as these individuals may work their entire careers here providing 30-40 years of care. Certainly, the city of Windsor's support of post-graduate medical education (\$24,000/year

Schulich School of Medicine & Dentistry – Windsor Campus
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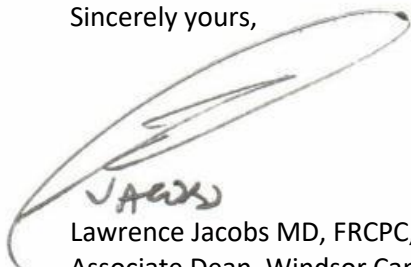


for enrichment activity) plays a role in this successful retention effort. Further, medical education does provide an attraction to recruitment. Since 2008, we have seen an absolute increase in specialists by 31% and family physicians by 35%. Unfortunately, prior to this, Windsor-Essex was estimated to have the worst access to physicians outside of northern Ontario. So, despite this success, we still have more to do.

I would caution against solutions focused solely or predominantly on recruitment from other communities. While this may provide instant metrics of success, it rarely leads to long-term stability in physician numbers as these efforts pit community against community and disrupt healthcare relationships in a time where supply and demand do not match. Medical Education is a long-term but sustainable solution to health care access. Efforts that improve the learning experience or those that increase the absolute number of trainees will have the most impact on a community. I firmly believe that focusing on these areas will best serve the interests of patients in Windsor-Essex.

Windsor-Essex needs more doctors and certainly communities such as West-Windsor and the downtown core are especially in need. The Schulich School of Medicine and Dentistry commits to our social responsibility of providing healthcare solutions to this region and is proud of the impact we have had to date. That said, complex problems never have simple solutions. I would certainly offer my experience to assist the community in any strategy considered.

Sincerely yours,

A handwritten signature in black ink, appearing to read 'L. Jacobs', is written over a large, light-colored oval scribble.

Lawrence Jacobs MD, FRCPC, FACP
Associate Dean, Windsor Campus
Assistant Professor, Medicine, Western University

Item No. 8.2



Committee Matters: SCM 239/2023

Subject: Response to Open Streets Petition - City Wide

Moved by: Councillor Jo-Anne Gignac
Seconded by: Councillor Mark McKenzie

Decision Number: **CSSC 206**

That the report from the Manger, Culture and Events, dated August 21, 2023, entitled "Response to Open Streets Petition" **BE RECEIVED** for information; and further,

That beginning in 2024, City Council **APPROVE** Open Streets moving to an east-side route including the Olde Riverside, Pillette Village, Ford City, Ottawa and Erie Street Business Improvement Areas (BIAs); and further,

That in 2025 the route **RETURN** to the previously approved downtown/west-side route including the Walkerville, Wyandotte Town Centre, Downtown, and Sandwich Business Improvement Areas (BIAs); and further,

That in future years, the City **CONTINUE** to alternate between the newly proposed east-side route and the previously approved downtown/west-side route; and further,

That administration **BE REQUESTED** to provide information related to options regarding holding Open Streets events twice a year including feasibility and costs, one in the downtown/west-side route and one in the new east-side route.

Carried.

Report Number: S 107/2023
Clerk's File: SPL/12373

Clerk's Note:

1. The recommendation of the Community Services Standing Committee and Administration are **not** the same.
2. Please refer to Item 8.4 from the Community Services Standing Committee held on September 6, 2023.
3. To view the stream of this Standing Committee meeting, please refer to:
<http://csg001-harmony.sliq.net/00310/Harmony/en/PowerBrowser/PowerBrowserV2/20230906/-1/9431>

Subject: Response to Open Streets Petition - City Wide

Reference:

Date to Council: September 6, 2023
Author: Michelle Staadegaard,
Manager, Culture and Events
519-253-2300 ext. 2726
mstaadegaard@citywindsor.ca

Recreation and Culture
Report Date: 8/21/2023
Clerk's File #: SPL/12373

To: Mayor and Members of City Council

Recommendation:

THAT the report titled "Response to Open Streets Petition" **BE RECEIVED** for information; and further,

THAT beginning in 2024, City Council **APPROVE** Open Streets moving to an east-side route including the Olde Riverside, Pillette Village, Ford City, Ottawa and Erie Street Business Improvement Areas (BIAs); and further,

THAT in 2025 the route **RETURN** to the previously approved downtown/west-side route including the Walkerville, Wyandotte Town Centre, Downtown, and Sandwich Business Improvement Areas (BIAs); and further,

THAT in future years, the City **CONTINUE** to alternate between the newly proposed east-side route and the previously approved downtown/west-side route.

Executive Summary:

N/A

Background:

Open Streets are unique events that have been held in over 100 cities in North America and involve the temporary closure of roadways to vehicular traffic, allowing people to use them for a host of healthy and fun physical activities like walking, cycling, yoga, dancing and fitness.

The goals of Open Streets are to encourage active transportation and physical activity; encourage community building activities; promote community-led active living initiatives; showcase recently improved streetscapes; provide an opportunity for local businesses and organizations to participate along the route.

In February of 2016, City Council adopted CR87/2016, which granted approval for two Open Streets Pilot Projects in July and September of that year.

On May 29, 2017, through report #C76/2017, Administration reported to City Council on the completion and success of the 2016 Open Streets Pilot Project and informed Council on the receipt of a 2017 Ontario 150 Grant which was to be used to continue Open Streets in 2017. City Council formally approved "Open Streets Windsor: An Ontario 150 Celebration" through CR313/2017. The event date was confirmed for September 17, 2017, with extended hours from 10:00 am – 5:00 pm and a budget of \$74,400. It was another successful event.

At the January 15, 2018, Special Meeting - 2018 Operating and Capital Budget, report #B27/2018 approved an Open Streets Windsor 2018 event to be held on September 23, 2018, from 10:00 am to 3:00 pm, following the event route adopted in 2017, with a road closure from 9:00am to 4:00pm. C196\2017 also granted approval of Budget Issue (Coordinator, Programs and Events) which allowed for the hiring of a new position to who would act as the Project Manager to Open Streets.

Following the 2018 Open Streets event an analysis of the 2018 route, along with potential alternate future routes was conducted. Feedback was received from the public, BIAs, the Special Events Resource Team (SERT) and Administration. In 2019, CAO 40-2019 approved a partial route change to improve the experience for participants and to assist in the maximization of available resources. This involved moving a portion of the event from University Avenue to Riverside Drive.

In 2019, the following route was adopted: Sandwich Street between Brock Street and Rosedale Avenue (roundabout), Riverside Drive West between Rosedale Avenue and Caron Avenue, Caron Avenue between Riverside Drive West and University Avenue, University Avenue West between Caron Avenue and Aylmer Avenue, Aylmer Avenue between University Avenue East and Wyandotte Street East, Wyandotte Street East between Aylmer Avenue and Albert Road, Albert Road between Wyandotte Street East and Edna Street, Edna Street between Albert Road and Drouillard Road, Drouillard Road between Wyandotte Street East and Richmond Street.

Due to the COVID-19 pandemic and restrictions around gathering limits, Open Streets 2020 was cancelled via CR#437/2020.

In 2021 and 2022, Open Streets continued to follow the 2019 route. Feedback from businesses, participants and organizations was positive and the events were deemed a great success with an estimated 20,000 visitors participating throughout each one-day event.

For 2023, route additions include University Avenue from Caron Avenue through Charles Clark Square to Riverside Drive, Riverside Drive to Aylmer Avenue, and Aylmer Avenue from Riverside to Wyandotte Avenue before continuing as in previous years.

Discussion:

Since the pilot in 2016, Open Streets have been hosted along an eight-kilometer (8 km) route in partnership with the Sandwich, Downtown Windsor, Wyandotte Town Centre, Walkerville and Ford City BIAs along with hundreds of businesses, organizations, performers and volunteers.

On February 13, 2023, Councillor Gignac presented a petition on behalf of the Olde Riverside Town Centre BIA and Pillette Village BIA regarding Open Streets with a request that the event either commence on the East side of Windsor or be extended to include the areas listed.

In March 2023, the City's Special Events staff and members of SERT met to discuss the options for an eastern-focused route in 2024 of similar size, goals and scope as the downtown/west-side route. City Administration will work with SERT and the east-side BIAs (Olde Riverside, Pillette Village, Ottawa, Ford City and Erie Street) to establish a route that ensures participant enjoyment and safety. By offering a new route, it will allow Administration to engage BIAs in the eastern area of the City, gather feedback post event and make any required adjustments following the first year of the new east-side route. Feedback gathering and analysis would include interviewing and surveying participants, residents, business owners, volunteers, partners and members of SERT to confirm and mitigate the success and areas for improvement of the new route.

To ensure the event falls in line with approved budgets and available resources, Administration is recommending that an approximate 8-kilometre route continue to be offered, alternating between an east end route (even years) and a west end route (odd years) on an annual basis.

The routes are noted below and would be contingent on construction work, if applicable.

East end route: Wyandotte Street East between Jefferson Boulevard & Drouillard Road, Drouillard Road between Wyandotte Street East & Seminole Street, Seminole Street between Drouillard Road & Walker Road, Walker Road between Seminole Street & Ottawa Street, Ottawa Street between Walker Road & Parent Avenue, Parent Avenue between Ottawa Street & Erie Street, and Erie Street between Parent Avenue & Howard Avenue.

West end route: Sandwich Street between Brock Street & Rosedale Avenue (roundabout), University Avenue West between Rosedale Avenue & Huron Church Road, Huron Church Road between University Avenue West & Riverside Drive West, Riverside Drive West between Huron Church Road & Caron Avenue, Caron Avenue between Riverside Drive West & University Avenue, University Avenue West between Caron Avenue & Aylmer Avenue, Aylmer Avenue between University Avenue East & Wyandotte Street East, Wyandotte Street East between Aylmer Avenue & Albert Road, Albert Road between Wyandotte Street East & Edna Street, Edna Street between Albert Road & Drouillard Road and Drouillard Road between Wyandotte Street East & Richmond Street.

Risk Analysis:

There are a number of safety risks identified with Open Streets that were identified in earlier reports to Council. These risks are vetted by SERT on an annual basis and mitigated accordingly to allow the event to operate successfully and safely.

The request to move the route would contain no new safety risks. As mentioned in the discussion section of this report, the members of SERT have been consulted and would need to approve any route for safety and security.

There is a significant risk that expanding the footprint would require additional resources in terms of staff and budget, which are not available. By simply moving the route while keeping the overall distance the same, there should be no additional financial or human resource requirements.

Climate Change Risks

Climate Change Mitigation:

N/A

Climate Change Adaptation:

N/A

Financial Matters:

In 2019, Council approved an annual operating budget of \$61,500 for the Recreation & Culture Department to host Open Streets Windsor (B7/2019.) The budget is to fund expenditures such as marketing/advertising, police, crossing guards, EMS, barricades, fencing, signage, volunteers, sanitation and refuse.

From 2019 – 2022, with the exception of 2020 when Open Streets was cancelled due to COVID, total event costs have been kept under the approved budget. The proposed new route for the pilot project has the same overall distance as the old route. No additional operating budget is required.

Consultations:

SERT (Special Events Resource Team)

Conclusion:

The City of Windsor recognizes the importance of special events and festivals in enhancing the quality of life, tourism, culture, recreation, and education, all of which would not be possible without the invaluable services of volunteers, community groups, and sponsors that add their support and skills to enhancing the community events.

If approved by City Council, Administration will look to move the existing Open Street route opportunities as requested by the petition, through the creation of a new East end route for 2024, as outlined in this report.

Planning Act Matters:

N/A

Approvals:

Name	Title
Michelle Staadegaard	Manager, Culture & Events
Tracy Ou	FPA, Recreation & Culture
Jen Knights	Executive Director Recreation and Culture
Ray Mensour	Commissioner, Community Services
Tony Ardovini	On behalf of Commissioner, Corporate Services/Chief Financial Officer & City Treasurer
Joe Mancina	Chief Administrative Officer

Notifications:

Name	Address	Email
SERT		

Appendices:



Subject: Response to CQ 20-2023 – Feasibility of Expanding Outreach Services and 311 Operating Hours– City Wide

Moved by: Councillor Mark McKenzie
Seconded by: Councillor Jo-Anne Gignac

Decision Number: **CSSC 208**

THAT the report of the Manager, Homelessness and Housing Support in response to CQ 20-2023 Feasibility of Expanding Outreach Services and 311 Operating Hours **BE RECEIVED** for information; and,

THAT City Council **ENDORSE** recent improvements made to the Homelessness Street Outreach team's schedule and improved access to such services through the 311 Contact Centre as outlined in this report; and,

THAT City Council **DIRECT** Administration to report back to Council with more data and information about the impact of recent improvements to the Homelessness Street Outreach team's schedule and after-hours homelessness response initiated through 311, before deciding if further expansion is needed; and,

THAT City Council continue to **SUPPORT** programs and services that advance the goals of the 10-year Housing and Homelessness Master Plan, and the creation and expansion of affordable and supportive housing.

That administration **BE REQUESTED** to report back to the Community Services Standing Committee, as to how the City will attempt to draw the various organizations together in an effort to collaborate and capitalize on the programs that they are prepared to offer.

Carried.

Report Number: S 118/2023
Clerk's File: GM/7755

Clerk's Note:

1. The recommendation of the Community Services Standing Committee and Administration are **NOT** the same.
2. Please refer to Item 8.1 from the Community Services Standing Committee held on October 4, 2023.
3. To view the stream of this Standing Committee meeting, please refer to:
<http://csg001-harmony.sliq.net/00310/Harmony/en/PowerBrowser/PowerBrowserV2/20231004/-1/9436>

Subject: Response to CQ 20-2023 – Feasibility of Expanding Outreach Services and 311 Operating Hours– City Wide

Reference:

Date to Council: October 4, 2023
Author: Jennifer Tanner
Manager, Homelessness and Housing Support
519-255-5200 x 5250
jtanner@citywindsor.ca
Housing and Children's Services
Report Date: 9/12/2023
Clerk's File #: GM/7755

To: Mayor and Members of City Council

Recommendation:

THAT the report of the Manager, Homelessness and Housing Support in response to CQ 20-2023 Feasibility of Expanding Outreach Services and 311 Operating Hours **BE RECEIVED** for information; and,

THAT City Council **ENDORSE** recent improvements made to the Homelessness Street Outreach team's schedule and improved access to such services through the 311 Contact Centre as outlined in this report; and,

THAT City Council **DIRECT** Administration to report back to Council with more data and information about the impact of recent improvements to the Homelessness Street Outreach team's schedule and after-hours homelessness response initiated through 311, before deciding if further expansion is needed; and,

THAT City Council continue to **SUPPORT** programs and services that advance the goals of the 10-year Housing and Homelessness Master Plan, and the creation and expansion of affordable and supportive housing.

Executive Summary:

N/A

Background:

At the July 10, 2023 meeting of City Council, Councillor Agostino asked the following question:

CQ 20-2023 “That administration report back to city council regarding all municipal and community outreach programs currently available and research and analyze the feasibility of expanding operating hours and services including having one dedicated number (i.e., 311/211) related to outreach support in our community.”

Discussion:

This report provides an overview of key outreach services funded through the City of Windsor as well as those led by other organizations. Such outreach services are intended to support Windsor-Essex’s most vulnerable residents, encompassing those grappling with homelessness, mental health challenges, addiction issues, trauma, untreated or neglected health problems, or a combination thereof.

Access to different types of outreach services varies depending on the organization that is delivering the service and the time of day a service is available. For example, support from a Family Services Windsor-Essex (FSWE) Homelessness Street Outreach Worker (herein referred to as “Homelessness Outreach”) can currently be requested by calling 311 during their regular business hours which are Monday to Friday from 8:00 am to 4:00 pm or by contacting Family Services Windsor Essex after-hours until midnight each day. Another key outreach service, the Nurse Police Team (NPT) can be requested through a call to Windsor Police Services non-emergency phone line, during the NPT hours of operation. A high-level summary of the various outreach teams and their hours of operation are provided in Appendix A: Key Outreach Services in Windsor and Hours of Operation Table. More specific details about the outreach services along with some exciting changes recently implemented are included in the following two sections.

Outreach Services – City Funded

To support those experiencing homelessness and those who sleep outdoors, the City of Windsor has allocated municipal and provincial funding to Family Services Windsor-Essex (FSWE) to deliver outreach services. Homelessness Outreach Workers interact with people experiencing homelessness, through a variety of progressive engagement techniques, make referrals to emergency shelters, and connect people with appropriate support services that will help them meet their housing, health, financial and social needs. People living outdoors may be disconnected from community services and it often takes multiple interactions to build trust before a person is willing to accept help.

There is a team of five (5) Homelessness Outreach Workers, with four (4) dedicated to the City of Windsor and one (1) dedicated to the County of Essex. In addition, the City also allocates federal funding to the Can-Am Indian Friendship Centre for an Indigenous Outreach Worker which offers specific culturally appropriate supports for Indigenous individuals experiencing homelessness. Indigenous Outreach supports are accessed by contacting Can-Am directly, not through the 311 process.

Current Process to Report Homelessness Concerns

If any member of the public notices someone who appears to be experiencing homelessness or who has set up a homeless encampment with structures or tents, they can request a Homelessness Response through 311.

When the Homelessness Outreach team receives a request for service through 311, they will triage and action the request within 24 hours, where a previous interaction had not already been completed recently. Given that the team consists of only four staff assigned in Windsor, it is not possible to guarantee a quicker response at this time. Additional funding would be required to increase the number of Homelessness Outreach Workers in order to improve response times.

As mentioned above the role of a Homelessness Outreach Worker is to engage with people experiencing homelessness. The workers can encourage people to access supports such as an emergency shelter, but they do not have the authority to “move people along” or compel them to leave a public space.

There are other homelessness services funded through the City of Windsor that complement the work of Homelessness Outreach Workers, including emergency shelters and the Homelessness and Housing Help Hub (H4).

Recent Improvements to Outreach Services

Based on feedback received by residents, businesses, and City Council, a new schedule for the Homelessness Outreach team was implemented during the summer of 2023 at no additional cost to the municipality. The changes were intended to better meet the needs of the community, and support those experiencing homelessness as well as those who are panhandling. This included expanding their hours of operation, extending service from 5 to 7 days a week and establishing regular service routes in six areas of the City (i.e., Downtown Windsor, Jackson Park, University Avenue West, Ford City, West Windsor and Walkerville).

The team has also begun offering on-call hours in the late evening, 7 days per week. When requests for services come in during the on-call hours, they are triaged by an FSWE Manager who determines if a Homelessness Outreach Worker should be dispatched to attend a location or if the request is better addressed during their regular operating hours or by another community service (e.g. NPT). During these on-call hours, the Homelessness Outreach Workers are not on duty, but rather on standby. If they are called in to respond to a request for service, they would have to travel to the designated location, which may take up to one hour. More details about the Homelessness Outreach team’s regular hours of operation and on-call hours are provided in Appendix A: Key Outreach Services in Windsor and Hours of Operation Table.

Additionally, since April 2023, a Homelessness Outreach Worker has also been riding along with Windsor Police approximately two days per week to respond to panhandling concerns in various locations, intersections and boulevards throughout Windsor. There have been positive results from this partnership, including helping people secure income support through Ontario Works and a better understanding of panhandling in the

city. These service improvements have been achieved within the existing municipal and provincial budgets and no additional funding was required.

Outreach Services in Windsor Led by Other Organizations

There are also outreach and mobile services in Windsor that are led and funded by other organizations and dovetail with the Homelessness Outreach service to provide additional supports for vulnerable people in our community.

Other outreach services are described in the paragraphs below and a high-level summary of the contact information and hours of operation can be found in Appendix A.

- Mobile Outreach and Support Team (MOST) Van - supports the needs of those who have mental health or addiction challenges, require housing, or are street-involved. MOST is made up of a driver trained to support those with physical disabilities, a Social Worker, and an Outreach Worker. MOST will travel via an accessible van to Downtown and West-End Windsor locations with supplies such as food and personal care items while offering services. MOST is a collaborative initiative between Hôtel-Dieu Grace Healthcare (HDGH), the Canadian Mental Health Association – Windsor/Essex County Branch (CMHA WECB), and FSWE.
- Nurse Police Team (NPT) which pairs Windsor Police Officers with nursing professionals from Windsor Regional Hospital (WRH) to offer proactive care for those struggling with mental health and substance use disorders. The NPT team tries to divert people from the hospital emergency department by providing wound care and preventative care to individuals on the street.
- Community Outreach & Support Team (COAST) is a partnership between Windsor Police Services (WPS) and Hotel Dieu Grace Healthcare (HDGH) to provide on-site crisis and mental health assessments in Windsor to individuals aged 16 and above who are facing complex mental health and psychosocial issues.
- Mobile Crisis Rapid Response Team (MCRRT) is another partnership between WPS and HDGH which responds to dispatched calls for individuals presenting with symptoms of mental illness, substance abuse, behavioural disorders, or people in acute crises within Windsor.
- Pozitive Pathway Needle Syringe Program provides harm reduction education and supplies, as well as education about wound care.
- Windsor Essex Community Health Centre's Hep C Treatment and Support Team works to ensure those diagnosed with or at risk of acquiring Hepatitis C have access to specialized care.
- Downtown Windsor Community Collaborative Street Lights is a partnership between DWCC, Parkwood Gospel Church and Exchange Church that connects people with no fixed address to break down barriers and build compassion and advocacy.
- Society of Saint Vincent de Paul Street Outreach finds those lost to the system and brings them in for required services and support.
- New Beginnings Youth Outreach Workers help young people with a variety of needs including job searches, help finding housing and meeting basic needs such as food, clothing and hygiene care.

- Windsor Residence for Young Men Aftercare and Outreach Program provides supports for former and potential clients with housing retention, crisis response, referrals and/or emotional support.

In addition to these outreach services in Windsor, there are several similar services including COAST in Amherstburg and LaSalle, MCRRT and Mental Health Response Unit (MHRU) in Essex County.

All Windsor and Essex County residents can access support for mental health emergencies by contacting the Community Crisis Centre:

- By phone at 519-973-4435, which is available 24/7
- Walk-in service within the Crisis and Mental Wellness Centre at 744 Ouellette
 - Monday to Friday from 8:00 am to 10:00 pm
 - Saturday and Sunday from 8:00 am to 8:00 pm
- Or by attending a local emergency department after these hours

Coordinating and Accessing Outreach Services

Many of the outreach services described above or listed in Appendix A, meet regularly at the Community Outreach table, chaired by Family Services Windsor Essex, to discuss and coordinate their respective services. However, arranging for one centralized phone number for all these services would be very complex.

Currently, there is no coordinated infrastructure in place to have one single dedicated phone number to reach all of these various outreach and mobile services. This type of service would require the integration of numerous systems and trauma-informed professionally trained staff to assess and triage each call-in order to appropriately direct to the correct service provider or determine if emergency services would be required. This would be a complex and long-term undertaking requiring systemic change, appropriate funding and resources to implement and operate.

Recognizing that there are City funded services that are within the control of the municipality (i.e., Homelessness Outreach, needle pick-ups, etc.) that could benefit from one dedicated line, Administration has already implemented changes to leverage the existing 311 system that will assist residents and businesses with homelessness response services.

311 System Enhancements to Access Outreach Services

When CQ 20-2023 was posed by Council, Administration began working to find potential enhancements to the Homelessness Outreach service that could be acted on quickly, using the 311 Contact Centre as the dedicated line. As a result, the following changes are being implemented:

1. The first improvement strategy is the creation of a new 311 online service request that will allow the public to request homelessness supports outside of the 311 Contact Centre's current hours of operation. The service request called

“Homelessness Report” will be available 24/7 utilizing the 311Windsor mobile app as well as 311online. Once a request is created, it is automatically forwarded directly to the Homelessness Outreach team for their review, triage and action during their hours of operation, as described further above. Leveraging the existing 311 reporting system not only achieves efficiencies but also allows for the collection of data for future analysis and decision-making. There is no cost to implement this solution.

2. The next immediate strategy that can be implemented to streamline access to Homelessness Outreach is by utilizing the existing 311 phone system and after-hours interactive voice response (IVR) messaging. The after-hours messaging will be adjusted to include a “press 1” option whereby homelessness response calls would be automatically routed to the Homelessness Outreach team. This provides not only an easy number for the public to remember but also an effective and simple solution for callers to connect directly with a service provider. Data can be collected on how many calls come in after hours seeking homelessness support. The cost to make changes to the 311-telephony infrastructure and implement this solution is less than \$2000 and can be absorbed within the existing operating budget.

By utilizing 311 services, and implementing the two solutions outlined above as a first step, Administration can fulfill the “one number to remember” request and also collect more reliable data about the number of requests for homelessness supports and the location of these concerns, as well as utilize standardized workflow processes and improve coordination between the Homelessness Outreach team and other City departments (e.g. By-Law, Parks).

It is important to note that while these changes do simplify and expand access to Homelessness Outreach supports, they do not provide an immediate or 24/7 response from the Homelessness Outreach team due to the limitations of their hours of operation, the limited number of Homelessness Outreach Workers, and the need to prioritize requests from all areas of the city and varying degrees of urgency. Each request for a Homelessness Response needs to be triaged to determine if Homelessness Outreach is the appropriate support service when it can be actioned by the Homelessness Outreach team, or if it would be more appropriate to request a response from another outreach service or Windsor Police Services.

Expanding 311 Hours of Operation

Currently, the 311 Contact Centre operates Monday to Friday from 8:00 am to 4:00 pm. These hours align with the peak call volume times. When first established in 2005, the 311 Call Centre hours were Monday to Friday from 7:00 am to 10:00 pm and Saturday from 8:00 am to 4:00 pm. As data on call volumes and service levels became available for analysis, these initial hours of operation were scaled back over the years to minimize costs and maximize efficiencies using the call volume data. In addition, 311 rolled out and promoted alternate self-serve channels such as 311online and the 311Windsor mobile app which offered 24/7 options for residents to report issues. Despite this reduction of operating hours to achieve efficiencies, during the last 10 years, the Contact Centre continues to respond to an average of over 140,000 311 inquiries annually.

To provide further information for Council's consideration, a high-level analysis was undertaken to provide estimated base budget costs to expand the 311 hours of operation to align with the Homelessness Outreach regular operating hours and on-call hours of service (16 hours/day/7 days per week) as well as potential provision of call answering to include after-hours (24/7). For the purpose of this high-level analysis, the existing positions and scheduling model (based on 8-hour shifts) was used. Projections are based on the minimum scheduling coverage required to operate per each option and in accordance with the Collective Agreements, Corporate Human Resource Policies and labour laws. The estimated staffing level coverage is in line with other 24/7 operations such as 211 and Windsor Police Services. Variables in call volume would have the potential to increase minimum staffing requirements and thereby further impact the resources required.

Table 1

	Projections for 311 Increased Service Hours				
	Hours of Service	Staffing Levels	Estimated base budget	Increase	% increase
Current Operating Hours	8 hours day/M-F/5 days week	21	1,962,658		
Option #1 Align with Homelessness Outreach hours	16 hours day/7 days week	33	3,232,400	1,269,742	65%
Option #2 Include after-hours	24/7	40	4,032,271	2,069,613	105%

To expand the existing Contact Centre hours of operation from the current Monday to Friday, 8:00 am to 4:00 pm, to 16 hours/7 days a week or 24/hour/7 days a week would require the commitment of significant resources. Should Council wish to pursue the expansion of Contact Centre hours as outlined above, a comprehensive business case including further research and fulsome analysis would be required to assess, develop and recommend a preferred operational model and detailed budget.

In addition to this, if the 311 Contact Centre hours were to be expanded, more funding would be needed to increase the number of Homelessness Outreach Workers to be able to respond to requests for services up to midnight or 24/7, rather than just offering on-call supports during the later hours of the evening. Also, when responding to requests for a Homelessness Response, the Outreach Workers' safety must be prioritized. Outreach Workers often operate in teams of two or where feasible, have a Windsor Police Officer present during the response in order to mitigate safety risks which has an impact on the response time and the cost of delivering the service. Determining the exact cost increase would require coordination with FSWE to determine the optimum staffing levels required to meet the service needs as well as their own health and safety and other human resources policies.

Other Options Considered

Other options for a dedicated line were explored and the findings are detailed below:

- A private or external social service call center was initially explored, and it was determined that it would not be a viable option when compared to the City's in-house 311 system for several reasons. The 311 system offers the advantage of consolidated data tracking in one unified system. All inquiries, complaints, and requests are recorded and managed within a single, integrated platform with 311. In contrast, a private call center will not have the same integrated and data tracking that 311 utilizes, which increases inefficiencies and complexity in managing and analyzing information. Additionally, the City's 311 staff and system have a better understanding of local issues and needs, resulting in more efficient and tailored responses to citizen inquiries. Finally, there's a cost factor to consider. Using a private call center can come with significant expenses, including one-time capital set-up and contract fees and additional operational costs.
- 211 is a free and confidential helpline that easily connects people to community, health, social and government services. Trained and accredited community navigators ask the right questions to get a picture of a person's needs. Navigators then provide referrals to services and programs connecting callers to the right supports. When people need additional help, 211 navigators will advocate on their behalf or warm transfer callers in crisis to crisis support lines or emergency services. The backbone of the 211 service is a comprehensive and integrated provincial resource database which contains information about programs and the agencies delivering them. The 211 service is delivered by a network of Regional Service Partners. The City of Windsor is the Regional Service Partner for the southwest region of the province encompassing Essex, Lambton, Chatham-Kent, Elgin, Oxford and Middlesex counties. Our trained Contact Centre staff respond to 211 inquiries during our regular operating hours Monday to Friday 8:00 am to 4:00 pm and handled over 23,000 inquiries in 2022. After-hours 211 service is provided by other partners in the integrated network. 211 is available to all people in the Province of Ontario 24/7 by phone, text, chat or email. 911 to report situations that require an immediate emergency response from police, fire or ambulance.

While 211 serves as a valuable resource for connecting callers to various programs, it's important to note that it operates independently from municipal processes related to homelessness and outreach initiatives. Additionally, it lacks the capacity to gather the municipal specific and detailed homelessness data that is necessary for these initiatives.

Risk Analysis:

The risk of expanding 311 hours of operation to access Homeless Outreach services is that it will cost a significant amount of money and may create an expectation from the public that response times will be immediate, like a call to 911. And without increasing the number of Homelessness Outreach Workers, their response time to requests for service will continue to be up to 24 hours. If a person experiencing homelessness is

facing an urgent situation, such as the risk of hypothermia during extreme cold events, the public should continue to contact 911 for an immediate response.

The public may also have an expectation that if they request a homelessness response that will result in the person experiencing homelessness being “moved along” or forced to leave a public space, which is not the role of Homelessness Outreach Workers and could infringe on the person’s rights to occupy a public space. Expansion of 311 and/or Outreach will not necessarily reduce visible homelessness in our community. Real reductions in homelessness require systemic changes to decrease poverty and increase the supply of affordable and supportive housing.

Climate Change Risks

Climate Change Mitigation:

N/A

Climate Change Adaptation:

N/A

Financial Matters:

311 Budget

Currently the residents can access one of the trained Customer Contact Representatives Monday to Friday 8:00 am to 4:00 pm. With the current operating model, the Customer Contact Centre 21 positions currently support this level of service. Salary and fringe expenses amount to \$1,905,558. Other costs such as training, expected overtime coverage and Cxone phone platform bring the total to \$1,962,658.

Option #1 to expand the hours of operation to 16 hours/7days a week will require an increase in the staffing complement by 12 positions to provide adequate coverage for the expanded hours. These added positions will result in an increase of \$1,193,648 in salary and fringe costs and \$76,094 in other costs to total cost of \$3,232,400.

Option #2 offers 24 hours/7days a week service at the 311 Call Centre. This will further increase staffing by 19 positions to cover all shifts around the clock. Under Option 3, salary and fringe will increase by \$1,916,207 and other costs by \$153,406 from the current model to the total cost of \$4,032,271.

The Customer Contact Centre does not have the resources to offer any service expansion without requesting additional operating budget. Extending service to 16 hours /7 days a week will require an increase of \$1,269,742 or 65% from the current budget. The required increase escalates to \$2,069,613 or 105% with the 24/7 operating model.

Homelessness Budget

For the 2023 – 2024 fiscal year, the City of Windsor has been allocated \$20.2 million in provincial and federal funding to provide a variety of homelessness programs including, but not limited to, the H4, emergency shelters, and outreach services. The City of Windsor also contributes \$2.45 million towards these homelessness programs. The Homelessness Outreach program is funded a total of \$366,184 through both the Province of Ontario’s Ministry of Municipal Affairs and Housing (MMAH) Homelessness Prevention Program (HPP) and the City of Windsor (\$277,703 HPP, \$138,481 City). Administration will continue to advocate for increased provincial and federal funding to support these programs. Requests for municipal funding to support and expand programs and services will be brought forward, as needed, through the budget development process.

Consultations:

Alena Sleziak – Manager, Customer Contact Centre

Doran Anzolin – Executive Initiatives Coordinator, Human and Health Services

Jennifer Tanner – Manager, Homelessness and Housing Support

Kelly Goz – Manager, Homelessness and Housing Support (A)

Linda Higgins – Manager, Intergovernmental Funding and Human Services Integration

Emilie Dunnigan – Manager, Development Revenue & Financial Administration

Tracy Ou – Financial Planning Administrator

Conclusion:

Immediate enhancements to access services through 311 during the Contact Centre’s regular hours of operation and after hours through the 311Windsor mobile app and 311 online are available should council approve. It is recommended that Administration be approved to implement the immediate enhancements through 311 and spend at least six months reviewing data collected about requests for services and study the impact on homelessness in our community and report back to Council.

Planning Act Matters:

N/A

Approvals:

Name	Title
Jennifer Tanner	Manager, Homelessness and Housing Support
Kirk Whittal	Executive Director, Housing and Children’s Services
Andrew Daher	Commissioner, Human and Health Services
Janice Guthrie	Commissioner, Corporate Services/CFO

Name	Title
Ray Mensour	Commissioner, Community Services
Joe Mancina	Chief Administrative Officer

Notifications:

Name	Address	Email
Joyce Zuk, Family Services Windsor- Essex		jzuk@fswe.ca
Chief Jason Bellaire, Windsor Police Services		jbellaire@windsorpolice.ca
David Musyj, Windsor Regional Hospital		David.Musyj@wrh.on.ca
Bill Marra, Hôtel-Dieu Grace Healthcare		Bill.Marra@hdgh.org
Sonja Grbevski, Canadian Mental Health Association Windsor-Essex County		sgrbevski@cmha-wecb.on.ca
Bruce Krauter, Essex-Windsor Emergency Medical Services		bkrauter@countyofessex.on.ca
Insp. Angela Ferguson, Ontario Provincial Police (OPP)		angela.ferguson@opp.ca
Sgt. Mike Agostinis, Lasalle Police		magostinis@lasallepolice.ca
Michael Brennan, Pozitive Pathways		executivedirector@pozitivepathways.com
Jennifer Wilson, Saint Vincet de Paul		wilson.jennifer33@gmail.com

Bob Cameron, Downtown Windsor Community Collaborative – Street Lights		bob@dwcc.ca
Rita Taillefer, Windsor Essex Community Health Centre		rtaillefer@wehc.org
Brian Harrison, Parkwood Outreach Centre		bharrison@parkwoodwindsor.com

Appendices:

- 1 Appendix A - Key Outreach Services in Windsor and Hours of Operation Table

Appendix A: Key Outreach Services in Windsor and Hours of Operation Table

Service	How to Access Service	Mon	Tues	Wed	Thurs	Fri	Sat	Sun
<i>Funded by The City of Windsor</i>								
Homelessness Outreach (Regular operating hours)	Call 311 (live answer M – F, 8am to 4pm) or 311Windsor Mobile App or 311 online	7:30am - 3:30pm	7:30am - 3:30pm	7:30am - 6:00pm	7:30am - 6:00pm	7:30am - 6:00pm	10:00am-6:00pm	10:00am-6:00pm
Homelessness Outreach (On call hours)	Call 311 (to be routed to FSWE) or 311Windsor mobile App or 311online	9:00pm - 12:00am	9:00pm - 12:00am	9:00pm - 12:00am	9:00pm - 12:00am	9:00pm - 12:00am	6:00pm-12:00am	6:00pm - 12:00am
CAN AM Indian Friendship Centre - Indigenous Outreach Worker	519-253-3243	-	1:00pm – 5:00pm	-	1:00pm – 5:00pm	-	-	-
<i>Led & Funded by Other Organizations</i>								
MOST Van	(226) 787-5724	5:00pm - 9:00pm	5:00pm - 9:00pm	5:00pm - 9:00pm	5:00pm - 9:00pm	5:00pm - 9:00pm	-	-
Nurse Police Team	519-258-6111 (Police Non-Emergency)	-	-	-	-	1:00pm-1:00am	1:00pm-1:00am	1:00pm-1:00am
MCRRT	519-258-6111	9:00am – 10:00pm	9:00am –	9:00am –	9:00am – 10:00pm	9:00am –	-	-

	(Police Non-Emergency)		10:00pm	10:00pm		10:00pm		
Windsor COAST	519-258-6111 (Police Non-Emergency)	8:00am – 4:00pm	8:00am – 4:00pm	8:00am – 4:00pm	8:00am – 4:00pm	8:00am – 4:00pm	-	-
Essex MHRU	519-723-4600	8:00am – 6:00pm	8:00am – 6:00pm	8:00am – 6:00pm	8:00am – 6:00pm	-	-	-
Leamington MHRU	519-324-0123	-	8:00am – 6:00pm	8:00am – 6:00pm	8:00am – 6:00pm	8:00am – 6:00pm	-	-
LaSalle MHST	519-250-5048	* Hours Varied						
Pozitive Pathways	519-973-0222 x 109	1:30pm-3:30pm (Glengarry Area)	-	1:00pm-4:00pm	*Bi-Weekly Leamington 1:00pm-4:00pm	-	-	-
Saint Vincent de Paul	519-890 - 7215	-	-	5:00pm-8:00pm (depend s on daylight)	-	-	-	7:00am-10:00am
Street Lights	519-903-7629	9am-11am	9am-11am	5:00pm-7:00pm	-	-	9am-11am	-
Windsor Essex Community Health Centre	519-890-6392	-	8:30am-4:30pm	8:30am-4:30pm (County)	-	8:30am-4:30am DT core	-	-
Red Frogs	519-948-7055	-	-	-	-	-	10:00pm-3:00am *every other Saturday	-



Outreach Services

**Presentation from Family Services
Windsor Essex to the Community
Services Standing Committee at their
October 4, 2023 meeting.**

Outreach Role

“Outreach is primarily directed toward finding homeless people who might not use services due to lack of awareness or active avoidance...and who would otherwise be ignored or underserved”



– *Dr. Gary Morse, Psychologist and Mental Health Advocate*

Outreach Role



- Find individuals experiencing homelessness
- Assess, add to BNPL or update BNPL
- Support accessing services in the community

Why Outreach is Different from Other Services



- Know people by name
- Know their needs and who they are connected to
- Re-find or reconnect
- Establish long-term relationships with people

Outreach Team



5 Staff



City

2 teams of 2 workers

County

1 worker

New Outreach Schedule

	<u>Day Shift - City routes</u>		<u>Day Shift – County</u>	<u>On Call</u>
	Team A (2 FTE)	Team B (2 FTE)	1 FTE	
Mon	7:30am-3:30pm		9:00am-5:00pm	9:00pm-12:00am
Tues	7:30am-3:30pm		9:00am-5:00pm	9:00pm-12:00am
Wed	7:30am-3:30pm	10:00am-6:00pm	9:00am-5:00pm	9:00pm-12:00am
Thurs	7:30am-3:30pm	10:00am-6:00pm	9:00am-5:00pm	9:00pm-12:00am
Fri	7:30am-3:30pm	10:00am-6:00pm	9:00am-5:00pm	9:00pm-12:00am
Sat		10:00am-6:00pm		6:00pm-12:00am
Sun		10:00am-6:00pm		6:00pm-12:00am

Outreach Response



- Referrals will come through 311 system
- All other referrals will be logged into 311 system by FSWE staff
- All calls are triaged, and the best/most appropriate response is determined

Questions?



Subject: Official Plan & Zoning Bylaw Amendments Z 024-23 [ZNG-7069] & OPA 175 [OPA-7072] 1027458 Ontario Inc. Multiple Dwelling Development Banwell & McHugh (South) - Ward 7

Moved by: Councillor Angelo Marignani
Seconded by: Councillor Fred Francis

Decision Number: **DHSC 559**

THAT the City of Windsor Official Plan Volume II – East Riverside Secondary Plan Schedule ER-2 **BE AMENDED** by changing the land use designation of Block 8, Plan 12M-425, City of Windsor from Business Park to “Banwell Road Mixed Use Corridor”; and,

THAT the City of Windsor Official Plan, Volume II, Part 1 – Special Policy Areas, **BE AMENDED** by adding site specific policies as follows:

1.# SOUTH WEST CORNER OF BANWELL ROAD AND MCHUGH STREET

1.#.1 The property described as Block 8, Plan 12M-425, in the City of Windsor, known municipally as 0 McHugh Street, is designated a special policy area on Schedule A: Planning Districts and Policy Areas in Volume I – The Primary Plan.

1.#.2 Notwithstanding Section 2.7.5.5 of the Official Plan, Volume II:

- a) Medium Profile Residential Buildings shall be permitted on the subject property; and
- b) High Profile Residential Buildings shall be permitted within 30 metres of Banwell Road on the subject property; and,

THAT the City of Windsor Zoning By-law 8600 **BE AMENDED** for the lands described as Block 8, Plan 12M-425 from MD1.4 to HCD2.7; and,

THAT the hold prefix **BE REMOVED** when the applicant/owner submits an application to remove the holding prefix and the following condition is satisfied:

- a) an updated Transportation Impact Study is prepared and submitted to the satisfaction of the City Engineer; and,

THAT subsection 1 of Section 20 of the City of Windsor Zoning By-law 8600 **BE AMENDED** for the lands described as Block 8, Plan 12M-425 by adding site specific regulations as follow:

4xx. **SOUTH WEST CORNER OF BANWELL ROAD AND MCHUGH STREET**

For the lands described as Block 8, Plan 12M-425, the following shall be additional permitted uses:

i. residential care facility;

ii. multiple dwelling;

subject to the regulations in Section 12.2.5, and,

Building height – Maximum – within 30m of Banwell Road - 30m maximum

Building height – Maximum – remainder of site - 26 m

(ZDM 15; ZNG/7069); and,

THAT the site plan control officer **BE REQUESTED** to explore the possibility of a vehicle access area to Banwell Road.

Report Number: S 121/2023
Clerk's File: Z/14648 & Z/14647

Clerk's Note:

1. The recommendation of the Development & Heritage Standing Committee and Administration are **NOT** the same.
2. Please refer to Item 7.2 from the Development & Heritage Standing Committee held on October 3, 2023.
3. To view the stream of this Standing Committee meeting, please refer to:
<http://csg001-harmony.sliq.net/00310/Harmony/en/PowerBrowser/PowerBrowserV2/202320231003/-1/9435>

Subject: Official Plan & Zoning Bylaw Amendments Z 024-23 [ZNG-7069] & OPA 175 [OPA-7072] 1027458 Ontario Inc. Multiple Dwelling Development Banwell & McHugh (South) - Ward 7

Reference:

Date to Council: October 3, 2023

Author: Jim Abbs

Senior Planner

519-255-6543 x6317

jabbs@citywindsor.ca

Report Date: September 13, 2023

Clerk's File #: Z/14648 & Z/14647

To: Mayor and Members of City Council

Recommendation:

THAT the City of Windsor Official Plan Volume II – East Riverside Secondary Plan Schedule ER-2 **BE AMENDED** by changing the land use designation of Block 8, Plan 12M-425, City of Windsor from Business Park to “Banwell Road Mixed Use Corridor”; and,

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1.#.2 Notwithstanding Section 2.7.5.5 of the Official Plan, Volume II:

- a) Medium Profile Residential Buildings shall be permitted on the subject property; and
- b) High Profile Residential Buildings shall be permitted within 30 metres of Banwell Road on the subject property; and,

THAT the City of Windsor Zoning By-law 8600 **BE AMENDED** for the lands described as Block 8, Plan 12M-425 from MD1.2 to HCD2.7; and,

THAT the hold prefix **BE REMOVED** when the applicant/owner submits an application to remove the holding prefix and the following condition is satisfied:

- a) an updated Transportation Impact Study is prepared and submitted to the satisfaction of the City Engineer; and,

THAT subsection 1 of Section 20 of the City of Windsor Zoning By-law 8600 **BE AMENDED** for the lands described as Block 8, Plan 12M-425 by adding site specific regulations as follow:

4xx. SOUTH WEST CORNER OF BANWELL ROAD AND MCHUGH STREET

For the lands described as Block 8, Plan 12M-425, the following shall be additional permitted uses:

- i. residential care facility;
- ii. multiple dwelling;

subject to the regulations in Section 12.2.5, and,

Building height – Maximum – within 30m of Banwell Road - 35m

Building height – Maximum – remainder of site - 26 m

A vehicle access area to Banwell Road is prohibited.

(ZDM 15; ZNG/7069)

Executive Summary:

N/A

Background:

This site is part of the East Riverside Secondary Plan area. That plan designated a large area of agricultural land within the City of Windsor for various forms of urban development. Over time, the majority of the area has developed, seeing the creation of primarily low profile residential areas interspersed with institutional, recreation and multiple dwelling uses.

One of the uses contemplated in the East Riverside Secondary Plan included an area set aside for Business Park (light industrial, commercial) use. The site of this development is subject to that Business Park designation. This designation, along with the lands designated Banwell Road Mixed Use Corridor were intended to form a commercial center for the secondary plan area.

Since the approval of the East Riverside Secondary Plan, commercial uses locating at the intersection of Banwell Road and Tecumseh Road East has taken on much of the commercial role anticipated for the intersection of Banwell and Mchugh/McNorton.

Other notable developments within the immediate area include a Mixed Use development on the Northeast corner of Banwell and McNorton (*ELM Windsor Inc, Z-013/17 approx 100 units*) and a Multiple Dwelling development immediately south of the Banwell Community Church (*Maple Leaf Homes Z-010/22 approx 130 units*). Both have been approved by City Council through Site Specific Zoning By-law amendments, but have not yet commenced construction.

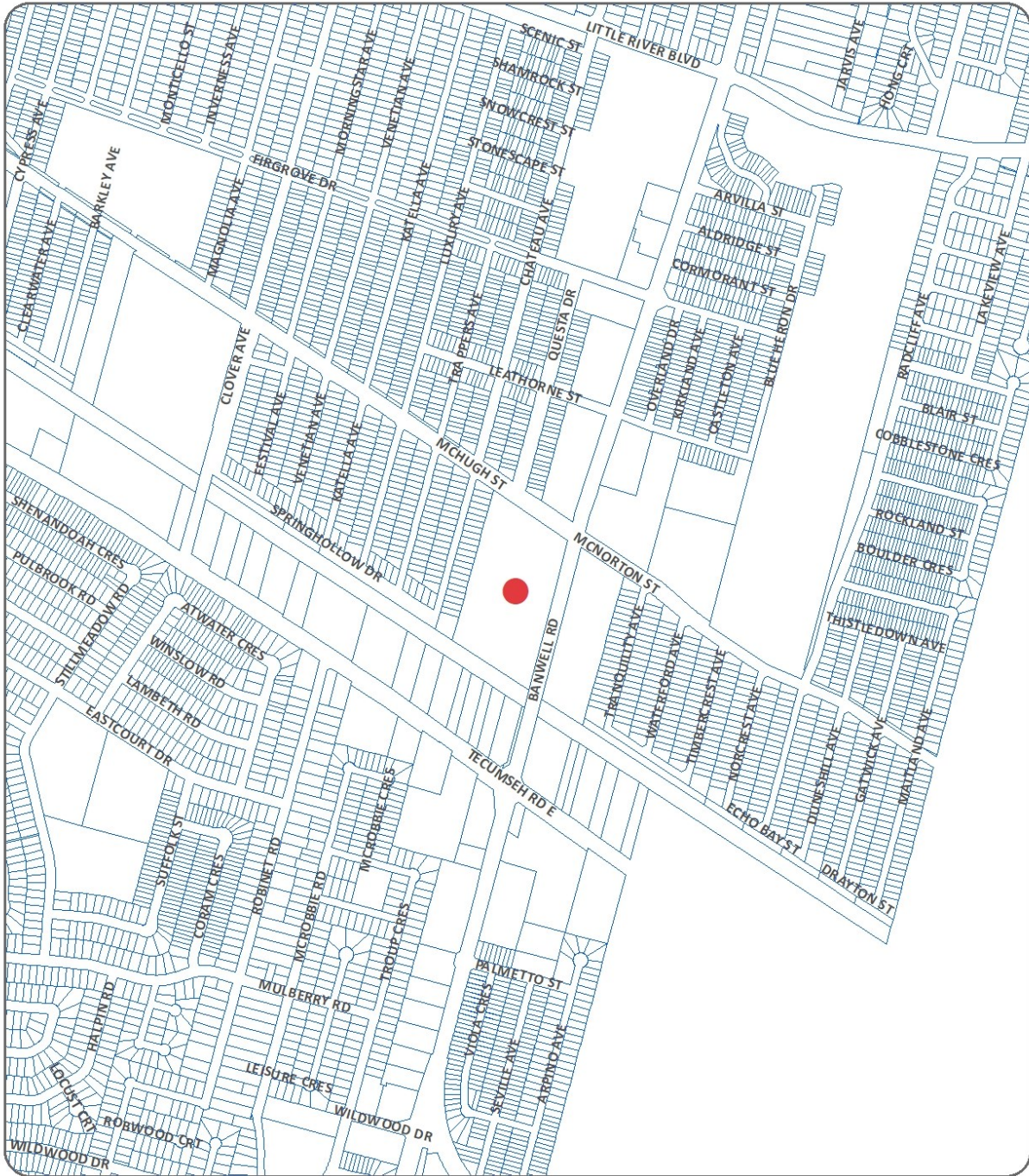
Application Information:

Location: 0 McHugh St – Part of Blocks 8 & 9, Plan 12M-425

Ward: 7 **Planning District:** East Riverside **ZDM:** 15

Registered Owner\Applicant: 1027458 Ontario Inc.

Agent: Dillon Consulting (Theresa O’Neil)



KEY MAP - Z-024/23, ZNG-7069, OPA 175, OPA-7072



● SUBJECT LANDS

Figure 1: Location Map



NEIGHBOURHOOD MAP - Z-024/23, ZNG-7069
& OPA 175, OPA-7072



SUBJECT LANDS

Proposal:

This application is one of three by the same developer along the West Side of Banwell Road between the Via Rail line and Firgrove Street. (see Appendix 1: Concept Plan) This phase of the development includes one (1) six (6)-storey multiple dwelling residential building with 72 units; one (1) eight (8)-storey multiple dwelling residential building with 96 units; two (2) 10-storey multiple dwelling residential buildings each with 120 units; For a total of 408 residential units.

A two (2)-storey business office; and a residential care facility is proposed for the McHugh Avenue frontage of the site and is intended to be used by a local charitable organization. This site of the residential care facility and business office was the subject of a recent successful consent to sever application to prepare the area for that development.

It is intended that the business office will have; 1,860m² of Gross Floor Area (GFA) and the residential care facility will be smaller at 499m² GFA.

The site is proposed to have driveway access from McHugh Street and will include a surface parking area with 531 parking spaces for the residential dwellings; 60 parking spaces for the business office; and 12 parking spaces for the residential care facility to accommodate on-site parking, for a total of 603 parking spaces. No reduction in parking is being requested for this development.

The multiple dwelling residential buildings on the South Site are intended to be developed in two phases. South Site - Phase 1 will be constructed first and includes 2 ten storey (35m) buildings fronting Banwell Road. Phase 2 will include the construction of the 8 storey building and the 6 storey building.

The applicant is requesting a site specific policy area to permit High profile development (greater than 4 storeys) on the site and to permit "Community Institutions" as a permitted use on the portion of the site fronting McHugh Street.

The applicant is requesting a Zoning By-law amendment that would add "Multiple Dwelling" subject to the regulations of the RD3.2 zone" and Residential Care Facility as a permitted use. As well the applicant is requesting an increase in the permitted height to 35m and a reduction in landscaped open space from 35% as required in the RD3.2 zone to 24.5%.

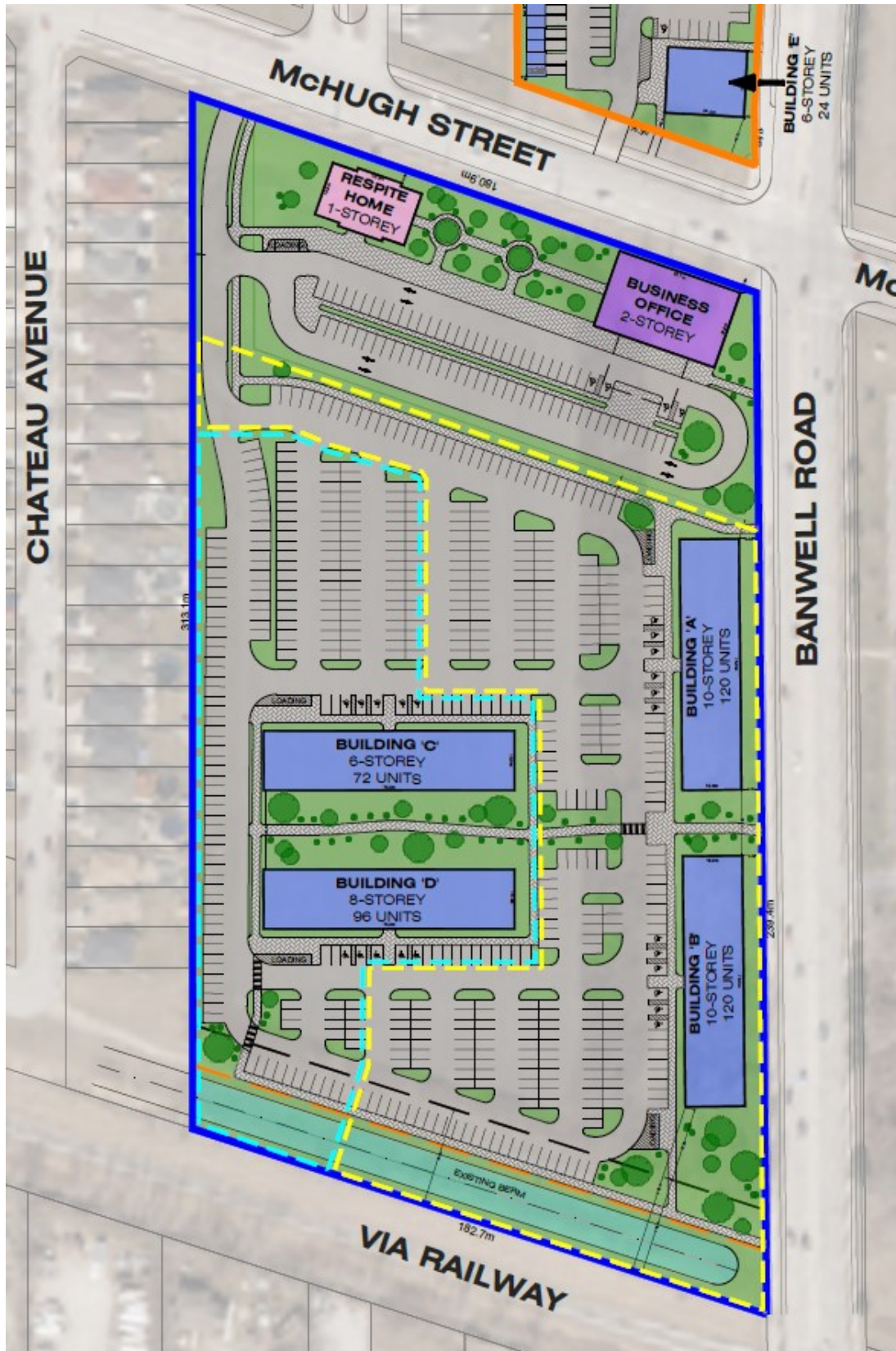


Figure 2: Concept Plan

Site Information:

Official Plan	Zoning	Current Use	Previous Use
Mixed Use Node Schedule D – Land Use, (OPA#159) Business Park (BP) Schedule ER-2: Land Use Plan, Secondary Plan East Riverside Planning Area Business Park (Schedule D – Land use	MD1.2 and S.20(1)112	Vacant	Vacant
Width	Depth	Area	Shape
+/-180 m	+/-313 m	5.35 ha	irregular
All measurements are approximate.			

Neighbourhood Characteristics:

This proposed development is located at 0 McHugh at the South west corner of Banwell Road and McHugh Street. The site is currently vacant and is bounded as follows:

North –townhouse dwellings and vacant land

East - Banwell Community Church, Huron Terrace Retirement Home, single detached dwellings, semi-detached dwellings, townhomes, East Riverside Park, and Tecumseh Arena;

South - CN Rail, Commercial, and Light Industrial;

West - Single detached dwellings and townhomes

Discussion:

Provincial Policy Statement 2020 (PPS):

The Multiple Dwelling development is an infill development (a development on underutilized or vacant land within the context of an existing urban or built up area) consistent with the Provincial Policy Statement (PPS) in that the development promotes the efficient use of existing land, promotes cost-effective development patterns and standards to minimize land consumption and servicing costs. Related to this direction, the PPS states:

“1.1.1 b) accommodating an appropriate range and mix of residential (including second units, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs”

e) promoting cost-effective development patterns and standards to minimize land consumption and servicing costs;”

The requested Official Plan and zoning bylaw amendments promotes cost-effective development by redeveloping an under-utilized vacant site. Allowing the proposed multiple dwelling development in this location contributes to minimizing land consumption and servicing costs by using a site that already has available trunk infrastructure in the immediate area.

The PPS also states:

“1.1.2 Sufficient land shall be made available to accommodate an appropriate range and mix of land uses to meet projected needs for a time horizon of up to 20 years.”

The PPS requires that land be available to diversify developments to meet the future needs of the community. Subsequent to the proposed Official Plan Amendment The multiple dwelling development will be consistent with that requirement by accommodating new residential construction on lands designated for that purpose.

The PPS also states:

“1.4.1 To provide for an appropriate range and mix of housing types and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall:

- a. maintain at all times the ability to accommodate residential growth for a minimum of 10 years through residential intensification and redevelopment and, if necessary, lands which are designated and available for residential development; and*
- b. maintain at all times where new development is to occur, land with servicing capacity*

sufficient to provide at least a three-year supply of residential units available through lands suitably zoned to facilitate residential intensification and redevelopment, and land in draft approved and registered plans.”

The proposed multiple dwelling development is consistent with the PPS by developing as part of a planned residential neighbourhood. This development will help to provide additional residential inventory within the City of Windsor.

The PPS also states:

“1.4.3 Planning authorities shall provide for an appropriate range and mix of housing types and densities to meet projected requirements of current and future residents of the regional market area by:

a. permitting and facilitating:

- 1. all forms of housing required to meet the social, health and well-being requirements of current and future residents, including special needs requirements; and*
- 2. all forms of residential intensification, including second units, and redevelopment in accordance with policy 1.1.3.3;*

b. directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;

c. promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed; and”

Approving the Official Plan Amendment and Zoning By-law Amendment would support residential development using the infrastructure that is already in place, instead of requiring more expenditure on new trunk infrastructure in an unplanned area. In terms of supporting active transportation and transit, the proposed development is in close proximity to Transit Windsor service, and in close proximity to an extensive trail system.

Official Plan:

Section 3.3.2.2 of the Urban Structure Plan contained in the City of Windsor Official Plan identifies Barwell Road as a City Corridor. Residential development on a City Corridor may include high profile (26 to 58 metres in height), medium profile (14 to 26 metres in height) and residential over retail at street, as well as row housing and lofts. The proposed development conforms with this policy.

The sites are designated “Mixed Use Node” and Business Park in the City of Windsor Official Plan, the Mixed Use Node designation indicates that Residential intensification shall be directed to the Mixed Use Nodes and areas in proximity to those Nodes. Within these areas Medium Profile buildings, up 4 storeys in height shall be permitted.

This site is identified as “Business Park” in the East Riverside Secondary Plan and on Schedule D –Land Use in Volume 1 of the City of Windsor Official Plan. The proposed Multiple Dwellings and Residential Care Facility would not be permitted in this designation, however the Business Office proposed would be permitted.

To help harmonize designations between Schedule D of Volume 1 of the Official Plan and Schedule ER-2 of the East Riverside Secondary Plan, the Planning Department recommends that Schedule ER-2 be modified to identify the subject site as Banwell Road Mixed Use Corridor. This would permit both the residential and business office uses on the site.

To prevent confusion between the conflicting sections of the Official Plan related to the height or profile of the development, the Planning Department recommends that a Site Specific policy area be applied to confirm that High Profile Buildings are permitted on this site within 30 metres of Banwell Road and Medium Profile buildings are permitted on the balance of the site. The site specific policy area would be implemented through a site specific zone (see Zoning Section).

Zoning:

The site is currently zoned MD1.4 with a site specific provision that permits an athletic and/or sports facility as a permitted use and prohibits building with 30m of the Railway Right of way. (S.20(1)112)

The applicant is requesting that the CD2.7 zone be applied to the site as it would be consistent with the requested Official Plan Amendment and the Mixed Use Node designation within the City’s Official Plan. The Planning Department concurs with the requested CD2.7 zone as it had been specifically designed for the mixed use area identified in the East Riverside Secondary Plan and contains uses and regulations appropriate for that area, and it would be consistent with the zone category on the Northeast corner of Banwell Road and McNorton Street.

The application of the regulations contained in the RD3.2 zone is appropriate because those regulations are designed for the Multiple Dwelling and Residential Care facility uses and the forms that those uses take. The regulations of the RD3.2 zone are generally designed to ensure that Multiple dwelling(s) are located on sites that are sized appropriately for the number of units proposed for the development.

Building Heights

The Planning Department is able to support the request for increased height on the site because the Urban Structure Plan designates Banwell Road as a City Corridor. Residential development on a City Corridor may include high profile (26 to 58 metres in height), medium profile (14 to 26 metres in height) on roads designated as City Corridors. More specifically, high profile residential buildings (up to a maximum height of 35 metres) are supported as part of Phase 1 of the development on the Banwell Road frontage. Medium profile residential buildings (up to a maximum height of 26 metres) are supported as part of Phase 2 of the development adjacent to the existing low profile residential dwellings on Chateau Avenue.

Request for reduction of landscaped open space

The Planning Department does not support the request for reduced landscaped open space from 35% to 24.0%. The site statistics provided for this phase of the development indicates that a landscaped open space of 38.2% is available on the site. As well the applicant is oversupplying (530 provided, 510 required) parking that could be used to enhance the Landscaped open space area if necessary.

Application of Hold Prefix

Transportation Planning indicates that the Transportation Impact Statement requires further refinement, as it is unclear whether the TIS fully accounted for the proposed residential units permitted within the ELM Windsor development on the northeast corner of the Banwell/McNorton intersection. While this update could be completed as part of the Site Plan Control process where mitigation of any transportation issues will be included as conditions within a site plan control agreement, it is important that a revised TIS be part of the zoning to ensure that the revised TIS is completed before the issuance of a building permit for the Multiple Dwelling portion of the site.

Climate Change Risks

Climate Change Mitigation:

The site will be subject to a site plan control agreement and will release storm water to the municipal system at a rate determined by a storm water management plan and development servicing plan that will be reviewed and approved by the City's Public Works department

Climate Change Adaptation:

The development on the site is close to existing bus routes and also community recreational opportunities. This will encourage the use of public transit and walking as modes of transportation, thereby helping to minimize the City's carbon footprint.

Financial Matters:

N/A

Consultations:

A Public Information Centre (PIC) was held by the developer's consultant on April 19, 2023. The approximately 150 residents attended. The results of the PIC are attached as Appendix 3.

As required in the Planning Act, notice was provided by advertisement in the Windsor Star. In addition, a courtesy notice to all property owners and tenants within 120 metres (400 feet) of the subject parcel were sent by mail.

The applications and relevant supporting studies were circulated to commenting agencies. Those responses are included as Appendix 2 – Agency Comments.

Conclusion:

The proposed use of this site as a development containing Multiple Dwelling structures containing 408 units represents an efficient development that will have no adverse impact on the well-being of the City of Windsor. The proposed development represents an appropriate residential use, adds to the range and mix of uses and will not cause any environmental or public health and safety concerns. This development is consistent with the Provincial Policy Statement.

The proposed Multiple Dwelling represents a housing type and density that meets the requirements of current and future residents, that meets the social, health and well-being of current and future residents, represents a form of residential intensification, is set in a location with access to infrastructure, public service facilities, and is close to commercial land uses.

The proposed Multiple Dwelling is located within a growing area that contains other Multiple Dwelling buildings in various stages of development of similar form and is compatible within its context.

The proposed Official Plan and zoning by-law amendments are consistent the PPS, conform with the policy direction of the City of Windsor Official Plan (as recommended for amendment), are compatible with existing and permitted uses in the surrounding neighbourhood, and constitute good planning.

Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Greg Atkinson Manager of Development/Deputy City Planner

Neil Robertson Acting City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP JM

Approvals:

Name	Title
Greg Atkinson	Manager of Development/Deputy City Planner
Neil Robertson	Acting City Planner
Wira Vendrasco	Deputy City Solicitor, Legal Services & Real Estate
Jelena Payne	Commissioner of Economic Development & Innovation
Joe Mancina	Chief Administrative Officer

Notifications:

Name	Address	Email
1027458 Ontario Inc.	949 Wilson Ave, Toronto ON M3K 1G2	jcoco@cocogroup.com
Dillon Consulting Limited (Theresa O’Niell)	3200 Dezeil Drive, Suite 608, Windsor ON N8W 5K8	toneill@dillon.ca
Councillor Marignani		

Appendices:

- 1 Combined Concept Plan
- 2 Liaison Comments
- 3 PIC Engagement Summary

APPENDIX D – CONSULTATION

ANNE-MARIE ALBIDONE – ENVIROMENTAL SERVICES

No concerns.

ENBRIDGE

After reviewing the provided drawing at Banwell Rd & McHugh St (North 'A' Site, North 'B' Site, South Site) and consulting our mapping system, please note that Enbridge Gas has active infrastructure around the proposed area. A PDF drawing has been attached for reference.

Please Note:

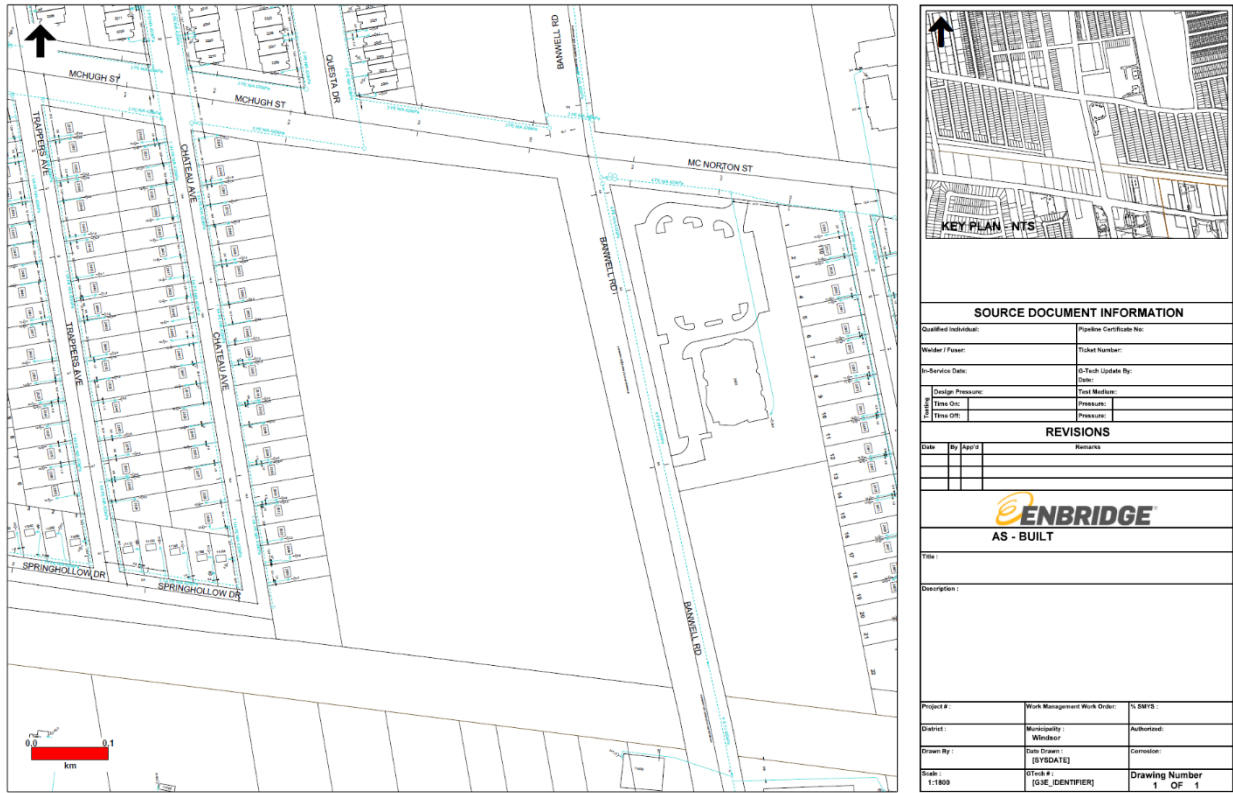
- 1. The shown piping locations are approximate and for information purposes only**
- 2. The drawings are not to scale**
- 3. This drawing does not replace field locates. Please contact Ontario One Call for onsite locates prior to excavating, digging, etc**

Enbridge Gas requires a minimum separation of 0.6m horizontal and 0.3m vertical from all of our plant less than NPS 16 and a minimum separation 1.0m horizontal and 0.6m vertical between any CER-regulated and vital pipelines. For all pipelines (including vital pipelines), when drilling parallel to the pipeline, a minimum horizontal clearance measured from the edge of the pipeline to the edge of the final bore hole of 1 m (3.3 ft) is required. Please ensure that this minimum separation requirement is maintained, and that the contractor obtains locates prior to performing any work and utilizes safe excavation practices while performing any work in the vicinity.

Also, please note the following should you find any abandoned infrastructure in the area:

- Any pipe that is excavated, please assume that it is live
- If during the course of any job, any pipe is found that is not on the locate sheet and is in conflict with your work, please call our emergency number (1-877-969-0999), and one of our Union Gas representatives will respond to determine if that plant is in fact live or dead
- Please note that our Enbridge Gas representative will respond to the live or dead call within 1-4 hours, so please plan your work accordingly

Please contact me if you have any further questions or concerns.



LANDSCAPE & URBAN DESIGN – STEFAN FEDIUK

Pursuant to the application for a zoning amendment (**Z 024/23**) to permit a High profile development (greater than 4 storeys) along Banwell Road with Multiple Dwelling with an excess height of 35m and a Residential Care Facility as a permitted use. Please also note the following comments:

Zoning Provisions for Parking Setback:

The application also requests reduced landscape open space to 24.5% on the subject, However, the Combined Concept Plan provided by the applicant does not demonstrate the need for this reduction on the South Site Properties, and lists them as 38.2% landscape open space to be provided. If the intent is to reduce the landscape open space is for a particular component of the South Properties, then more detailed information is required.

Per the Zoning Bylaw 8600, Section 24 – Parking, Loading and Stacking Provisions, specifically 24.20.5 Table 24.20.5.1 Requires Parking Spaces for Multiple dwelling containing a minimum of 5 dwelling Units, the rate is 1.25 parking spaces for each dwelling unit. South Site proposes 1.30 parking spaces per unit within the residential component. This represents an excess of 21 parking spaces than what is required.

Therefore, the proposed reduction of landscape area to 24.5% is not supported.

Tree Preservation:

The applicant has provided a Tree Inventory and Preservation Study (March 2023) for the entire three parcel development, prepared by Dillon Consulting, which documented 310 trees (27 on the property immediately north of the proposed development). A majority of the trees (84%) were identified as native species, including 20 Kentucky Coffeetree near the southern development parcel which are classified as an S2 Threatened SAR being very rare and

imperiled. The study implies that all 20 of the Kentucky Coffeetrees will be preserved. However, the TIP mapping and Detailed Tree Inventory spreadsheet indicate that two of these trees (tree # 176 & 186) are in good condition and scheduled for removal. Additionally, three of the Kentucky Coffeetrees (Tree # 177, 178, & 184) are extremely close to the proposed footprint of Building 'B' that their survival due to root damages will be impaired. This represents 25% of these SAR tree species would be removed or in jeopardy of removal. It is therefore recommended that the footprint for Building 'B' be moved northward to remove any risk of harm to this stand of SARs trees.

The remaining hedgerow of trees and scrub undergrowth running north-south through the middle of the South site, is dominated by Eastern Cottonwood, and Manitoba Maple (aka Boxelder). None of these are identified as SARs. As per Section 5.3 of the TIP report it is recognized that a Landscape Plan will be provided with the Site Plan Approval application to ensure that the loss to urban tree canopy has been compensated at a rate of caliper-to-caliper of tree trunk DBH to the satisfaction of the City Forester and the Planning Department's Landscape Architect.

Additionally, several trees on city owned land are proposed to be removed. Applicant is to provide confirmation from the City Forester that these tree may be removed and what if any compensation to the City of Windsor is required.

In addition, several endangered SAR Bat species and Butler's Gartersnake have been identified as potentially occurring in the vicinity of the proposed development. While suitable habitat for Butler's Gartersnake is not found on the site, there are suitable roosting habitats for bats. The proposed development would see the entire hedgerow removed due to the proposed site layout. The study recommends no removal between April 1 and September 30, and additional recorded monitoring is required to ensure that there are no bat species are roosting prior to any removals.

Climate Change:

The proposed development is oriented in a north-south direction through the South Site. The increased asphalt surface and removal of existing tree canopy will increase the urban heat island effect through the site and abutting residential properties to the west. The requirement for tree replacement and increased landscape area is essential to mitigate the impacts of the hard surface paving from a sustainability perspective. As recommended by the above Zoning comments, reduction of the proposed area of hard surface is necessary to increase climate change resiliency and reduce storm water runoff.

Urban Design:

While consistency is prescribed by the O.P. vol 2, Section 2.7.5, the design of the proposed Building's 'A' thru 'D', should ensure materials and details that allow for accessibility needs to help people with cognitive and visibility constraints identify different units readily.

Parkland Dedication:

All requirements will be determined at the time a Site Plan application is received.

CANADA POST – BRUNO DESANDO

This development, as described, falls within our centralized mail policy.

I will specify the condition which I request to be added for Canada Post Corporation's purposes.

- a) Canada Post's multi-unit policy, which requires that the owner/developer provide the centralized mail facility (front loading lockbox assembly or rear-loading mailroom [mandatory for 100 units or more]), at their own expense, will be in effect for buildings and complexes with a common lobby, common indoor or sheltered space.

Should the description of the project change, I would appreciate an update in order to assess the impact of the change on mail service.

If you have any questions or concerns regarding these conditions, please contact me. I appreciate the opportunity to comment on this project.

Lock-Box Assembly Requirements

*The complete Canada Post Standards Manual for Builders & Developers can be downloaded at:
https://www.canadapost.ca/cpo/mc/assets/pdf/business/standardsmanual_en.pdf*

Compartments Size

- *Horizontal lock-box models used in mailrooms must have the following minimums:*
 - o *Residential compartments must be at least 12.5 x 13.5 cm*
 - o *Commercial compartments at least 13.5 x 30.5 cm*
 - o *Parcel compartments at least 30.5 x 30.5 cm*
- *Vertical lock-box models must have min comp size of 25 x 12.5 cm. (Most models are 40 x 12.7 cm)*

Heights

- All lock-box assemblies must be installed in a manner that will not require the delivery employee to reach higher than 170cm or lower than 45cm when delivering to the equipment. With respect to horizontal lock-boxes, the limits above will likely mean that maximum number of compartments that can be included in each column of residential compartments would be eight

Rear-loading Lock-boxes

- *Projects with more than 100 units are required to be serviced via a rear-loading lock-box assembly.*
- *There must be a width of at least 100cm of working space from the back of the boxes to the wall.*
- *A ledge under the bottom row of boxes is also recommended in rear-loading designs. This ledge is to be directly under the bottom row of boxes (no space between ledge and bottom of boxes) and must stick out at least 20cm from the back of the boxes.*
 - *Mailroom door is required to provide a minimum 81cm opening*
 - *Lighting should be at least 100 lux (measured 75 cm from floor)*

Access

- *All buildings where the lock-boxes are required to be serviced from inside the building are required to install a Canada Post Crown lock in the building intercom. The intercom is pre-fabricated with an internal housing for the lock. The lock can be obtained from the local deliver supervisor.*
- *If the building has more than 100 units, a rear-loading lock-box assembly will be installed. The door to the Canada Post delivery area must be fitted with a specific model of deadbolt. This is because Canada Post will supply a key cylinder made specifically for the Canada Post key that will fit inside the deadbolt purchased by the developer.*

Numbering

- *Compartments should be numbered vertically and left to right on the delivery side of the boxes*

Grade-level Components

- If the development includes grade level retail or residential units, please take note that door-to-door delivery will not be provided to these units. Canada Post is happy to install a Community Mailbox to provide service to these units. Please coordinate a location with the Canada Post Delivery Planner for the area. If there is no room on the property for the Community Mailbox, service can be provided via another Community Mailbox in the area. Options to service the units from the tower (lobby) lock-boxes or via a front-loading lock box erected on the outside of the building can also be discussed with the Delivery Planner.

BUILDING DEPARTMENT – BARBARA RUSAN

Comments from the City of Windsor, Building Department relating to the subject line matter are as follows:

The Building Code Act, Section 8.(1) requires that a building permit be issued by the Chief Building Official for construction or demolition of a building. The building permit review process occurs after a development application receives approval and once a building permit application has been submitted to the Building Department and deemed a complete application.

Due to the limited Ontario Building Code related information received, review of the proposed project for compliance to the Ontario Building Code has not yet been conducted. It is strongly recommended that the owner and/or applicant contact the Building Department to determine building permit needs for the proposed project prior to building permit submission.

The City of Windsor Building Department can be reach by phoning 519-255-6267 or, through email at buildingdept@citywindsor.ca

In addition to the above please note: a Record of Site Condition will be required if the proposed land use changes to a more sensitive land use (for example: changing from an existing industrial or commercial use to a residential use).

TRANSIT WINDSOR – JASON SCOTT

Transit Windsor has no objections to this development. The closest existing transit route to this property is with the Lauzon 10. The closest existing bus stop is directly adjacent to this property on McHugh at Questa Northeast Corner providing direct transit access. Transit service will be greatly improved in this area with City Council's approval of Transit Windsor' 2023 operating budget and service plan as 2 new local routes will be introduced to replace the Lauzon 10. Both of these routes will be along McHugh and Banwell and will both have 2 way conventional transit service versus the existing one way loop with the Lauzon 10. They are proposed to be implemented in early 2024. This is consistent with our City Council approved Transit Master Plan. Transit Windsor is pleased to see the majority of the buildings being located directly adjacent to the roadway. This helps with passenger accessibility and active transportation by reducing the walking distance from the buildings to the road. The inclusion of sidewalks from the buildings in the interior of the proposal is also welcome so people don't have to walk through parking lots to get to the road.

JOHN WALSH – VIA RAIL

We are in receipt of your Liaison Submission dated 26 July 2023 for the above-captioned applications.

Upon our review, VIA must advise you that we are not in favour of Residential Development in such close proximity to our freight and passenger train right-of-way due to the obvious incompatibility.

Should the City still decide to progress the subject applications, the attached noise, vibration and safety mitigation measures must be addressed for any new development within 300m of VIA's Chatham Subdivision.

In addition to the aforementioned measures, it is also noted that the sight-triangle for the Banwell Road and VIA at-grade crossing could be compromised by the proposed development despite the

presence of automatic protection devices at the level crossing. Please consult with Transport Canada to ensure compliance with their regulations in this regard.

Kindly keep us informed on the progress of this application.

SITE PLAN CONTROL

The development proposal is subject to Site Plan Control pursuant to the Planning Act and City of Windsor By-law 1-2004. Where preceding development applications are required, inclusive of Official Plan and Zoning By-law Amendments, request for Site Plan Control Pre-Consultation may be made following completion of the requisite Development and Heritage Standing Committee meeting at <https://ca.cloudpermit.com/login>.

ENWIN

HYDRO ENGINEERING:

No Objection, provided adequate clearances are achieved and maintained.

ENWIN has existing underground along the south side of McHugh with 27,600 volt primary hydro distribution.

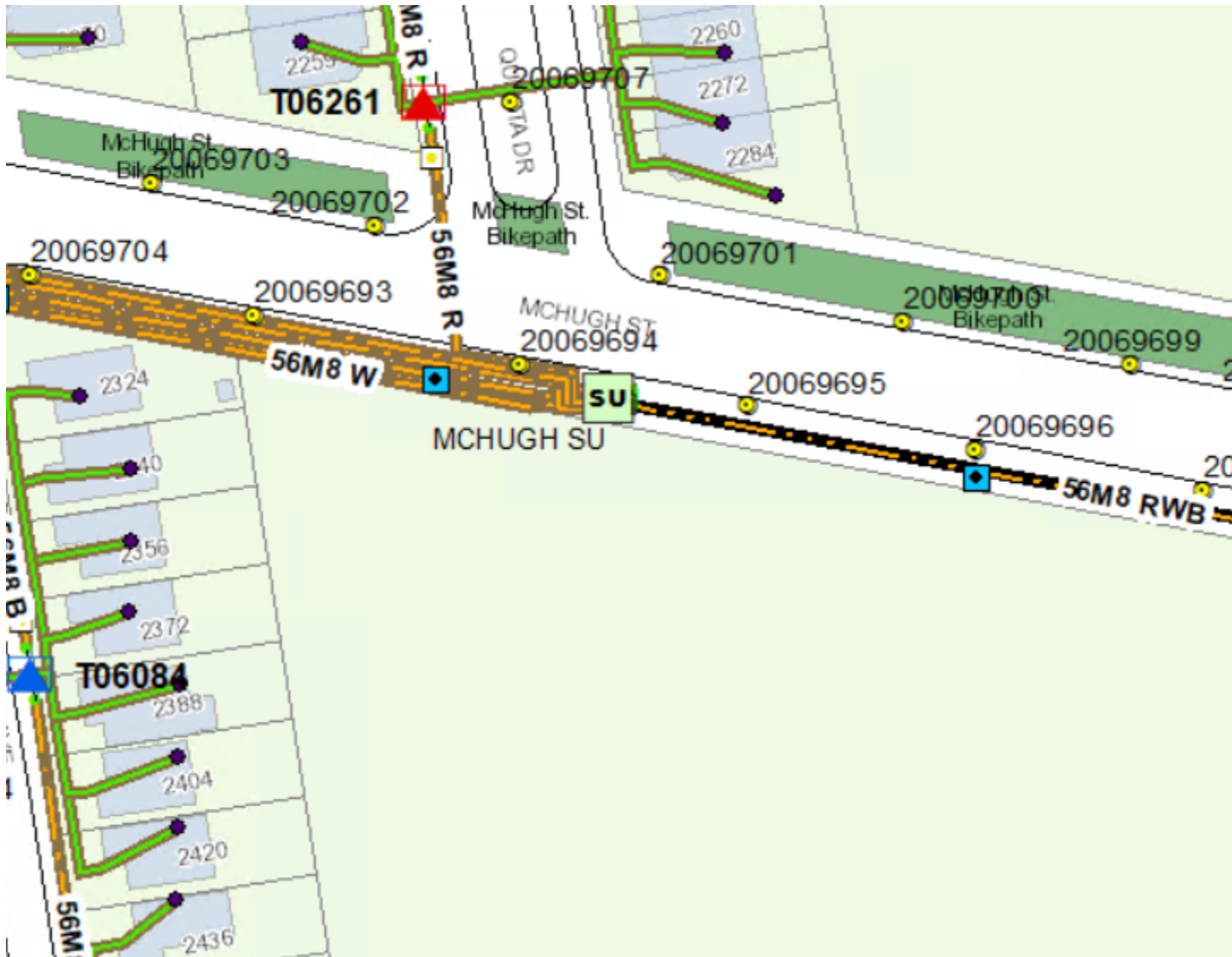
ENWIN has existing switching unit at the south west corner of McHugh with 27,600 volt primary hydro distribution.

ENWIN has existing streetlight poles on the south side of McHugh with underground 120/240 volt distribution.

ENWIN has existing streetlight poles on the north side of McHugh with underground 120/240 volt distribution.

Prior to working in these areas, we would suggest notifying your contractor and referring to the Occupational Health and Safety Act and Regulations for Construction Projects to confirm clearance requirements during construction.

Also, we suggest referring to the Ontario Building Code for permanent required clearances for New Building Construction.



WATER ENGINEERING:

Water Engineering has no objections.

ALICIA GOOD – ERCA

The City of Windsor has received an Application for an Official Plan Amendment 175 and a Zoning By- Law Amendment 024-23 to support the construction of a Mixed-Use development with multiple High Profile buildings on the subject lands.

The following is provided as a result of our review of Application for Official Plan Amendment OPA-175, and Zoning By-Law Amendment ZBA-024-23.

NATURAL HAZARDS AND REGULATORY RESPONSIBILITIES UNDER THE CONSERVATION AUTHORITIES ACT, O. REG 686/21, PPS

The following comments reflect ERCA's role in protecting people and property from the threats of natural hazards and regulating development hazards lands under Section 28 of the *Conservation Authorities Act*.

The above noted lands are subject to our Development, Interference with Wetlands and Alteration to Shorelines and Watercourses Regulation under the *Conservation Authorities Act* (Ontario Regulation No. 158/06). The parcel falls within the regulated area of the Parent Outlet Drain. The property owner will be required to obtain a Permit from the Essex Region Conservation Authority prior to any construction or site alteration or other activities affected by Section 28 of the *Conservation Authorities Act*.

FINAL RECOMMENDATION

Our office has no objection to the proposed OPA 175 and ZBA-024-023. We request continued circulation for this development as we may have further comment regarding the Stormwater Management Plan at the time of application for Site Plan Control. As noted above, the property owner will be required to obtain a Permit from the Essex Region Conservation Authority prior to any construction or site alteration or other activities affected by Section 28 of the *Conservation Authorities Act*.

If you have any questions or require any additional information, please contact the undersigned.

SITE PLAN CONTROL

The development proposal is subject to Site Plan Control pursuant to the Planning Act and City of Windsor By-law 1-2004. Where preceding development applications are required, inclusive of Official Plan and Zoning By-law Amendments, request for Site Plan Control Pre-Consultation may be made following completion of the requisite Development and Heritage Standing Committee meeting at <https://ca.cloudpermit.com/login>.

ZAID ZWAYYED - ZONING COORDINATOR

Below is the zoning review for the proposed development:

- **Current Zoning Designation:** MD1.4 Subject to a Site Specific provision S.20(1)112
- **Proposed Zoning Designation:** Site-specific to permit multiple dwelling and Residential Care Facility, subject to the provisions of RD3.2

- **Existing Use [as per historical Building Permit(s) / Planning Act Application(s)]:**
 - Vacant property

- **Proposed Use:**
 - Four Multiple Dwelling Buildings with 408 dwelling units, two storey business office, residential care facility building, and a parking area.

- **Section 5 – General Provisions**
 - **Railway Right of Way Provisions (5.60):**

- An Earth Berm having a minimum height of 2.50 m and slopes of 2.5 to 1 or greater to be provided along the property line abutting the railway right of way (5.60.1.1)
 - Minimum Separation of a residential building from the railway right of way (5.60.1.2):
 - 30.0 m (Required)
 - 31.0 m (Provided)
 - A security fence, 1.830 m high along the property line abutting the railway right of way, is required (5.60.5).
- **Zoning Provisions (12.2):**
 - **Minimum lot Frontage:**
 - 30.0 m (Required)
 - 180.78 m (Provided)
 - **Minimum Lot Area:**
 - 18,709.0 m² (Required)
 - 53,603.36 m² (Provided)
 - **Maximum Lot Coverage:**
 - 35.0% (Required)
 - 12.90% (Provided)
 - **Maximum Main Building Height:**
 - 35.0 m (Proposed) (Required)
 - The Applicant must confirm the proposed height (Provided)
 - **Minimum Landscaped Open Space Yard:**
 - 24.50% (Proposed) (Required)
 - 24.50% (Provided)
 - **Maximum Dwelling Unit Density:**
 - 188 units per ha (Required)
 - 108 Units per ha (Provided)
- **Section 24 - Parking, Loading and Stacking Provisions**
 - **Required Number of Parking Spaces:**
 - Dwelling Units (proposed rate 1.50 per unit): 530 spaces (Required)
 - Business Office: 41 (Required)
 - Residential Care Facility: The applicant must provide the number of beds to calculate
 - Total Provided Number of parking: 602 Spaces shown on the submission
 - **Required Number of Visitor Parking Spaces:**
 - 79 spaces (Required)
 - Designated visitor spaces must be shown

- **Required Number of Type A Accessible Parking Spaces:**
 - Number of beds must be provided to confirm (Required)
 - 10 spaces (Provided)
- **Required Number of Type B Accessible Parking Spaces:**
 - Number of beds must be provided to confirm (Required)
 - 16 spaces (Provided)
- **Access Aisles and curbcuts for accessible spaces:** curbcuts or ramps must be shown on the submission.
- **Required Number of Bicycle Spaces:**
 - Bicycle spaces are not shown on the submission
- **Required Loading Spaces:**
 - 5 spaces (Required)
 - 5 spaces (Provided)
- **Section 25 – Parking Area Provisions**
 - More information and dimensions are required to review for compliance with section 25.

TRACY TANG - HERITAGE PLANNING

There is no apparent built heritage concern with this property and it is located on an area of low archaeological potential.

Nevertheless, the Applicant should be notified of the following archaeological precaution.

1. Should archaeological resources be found during grading, construction or soil removal activities, all work in the area must stop immediately and the City's Planning & Building Department, the City's Manager of Culture and Events, and the Ontario Ministry of Citizenship and Multiculturalism must be notified and confirm satisfaction of any archaeological requirements before work can recommence.
2. In the event that human remains are encountered during grading, construction or soil removal activities, all work in that area must be stopped immediately and the site secured. The local police or coroner must be contacted to determine whether or not the skeletal remains are human, and whether the remains constitute a part of a crime scene. The Local police or coroner will then notify the Ontario Ministry of Citizenship and Multiculturalism and the Registrar at the Ministry of Government and Consumer Services if needed, and notification and satisfactory confirmation be given by the Ministry of Citizenship and Multiculturalism.

Contacts:

Windsor Planning & Building Department:

519-255-6543 x6179, ktang@citywindsor.ca, planningdept@citywindsor.ca

Windsor Manager of Culture and Events (A):

Michelle Staaedegaard, (O) 519-253-2300x2726, (C) 519-816-0711,
mstaaedegaard@citywindsor.ca

Ontario Ministry of Citizenship and Multiculturalism

Archaeology Programs Unit, 1-416-212-8886, Archaeology@ontario.ca

Windsor Police: 911

TRANSPORTATION PLANNING – CLARE AMICARELLI

- The Official Plan classifies Banwell Road as a Class 2 Arterial with a required right-of-way width of 30 metres, as per Schedule X. The existing right-of-way width is sufficient; therefore, a conveyance is not required.
- The Official Plan classifies McHugh St as a Class 2 Arterial with a required right-of-way width of 36 metres, as per Schedule X. The existing right-of-way width is sufficient; therefore, a conveyance is not required.
- A corner cut-off of 6.1 metres x 6.1 metres is required at the corner of Banwell Rd and McHugh St for a signalized intersection.
- Per the Official Plan, a sidewalk is required on both sides of an Arterial Road. A sidewalk construction or contribution is required along the frontage of McHugh St, as per Engineering Right-of-Way's comments.
- All parking must comply with ZBL 8600, otherwise a parking study may be required.
 - Development requires 32 bicycle spaces as per ZBL 8600. Bicycle spaces must comply with ZBL for number and dimensions of spaces and must be clearly shown on revised site plan.
 - Total GFA is required in order to determine if loading spaces proposed are sufficient according to the ZBL.
 - Aisle width must be 6 metres in order to have adequate width for turning maneuvers
- Transportation Planning has reviewed the Transportation Impact Study titled, "Banwell and McHugh Mixed Use Developments" conducted by Dillon Consulting Ltd. in March 2023. Transportation Planning has the following comments:
 - A revised TIS is required, as the TIS submitted does not include any developments along the east side of Banwell Rd. The revised TIS must account for any traffic that will be generated by the properties east of Banwell Rd, specifically at the southeast corner of Banwell Rd and Leathorne St by using estimated traffic volumes based on the existing and permitted zoning of those properties.
- A reciprocal access agreement is required in order for the severed lot to have access to McHugh St. The reciprocal access must be maintained and it should be in line with Questa Dr for safety purposes.
- All new exterior paths of travel must meet the requirements of the Accessibility for Ontarians with Disabilities Act (AODA).
- All new accesses shall conform to the TAC Geometric Design Guide for Canadian Roads and the City of Windsor Standard Engineering Drawings AS-204.

- All proposed driveways must be 7-9 metres total at the property line (minimum 3.5m/lane, maximum 4.5m/lane)
- As noted above, the proposed access onto McHugh St should be in line with Questa Dr for safety purposes.

ENGINEERING – ROB PERISINOTTI

We have reviewed the subject Rezoning application and have the following comments:

Sewers

There is a 250mm PVC sanitary sewer located within the Banwell Road right-of-way and a 300mm PVC sanitary sewer located in the McHugh Street right-of-way.

There is a 975mm RCP Storm sewer located within the Banwell Road right-of-way and a 375mm PVC storm sewer located within the McHugh Street right-of-way.

There is a 2100mm RCP sanitary trunk sewer located at the south side of the property within an existing easement A complete Servicing study will be required for all proposed services inclusive of all proposed developments.

Right-of-Way

Banwell Road is classified as Class II Arterial Road according to the Official Plan requiring a right-of-way width of 30 meters; the current right-of-way is sufficient; therefore, a conveyance is not required at this time.

McHugh St is classified as a Class II Arterial Road according to the Official Plan with a required right-of-way width of 36 metres, as per Schedule X. The existing right-of-way width is sufficient; therefore, a conveyance is not required.

A corner cut off of 6.1 meters x 6.1 meters is required at the south west corner of Banwell Rd and McHugh St.

The north side of the property is deficient in municipal sidewalk. Construction of concrete municipal sidewalk will be required to tie into the existing sidewalk at the west property line to the corner of McHugh St. and Banwell Rd.

At the time of SPC application the following will be required:

1. Stormwater Management Report
 2. Offsite works to alter Banwell Road if required by Transportation Planning.
- In summary we have no objection to the proposed development, subject to the following requirements:

Servicing Study – The owner agrees, at its own expense, to retain a Consulting Engineer to provide a detailed servicing study report on the impact of the increased flow to the existing municipal sewer systems, satisfactory in content to the City Engineer and prior to the issuance of a construction permit. The study shall review the proposed impact and recommend mitigating measures and implementation of those measures.

Corner Cut-Off – The owner(s) agrees, prior to the issuance of a construction permit, to gratuitously convey a 6.1m x 6.1m corner cut-off at the south west corner of Banwell Rd and McHugh St.

Sidewalks -The owner(s) agrees, to construct at their expense and according to City of Windsor Standard Specifications, a concrete sidewalk along the entire McHugh Street frontage of the subject lands. All work to be to the satisfaction of the City Engineer;

If you have any further questions or concerns, please contact Amy Kurek, of this department at akurek@citywindsor.ca



SOUTH SITE - RESIDENTIAL DEVELOPMENT DETAILS:

TOTAL PROPOSED UNITS:	408
BUILDING 'A':	10-STOREY, 120 UNITS
BUILDING 'B':	10-STOREY, 120 UNITS
BUILDING 'C':	6-STOREY, 72 UNITS
BUILDING 'D':	8-STOREY, 96 UNITS
PROPOSED PARKING SPACES:	531
PROPOSED PARKING RATE:	1.30
LANDSCAPED OPEN SPACE:	
(incl. trails):	38.2%
LOT COVERAGE:	12.7%

SOUTH SITE - BUSINESS OFFICE DEVELOPMENT DETAILS:

GFA:	1860m ²
REQUIRED PARKING SPACES:	56
PROVIDED PARKING SPACES:	60

SOUTH SITE - RESPITE HOME DEVELOPMENT DETAILS:

GFA:	499m ²
REQUIRED PARKING SPACES:	12
PROVIDED PARKING SPACES:	12

NORTH 'B' SITE - RESIDENTIAL DEVELOPMENT DETAILS:

TOTAL PROPOSED UNITS:	180
BUILDING 'E':	6-STOREY, 24 UNITS
BUILDING 'F':	6-STOREY, 78 UNITS
BUILDING 'G':	6-STOREY, 78 UNITS
PROPOSED PARKING SPACES:	270
PROPOSED PARKING RATE:	1.50
LANDSCAPED OPEN SPACE:	
(incl. trails):	36.4%
LOT COVERAGE:	23.2%

NORTH 'A' SITE - RESIDENTIAL DEVELOPMENT DETAILS:

TOTAL PROPOSED UNITS:	156
BUILDING 'H':	6-STOREY, 78 UNITS
BUILDING 'I':	6-STOREY, 78 UNITS
PROPOSED PARKING SPACES:	227
PROPOSED PARKING RATE:	1.45
LANDSCAPED OPEN SPACE:	
(incl. trails):	24.5%
LOT COVERAGE:	9.02%

1027458 ONTARIO LTD.
BANWELL AND McHUGH - MIXED USE DEVELOPMENTS

CONCEPTUAL DEVELOPMENT PLAN
April 4, 2023

BANWELL AND McHUGH - SOUTH SITE (± 5.35 ha / 13.23 ac)	BANWELL AND McHUGH - NORTH 'A' SITE (± 1.43 ha / 3.54 ac)	BANWELL AND McHUGH - NORTH 'B' SITE (± 1.66 ha / 4.11 ac)	PROPOSED MULTI-UNIT RESIDENTIAL	PROPOSED LANDSCAPED OPEN SPACE	SOUTH SITE - PHASE 1
30m BUFFER FROM RAILWAY	EXTENT OF BERM	EXISTING BERM	PROPOSED BUSINESS OFFICES	PROPOSED RESPITE HOME	SOUTH SITE- PHASE 2

File Location:
c:\pw working directory\projects 2022\dillon_32mru\dms63328\22-5144 - banwell and mchugh - concept plan.dwg
April, 04, 2023 1:22 PM

MAP/DRAWING INFORMATION
THIS DRAWING IS FOR INFORMATION PURPOSES ONLY. ALL DIMENSIONS AND BOUNDARY INFORMATION SHOULD BE VERIFIED BY AN O.L.S PRIOR TO CONSTRUCTION.
CREATED BY: MRU
CHECKED BY: TJO
DESIGNED BY: MRU

SCALE: 1: 2000 (11X17)



PROJECT: 22-5144
STATUS: DRAFT
DATE: 04/03/2023



DILLON
CONSULTING

1027458 ONTARIO INC.

Public Information Centre Engagement Summary

April 19, 2023

East Riverside, South Neighbourhood, Windsor, Ontario

Banwell & McHugh Mixed Use Developments

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- B Figure 4.0 – Conceptual Development Plan
- C Notice of Resident’s Meeting
- D PIC Presentation Boards
- E Resident’s Comments
- F Record of Attendance and Location Map



1.0 The Public Information Centre

The Public Information Centre (PIC) was held on April 19, 2023 on behalf of our client 1027458 Ontario Inc., by Dillon Consulting Limited in support of proposed mixed use developments located within the South Neighbourhood of the East Riverside Planning Area in the City of Windsor Ontario. The proposed mixed use developments include nine (9) multiple dwelling buildings with a total of 744 units, an office building, and a residential care facility (refer to Appendix A – Figure 1.0 – Location Map).

The proposed mixed use development is broken into three sites (refer to Appendix B – Figure 4.0 – Conceptual Development Plan) which includes the following:

- North 'A' Site
 - Two (2), six (6)-storey multiple dwelling buildings;
 - 156 units proposed; and,
 - 227 parking spaces provided.
- North 'B' Site
 - Three (3), six (6)-storey multiple dwelling buildings;
 - 180 units proposed; and,
 - 270 parking spaces provided.
- South Site
 - One (1), six (6)- storey multiple dwelling building;
 - One (1), eight (8)-storey multiple dwelling building;
 - Two (2), ten-storey multiple dwellings;
 - 408 units proposed;
 - 531 residential parking spaces provided;
 - One (1), two (2)-storey Office Building;
 - 60 office parking spaces provided;
 - One (1) Residential Care Facility; and,
 - 12 Residential Care Facility parking spaces provided.

The proposed mixed use developments require the following development applications:

- Official Plan Amendment to the City of Windsor's Official Plan;
- Official Plan Amendment to the East Riverside Planning Area Secondary Plan; and,
- Zoning By-law Amendment to the City of Windsor Zoning By-law 8600.

The purpose of the Public Information Centre was to provide additional information to residents on the proposed developments, provide information about the planning process, and discuss/collect comments and questions relating to the proposed development.

A copy of the Notice of PIC is provided in Appendix C.

1.1 Format

The Public Information Centre (PIC) was held in-person at the Windsor Family Credit Union (WFCU Centre) in the City of Windsor on April 19th, 2023 from 6:00pm – 8:00pm. The Public Information Centre was conducted in a drop-in format, with presentation boards displaying the following information (refer to Appendix D):

- Introduction Board;
- Welcome Board;
- Figure 1.0 – Location Map;
- Figure 2.0 – City of Windsor: Official Plan Designations;
- Figure 3.1 – City of Windsor: Zoning By-law 8600; North Site 'A' & 'B'
- Figure 3.1 – City of Windsor: Zoning By-law 8600; South Site
- Figure 4.0 – Conceptual Development Plan - Master Plan;
- Figure 5.0 – Conceptual Development Plan – North Site 'A' & 'B';
- Figure 6.0 – Conceptual Development Plan – South Site
- Figure 7.0 – City of Windsor: East Riverside Major Roads;
- Figure 8.1 – Shadow Impact Study – March 21, 7:00 AM-10:00 AM;
- Figure 8.2 – Shadow Impact Study – March 21, 3:00 PM-5:00 PM;
- Figure 9.1 – Shadow Impact Study – June 21, 6:00 AM-10:00 AM;
- Figure 9.2 – Shadow Impact Study – June 21, 12:00 PM-6:00 PM;
- Figure 10.1 – Shadow Impact Study – September 21, 7:00 AM-10:00 AM;
- Figure 10.2 – Shadow Impact Study – September 21, 12:00 PM-5:00 PM;
- Figure 11.1 – Shadow Impact Study – December 21, 8:30 AM-11:00 AM;
- Figure 12.2 – Shadow Impact Study – December 21, 2:00 PM-3:30 PM;

Resident feedback was obtained in the following ways:

- Visiting location map – residents were asked to identify with a sticker where they were visiting from in relation to the site;
- Individual discussions – conducted throughout the evening between residents and the representatives from Dillon Consulting Limited;
- Comment forms – collected in-person at the Public Information Centre, with the option for comments to be mailed or emailed to Dillon Consulting Limited at a later date and accepted up until 11:59pm on May 3, 2023; and

- A project specific email address was provided.

1.2 Attendance

There were approximately 150 residents in attendance for the PIC on April 19th, 2023. While a total of approximate 150 residents was observed by Dillon Consulting Limited, it is noted that only one (1) member per household was asked to provide information for sign-in. As such, the total number of residents reflected on the record of attendance is seventy-five (75).

The following table provides locational information of the attendees at the Public Information Centre, based on proximity to the proposed developments.

Attendance	Within 120 Meters	Within 121 Meters – 500 Meters	Beyond 500 metres
Number of Attendees	35	33	7
Percentage of Attendees	47%	44%	9%

A copy of the comments received has been included in Appendix E. A copy of the redacted record of attendance along with the location map identifying where residents were visiting from has been included in this summary as Appendix F.



2.0 Comments Received

A total of seventy-nine (79) comment forms were collected by receipt through in-person drop-box, approximately twenty-two (22) emails, and approximately ten (10) of phone calls were received. Tenants and land owners who responded provided valuable feedback to project staff.

Questions and concerns received during the meeting, through email, and through phone calls have been noted. Responses to the comments received are provided in this engagement summary.

The following table provides an overview of the comments received based on location:

Type of Comment	Within 120 Meters	Within 121 Meters – 500 Meters	No Address Provided
Comment Form	6	3	70
Email	6	3	2
Phone Call	9	1	0
TOTALS	21	7	72

The following table provides an approximate overview of the content of the comments received, broken down by category:

Category	Response Rates
Transportation & Traffic	30 %
Stormwater & Flooding	26 %
Lighting	14 %
Safety & Privacy	13 %
Concept Plan Design	6 %
Land Use Concerns	6 %
Wildlife & Vegetation	3 %
Noise	1 %
Air Quality	1 %

As indicated by the above table, comments received were about transportation / traffic and stormwater and flooding, followed by comments about lighting as well as safety and privacy.

The table below outlines the frequent topics observed through the comments received and the development team's response to each. Land Use planning matters have been considered below. Other comments not related to land use planning principles have been noted.

COMMENTS RECEIVED	RESPONSES
<p>Wildlife and Vegetation:</p> <ol style="list-style-type: none"> 1. What about the animals that are back there? 2. Concern that the natural habits of rabbits, deer, turkey, falcons and pheasants will be destroyed and that they will go away; 3. The kids will not be able to enjoy nature; 4. Green space will be permanently removed; and, 5. Where is the environmental study on increased housing? <p>Safety and Privacy:</p> <ol style="list-style-type: none"> 6. Request for a Vinyl Fence to be proposed to stop the light and people from cutting through; 7. Wood fence will break down in a few years; 8. A 10ft vinyl fence is requested to be proposed instead of wood along the properties east of Questa Drive; 9. Concerns about neighborhood security; 10. Concerns six (6)-storey building would infringe on privacy; 11. A lot of natural trees will be destroyed and will no more provide privacy and enjoyment; and, 12. More people will mean more criminals in the area and increased crimes. 	<p>Wildlife and Vegetation:</p> <ul style="list-style-type: none"> • A Tree Inventory and Preservation Study (TIPS), reviewing the natural site features present on the subject sites, has been completed as part of the background studies required for the development. A preliminary screening for Species at Risk (SAR) was also carried out as part of the process. • To the north of the proposed developments is the Elizabeth Kishkon Park and east of Banwell Road, along Blue Heron Drive, is the East Riverside Park. These parks are part of the Greenway System for future residents (OP 4.2.1.2; OP, Vol. II, Schedule ER-3), providing a network of pedestrian and cycling routes. The area is well supported by existing open space areas and greenway infrastructure. <p>Safety and Privacy:</p> <ul style="list-style-type: none"> • At present, a 1.8 m (5.9 ft) high, board-on-board fence is the industry standard to provide privacy for existing residential dwellings that are adjacent to the west lot line of the proposed developments. • The proposed fence is in keeping with the municipal requirements. • The proposed fence would limit the light (i.e. from cars entering or existing the proposed parking spaces) on the properties adjacent to the west lot line of the proposed developments. • Full cut-off lighting is a requirement of Site Plan Control to limit site trespass onto adjacent property. • Appropriate setbacks and landscaping will be provided to create barriers.
<p>Concept Plan Design:</p> <ol style="list-style-type: none"> 13. Resident concerns over limited proximity of the buildings that may affect 'Site Lines'; 14. A wider buffer is needed between the homes and the proposed building; 	<p>Concept Plan Design:</p> <ul style="list-style-type: none"> • Through additional required Approvals processes (i.e. Site Plan Control), options will be considered for optimizing building placement, ensuring an appropriate balance between the proposed density and the existing community. • Vehicular site lines at intersections will be reviewed to ensure safe access to the road network.

<p>15. Want the building further away from the backyards;</p> <p>16. Concern over limited sunlight into backyards in the summer months;</p> <p>17. Shadows from high-rise buildings are a major concern;</p> <p>18. The lack of sunlight due to the tall buildings in our back yard will impact the heating and cooling bills;</p> <p>19. Concerns over garbage and rat infestation backing into our backyard; and,</p> <p>20. What is the purpose of a 10' clearance from our lot line to the fence (referring to North 'A' Site)?</p>	<ul style="list-style-type: none"> • The proposed locations of the buildings adhere to the minimum setback requirements of the City of Windsor and provide adequate separation between the properties. • The proposed conceptual development has been designed to reduce shadow impacts on the existing neighbourhood. A Shadow Impact Study has been completed as part of the background studies required for the development. • Site Plan Control approval will address the location of garbage storage and disposal. • A fence is proposed to be located along the property line between the proposed development and existing residential dwellings.
<p>Land Use Concerns:</p> <p>21. How is it possible to build these buildings now because for 20 years one couldn't?</p> <p>22. When we move into this neighborhood, we were told only a commercial plaza is allowed to be built here. How is this changing now?</p> <p>23. North Site 'A' should be a park site, or building of a new retirement home as there is an aging population in the area;</p> <p>24. Concerns over the density proposed;</p> <p>25. Prefer single detached houses or townhouses;</p> <p>26. Prefer a 3 storey building that would help keep property value;</p> <p>27. Prefer 1-storey commercial or park property.</p> <p>28. There are a lot of empty land around Windsor, why not move there?</p> <p>29. This project is zoned for commercial not residential. How is it possible?</p> <p>30. The development seems an oversaturation of the area; and.</p> <p>31. The character of the neighborhood will be changed because of the high density.</p>	<p>Land Use Concerns:</p> <ul style="list-style-type: none"> • The proposed development is consistent with the Provincial Policy Statement 2020 (PPS) policies, providing for an appropriate range and mix of housing types and densities in order to meet projected needs of current and future resident (PPS 1.4.1). As well, the proposed development demonstrates a land use pattern which efficiently uses land and resources, and supports active transportation (PPS 1.8.1). • The proposed development promotes a land use pattern with increased density, introducing a range and mix of residential and commercial uses to the area and encouraging transit-supportive development that minimizes land consumption and servicing costs, as intended by the PPS (1.1.1 (b, e)). • Both Banwell Road and McHugh Street are classified as a Class II Arterial Roads in the City of Windsor Official Plan (OP) (OP), Vol. II, 2.7.9.12 (e)), where medium and high-density residential developments, as well as mixed use developments are encouraged to be located by the City of Windsor OP (OP, 6.9.2.3 and 7.2.6.18 (c)). The proposed developments are located along Banwell Road which is considered a City Corridor, as per Schedule J - Urban Structure Plan in the City of Windsor Official Plan. Higher density land uses, such as the proposed development, are encouraged along City Corridors (OP, 3.3.2.1). • In July 2022, the City of Windsor adopted Official Plan Amendment No. 159 to provide direction and implement policies regarding intensification within the City. Through this Amendment, changes to land uses and land use policies came into effect that designates the Banwell Road and McHugh Street intersection as a Mixed Use Node. • A site-specific Official Plan Amendment to the City of Windsor Official Plan is proposed for the Mixed Use Node land use designation to add High Profile residential dwelling units as a

	<p>permitted use. In addition, a site-specific Official Plan Amendment to the East Riverside Planning Area Secondary Plan will also be required to change the Subject Area to the Banwell Road Mixed Use Corridor land use designation with the added permitted use of Community Institutions.</p> <ul style="list-style-type: none"> • A Zoning By-law Amendment is proposed to rezone the Subject Area from General Commercial (East Riverside) (CD2.7) with S.20(1)218, General Commercial (East Riverside) (CD2.7), and Manufacturing District 1.4 (MD1.4) with S.20(1)112 to a site specific General Commercial (East Riverside) (CD2.7) with the Addition of "Multiple Dwelling" and "Residential Care Facility" as permitted uses, following site-specific Residential District 3.2 (RD3.2) zone standards, with an increased Main Building Height of 35.0m meters and Landscaped Open Space Yard of 24.5%. • This application(s) are specific to these properties, other vacant lands in the immediate area are not part of the analysis. • Site Plan Control approval, which will occur after these approvals, will confirm design elements, landscaping, and architectural features that harmonize with the neighborhood character will be considered.
<p>Transportation & Traffic:</p> <ol style="list-style-type: none"> 32. Concerns about traffic problems already existing in the area and adding many more cars; 33. Concerns that there won't be enough parking for 2 cars per unit so people will park on the streets and no room left for resident visitors; 34. Concerns over driver and pedestrian's safety; 35. Where will people park? Questa drive and Chateau drive is already full of street parking due to newly built residential buildings and raise concerns over street parking; 36. Exits onto Banwell is very dangerous now with limited site lines due to fencing and bushes. 37. How will traffic safety be maintained as the neighborhoods have a lot of kids playing on the streets? 38. What will be the school bus routes? 39. Will there be electric vehicle charging stations? 	<p>Transportation & Traffic:</p> <ul style="list-style-type: none"> • The City is responsible for traffic calming measures. Residents concerned about dangerous driving conditions or pedestrian safety should submit a formal submission to the City through 311. • At present, the City of Windsor's minimum zoning requirement for parking spaces for a Multiple Dwelling (i.e. multi-unit residential building) is 1.25 parking spaces per dwelling unit. A parking rate is provided for each site that is above the required minimum. The proposed parking rates are 1.45 parking spaces per dwelling unit (North 'A' Site), 1.50 parking spaces per dwelling unit (North 'B' Site), and 1.30 parking spaces per dwelling unit (South Site). A total of 1,028 parking spaces are proposed to support the 744 residential units, with an additional 72 parking spaces provided to support the business office and respite home. • A Traffic Impact Study (TIS) was completed to determine the transportation related impacts of the proposed development, including the determination for potential modifications to infrastructure to accommodate traffic generated by the proposed development. • Construction Routes are prescribed by the City to ensure there is no undue negative impacts on the surrounding residential areas. • School bus routes are determined by the local School Boards. • Through additional required Approvals processes (i.e. Site Plan Control), the inclusion of electric vehicle charging stations to accommodate future residents with electric vehicles will be considered.



<p>40. How are trucks allowed on these roads? Delivery vehicles are parking in front of our houses which is unacceptable.</p> <p>41. Where is the pedestrian crosswalk at Firgrove and Banwell?</p> <p>42. Residents strongly suggest a new traffic light at Banwell & Firgrove be installed for pedestrian safety;</p> <p>43. Need stop lights to get our onto Banwell from Leathorne & Firgrove; and,</p> <p>44. Residents suggest a parking permit be required for people to park in front of our houses; and,</p> <p>45. What will happen with the construction traffic?</p>	<ul style="list-style-type: none"> • A request for a neighbourhood parking permit system should be submitted to 311. • Specific design details such as: loading spaces, drive aisles, adequate turning radii for fire trucks, delivery and other vehicles are considered during Site Plan Control. • Specific parking questions / issues about existing residential dwellings / buildings in the area should be forwarded to 311.
<p>Lighting:</p> <p>46. Concern that lights will be in the backyard and bedrooms of residents.</p>	<p>Lighting:</p> <ul style="list-style-type: none"> • Site Plan Control approval requires “full cut-off lighting” ensuring lighting stays within the subject property.
<p>Stormwater / Flooding:</p> <p>47. General concerns about Flooding in the residential homes.</p> <p>48. Can the infrastructure in the area accommodate all these new residents?</p> <p>49. Can the sewer system accommodate these new residents? There are a lot of sewer back-up and flooding, which will in turn increase insurance due to excess claims. The increased residents will increase sewer output;</p> <p>50. The retention pond at Blue Heron has actually overflowed in 2016, along with numerous houses on Kirkland. So, the existing retention pond will definitely not be enough for the proposed development and additional residents. Explain;</p> <p>51. The Site has water pooling and flooding happens every storm.</p>	<p>Stormwater / Flooding:</p> <ul style="list-style-type: none"> • A Stormwater Management Report was completed to ensure that the development can be accommodated within the existing system. • A Functional Servicing Report was completed, evaluating the capacity of existing infrastructure to support the proposed development. • Parking lots act as retention areas for stormwater. This is a requirement in all multi-residential and commercial developments in Windsor.



<p>How is that being addressed?; and, 52. We would like the berm to stay as it helps with flooding.</p> <p>Noise: 53. The noise in the area will increase due to added cars and people.</p>	<p>Noise:</p> <ul style="list-style-type: none"> • A Noise and Vibration Assessment was completed regarding the impact of the rail line on the proposed land uses. • Noise assessments related to existing residential and proposed residential uses and increase in road noise are not completed.
<p>Air Quality: 54. Air quality will go down with all the new cars and added traffic.</p>	<p>Air Quality:</p> <ul style="list-style-type: none"> • Air quality related to the increase in vehicular traffic is not studied.
<p>Other: 55. General concern that the value of properties will decrease; 56. The property value of the condos will be higher than the residential houses. 57. Who will maintain the properties and the surrounding areas? 58. Are there adequate schools associated with the new developments? Are the City and schools engaged? 59. Property taxes will go up; 60. Will these be condos or apartment for rent?; and, 61. What income group is coming here?</p>	<p>Other:</p> <ul style="list-style-type: none"> • All concerns related the proposed development have been noted. The purpose of the Public Information Centre was to obtain feedback to determine the best possible use for the site. As the development proceeds, consideration will be taken regarding comments and concerns provided by residents. • There are a number of concerns raised that are not land use questions: <ul style="list-style-type: none"> ○ Tenure (ownership vs rental); ○ Taxes / mill rate; ○ Income level of the existing or future residents; and ○ Property values are not salient in the discussion around land use.
<p>Project Information Concerns: 62. The PIC was unsuccessful and a group discussion is demanded. 63. Preference for more than only the people within 120 m to be notified for future meetings.</p>	<p>Project Information Concerns:</p> <ul style="list-style-type: none"> • Future public meetings and information about this proposed development will be communicated in the future. • In our experience, an Open House Drop-In format is appropriate for this type of approval. • The 120m area for notification of the meeting is a requirement of the Planning Act.

A copy of the provided comments has been included in Appendix E.

Next Steps

As the project progresses there will be additional opportunities for the surrounding land owners to provide comments and concerns, which is a statutory requirement under the Planning Act. These meetings will offer the public, local residents and interested stakeholders the opportunity to review the project and continue to offer feedback.

Appendix A

Figure 1.0 – Location Map





1027458 ONTARIO INC.
 BANWELL AND McHUGH -
 MIXED USE DEVELOPMENTS
 PUBLIC INFORMATION CENTRE

LOCATION MAP
 FIGURE 1.0



BANWELL AND McHUGH -
 NORTH 'A' SITE
 (±1.43ha / 3.54ac)



BANWELL AND McHUGH -
 NORTH 'B' SITE
 (±1.66ha / 4.11ac)



BANWELL AND McHUGH -
 SOUTH SITE
 (± 5.35ha / 13.23ac)



PROJECT:
 STATUS: DRAFT
 DATE: 12/04/2023

File Location:
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 22-5266 - banwell sites - addresses.dwg
 April, 12, 2023 10:00 AM

SOURCE: CITY OF WINDSOR
 City Council Agenda - Monday, October 30, 2023
 Page 86 of 457

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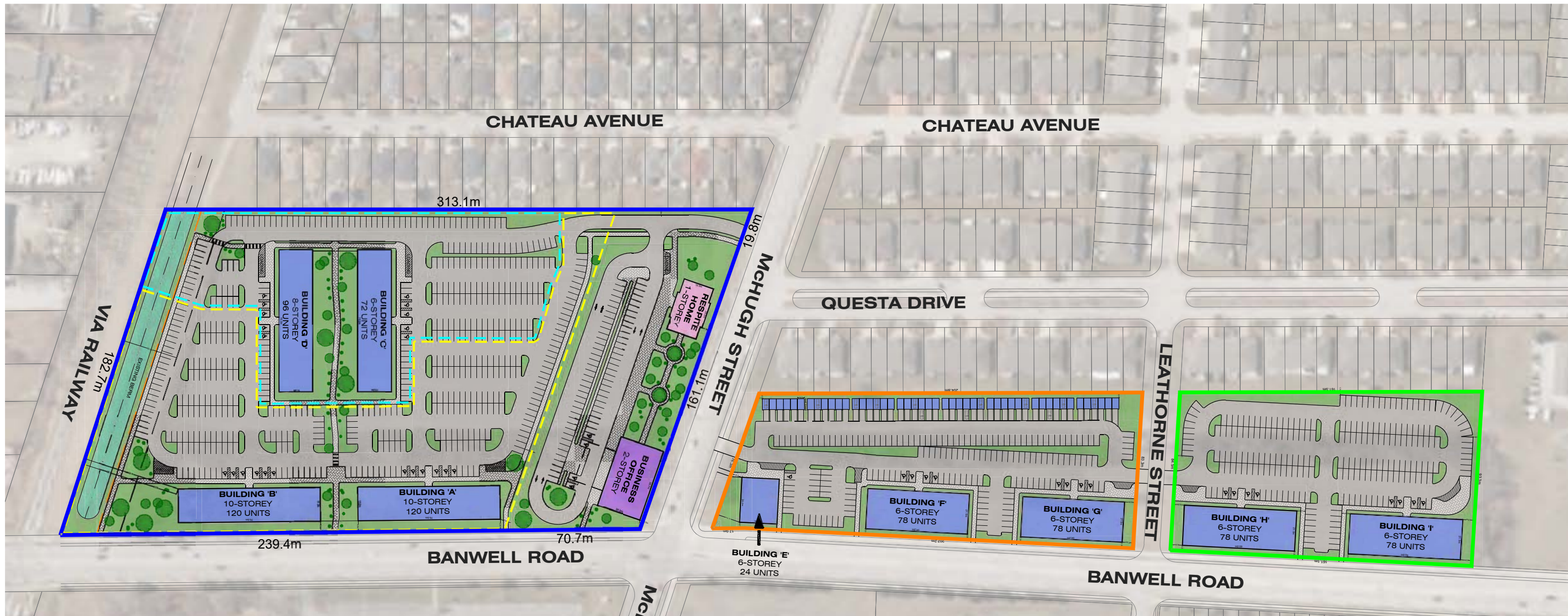
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 DESIGNED BY: RR

SCALE: N.T.S.



Appendix B

Figure 4.0 – Conceptual Development Plan



SOUTH SITE - RESIDENTIAL DEVELOPMENT DETAILS:

TOTAL PROPOSED UNITS:	408
BUILDING 'A':	10-STOREY, 120 UNITS
BUILDING 'B':	10-STOREY, 120 UNITS
BUILDING 'C':	6-STOREY, 72 UNITS
BUILDING 'D':	8-STOREY, 96 UNITS
PROPOSED PARKING SPACES:	531
PROPOSED PARKING RATE:	1.30
LANDSCAPED OPEN SPACE:	
(incl. trails):	38.2%
LOT COVERAGE:	12.7%

SOUTH SITE - BUSINESS OFFICE DEVELOPMENT DETAILS:

GFA:	1860m ²
REQUIRED PARKING SPACES:	56
PROVIDED PARKING SPACES:	60

SOUTH SITE - RESPITE HOME DEVELOPMENT DETAILS:

GFA:	499m ²
REQUIRED PARKING SPACES:	12
PROVIDED PARKING SPACES:	12

NORTH 'B' SITE - RESIDENTIAL DEVELOPMENT DETAILS:

TOTAL PROPOSED UNITS:	180
BUILDING 'E':	6-STOREY, 24 UNITS
BUILDING 'F':	6-STOREY, 78 UNITS
BUILDING 'G':	6-STOREY, 78 UNITS
PROPOSED PARKING SPACES:	270
PROPOSED PARKING RATE:	1.50
LANDSCAPED OPEN SPACE:	
(incl. trails):	36.4%
LOT COVERAGE:	23.2%

NORTH 'A' SITE - RESIDENTIAL DEVELOPMENT DETAILS:

TOTAL PROPOSED UNITS:	156
BUILDING 'H':	6-STOREY, 78 UNITS
BUILDING 'I':	6-STOREY, 78 UNITS
PROPOSED PARKING SPACES:	227
PROPOSED PARKING RATE:	1.45
LANDSCAPED OPEN SPACE:	
(incl. trails):	24.5%
LOT COVERAGE:	9.02%

1027458 ONTARIO LTD.
BANWELL AND McHUGH -
MIXED USE DEVELOPMENTS
PUBLIC INFORMATION CENTRE

CONCEPTUAL DEVELOPMENT PLAN
Figure 4.0

BANWELL AND McHUGH - SOUTH SITE (± 5.35 ha / 13.23 ac)	BANWELL AND McHUGH - NORTH 'A' SITE (± 1.43 ha / 3.54 ac)	BANWELL AND McHUGH - NORTH 'B' SITE (± 1.66 ha / 4.11 ac)	PROPOSED MULTI-UNIT RESIDENTIAL	PROPOSED LANDSCAPED OPEN SPACE	SOUTH SITE - PHASE 1
30m BUFFER FROM RAILWAY	EXTENT OF BERM	EXISTING BERM	PROPOSED BUSINESS OFFICES	PROPOSED RESPITE HOME	SOUTH SITE- PHASE 2

File Location:
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SCALE: 1: 2000 (11X17)



PROJECT: 22-5144
STATUS: DRAFT
DATE: 12/04/2023

Appendix C

Notice of Resident's Meeting



Public Information Centre

Proposed Mixed Use Development Banwell Road and McHugh Street

On behalf of our client, 1027458 Ontario Inc., Dillon Consulting Limited is hosting a Public Information Centre to introduce three (3) proposed mixed use developments located in the South Neighbourhood area of the East Riverside Planning Area in the City of Windsor. During this session, guests will be able to:

- Meet the project team;
- Become informed about the planning process; and
- Discuss comments and questions relating to the proposed developments.

This meeting is the next step in the planning process to permit development of these lands for the proposed:

- **North 'A' Site** – Two (2), six (6)-storey multiple dwelling buildings with 156 dwelling units and 227 parking spaces total;
- **North 'B' Site** – Three (3), six (6)-storey multiple dwelling buildings with 180 dwelling units and 270 parking spaces total;
- **South Site** – One (1), six (6)-storey multiple dwelling, one (1), eight (8)-storey multiple dwelling, and two (2), 10-storey multiple dwellings with 408 units and 531 parking spaces total, one (1) two (2)-storey Office Building with associated parking; and one (1) Residential Care Facility with associated parking.

This meeting is being held in advance of a statutory public meeting required under the *Planning Act* to obtain approvals. The City of Windsor will be inviting all residents and landowners within 120 metres of the property limits to additional meetings in the near future.

The Public Information Centre will be a drop-in format with project materials available for viewing and representatives available to chat.

We are looking for your input and comment. Written comments, via mail or email, will be accepted until May 3, 2023.



Public Information Centre

Date:

Wednesday, April 19, 2023

Time:

6:00 pm to 8:00 pm

Location:

WFCU Centre – Reception Hall
8787 McHugh Street, N8S 0A1

Contact:

Theresa O'Neill, Planner
Dillon Consulting Limited
3200 Deziel Drive, Suite 608, Windsor, N8W 5K8
T – 519.438.1288 ext.1286
southneighbourhood@dillon.ca

Application to City of Windsor

Public
Information Centre

WE
ARE
HERE

Summary of Resident
Comments

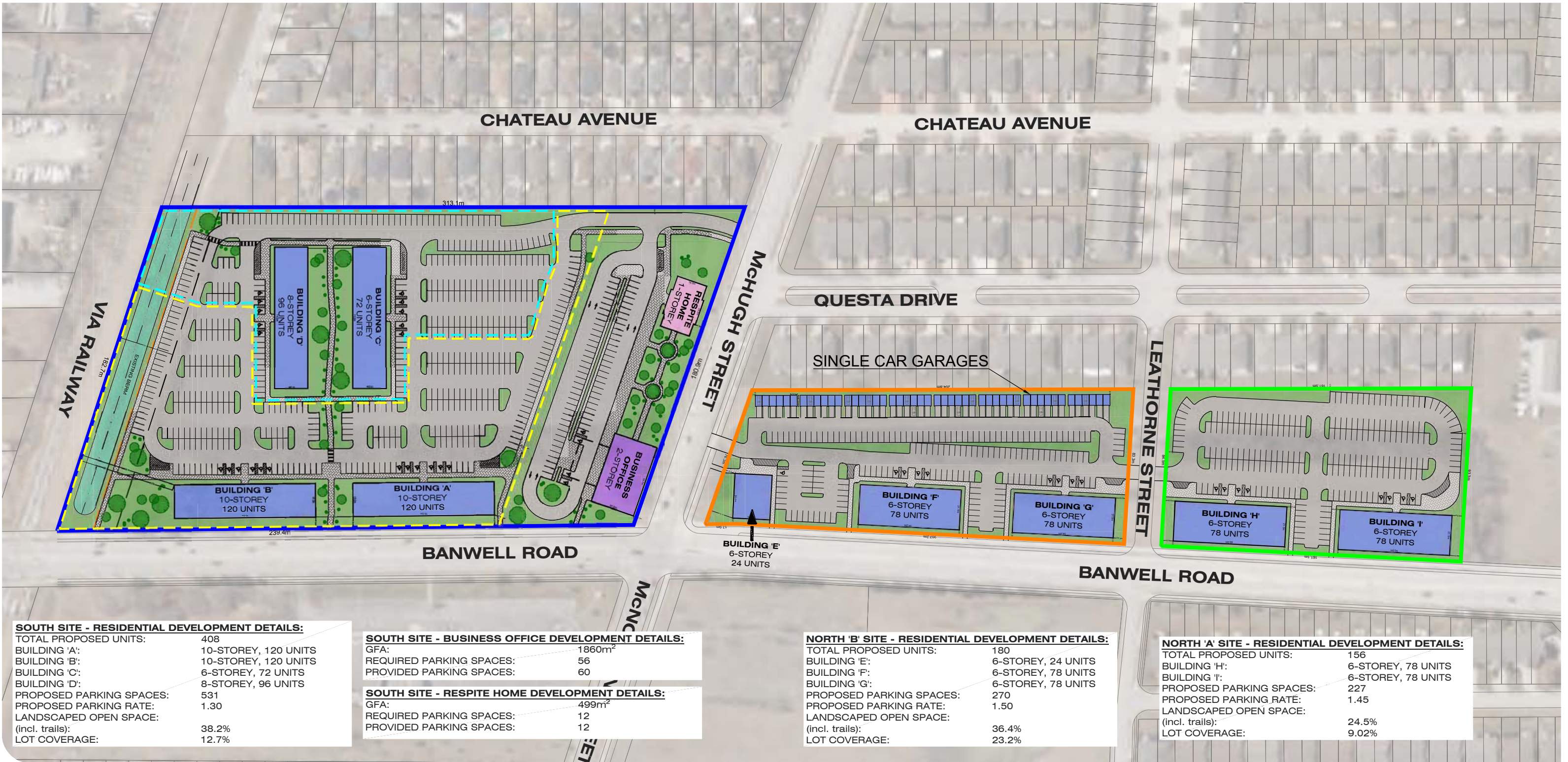
Application Review by
City of Windsor

Development & Heritage
Standing Committee

Council Meeting



April 5, 2023



SOUTH SITE - RESIDENTIAL DEVELOPMENT DETAILS:

TOTAL PROPOSED UNITS:	408
BUILDING 'A':	10-STOREY, 120 UNITS
BUILDING 'B':	10-STOREY, 120 UNITS
BUILDING 'C':	6-STOREY, 72 UNITS
BUILDING 'D':	8-STOREY, 96 UNITS
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PROPOSED PARKING RATE:	1.30
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(incl. trails):	38.2%
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PROPOSED PARKING RATE:	1.50
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PROPOSED PARKING RATE:	1.45
LANDSCAPED OPEN SPACE:	
(incl. trails):	24.5%
LOT COVERAGE:	9.02%

1027458 ONTARIO LTD.
BANWELL AND McHUGH -
MIXED USE DEVELOPMENTS

CONCEPTUAL DEVELOPMENT PLAN
April 4, 2023

BANWELL AND McHUGH - SOUTH SITE (± 5.35 ha / 13.23 ac)	BANWELL AND McHUGH - NORTH 'A' SITE (±1.43 ha / 3.54 ac)	BANWELL AND McHUGH - NORTH 'B' SITE (±1.66 ha / 4.11 ac)	PROPOSED MULTI-UNIT RESIDENTIAL	PROPOSED LANDSCAPED OPEN SPACE	SOUTH SITE - PHASE 1
30m BUFFER FROM RAILWAY	EXTENT OF BERM	EXISTING BERM	PROPOSED BUSINESS OFFICES	PROPOSED RESPITE HOME	SOUTH SITE- PHASE 2

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SCALE: 1: 2000 (11X17)



PROJECT: 22-5144
STATUS: DRAFT
DATE: 04/03/2023

Appendix D

PIC Presentation Boards



PUBLIC INFORMATION CENTRE

EAST RIVERSIDE
SOUTH NEIGHBOURHOOD
MIXED USE DEVELOPMENTS

The Public Information Centre is being hosted to introduce and provide information on three (3) proposed mixed-use developments.

The Public Information Centre is being hosted in a “drop-in” format with representatives available to take comments and answer questions.


The goals of the Public Information Centre are as follows:

1. Provide information on the planning process and development approvals process;
2. Provide information on the proposed mixed-use developments;
3. Provide details on anticipated timelines;
4. Collect comments and feedback from attendees to help guide the proposed developments as they continue through the development process.

Collecting Feedback:

1. Comment forms are available for attendees to complete;
2. Comments may also be emailed to **southneighbourhood@dillon.ca**;
3. All comments will be compiled at the end of the meeting and will be included in a report summarizing the meeting;
4. All comments will be accepted until **May 3, 2023.**

To Submit Comments: southneighbourhood@dillon.ca


DILLON
CONSULTING

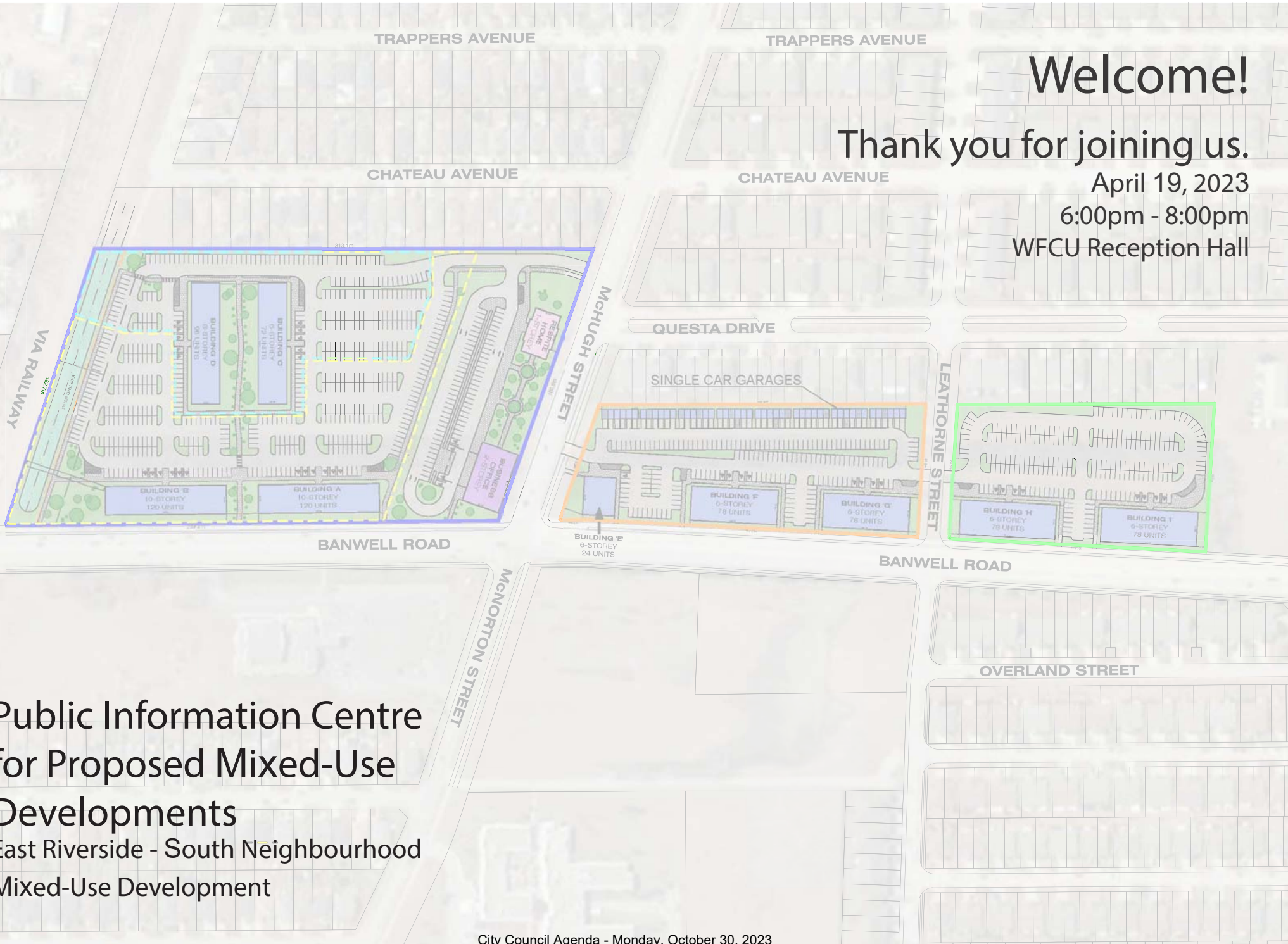
Welcome!

Thank you for joining us.

April 19, 2023

6:00pm - 8:00pm





WFCU Reception Hall



Public Information Centre
for Proposed Mixed-Use
Developments
East Riverside - South Neighbourhood
Mixed-Use Development

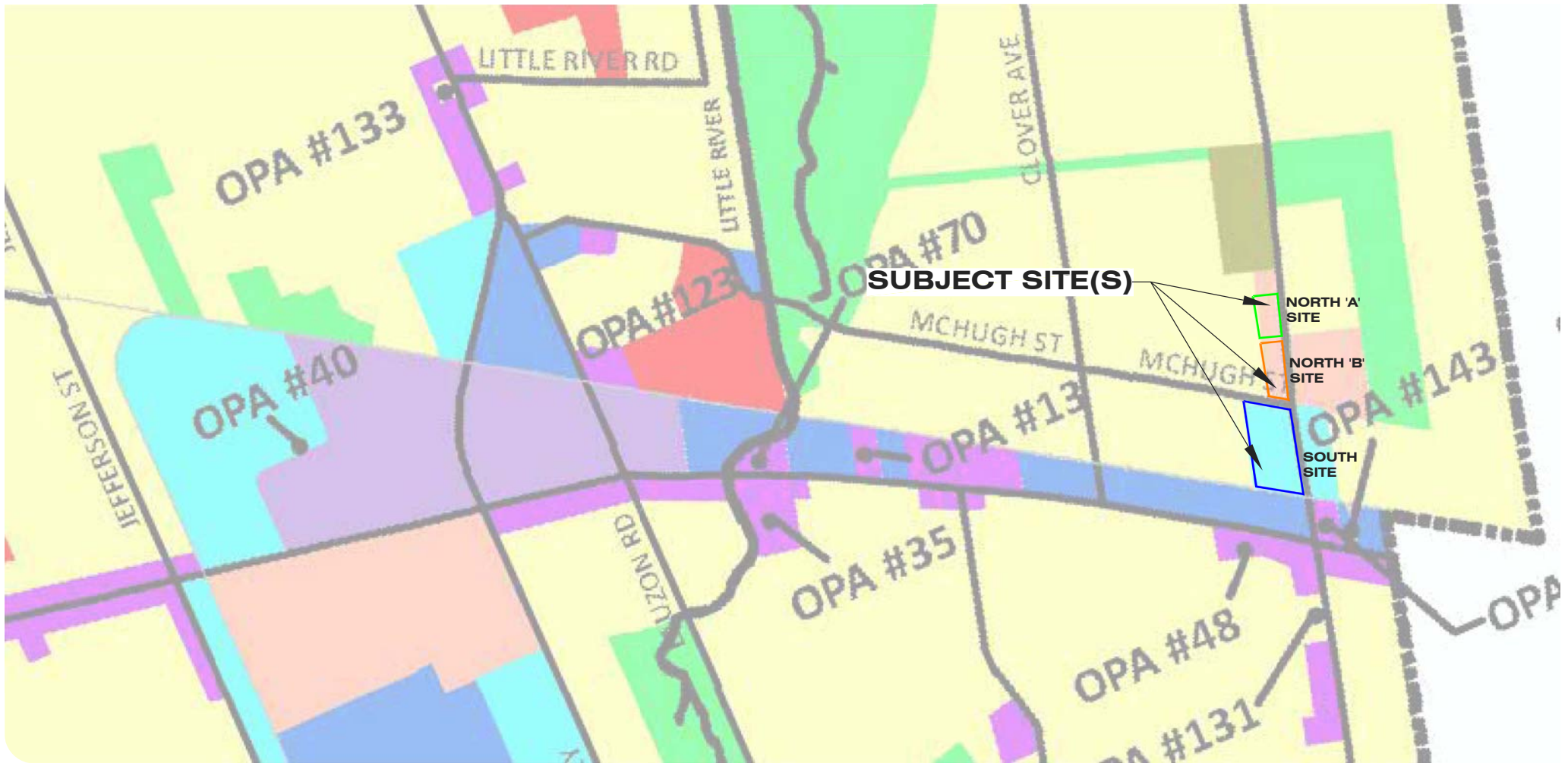


1027458 ONTARIO INC.
 BANWELL AND McHUGH -
 MIXED USE DEVELOPMENTS
 PUBLIC INFORMATION CENTRE

 BANWELL AND McHUGH - NORTH 'A' SITE (±1.43ha / 3.54ac)	 BANWELL AND McHUGH - NORTH 'B' SITE (±1.66ha / 4.11ac)	 BANWELL AND McHUGH - SOUTH SITE (± 5.35ha / 13.23ac)	
<small>PROJECT:</small> STATUS: DRAFT DATE: 12/04/2023			


LOCATION MAP
 FIGURE 1.0


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 SCALE: N.T.S.





1027458 ONTARIO LIMITED
 BANWELL AND MCHUGH -
 MIXED USE DEVELOPMENTS
PUBLIC INFORMATION CENTRE

**EXISTING CITY OF WINDSOR OFFICIAL
 PLAN DESIGNATION
 FIGURE 2.0**

 BANWELL AND MCHUGH -
 NORTH 'A' SITE
 (± 1.43ha / 3.54ac)

 BANWELL AND MCHUGH -
 NORTH 'B' SITE
 (± 1.66ha / 4.11ac)

 BANWELL AND MCHUGH -
 SOUTH SITE
 (± 5.35ha / 13.23ac)

 BUSINESS PARK	 RESIDENTIAL	 WATERFRONT RESIDENTIAL	 MIXED USE
 MAJOR INSTITUTIONAL	 OPEN SPACE	 WATERFRONT RECREATION	 NATURAL HERITAGE
 COMMERCIAL CENTRE	 COMMERCIAL CORRIDOR	 WATERFRONT PORT	 INDUSTRIAL

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SCALE: N.T.S.



PROJECT: 22-5144 & 22-5266
 STATUS: DRAFT
 DATE: 04/14/2023

SOURCE: THE CITY OF WINDSOR OFFICIAL PLAN
 - SCHEDULE D: LAND USE



1027458 ONTARIO LIMITED
BANWELL AND McHUGH - MIXED USE DEVELOPMENTS
PUBLIC INFORMATION CENTRE

EXISTING CITY OF WINDSOR ZONING BY-LAW 8600
FIGURE 3.1

BANWELL AND McHUGH - NORTH 'A' SITE (±1.43ha / 3.54ac)	BANWELL AND McHUGH - NORTH 'B' SITE (±1.66ha / 4.11ac)	BANWELL AND McHUGH - SOUTH SITE (±5.35ha / 13.23ac)	ZONE BOUNDARY	SPECIFIC ZONING EXEMPTIONS	SPECIFIC ZONING REGULATION
			RD - Residential Districts CD - Commercial Districts	ID - Institutional Districts MD - Manufacturing Districts	GD - Green Districts

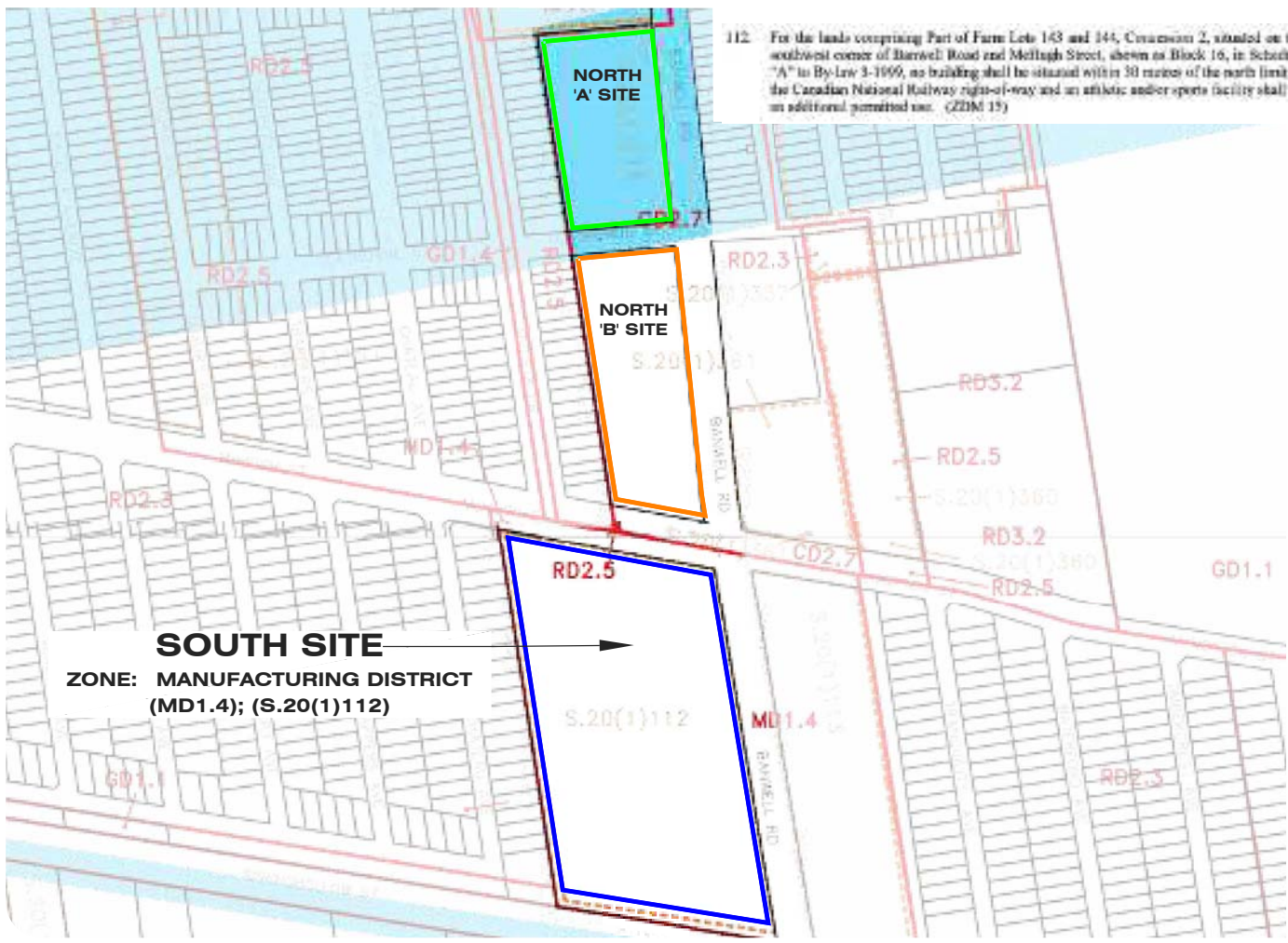
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SOURCE: THE CITY OF WINDSOR - ZONING BY-LAW 8600 MAP

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SCALE: N.T.S.

PROJECT: 22-5144 & 22-5266
STATUS: DRAFT
DATE: 17/04/2023



112. For the lands comprising Part of Farm Lots 143 and 144, Concession 2, situated on the southwest corner of Banwell Road and McHugh Street, shown as Block 16, in Schedule "A" to By-Law 3-1999, no building shall be situated within 30 metres of the north limit of the Canadian National Railway right-of-way and an athletic and/or sports facility shall be an additional permitted use. (ZDM 15)

18.4 MANUFACTURING DISTRICT 1.4 (MD1.4)

18.4.1 PERMITTED USES

- | | |
|--|---|
| <ul style="list-style-type: none"> Auto/Service Service Bakery Business Office Commercial School Food Catering Service Food Packaging Facility | <ul style="list-style-type: none"> Manufacturing Facility Medical Appliance Facility Medical Office Micro-Brewery Professional Studio Research and Development Facility |
|--|---|

Any of the following Auxiliary Uses:

- | | |
|--|---|
| <ul style="list-style-type: none"> Child Care Centre Club Convenience Store Food Convenience Store Food Outlet - Drive-through Food Outlet - Take-out Gas Bar | <ul style="list-style-type: none"> Health Studio Personal Service Shop Restaurant Restaurant with Drive-through Warehouse Wholesale Store |
|--|---|

Any of the following Existing Uses:

- Motor Vehicle Dealership
- Sports Facility
- Transport Terminal

Any use accessory to any of the above uses, including a Retail Store

18.4.2 PERMITTED USES

- Outdoor Storage Yard

18.4.5 PROVISIONS

- | | |
|---|----------------------------------|
| 1 Lot Width - minimum | 30.0 m |
| 4 Building Height - maximum | 20.0 m |
| 5 Front Yard Depth - minimum | 9.0 m |
| 6 Rear Yard Depth - minimum | 9.0 m |
| From a rear lot line that abuts a lot on which a dwelling or dwelling unit is located | |
| 7 Side Yard Width - minimum | 9.0 m |
| From a side lot line that abuts a lot on which a dwelling or dwelling unit is located or from a side lot line that abuts a street | |
| 8 Landscaped Open Space Yard - minimum | 11.0% of lot area |
| 10 Gross Floor Area - Retail Store - maximum | 25.0% of the GFA of the lot area |
| 30 All activities or uses shall take place entirely within a fully enclosed building. This provision does not apply to the following activity or use: child care centre, gas bar, loading area, outdoor eating area, parking area, parking space, sports facility, or refueling area. | |

1027458 ONTARIO LIMITED
BANWELL AND McHUGH - MIXED USE DEVELOPMENTS
PUBLIC INFORMATION CENTRE

EXISTING CITY OF WINDSOR ZONING BY-LAW 8600
FIGURE 3.2

- BANWELL AND McHUGH - NORTH 'A' SITE (±1.43ha / 3.54ac)
- BANWELL AND McHUGH - NORTH 'B' SITE (±1.66ha / 4.11ac)
- BANWELL AND McHUGH - SOUTH SITE (±5.35ha / 13.23ac)

- ZONE BOUNDARY
- SPECIFIC ZONING EXEMPTIONS
- SPECIFIC ZONING REGULATION

- | | | |
|----------------------------|------------------------------|----------------------|
| RD - Residential Districts | ID - Institutional Districts | GD - Green Districts |
| CD - Commercial Districts | MD - Manufacturing Districts | |

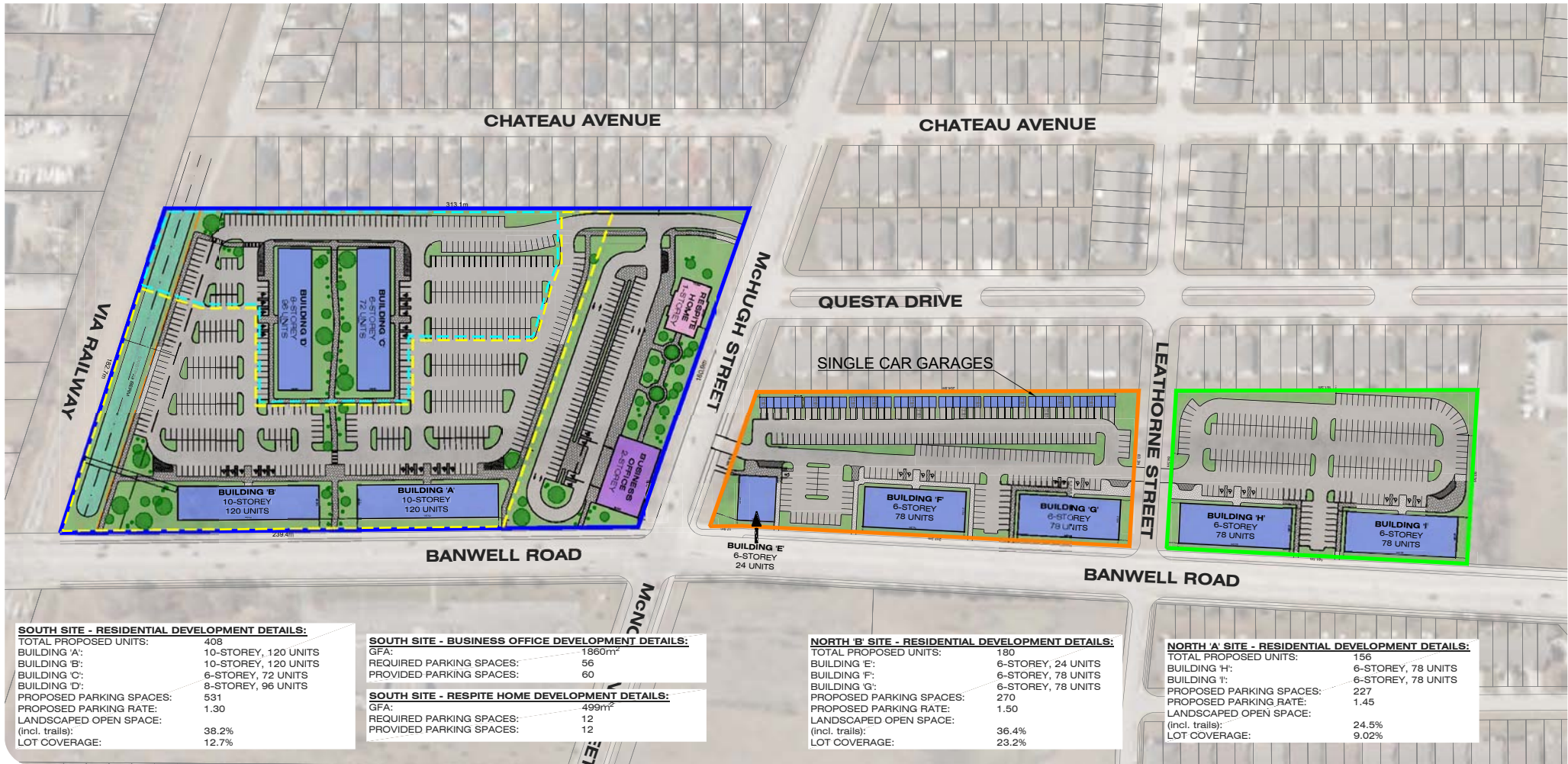
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MAP/DRAWING INFORMATION
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DESIGNED BY: RR

SCALE: N.T.S.

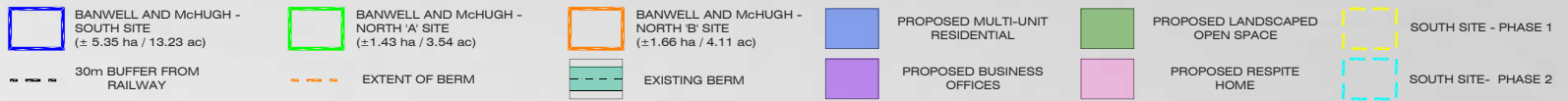


PROJECT: 22-5144 & 22-5266
STATUS: DRAFT
DATE: 17/04/2023



1027458 ONTARIO LTD.
BANWELL AND McHUGH -
MIXED USE DEVELOPMENTS

CONCEPTUAL DEVELOPMENT PLAN
FIGURE 4.0



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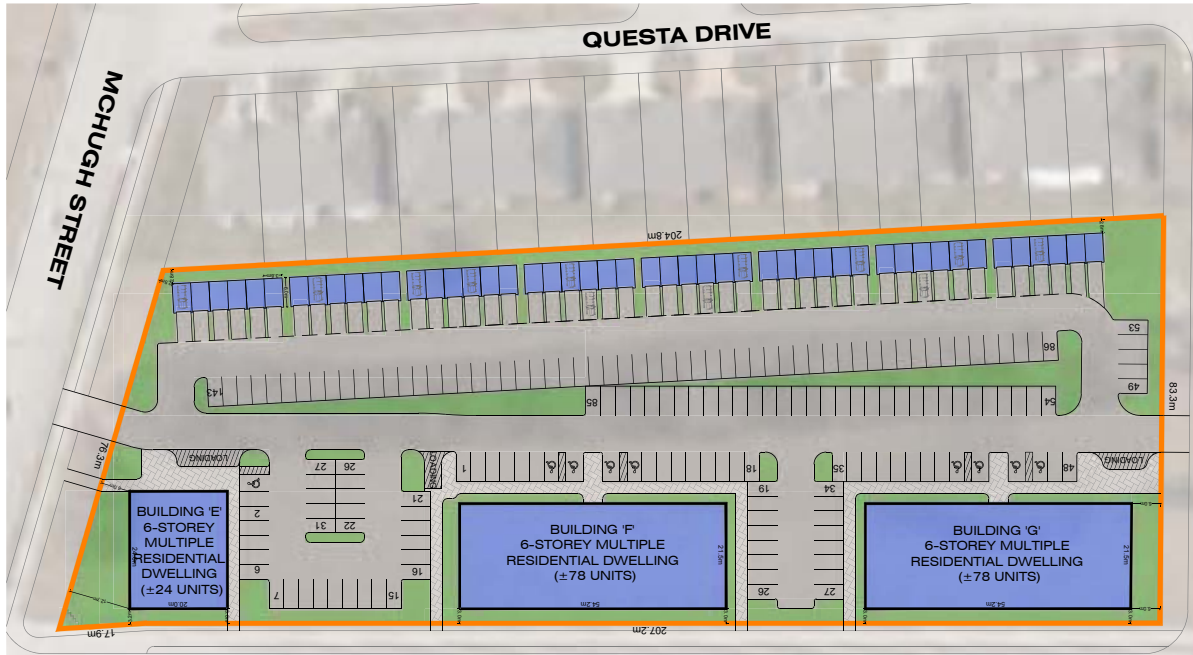
SOURCE: THE COUNTY OF ESSEX INTERACTIVE MAPPING (2021)

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SCALE: 1: 2000 (11X17)



PROJECT: 22-5144
STATUS: DRAFT
DATE: 04/03/2023



BANWELL ROAD

BANWELL ROAD

NORTH 'A' SITE - RESIDENTIAL DEVELOPMENT DETAILS:


PROPOSED UNITS:	156
PROPOSED PARKING SPACES:	227
PROPOSED PARKING RATE:	1.45
LANDSCAPED OPEN SPACE: (incl. trails):	24.5%
LOT COVERAGE:	9.02%

NORTH 'B' SITE - RESIDENTIAL DEVELOPMENT DETAILS:

PROPOSED UNITS:	180
PROPOSED PARKING SPACES:	270
PROPOSED PARKING RATE:	1.50
LANDSCAPED OPEN SPACE: (incl. trails):	36.4%
LOT COVERAGE:	23.2%

1027458 ONTARIO LTD.
BANWELL AND McHUGH -
MIXED USE DEVELOPMENTS
PLANNING JUSTIFICATION REPORT

**CONCEPTUAL DEVELOPMENT PLAN -
NORTH SITE 'A' AND 'B'
FIGURE 5.0**

 BANWELL AND McHUGH - NORTH 'A' SITE (± 1.43 ha / 3.54 ac)	 PROPOSED BUILDINGS	 PROPOSED LANDSCAPED AREA	 PROPOSED SIDEWALK	 PROPOSED PAVEMENT
 BANWELL AND McHUGH - NORTH 'B' SITE (± 1.66 ha / 4.11 ac)				

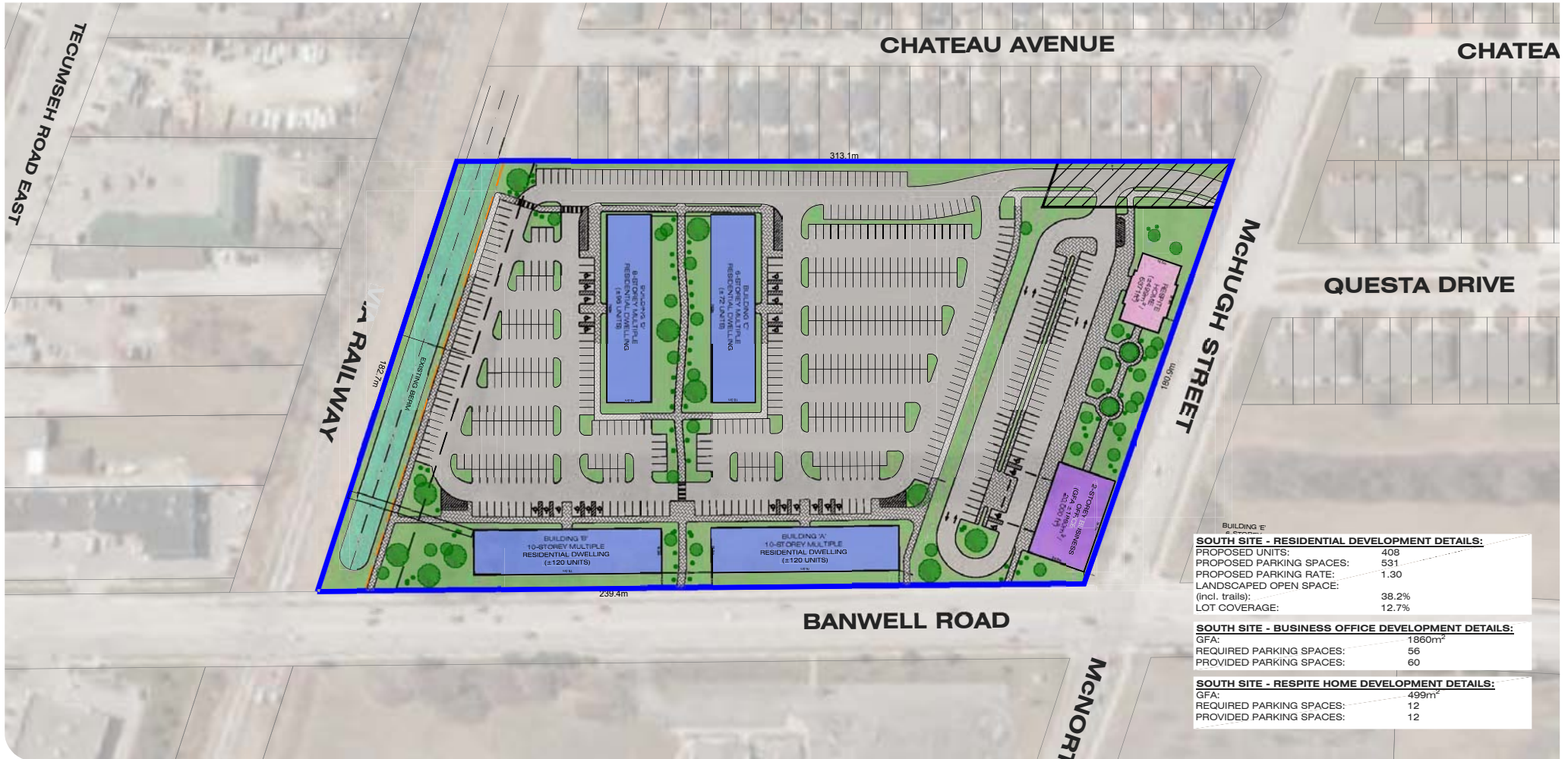
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DESIGNED BY: MRU

SCALE: 1:1000 (11X17)



PROJECT: 22-5266
STATUS: DRAFT
DATE: 12/06/2022



SOUTH SITE - RESIDENTIAL DEVELOPMENT DETAILS:	
PROPOSED UNITS:	408
PROPOSED PARKING SPACES:	531
PROPOSED PARKING RATE:	1.30
LANDSCAPED OPEN SPACE:	
(incl. trails):	38.2%
LOT COVERAGE:	12.7%

SOUTH SITE - BUSINESS OFFICE DEVELOPMENT DETAILS:	
GFA:	1860m ²
REQUIRED PARKING SPACES:	56
PROVIDED PARKING SPACES:	60

SOUTH SITE - RESPITE HOME DEVELOPMENT DETAILS:	
GFA:	499m ²
REQUIRED PARKING SPACES:	12
PROVIDED PARKING SPACES:	12

1027458 ONTARIO LTD.
BANWELL AND McHUGH -
MIXED USE DEVELOPMENTS
PLANNING JUSTIFICATION REPORT

CONCEPTUAL DEVELOPMENT PLAN -
SOUTH SITE
FIGURE 6.0

BANWELL AND McHUGH - SOUTH SITE (± 5.35 ha / 13.23 ac)	AREA SUBJECT TO SHARED ACCESS AGREEMENT (± 0.13ha / 0.32ac)	PROPOSED MULTI-UNIT RESIDENTIAL	PROPOSED LANDSCAPED OPEN SPACE	PROPOSED RESPITE HOME
30m BUFFER FROM RAILWAY	EXTENT OF BERM	EXISTING BERM	PROPOSED BUSINESS OFFICE BUILDING	PROPOSED SIDEWALK / MULTI-USE TRAIL

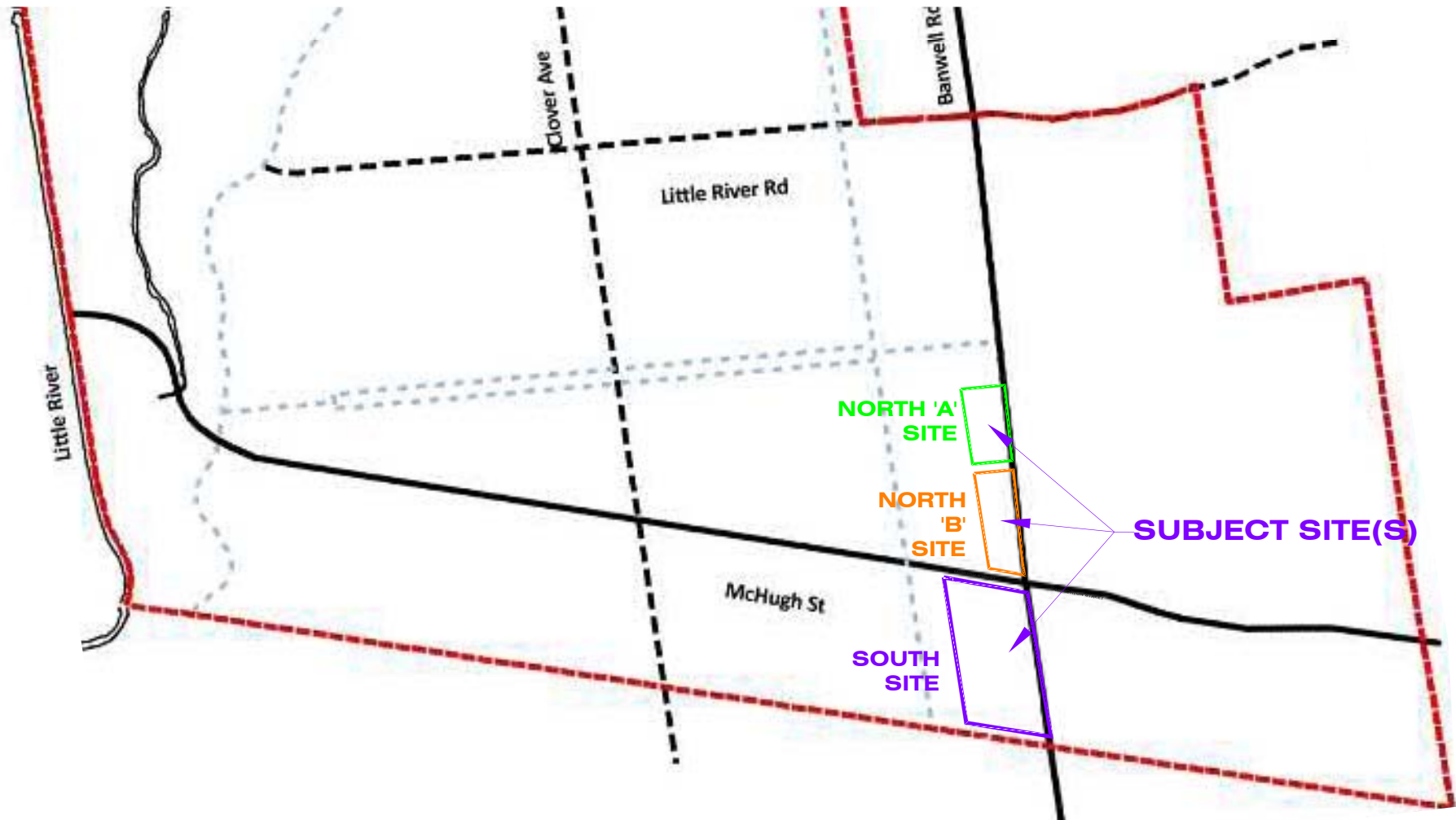
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 March, 27, 2023 11:09 AM

SOURCE: THE COUNTY OF ESSEX INTERACTIVE MAPPING (2021)

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SCALE: 1:1500 (11X17)


PROJECT: 22-5144
 STATUS: DRAFT
 DATE: 03/21/2023




1027458 ONTARIO INC.
 BANWELL AND McHUGH -
 MIXED USE DEVELOPMENTS
 PUBLIC INFORMATION CENTRE

**CITY OF WINDSOR: EAST RIVERSIDE
 MAJOR ROADS
 FIGURE 7.0**


 BANWELL AND McHUGH -
 NORTH 'A' SITE
 (±1.43ha / 3.54ac)


 BANWELL AND McHUGH -
 NORTH 'B' SITE
 (±1.66ha / 4.11ac)

 BANWELL AND McHUGH -
 SOUTH SITE
 (± 5.35ha / 13.23ac)

 Class II Arterial

 Class II Collector

 Class II Collector

 Boundary of Planning Area

File Location:
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SCALE: N.T.S.



PROJECT: 22-5144
 STATUS: DRAFT
 DATE: 14/04/2023

SOURCE: CITY OF WINDSOR - OFFICIAL PLAN,
 CHAPTER 2, SCHEDULE 'ER-4': MAJOR ROAD



MARCH 21 - 7:00 AM



MARCH 21 - 8:00 AM




MARCH 21 - 9:00 AM





MARCH 21 - 10:00 AM

1027458 ONTARIO INC.
 Banwell and McHugh
 Mixed Used Development
 PUBLIC INFORMATION CENTRE

SHADOW IMPACT STUDY
 MARCH 21ST - 7:00 AM - 10:00 AM
 FIGURE 8.1

 BANWELL AND MCHUGH - SOUTH SITE
 (± 5.35 ha / 13.23 ac)

 BANWELL AND MCHUGH - NORTH 'A' SITE
 (± 1.43 ha / 3.54 ac)

 BANWELL AND MCHUGH - NORTH 'B' SITE
 (± 1.66 ha / 4.11 ac)

 PROPOSED SHADOW

 EXISTING SHADOW

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PROJECT: 22-5144
 STATUS: FINAL
 DATE: 2023/04/14



1027458 ONTARIO INC.
 Banwell and McHugh
 Mixed Used Development
 PUBLIC INFORMATION CENTRE

SHADOW IMPACT STUDY
 MARCH 21ST - 3:00 PM - 5:00 PM
 FIGURE 8.2

BANWELL AND McHUGH - SOUTH SITE
 (± 5.35 ha / 13.23 ac)

BANWELL AND McHUGH - NORTH 'A' SITE
 (± 1.43 ha / 3.54 ac)

BANWELL AND McHUGH - NORTH 'B' SITE
 (± 1.66 ha / 4.11 ac)

PROPOSED SHADOW

EXISTING SHADOW

File Location:
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 CHECKED BY: TJO
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SCALE: N.T.S





PROJECT: 22-5144
 STATUS: FINAL
 DATE: 2023/04/14




1027458 ONTARIO INC.
 Banwell and McHugh
 Mixed Used Development
 PUBLIC INFORMATION CENTRE

SHADOW IMPACT STUDY
 JUNE 21ST - 6:00 AM - 10:00 AM
 FIGURE 9.1

 BANWELL AND McHUGH - SOUTH SITE
 (± 5.35 ha / 13.23 ac)

 BANWELL AND McHUGH - NORTH 'A' SITE
 (± 1.43 ha / 3.54 ac)

 BANWELL AND McHUGH - NORTH 'B' SITE
 (± 1.66 ha / 4.11 ac)

 PROPOSED SHADOW

 EXISTING SHADOW

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
PROJECT: 22-5144
 STATUS: FINAL
 DATE: 2023/04/14




1027458 ONTARIO INC.
 Banwell and McHugh
 Mixed Used Development
 PUBLIC INFORMATION CENTRE

SHADOW IMPACT STUDY
 JUNE 21ST - 12:00 PM - 6:00 PM
 FIGURE 9.2

 BANWELL AND McHUGH - SOUTH SITE
 (± 5.35 ha / 13.23 ac)

 BANWELL AND McHUGH - NORTH 'A' SITE
 (± 1.43 ha / 3.54 ac)

 BANWELL AND McHUGH - NORTH 'B' SITE
 (± 1.66 ha / 4.11 ac)

 PROPOSED SHADOW

 EXISTING SHADOW

File Location:
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SCALE: N.T.S



PROJECT: 22-5144
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 DATE: 2023/04/14



1027458 ONTARIO INC.
 Banwell and McHugh
 Mixed Used Development
 PUBLIC INFORMATION CENTRE

SHADOW IMPACT STUDY
 SEPTEMBER 21ST - 7:00 AM - 10:00 AM
 FIGURE 10.1

BANWELL AND McHUGH - SOUTH SITE
 (± 5.35 ha / 13.23 ac)

BANWELL AND McHUGH - NORTH 'A' SITE
 (± 1.43 ha / 3.54 ac)

BANWELL AND McHUGH - NORTH 'B' SITE
 (± 1.66 ha / 4.11 ac)

PROPOSED SHADOW

EXISTING SHADOW

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 DESIGNED BY: RR

SCALE: N.T.S





PROJECT: 22-5144
 STATUS: FINAL
 DATE: 2023/04/14




1027458 ONTARIO INC.
 Banwell and McHugh
 Mixed Used Development
 PUBLIC INFORMATION CENTRE

SHADOW IMPACT STUDY
 SEPTEMBER 21ST - 12:00 PM - 5:00 PM
 FIGURE 10.2

 BANWELL AND McHUGH - SOUTH SITE
 (± 5.35 ha / 13.23 ac)

 BANWELL AND McHUGH - NORTH 'A' SITE
 (±1.43 ha / 3.54 ac)

 BANWELL AND McHUGH - NORTH 'B' SITE
 (±1.66 ha / 4.11 ac)

 PROPOSED SHADOW

 EXISTING SHADOW

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SCALE: N.T.S



PROJECT: 22-5144
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1027458 ONTARIO INC.
 Banwell and McHugh
 Mixed Used Development
 PUBLIC INFORMATION CENTRE

SHADOW IMPACT STUDY
 DECEMBER 21ST - 8:30 AM - 11:00 AM
 FIGURE 11.1

BANWELL AND McHUGH - SOUTH SITE
 (± 5.35 ha / 13.23 ac)

BANWELL AND McHUGH - NORTH 'A' SITE
 (±1.43 ha / 3.54 ac)

BANWELL AND McHUGH - NORTH 'B' SITE
 (±1.66 ha / 4.11 ac)

PROPOSED SHADOW

EXISTING SHADOW

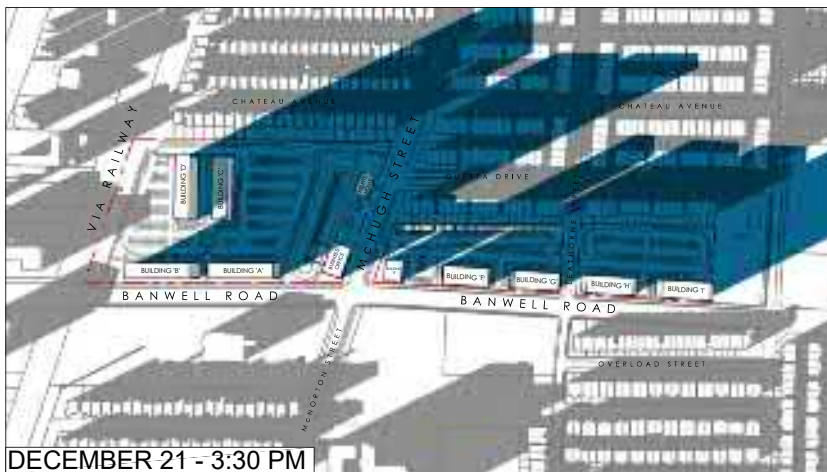
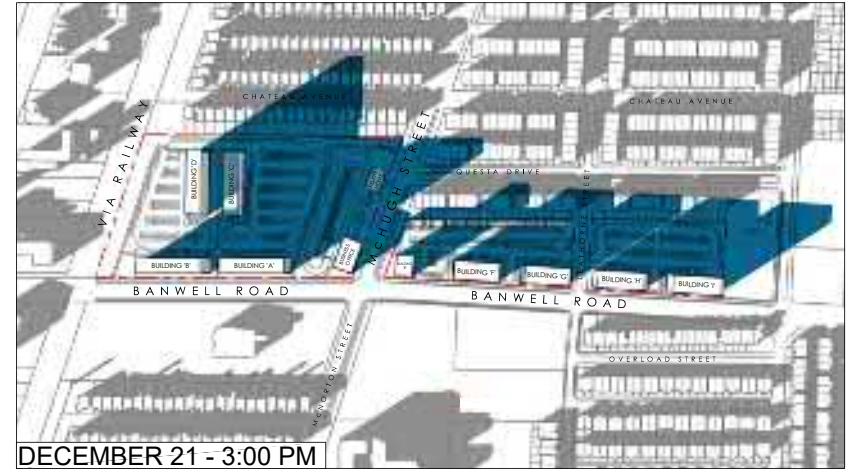
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



PROJECT: 22-5144
 STATUS: FINAL
 DATE: 2023/04/14




1027458 ONTARIO INC.
 Banwell and McHugh
 Mixed Used Development
 PUBLIC INFORMATION CENTRE

SHADOW IMPACT STUDY
 DECEMBER 21ST - 2:00 PM - 3:30 PM
 FIGURE 11.2

 BANWELL AND McHUGH - SOUTH SITE
 (± 5.35 ha / 13.23 ac)

 BANWELL AND McHUGH - NORTH 'A' SITE
 (± 1.43 ha / 3.54 ac)

 BANWELL AND McHUGH - NORTH 'B' SITE
 (± 1.66 ha / 4.11 ac)

 PROPOSED SHADOW

 EXISTING SHADOW

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PROJECT: 22-5144
 STATUS: FINAL
 DATE: 2023/04/14

Appendix E

Resident's Comments



East Riverside – South Neighbourhood – Mixed Use Development Comment Form

Residents Meeting – April 19, 2023

Please complete this form and return it to Dillon Consulting Limited. Information will be collected in accordance with Ontario’s Municipal Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record.

Name:	Email:
North ‘A’ Site:	North ‘B’ Site:
South Site:	

Please see reverse of page



General Comments and Feedback:

I have lived in Quetta for 22 years. I realize that there is a need for roads. I'm against that the lights will come into my back. I would want a fence (mixyl) to stop the lights & people sitting through. I am worried about security and flooding. There won't be enough parking for 2 cars so people will park on the street and no room left for our visitors.

Dillon Consulting Limited
3200 Deziel Drive, Suite 608
Windsor, ON N8W 5K8

Tel: 519-438-1288 Ext. 1286

Email:

southneighbourhood@dillon.ca

Attention: Theresa O'Neill, Planner

Please deposit this form in the comment box or email comment responses.

All comments will be received until May 3, 2023

East Riverside – South Neighbourhood – Mixed Use Development Comment Form

Residents Meeting – April 19, 2023

Please complete this form and return it to Dillon Consulting Limited. Information will be collected in accordance with Ontario’s Municipal Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record.

Name: [REDACTED]	Email: [REDACTED]
North ‘A’ Site:	North ‘B’ Site:
South Site:	

Please see reverse of page



General Comments and Feedback:

AS A NEIGHBOUR OF 1732 BANWELL RD, WE ARE CONCERNED ABOUT LIMITED APPROXIMATEY OF THE BUILDINGS THAT MAY AFFECT "SITE LINES" FOR DRIVER'S + PEDESTRIAN'S SAFETY. (WE "WITNESSED" A "FATALITY" AT BANWELL + FIRGROVE, IN 2022, PROBABLY DUE TO THE CURRENT POOR "SITE LINES" FROM RECENTLY BUILT BENCING + HIGH RISE BUILDINGS @ 1675 BANWELL).

ALSO, WE STRONGLY FEEL, THERE SHOULD BE A TRAFFIC LIGHT INSTALLED @ BANWELL + FIRGROVE. "THIS COULD SAVE MANY LIVES."

Dillon Consulting Limited
3200 Deziel Drive, Suite 608
Windsor, ON N8W 5K8

Tel: 519-438-1288 Ext. 1286

Email:

southneighbourhood@dillon.ca

Attention: Theresa O'Neill, Planner

Please deposit this form in the comment box or email comment responses.

All comments will be received until May 3, 2023

East Riverside – South Neighbourhood – Mixed Use Development Comment Form

Residents Meeting – April 19, 2023

Please complete this form and return it to Dillon Consulting Limited. Information will be collected in accordance with Ontario's Municipal Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record.

Name: [REDACTED]	Email: [REDACTED]
North 'A' Site:	North 'B' Site: 2248 Questa Dr.
South Site:	[REDACTED]

Please see reverse of page

General Comments and Feedback:

Said frankly very "honest"
 I am quite disappointed that my quiet residential neighborhood is going to be saturated with ¹⁰ HUGE DEVELOPMENTS!! These 6 Storey Buildings will infringe upon our privacy and the amount of sunlight we will enjoy in the Summer months (all year really!!) ~~the~~ the lights that will be erected ~~then~~ ^{cause} our backyards and bedrooms to be lit up like a runway!!! The infrastructure in this area cannot possibly accomodate all these new residents. There

will be no way to exit from the area housing Buildings A, B, C, D other than McHugh. We already have experienced multiple ^{car} accidents on McHugh and Banwell because of the amount of cars on the road. I would like to request a 10ft "Vinyl" fence between our properties on the east side of Questa and not wood!! Wood

Please deposit this form in the comment box or email comment responses.

Dillon Consulting Limited
 3200 Deziel Drive, Suite 608
 Windsor, ON N8W 5K8

Tel: 519-438-1288 Ext. 1286
 Email: southneighbourhood@dillon.ca

Attention: Theresa O'Neill, Planner

* The value of our properties ^{will} decrease!!!
 What about our sewer system? We've already experienced flooding?
 I'm not happy about all the natural trees that will be destroyed. They provide us with privacy ^{enjoyment}
 would create too much maintenance and who will do the upkeep!!
 All comments will be received until May 3, 2023



East Riverside – South Neighbourhood – Mixed Use Development Comment Form

Residents Meeting – April 19, 2023

Please complete this form and return it to Dillon Consulting Limited. Information will be collected in accordance with Ontario's Municipal Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record.

Name:	Email:
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback:

Sewerbackup - Flooding - into our homes = Sewerbackup Claims, increasing insurance prices.

traffic - safety. - neighbourhood is full of kids. that play on streets.

Shadow from buildings

McHugh / Banwell / side streets already horrible to get out of

where will the traffic flow? hopefully not through our streets that are already tightly packed.

parking? where will people park? Questa is already having issues with the new buildings on Banwell

not wanting to pay for parking - they are parking on their streets already.

we also on Chateaux have no where to park - Drive through our streets - there is no parking.

What about the animals that are back there?

Please deposit this form in the comment box or email comment responses.

All comments will be received until May 3, 2023

Colo caught build for 20+ years now
you can't

Dillon Consulting Limited
3200 Deziel Drive, Suite 608
Windsor, ON N8W 5K8

Tel: 519-438-1288 Ext. 1286

Email:

southneighbourhood@dillon.ca

Attention: Theresa O'Neill, Planner

East Riverside – South Neighbourhood – Mixed Use Development

Comment Form

Residents Meeting – April 19, 2023

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Name: [REDACTED]	Email: [REDACTED]
North 'A' Site: SHOULD BE A PARK SITE, OR BUILDING OF NEW RETIREMENT HOME. AGING POPULATION IN AREA.	North 'B' Site:
South Site:	

Please see reverse of page



General Comments and Feedback:

INCREASED POPULATION - 744 UNITS X AVG. 2/PER UNIT
1488 PEOPLE, BETWEEN 744 AND 1488 + VEHICLES
INCREASED SEWER OUTPUT, TRAFFIC, NOISE.

RETENTION POND AT BLUE HERON - IT ACTUALLY
OVER FLOWED IN 2016, ALONG WITH NUMEROUS
HOUSES ON KIRKLAND (WHERE I LIVE) SO OBVIOUSLY/
RETENTION POND COULD POSSIBLY NOT
BE SUFFICIENT FOR ALL OF THIS

ADDITIONAL PEOPLE IN A SMALL AREA.
NEED STOP LIGHTS TO GET OUT ONTO
BANWELL FROM LEATHORNE + FIRGROVE
VERY DANGEROUS NOW WITH LIMITED
SITE LINES DUE TO FENCING + BUSHES.

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Name:	Email:
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page



General Comments and Feedback:

I am concern about the new project that is coming to our neighbourhood. Our street is already packed with lots of traffic and building new project would bring chaos. More people means more crowd. We are concern about the garbage that will be. Our kids will not be able to enjoy nature freely since ~~but~~ destroying nature and building ground apartment complex for the sake of money.

Our neighbourhood is concern about safety, environment, ~~and~~ freedom of kids. garbage.

I will like to stop the project that is being plan posed.

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Name: [REDACTED]	Email: [REDACTED]
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback:

We are very concerned about the density of the proposed development, especially the inclusion of multiple 8 storey buildings. As local residents, we are very concerned that the development will exit onto McHugh - as the only exit. Congestion of traffic will be a huge problem on McHugh and Barwell.

Additional concerns that development work with city and schools to ensure that are adequate schools associated with every new development.

This project permanently removes green space !!

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Name: [REDACTED]	Email: [REDACTED]
North 'A' Site:	North 'B' Site:
South Site: Property value will go down on my house. 2628 Chateaux Ave.	

Please see reverse of page

General Comments and Feedback:

I have been a original owner of my house. We back onto the field on Bonwell 2628 Chateaub Ave. We enjoy the deer, Turkeys, falcons, pheasants all the ~~the~~ time. We have maintained a large area of grass attached to the field for the entire time we have lived there. I have a pool, and now a large condo will block the sun I get in my back yard, and plus take away my privacy. Question is ~~is~~, we will have parking issue which we already do in are area. What about garbage. I don't want deal with rats! What are you plan is for privacy? I would like a privacy fence placed at least 10 feet tall vinyl. Also want the buildings farther away. Also what about the lights? ~~what about~~ I don't want street lights in my backyard. The noise the lights. Everything we do not support this. I would rather have house behind my house, then condos. This is agreed! ~~that~~

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No respect for the people who have lived here for 20+ year!

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Name:	Email:
North ‘A’ Site:	North ‘B’ Site:
South Site:	

Please see reverse of page



East Riverside – South Neighbourhood – Mixed Use Development

Comment Form

Residents Meeting – April 19, 2023

General Comments and Feedback:

QUESTA RESIDENT: NOT INTERESTED IN THIS DEVELOPMENT
AT ALL. KEEP THE NEIGHBOURHOOD SINGLE FAMILY
TOWN HOUSES OR HOUSES.

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East Riverside – South Neighbourhood – Mixed Use Development Comment Form

Residents Meeting – April 19, 2023

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Name: [REDACTED]	Email: [REDACTED]
North 'A' Site: Concerns for construction of 744 residential units on Banwell plus another 157 units across the road!	North 'B' Site:
South Site:	

Please see reverse of page



East Riverside - South Neighbourhood - Mixed Use Development

Comment Form

Residents Meeting - April 19, 2023

General Comments and Feedback:

PS AT 88 YRS OLD THE ONLY GOOD SIDE OF IT, I MIGHT NOT SEE IT TO PUT UP WITH IT THERE IS A GOOD ENDING TO (CONGESTION) 55 YRS MAY GOD BE WITH US

I moved from 1251 Parkview close to the new construction at corner of Laurier Rd & McHugh. I was a senior 88 yrs old I am not living in the past, I believe in people having new condos apt's Home etc

But not overloading on the above enough is enough moved to Chateau Lake Obo wife + I. Love it, until now my understanding 6 new apts on Banwell - ridiculous (overcrowding TRAFFIC CONGESTION THE ALMIGHTY BUCK GREEN FROM Please deposit this form in the comment box or email comment responses.

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OUTSIDE MONEY INSIDE MONEY

Project #'s: 22-5144, 22-5266

I MIGHT AS WELL AS ...



East Riverside – South Neighbourhood – Mixed Use Development Comment Form

Residents Meeting – April 19, 2023

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Name: [REDACTED]	Email: [REDACTED]
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page



General Comments and Feedback:

I understand the need for more housing soon but should it not be more family type dwellings instead of apartments? I am happy to see this empty land being developed - it has been discussed several times over the last few years - but this seems like over saturation!

I live on Arvilla and I usually have to wait for a significant time to be able to turn onto Banwell due to the traffic. This construction will definitely affect the traffic flow on Banwell both north & south. The present condition of Banwell Road surface is not ideal especially in the winter months with freezing on the surface as it buckles up. As a retired teacher I am concerned if there

is a significant increase in children living in these units where will they attend school? Tecumseh Vista Academy is already over capacity. I taught Grade 2 in a portable at Eastwood School for 12 years. This is not an ideal situation. Has any thought been given to the added impact of water drainage & flood prevention? We endured that sewer fl in 2016 & little has been done since to prevent that again. Adding this increase in units will definitely impact the problem. As an 80 year old this will not impact me as much as all the younger people living in the nearby housing

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of this construction.

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Name:	[REDACTED]	Email:	
North 'A' Site:		North 'B' Site:	
South Site:			

Please see reverse of page

East Riverside – South Neighbourhood – Mixed Use Development

Comment Form

Residents Meeting – April 19, 2023

General Comments and Feedback:

Safety of my husband and I who have health conditions - and sold our home in Riverside to live in a SAFE, QUIET, neighbourhood without 100's of more traffic and residents of ~~nearby~~ BANWELL from train tracks to FIRGROVE driving over speed limit on Chateau St.

ALSO

EULFEN Mc CANN

- ① Definitely danger of flooding, because of overflow
- ② School buses drive speed down

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Name: [REDACTED]	Email: [REDACTED]
North 'A' Site:	North 'B' Site: 9
South Site:	

Please see reverse of page

East Riverside – South Neighbourhood – Mixed Use Development

Comment Form

Residents Meeting – April 19, 2023

General Comments and Feedback:

CHATEAU

I LIVE ON A CORNER. I ALREADY HAVE PROBLEMS WITH PEOPLE FROM ACROSS THE STREET PARKING TOO CLOSE TO MY DRIVEWAY (SOMETIMES, 3 DAYS) THEY DO IT TO EVERYONE ON THIS-NORTH SIDE OF McHUGH ^{SOUTH}
MANY TIMES I HAVE A PROBLEM WITH TRAFFIC BECAUSE THIS IS ALSO A BUS LINE
MY CONCERN IS FLOODING, IS THERE GOING TO BE A INFRASTRUCTURE IN PLACE TO TAKE CARE OF THIS
MY HEALTH IS POOR - I HATE THE SMELL OF ~~EXHAUST~~ EXHAUST PIPES. I HAVE A BAD HEART AND TROUBLE BREATHING



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East Riverside – South Neighbourhood – Mixed Use Development Comment Form

Residents Meeting – April 19, 2023

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Name: [REDACTED]	Email: [REDACTED]
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page

East Riverside – South Neighbourhood – Mixed Use Development

Comment Form

Residents Meeting – April 19, 2023

General Comments and Feedback:

Make the roads on Banwell, McHugh, Clower and surrounding areas safe from speeding cars. Build grade schools in area. Address parking for condos so people can park at the condos and not on our streets. Install traffic lights at Little River Road & Banwell, install traffic lights at Firgrove & Banwell for better traffic control. No environmental study on increased housing.

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Name: [REDACTED]	Email:
North 'A' Site: [REDACTED]	North 'B' Site:
South Site:	

Please see reverse of page



Comment Form

Residents Meeting – April 19, 2023

General Comments and Feedback: 7 PM City

- Worry about:

- traffic
- parking on Ovesta + Chateau from apartment residents
- flooding
- traffic already bad at light. We have all Condos now. Apartments will be overkill.

- Condo's or apartments for rent

- Heared to income?

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Comment Form

Residents Meeting – April 19, 2023

General Comments and Feedback:

I oppose the building of this large hi-rise development in my neighbourhood. This will change the character of our neighbourhood, making it very high density. The local infrastructure is not prepared for this. I am particularly concerned about the sewers + traffic. Having had basement flooding 5 years ago, I cannot endure another flood. Banwell is only 4 lanes + the attempt at improving the Banwell - McHugh intersection is inadequate. Carving a left-turn lane out of another traffic lane (McHugh going east) is very dangerous, with the speed many drivers on Banwell make that left turn. I am part of a large group of Seniors, living in this area, whose lives will be totally disrupted by this development. I am also concerned that many existing residents will attempt to sell and property values will fall. *Margaret Kappeler*

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General Comments and Feedback:

- TRAFIC is horrible now & people (pedestrians) have been hit & killed at the corner of Banwell & Fugiove trying to get across.
- Delivery people are parking on Banwell in front of the apartments and there are no stopping signs already in place.
- There is a childrens playground on Banwell and there are no signs relating to this for traffic.

We definitely need something done about speeding traffic on Banwell between McHugh & Little River Rds.

Action must be taken before all the new buildings are constructed. We need action on the traffic problem before it all gets worse. More people means more traffic!



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East Riverside – South Neighbourhood – Mixed Use Development Comment Form

2248

Residents Meeting – April 19, 2023

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Name: [Redacted]	Email: [Redacted]
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page



General Comments and Feedback:

WE LIVED Here 23 YRS. - PARTIALLY FLOODED ONCE.
I BELIEVE the BERM HAS SERVED to HELP with FLOODING
WOULD Like to Keep it. AS EXTRA PRECAUTION

AS FAR AS PARKING - A PARKING PERMIT OUT FRONT OF
OUR ADDRESS.

WHAT IS the PURPOSE OF 10' CLEARANCE
FROM OUR LOT LINE to the fence?



Four horizontal lines for additional handwritten comments.

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Name: [REDACTED]	Email:
North 'A' Site: TRAFFIC & SAFETY FLOODING - PRIVACY Fence NO MAINTENANCE MAINTENANCE 10' H. ALSO WILL THEIR PROPERTY LEVEL BE HIGHER than OURS.	North 'B' Site:
South Site:	

Please see reverse of page



General Comments and Feedback:

- TRAFFIC IS HORRIBLE ALREADY - HOW MUCH MORE CAN WE STAND

- TRUCKS ALLOWED ON ROAD NOW - SEMI TRUCK WENT DOWN THE OTHER DAY

- A PERSON WAS ALREADY KILLED AT BANWELL + FERGROVE

- PEOPLE PARKING IN FRONT OF THE APARTMENT WITH FLASHERS ON
EVEN THOUGH THERE ARE NO PARKING SIGNS

- NOT ENOUGH ~~STREET~~ PARKING SPOTS FOR ALL THE RESIDENCE IN THE APARTMENTS SO THEY ARE ALL PARKING ON OUR STREETS - IF YOU HAVE COMPANY THERE IS NO WHERE FOR THEM TO PARK

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- THERE IS A CHILDRENS PLAYGROUND AT ELIZABETH KESHKON PARK

- CARS DRAG RACE AT NIGHT

- THE ~~SOUND~~ ^{SOUND} OF THE CARS REFLECT ~~OFF~~ ^{OFF} THE BUILDINGS

- NO ONE GOES 50 MILES PER HOUR

Please deposit this form in the comment box or email comment responses.

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- WHERE IS OUR PEDESTRIAN CROSS WALK AT FERGROVE + BANWELL



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Name: [REDACTED]	Email: [REDACTED]
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page

East Riverside – South Neighbourhood – Mixed Use Development

Comment Form

Residents Meeting – April 19, 2023

General Comments and Feedback: The noise in area will increase.

- This area is a natural habitat for wild turkeys, Rabbits, etc
- The buildings are 6 stories high which will not give any privacy to condos on the east side of Questa.
- The car numbers for residence and their visitors will cause Questa to take the overflow.
- The traffic in this area will be congesting to say the least, when all is complete.
- The natural trees (mature) will be removed for parking. (More Positive Note.)
- limit the height of the buildings to no more than 3 stories would halve the numbers of residences and traffic
- The value of all Questa properties will be significantly reduced!!!

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(one) story → Can this be park property or perhaps commercial (variety)

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Name:	Email:
North 'A' Site:	North 'B' Site:
I'd expect from the likes of our current City of K. admin. & is why I voted	
South Site:	
<u>AGAINST DREW</u>	

Please see reverse of page

General Comments and Feedback:

Absolutely out rageous! I am disgusted at the leadership of Windsor East that would allow this absolute tyranny of BIG BUSINESS to RUIN neighbourhoods for our retirees & elderly.

I am fully against this RAPE of my neighbourhood. Shame & shame on you all. How about something that ENTHANCES the community for these people and still provides you with your PROFIT? I hate the idea of development but OBVIOUSLY something was going to get there. A luncheonette or coffee shop, dry cleaners, strip mall convenience store, library — THINGS they can walk to and ENJOY.

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AND THIS IS AN OUTRAGE AND THIS TRAVESTY + is exactly what

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Name:	Email:
North ‘A’ Site:	North ‘B’ Site:
South Site:	

Please see reverse of page



General Comments and Feedback:

- were just not happy about this project because, of the following reasons.
- 1.) every day heavy traffic,
 - 2.) for our safety for our family, no more privacy.
 - 3.) Insurance and Tax go up
 - 4.) Flooding Issue, before no problem.
 - 5.) no more sunrise, affecting our heating & cooling bill.
 - 6.) more people, more criminals in our area.
 - 7.) around windsor lot of empty land, they can move the project there.
 - 8.) not happy,

not good luck

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Name: [REDACTED]	Email: [REDACTED]
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback:

WE LIVE IN THIS NEIGHBOURHOOD FROM THE BEGINNING. WHEN WE MOVE IN, WE WERE TOLD THAT THERE ^{WAS} GOING TO BE PLAZA. PLAZA IS ACETABLE, BUT BIG BUILDING NOT. EVEN NOW, WE HAVE TO WAIT TO CROSS MC HUGH, IMAGINE WITH 600 CARS. WHERE IS PARK FOR KIDS? WHAT IS GOING TO HAPPEN TO FLOODING WHAT IS GOING TO HAPPEN TO DEERS THAT LIVE IN SMALL FOREST. YOU GUYS THINK OF MONEY, MONEY. THINK OF PEOPLE THAT ALREADY LIVE HERE. HOUSES ARE ACETABLE BUT BIG BUILDING IN THIS SMALL AREA NOT, NOT.

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Name:	Email:
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback: — writing for my neighbour who cannot write.

Flooding Concerns, Basement Flooding.

Street congestion — no lights.

— Composite solid fence to give privacy — and stop people walking through our properties.

— eviction of animals.

Air Quality — cars defrosting in morning engines running —

Absolutely

shocking.

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Name:	Email:
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback:

Flooding concerns - there is a subterranean stream running thru the proposed parking lot - It will be problematic for our all ready flood prone Basements.

- Need a composite solid fence Round parking lot we do not need lights in our bedroom windows all night -

- No traffic lights anywhere - Dangerous for all people

- Air Quality while building will Be awful and continue with traffic fumes.

- No space for animals we have deer, turkeys a fox - all sorts of creatures will have nowhere to go.

This is a terrible idea - a lot of modification is needed and a bit more compassion - This is completely

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Unacceptable - Please Rethink.

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Name:	Email:
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback:

This project will greatly impact the area in so many negative ways, it will create a lot of traffic that this neighbourhood is not equipt to handle including parking issues, safety issues - etc -

I strongly object the project as it will change the quite safe residential area to a crowded, unsafe busy area -



Dillon Consulting Limited
 3200 Deziel Drive, Suite 608
 Windsor, ON N8W 5K8

Tel: 519-438-1288 Ext. 1286
 Email:
southneighbourhood@dillon.ca

Attention: Theresa O'Neill, Planner

Please deposit this form in the comment box or email comment responses.

All comments will be received until May 3, 2023

East Riverside – South Neighbourhood – Mixed Use Development Comment Form

Residents Meeting – April 19, 2023

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Name: [REDACTED]	Email: [REDACTED]
North 'A' Site:	North 'B' Site:
South Site: We live directly behind Buildings C4D.	

Please see reverse of page

General Comments and Feedback:

- Vinyl privacy fence 10'
- no access @ berm
- lower storey buildings (maybe all 6 storey)
- indoor garbages
- move buildings closer to banwell. (C&D).
- Lighting (parking lot locations).
- wildlife displacement.
- flooding issues??
- where will overflow parking be?
- prefer single storey dwellings :)
- Excess traffic.
- school bus route.

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Name: [REDACTED]	Email: [REDACTED]
North 'A' Site:	North 'B' Site:
South Site: We live directly behind the buildings D and C on Chateau.	

Please see reverse of page

General Comments and Feedback:

I am not in agreement with the plans.

- I believe that the 2 buildings at the back of the property are too high
- Move the buildings closer to Banwell or on Mottugh
- We need a fence for sure
- Enclosed garbage areas
- No access at the barn - fence all the way through.
- Vinyl privacy fence 10 ft high
- Parking lighting will be a concern
- shade for my pool
- water that pools and floods the field every storm
- animals - lots of deer, turkey, coyotes

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Name:	Email:
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback:

Hello:

This project will affect the whole neighbourhood

- Flooding
- traffic volume, not safe^{for} families with kids! (intersects)
- no parking enough

Overall this project is really a bad news.

I object this project

not safe for our place and
our kids.

Crowded area,

pollution from cars will add
bad environment for the health.

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Name:	Email:
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback:

- The area may become flood prone
- The traffic volume will increase significantly
- The value of Questa residences will be reduced greatly!!! (∴ Taxes must be lowered.)
- Animal life will be chased out with turkeys
- Parking overflow will end up on Questa.
- Homes on Questa will lose privacy with 6 story high back yard neighbours

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East Riverside – South Neighbourhood – Mixed Use Development Comment Form

Residents Meeting – April 19, 2023

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Name: [REDACTED]	Email: [REDACTED]
North 'A' Site:	North 'B' Site:
South Site: 	

Please see reverse of page



General Comments and Feedback:

Traffic Jams, Safety, Flooding, 10 storey buildings

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Residents Meeting – April 19, 2023

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Name: [Redacted]	Email: [Redacted]
North 'A' Site: # 1904	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback:

744 UNITS WILL INVADE OUR NEIGHBOURHOOD.

CONGESTION RE: TRAFFIC INFRASTRUCTURE - WE ALREADY DEALT WITH FLOODING.

STOP LIGHTS - WILL THERE BE ANY?

IN OUR STREETS.

NO ROD IRON FENCING IN FRONT OF BUILDINGS - SIGNS LINES TO ENTER BANWELL

ROAD OBSTRUCTED.

THE QUESTIONS ARE SO MANY!! THE GATHERING HERE ARE FOLKS THAT EXPECTED TO BE SPOKEN TO AS A GROUP HAVING QUESTIONS + ANSWERS HEARD BY ALL. I FEEL INSULTED - ASK A QUESTION + ALL ANSWERS WERE WISHY WASHY - "OH COULD BE 6 STORES - COULD BY A 3 STOREY" "GARAGES?? OH MAYBE NOT!!" DETAILS RIDICULOUS - EVERYONE LEFT WITH NO SATISFACTION. "COCO?? YOU SAY" SHAME ON YOU!

Please deposit this form in the comment box or email comment responses.

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UPSET IS NOT EVEN CLOSE TO DESCRIBING THE MOOD HERE.

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Attention: Theresa O'Neill, Planner

East Riverside – South Neighbourhood – Mixed Use Development

Comment Form

Residents Meeting – April 19, 2023

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Name: [REDACTED]	Email: [REDACTED]
North 'A' Site: <i>even great impact on my quality of life. The proposed board fence behind the proposed building will fall down in a few years and are not not high enough! The big impact on the environment is disastrous! Shame on Coco!</i>	North 'B' Site: <i>The proposed building will fall down in a few years and are not high enough! The impact on the environment is disastrous!</i>
South Site: <i>Building these proposed building in a retirement community is taking a disadvantage of our elderly community</i>	

Please see reverse of page

General Comments and Feedback:

①
I am against this proposal. The traffic congestion will be increased and dangerous. There are already numerous accidents on Baswell. This will only increase. The impact on our sewer systems will be ~~strained~~ strained. Flooding will increase!!! The light pollution will increase. The sight lines onto Baswell will be obstructed. The natural ~~habitat~~ habitats will be destroyed, we ~~currently~~ currently have multiple bird species that live there. The limited sun exposure from the proposed buildings will limit my quality of life. The noise pollution from traffic will have an →

Please deposit this form in the comment box or email comment responses.

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Attention: Theresa O'Neill, Planner

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Name: [REDACTED]	Email: [REDACTED]
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback:

~~744~~ units = ↑ sewer ↑ flooding ↑ traffic ↑ noise ↑ congestion
'Figure & Banwell' → Black 'metal fence' = blind spot.
6 accidents, 1 death. *Need MORE traffic lights.

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Name:	Email:
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback:

I've read where they are wanting to build 10,000 units in the Windsor area.

* Why are we going to see almost 1/10 of those units in our little neighborhood? They should be spread out across the city.

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 Email: southneighbourhood@dillon.ca

Attention: Theresa O'Neill, Planner

[Redacted] 1829 Kirkland Ave.

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Name:	Email:
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback:

Why do your buildings need to look like 1970's
Soviet bloc architecture?

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Name:	Email:
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback:

Too much traffic - need lights at Ferguson & Buxwell
No stopping in front of present buildings to
deliver.

Longer lights at McHugh & Buxwell - when walking
can't make it across in one light.

3 lights on Buxwell between McHugh &
Ferguson. - please the traffic down

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Name: [REDACTED]	Email: [REDACTED]
North 'A' Site: <i>Current parking dilemma already from additional cars parking on Owista + Chateau from existing condos on Berwell.</i>	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback:

Loss of trees
~~Loss~~ *Not enough parking - already cars are parking on Questa & Chateau from the condos already on Barwell.*
Concern of all cars exiting on side roads & causing congestion
Need traffic lights on Barwell

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Name:	Email:
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback:

1. This project is not
zoned for Residential
It is zoned for commercial

Zoning

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East Riverside – South Neighbourhood – Mixed Use Development

Comment Form

Residents Meeting – April 19, 2023

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Name:	Email:
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback:

Limit height to ONLY 3 stories
this will reduce ~~the~~ the impact in
most areas or concerns by half.

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East Riverside – South Neighbourhood – Mixed Use Development

Comment Form


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Name: [REDACTED]	Email: [REDACTED]
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback:



 MY PROPERTY WILL LOSE VALUE, PARKING WILL HAPPEN ON THE
 SIDE STREETS AND WILL BE A NIGHTMARE FOR EXISTING RESIDENTS
 THE LIGHTS FROM PARKING LOTS WILL BE VERY BRIGHT.
 NATURE WILL BE GONE FROM AREA
 FLOODING WILL HAPPEN IN AREA – THE NOISE WILL INCREASE

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Name:	[Redacted]	Email:	[Redacted]
North 'A' Site:	[Redacted]	North 'B' Site:	[Redacted]
South Site:			

Please see reverse of page

General Comments and Feedback:

1. Unmanageable Traffic !!!
for such a small area
- traffic lights ~~needed~~

2. Sewer System needs upgrading
we already flood

3. Need high privacy
fence

4. High Buildings leave
us with NO view NO
Sunlight!

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This Project is not Zoned
Residential for ~~apartments~~ Apartment

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Name:	Email:
North 'A' Site:	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback:

Way too many dwellings!

Very concerned about the impact over 1,000 new cars will have on the neighbourhood traffic. It is getting difficult to cross McHugh street already with all the new construction in the WFCU area.

Tax payers should not be liable to pay for roads expansion to accommodate the traffic

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Attention: Theresa O'Neill, Planner

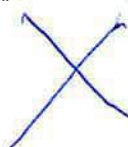
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Name: [REDACTED]	Email: [REDACTED]
North 'A' Site: 	North 'B' Site:
South Site:	

Please see reverse of page

General Comments and Feedback:

As a home owner on Questa Dr. I don't want this Project to move forward. My property's directly and negatively impacted by this project. The Value of my property is going to go down because of it. Privacy is a big concern. the bright lights at night is going to be a problem. Noise problems, traffic problems, Nature and wildlife will be destroyed. Floodings is also another of the problem will be having.

Vehicles will be parking in front of our houses all the time. this is a quiet and peaceful are. and we want it to be like this.

It should catch your attention that a single neighbor want this project. so please listen to the people.



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Neighbourhood, South <southneighbourhood@dillon.ca>

Attn: Theresa O'Neill, East Riverside-South Neighbourhood-Mixed Use Development

2 messages

Wed, May 3, 2023 at 2:57 PM

To: "Neighbourhood, South" <southneighbourhood@dillon.ca>

When we purchased our home on Questa Drive a large part of our choice was due to the beautiful green space behind, where we were reassured nothing major would be built. There is complete privacy day and night, We can open our drapes, open our windows and enjoy the retirement we have worked toward for many years, Wildlife is always there - eagles, hawks, deer, bunnies, doves mallard families, cardinals, blue jays, wild turkeys, squirrels, robins and many, many other small birds. There are always nice breezes. We get the early morning sun to start our day through our back patio doors This is a lovely quiet peaceful retirement community and our quality of living is wonderful.

We have easy access to drugstore, grocery stores, hospitals and other necessities of living, There is ample parking and we consider this area very safe to live in. This is a huge retirement community to which all these people made the choice to relocate because of what it represents to us for our retirement years.

This development will cause:

The **loss of green space a tree canopy, and wetland** which is so important to the environment, climate change and wildlife.

Between pavement and buildings, noxious car fumes, congestion of people and cars and loss of protective spaces for them, **wildlife will disappear.**

There will be about a 2 to 3 hours **loss of morning sunlight.** This light is very important to get in a middle unit that has no windows on the side, and for our mental health.

There will be a complete **loss of privacy** with hundreds of cars parked 20 feet from our bedroom window and six storey buildings overlooking our back deck and windows: now we will have to keep our blinds closed. With the cars there will be an excess of **fumes:** now we will have to close our windows. What will the effect be on those with COPD, asthma or any other respiratory ailments?

Excess noise - engines starting, stopping, car doors slamming, people talking: Ear plugs?

The car **lights** and apartment lights will light up our backyards all night long: now we will have to wear sleep masks.

The proposed 5.9 foot fence won't be the solution to any of these

There will be huge **garbage** containers outside our back door: stench and early morning garbage trucks.

Another concern is that with such a huge influx of people, there is more **crime.**

The **scariest outcome** of this development is the effect of 1300 plus additional vehicles concentrated in a 3 block area (in addition to the upcoming development on the other side of Banwell). The 3 buildings on Firgrove have already shown us: more traffic with speeding down our street, taking up our parking on our street, and one **fatality.** This area is too small to handle such an enormous number of people and vehicles safely, especially with many elderly who have canes, walkers, wheelchairs or just plain move a lot slower. Crossing the

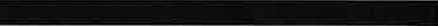
street will become a nightmare for them. I guess the answer would be for them to not even attempt to go anywhere. Just shut the door.

So our wonderful quality of life will now be shutting the door, closing the windows drawing the blinds, putting in ear plugs, and a sleep mask and live a different quality of life. That's progress?!



Neighbourhood, South <southneighbourhood@dillon.ca>

Wed, May 3, 2023 at 4:45 PM



We would like to thank you for taking the time to reach out to us with your concerns.

We acknowledge the many concerns raised regarding the proposed multi-unit residential buildings. Our team will be taking into consideration the thoughts shared during the Public Information Centre as well as the comments received through email and telephone.

The concerns relating to natural features, wildlife, potential shadow impacts, parking, traffic, the location of garbage facilities, and crime have been noted. These concerns will be taken into account during the City's review of the application. Issues relating to the location of the garbage disposal units would be addressed in later stages of the development approvals process, during Site Plan Control Approval (not yet initiated).

As per City requirements, a Natural Site Features Inventory & Preservation Plan, Shadow Impact Study and Traffic Impact Study (TIS) have been completed for the sites. Regarding keeping nearby residents informed, we are preparing an engagement summary of all of the comments and concerns raised at the meeting on April 19, 2022 as well as those via telephone and email, which will be available as part of the public record. The supporting materials associated with this proposal will be available on the City's website (*For Residents > Planning > Land Development > Development Applications > Current Development Applications*), at the time the City determines the application submission to be complete.

Future public meetings and information about this proposed development will be the responsibility of the City of Windsor as required by the *Planning Act*.

Please don't hesitate to reach out to your City Councilor, and the City Planner on file with any other comments or concerns.

Thank you,

Theresa

[Quoted text hidden]



Neighbourhood, South <southneighbourhood@dillon.ca>

Banwell, McHue, Chateau, Questa Project Concerns

2 messages

Thu, Apr 20, 2023 at 8:43 PM

To: southneighbourhood@dillon.ca

Hello Theresa,

Our property on Chateau Ave backs onto the area where the developer would like to build four apartment buildings.

Concerns:

- 1 The amount of dust this building project will create, My husband has asthma and I have COPD, which will make it impossible to use our back deck or yard during the building process.
2. Buildings create shade preventing sunlight from our property. We both suffer with SAD.
3. Flooding, infrastructure not in place to prevent future flooding.
4. Parking lots, cars idling will create a high level of exhaust fumes.
5. Apartment garbage disposal bins placed near our property line, i.e. odour, rats.
6. Safety concerns, increase in traffic volume and congestion on Chateau Ave, McHue St, Banwell Ave, Questa Ave, and other neighbouring streets.
7. Stress and anxiety created as a result of the noise and disruption to our lifestyle during the building process.
8. Request for compensation regarding our health and well being issues.

We do NOT support this proposal.

Neighbourhood, South <southneighbourhood@dillon.ca>

Mon, May 1, 2023 at 12:36 PM

We would like to thank you for taking the time to reach out to us with your concerns.

We acknowledge the many concerns raised regarding the proposed multi-unit residential buildings. Our team will be taking into consideration the thoughts shared during the Public Information Centre as well as the comments received through email and telephone.

The concerns relating to dust, potential shadow impacts, stormwater management, parking and traffic, and the location of garbage disposal areas have been noted. These concerns will be taken into account during the City's review of the application.

As per City requirements, a Shadow Impact Study, Stormwater Management Report, and Traffic Impact Study (TIS) have been completed for the sites. The materials associated with this proposal will be available on the City's website (*For Residents > Planning > Land Development > Development Applications > Current Development Applications*), at the time the City determines the application submission to be complete.

Issues relating to the location of the garbage disposal units and creation of dust would be addressed in later stages of the development approvals process, during Site Plan Control Approval (not yet initiated).

Future public meetings and information about this proposed development will be the responsibility of the City of Windsor as required by the *Planning Act*.

Please don't hesitate to reach out to our team, your City Councilor, and the City Planner on file with any other comments or concerns.

Thank you,

Theresa

[Quoted text hidden]



Neighbourhood, South <southneighbourhood@dillon.ca>

Dillon Comments 2

2 messages

Tue, May 2, 2023 at 10:24 PM

To: southneighbourhood@dillon.ca
Cc: "Marignani, Angelo" <amarignani@citywindsor.ca>

----- Forwarded message -----

Date: Tue, May 2, 2023, 9:51 p.m.
Subject: Dillon Comments 2

 **Dillon Comments 2.pdf**
6698K

Neighbourhood, South <southneighbourhood@dillon.ca>

Wed, May 3, 2023 at 4:33 PM

Cc: "Marignani, Angelo" <amarignani@citywindsor.ca>

Hello,

We would like to thank you for taking the time to reach out to us with your concerns.

We acknowledge the many concerns raised regarding the proposed multi-unit residential buildings. Our team will be taking into consideration the thoughts shared during the Public Information Centre as well as the comments received through email and telephone.

Future public meetings and information about this proposed development will be the responsibility of the City of Windsor as required by the *Planning Act*.

Please don't hesitate to reach out to your City Councilor and the City Planner on file with any other comments or concerns.

Thank you,

Theresa

[Quoted text hidden]



Neighbourhood, South <southneighbourhood@dillon.ca>

Dillon Comments File 1

2 messages

Tue, May 2, 2023 at 10:23 PM

To: "southneighbourhood@dillon.ca" <southneighbourhood@dillon.ca>
Cc: "Marignani, Angelo" <amarignani@citywindsor.ca>

----- Forwarded message -----

Date: Tue, May 2, 2023, 9:50 p.m.
Subject: Dillon Comments File 1

 **Dillon Comments.pdf**
3814K

Neighbourhood, South <southneighbourhood@dillon.ca>

Wed, May 3, 2023 at 4:32 PM

Cc: "Marignani, Angelo" <amarignani@citywindsor.ca>

Hello,

We would like to thank you for taking the time to reach out to us with your concerns.

We acknowledge the many concerns raised regarding the proposed multi-unit residential buildings. Our team will be taking into consideration the thoughts shared during the Public Information Centre as well as the comments received through email and telephone.

Future public meetings and information about this proposed development will be the responsibility of the City of Windsor as required by the *Planning Act*.

Please don't hesitate to reach out to your City Councilor and the City Planner on file with any other comments or concerns.

Thank you,

Theresa
[Quoted text hidden]



Neighbourhood, South <southneighbourhood@dillon.ca>

East Riverside -South neighborhood mixed use development

2 messages

Mon, May 1, 2023 at 7:23 PM

To: southneighbourhood@dillon.ca

Hello

my name is [REDACTED], which would be on the south site of the of the conceptual development plan.

I have various concerns if the these plans, become more than a concept in the near future .

We have lived at [REDACTED] since we built our home over 23 years ago .

My concerns and worry is not only for the health and safety of my family . It is also a safety concern to children and pedestrians in the area .

Due to the increase in the number of traffic going down our residential streets , parking which we already had issues with on our over crowded street .

This is already a problem with a recent build on Banwell with the condos and their parking issues that, have now spilled out onto the side road of Leathorne .

The exhaust from all these extra vehicles during build & after , will be a health hazard to us including excessive noise

These buildings, for one will be fairly close to our homes and will infringe on our privacy , of us enjoy our backyard & decks.

The buildings will also affect the enjoyment of seeing the sun raise everyday. The shadow effect is real for some will be greater than others who suffer from Sadds. It is a great concern for myself , my husband & my children who are 12 & 19.

The large scale parking lot that will wipe out most of the green space and nature that we enjoy on a daily basis . These field areas are home to many species, such as deer, various birds and on occasion coyotes .

There is also the scary fact of flooding !! This city has already had lots of flooding issues in Riverside and other parts . It is very scary , when it rains how much the water levels raise in that field behind our home !

These fields and used to be swamp land back in the 60's and to then put these massive buildings and parking lots in these areas will only increase the flood risk in our area !

There is also the overcrowding of our grade schools and high school is a huge issue already . There is no room to put anymore students without our children suffering from shortage of space in the classroom and shortage of teaching staff to meet the needs of our children currently.

With they being said , a lot more thought and effort needs to be put into the process of buildings of any type in this area.

There are a lot of residents that do not want to see these massive structure or parking lots in our backyards ! I urge you to please hear the plea of the residents of East Riverside and rethink planning and design of any stricter that would be put , this closely to our homes and green space.

I thank you for your time , in reviewing my concerns. I can only hope and pray that some sort of common ground can be met , when it comes to our beautiful east Riverside community and the residents they reside here.

Sent from my iPhone

Neighbourhood, South <southneighbourhood@dillon.ca>

City Council Agenda - Monday, October 30, 2023

Wed, May 3, 2023 at 4:13 PM

[REDACTED]

[REDACTED]

We would like to thank you for taking the time to reach out to us with your concerns.

We acknowledge the many concerns raised regarding the proposed multi-unit residential buildings. Our team will be taking into consideration the thoughts shared during the Public Information Centre as well as the comments received through email and telephone.

The concerns relating to traffic, vehicle noise, parking, the proposed location of the buildings C & D, potential shadow impacts, wildlife, flooding, and availability of community services have been noted. These concerns will be taken into account during the City's review of the application.

As per City requirements, a Traffic Impact Study (TIS), Shadow Impact Study, Natural Site Features Inventory & Preservation Plan, and Stormwater Management Report have been completed for the sites. Regarding keeping nearby residents informed, we are preparing an engagement summary of all of the comments and concerns raised at the meeting on April 19, 2022 as well as those via telephone and email, which will be available as part of the public record. The supporting materials associated with this proposal will be available on the City's website (*For Residents > Planning > Land Development > Development Applications > Current Development Applications*), at the time the City determines the application submission to be complete.

Future public meetings and information about this proposed development will be the responsibility of the City of Windsor as required by the *Planning Act*. The *Planning Act* requires only that residents within 120 meters of a proposed development site be notified of the proposal.

Please don't hesitate to reach out to our team, your City Councillor, and the City Planner on file with any other comments or concerns.

Thank you,

Theresa

[Quoted text hidden]



Neighbourhood, South <southneighbourhood@dillon.ca>

East Riverside-South neighborhood Development..

3 messages

Mon, May 1, 2023 at 6:45 PM

To: southneighbourhood@dillon.ca
Cc: amarigani@citywindsor.ca

Hello my name is [redacted]. South site of conceptual development plan .
My concerns are : first and foremost is location of building C , D being so close to our homes and invasion of privacy.
My second concern is the traffic that will be created from these units and the safety of our children and pedestrian. Also the parking on our street from tenants of buildings with more then one vehicle. We see on the other buildings that were built on banwell.
.3rd concern is noise and pollution from vehicle that are parked behind our home .
4th is potential flood that could be caused by massive parking lot behind our homes with very little green space .
5th concern is the construction dust and noise and influx of work vehicle...
6th concern is our property that could be vandalized due to no buffer between our homes and parking lot...
Myself and a few neighbour's of Chateau Ave have maintained the grass for the last 22 years. Behind our fences is approximately 30ft out before the farmers field , that stretches from McHugh to springhollow .
In closing I wish not to see these buildings become a reality... I'm much more in favorite for more homes or townhouse or the original plan to have it as a plaza which was promised when I build this home... thank you for your time and consideration...

Neighbourhood, South <southneighbourhood@dillon.ca>

Wed, May 3, 2023 at 4:06 PM

Cc: amarigani@citywindsor.ca

We would like to thank you for taking the time to reach out to us with your concerns.

We acknowledge the many concerns raised regarding the proposed multi-unit residential buildings. Our team will be taking into consideration the thoughts shared during the Public Information Centre as well as the comments received through email and telephone.

The concerns relating to the proposed location of the buildings C & D, traffic, parking, vehicle noise, flooding, disruptions due to construction have been noted. These concerns will be taken into account during the City's review of the application. A 1.8m (5.9 ft) high board-on-board privacy fence is proposed to be erected along the property line, providing a buffer between the proposed parking area and the existing residences.

As per City requirements, a Traffic Impact Study (TIS) and Stormwater Management Report have been completed for the sites. Regarding keeping nearby residents informed, we are preparing an engagement summary of all of the comments and concerns raised at the meeting on April 19, 2022 as well as those via telephone and email, which will be available as part of the public record. The supporting materials associated with this proposal will be available on the City's website (For Residents > Planning > Land Development > Development Applications > Current Development Applications), at the time the City determines the application submission to be complete.

Future public meetings and information about this proposed development will be the responsibility of the City of Windsor as required by the Planning Act. The Planning Act requires only that residents within 120 meters of a proposed development site be notified of the proposal.

Please don't hesitate to reach out to our team, your City Councillor, and the City Planner on file with any other comments or concerns.

Thank you,

Theresa

[Quoted text hidden]



Neighbourhood, South <southneighbourhood@dillon.ca>

Mixed use Development Banwell Rd. & McHugh St

2 messages

Tue, May 2, 2023 at 4:59 PM

To: "southneighbourhood@dillon.ca" <southneighbourhood@dillon.ca>, "amarignani@citywindsor.ca" <amarignani@citywindsor.ca>

Attention Theresa O'Neill and Angelo Marignani

My name is [REDACTED] I would like to provide you with the following questions, concerns and thoughts about the proposed development unveiled at the April 19th Public Information Centre.

How were people informed about the Public Information Centre? I certainly was not aware of the scheduling of this session nor were any of my neighbours that I have spoken with.

Who are the executive officers and owners of 1027458 Ontario Inc.?

The proposal includes 9 multiple dwelling buildings. Will these be condominiums , rental units or both? How many of each? If rentals, is there provision for rent geared to income housing?

The proposal outlines creation of 744 dwellings. This will significantly increase the population density in the area at a level which is out of line with existing concentrations. This will exacerbate traffic issues in the area and likely compromise safety of residents.

Has consideration been given to the adequacy of the existing roads?

I am concerned that the parking provisions may not contain all the needs of the unit residents and guests. I feel this way because the 3 buildings that have been built over the last couple of years to the north of this proposed development has led to spill over parking on Questa Blvd on a permanent basis.

I look forward to receiving a response to my questions and receiving more information about the timetable on this process. I also will appreciate being invited to further meetings and improved transparency

Thanks

Sent from Outlook

Neighbourhood, South <southneighbourhood@dillon.ca>

Wed, May 3, 2023 at 4:31 PM

Cc: "amarignani@citywindsor.ca" <amarignani@citywindsor.ca>

We would like to thank you for taking the time to reach out to us with your concerns.

We acknowledge the many concerns raised regarding the proposed multi-unit residential buildings. Our team will be taking into consideration the thoughts shared during the Public Information Centre as well as the comments received through email and telephone.

The concerns relating to traffic and parking have been noted. These concerns will be taken into account during the City's review of the application. At present, the City of Windsor's accepted *minimum* requirement for parking spaces for a Multiple Dwelling (i.e. multi-unit residential building) is 1.25 parking spaces per dwelling unit. We have provided a parking rate for each site that is above the required minimum. The proposed parking rates are 1.45 parking spaces per dwelling unit (North 'A' Site), 1.50 parking spaces per dwelling unit (North 'B' Site), and 1.30 parking spaces per dwelling unit (South Site). A total of 1,028 parking spaces are proposed to support the 744 residential units, with an additional 72 parking spaces provided to support the business office and respite home. I did not have involvement with the development of the three (3) buildings north of these sites, thus I am unable to compare our proposed parking rates to what was permitted for the development to the north.

The proposed residential units are proposed to be condominiums. At this time, I am not authorized to disclose information about the executive officers and owners of 1027458 Ontario Inc. Regarding notification of the PIC, our team issued notices via mail to all owners/tenants located 120 meters around the subject sites. The *Planning Act* requires that, at minimum, residents within 120 meters of a proposed development site be notified of the proposal.

As per City requirements, a Traffic Impact Study (TIS) has been completed for the sites. Regarding keeping nearby residents informed, we are preparing an engagement summary of all of the comments and concerns raised at the meeting on April 19, 2022 as well as those via telephone and email, which will be available as part of the public record. The supporting materials associated with this proposal will be available on the City's website (*For Residents > Planning > Land Development > Development Applications > Current Development Applications*), at the time the City determines the application submission to be complete.

Future public meetings and information about this proposed development will be the responsibility of the City of Windsor as required by the *Planning Act*.

Please don't hesitate to reach out to your City Councilor and the City Planner on file with any other comments or concerns.

Thank you,

Theresa

[Quoted text hidden]



Neighbourhood, South <southneighbourhood@dillon.ca>

Project no.22-5144 and 22-5266

2 messages

Thu, Apr 27, 2023 at 8:19 PM

[Redacted]
to: southneighbourhood@dillon.ca

Attention:Theresa O'Neil, Planner

The number of proposed apartment buildings is not needed as there are several new buildings in this area with lots of vacancies. These structures would create a monumental traffic problem. The land in this area tends to be low lying and there could be a flooding problem. This also could effect sewage, hydro and water. Also it would not be esthetically pleasing . Please come up with another plan.

Yours truly,

[Redacted]
Arvilla Street

Neighbourhood, South <southneighbourhood@dillon.ca>

Tue, May 2, 2023 at 9:01 AM

[Redacted]
We would like to thank you for taking the time to reach out to us with your concerns.

We acknowledge the many concerns raised regarding the proposed multi-unit residential buildings. Our team will be taking into consideration the thoughts shared during the Public Information Centre as well as the comments received through email and telephone.

The concerns relating to density, transportation, stormwater management, and servicing have been noted. These concerns will be taken into account during the City's review of the application.

As per City requirements, a Traffic Impact Study (TIS), stormwater management report, and functional servicing report have been completed for the sites. The supporting materials associated with this proposal will be available on the City's website (*For Residents > Planning > Land Development > Development Applications > Current Development Applications*), at the time the City determines the application submission to be complete.

Future public meetings and information about this proposed development will be the responsibility of the City of Windsor as required by the *Planning Act*.

Please don't hesitate to reach out to our team, your City Councilor, and the City Planner on file with any other comments or concerns.

Thank you,

Theresa

[Quoted text hidden]



Neighbourhood, South <southneighbourhood@dillon.ca>

Project Numbers 22-5144 and 22-5266

2 messages

Sat, Apr 29, 2023 at 9:06 AM

[REDACTED]
 To: southneighbourhood@dillon.ca

Attn Theresa O'Neil
 1-519-438-1288 ext 1286.

The project is for 744 units. When looking at the site plan there is nowhere near that amount of parking spaces. How can this project proceed when that's the case?

Yours truly
 [REDACTED]

 Neighbourhood, South <southneighbourhood@dillon.ca>

Tue, May 2, 2023 at 9:01 AM

[REDACTED]
 [REDACTED]
 We would like to thank you for taking the time to reach out to us with your concerns.

We acknowledge the many concerns raised regarding the proposed multi-unit residential buildings. Our team will be taking into consideration the thoughts shared during the Public Information Centre as well as the comments received through email and telephone.

The concerns relating to parking have been noted. These concerns will be taken into account during the City's review of the application. At present, the City of Windsor's accepted *minimum* requirement for parking spaces for a Multiple Dwelling (i.e. multi-unit residential building) is 1.25 parking spaces per dwelling unit. We have provided a parking rate for each site that is above the required minimum. The proposed parking rates are 1.45 parking spaces per dwelling unit (North 'A' Site), 1.50 parking spaces per dwelling unit (North 'B' Site), and 1.30 parking spaces per dwelling unit (South Site). A total of 1,028 parking spaces are proposed to support the 744 residential units, with an additional 72 parking spaces provided to support the business office and respite home.

As per City requirements, supporting materials have been completed for the sites. The supporting materials associated with this proposal will be available on the City's website (*For Residents > Planning > Land Development > Development Applications > Current Development Applications*), at the time the City determines the application submission to be complete.

Future public meetings and information about this proposed development will be the responsibility of the City of Windsor as required by the *Planning Act*. The *Planning Act* requires only that residents within 120 meters of a proposed development site be notified of the proposal.

Please don't hesitate to reach out to our team, your City Councilor, and the City Planner on file with any other comments or concerns.

Thank you,

Theresa

[Quoted text hidden]



Neighbourhood, South <southneighbourhood@dillon.ca>

Proposed Mixed Use Development Banwell Road and McHugh Street

2 messages

Fri, Apr 28, 2023 at 1:22 PM

To: 311@citywindsor.ca, amarignani@citywindsor.ca, mayoro@citywindsor.ca, southneighbourhood@dillon.ca

I'm a recent arrival in Windsor. My wife and I moved here from the southeast corner of British Columbia three years ago. At first, it was a shock leaving the Rockies and it's abundant wildlife behind. And yet, here we are in the Blue Heron area and we would not go back: we both love living in East Windsor!

The transition to this new life was made a lot easier for me because we live close to East Riverside Park. This park has been a life saver!! And for the past two years, my twice daily walks with our big dog has expanded to include Elizabeth Kishkon Park and the stretch of greenspace that runs from this park all the way to the railroad tracks along Banwell Road, the new proposed area for this massive apartment building complex!!

The reality of living in a flood zone is new to me... And yet, I understand the crucial importance of greenspace, as is this stretch of land on Banwell Road, and the capacity it has to retain, distribute and use excess water in such an ecologically sensitive environment. Nevermind the amazing diversity of wildlife that uses and lives in this corridor...

I also understand the pressing need for new accommodation. And I am very sympathetic to the voices of the young families and the new immigrants that want to make this beautiful Essex County their home. Their need is great indeed... Homes are in short supply...

And I say that unless Dillon Consulting and the city of Windsor can provide clear answers to the ecological regression that this proposal would create, I am definitely opposed to this project! Because there is an urgent need for new housing does not mean we should throw caution to the wind!!! Let's build wisely.

Yours truly,

Neighbourhood, South <southneighbourhood@dillon.ca>

Tue, May 2, 2023 at 9:01 AM

Cc: 311@citywindsor.ca, amarignani@citywindsor.ca, mayoro@citywindsor.ca

We would like to thank you for taking the time to reach out to us with your concerns.

We acknowledge the many concerns raised regarding the proposed multi-unit residential buildings. Our team will be taking into consideration the thoughts shared during the Public Information Centre as well as the comments received through email and telephone.

The concerns relating to the environment, wildlife, and stormwater management have been noted. These concerns will be taken into account during the City's review of the application.

As per City requirements, a Natural Site Features Inventory & Preservation Plan and Stormwater Management Report have been completed for the sites. Regarding keeping nearby residents informed, we are preparing an engagement summary of all of the comments and concerns raised at the meeting on April 19, 2022 as well as those via telephone and email, which will be available as part of the public record. The supporting materials associated with this proposal will be available on the City's website (*For Residents > Planning > Land Development > Development Applications > Current Development Applications*), at the time the City determines the application submission to be complete.

Future public meetings and information about this proposed development will be the responsibility of the City of Windsor as required by the *Planning Act*.

Please don't hesitate to reach out to our team, your City Councilor, and the City Planner on file with any other comments or concerns.

Thank you,

Theresa

[Quoted text hidden]



Neighbourhood, South <southneighbourhood@dillon.ca>

South neighbour hood project attention Theresa O'Neill,

2 messages

Fri, Apr 28, 2023 at 9:40 PM

To: "southneighbourhood@dillon.ca" <southneighbourhood@dillon.ca>
Cc: "mayoro@citywindsor.ca" <mayoro@citywindsor.ca>, "amarignani@citywindsor.ca" <amarignani@citywindsor.ca>

To everyone concerned

I was not at the meeting on April 19th but neighbour across Kirkland Ave and friends on Questa informed us of the outcome. Disappointed that not everyone in the area wasn't invited because it impacts everyone in East Riverside.

Very disappointed with our leadership in Windsor. Reading in The Windsor Star quoted by mayor D. Dickens all he wants is to make great improvements in urban tree coverage?
On the other hand per meeting at WFCU "limited invitees" they want to put "9" high rise apartment complex on West side on Banwell from Firgrove to Leathorne to McHugh to the rail tracks. That approximatly 750 units along with an administration building and a respite place. Doesn't make a bit of sense.

WHY is East Riverside being singled out for so many high rises causing overcrowding.
We already have 3 monstrous and ugly ones sitting on Banwell and Firgrove. Already had a fatality at that corner.
Secondly
God's creatures deers, birds, rabbits, geese, ducks, pheasants etc. are being driven out of the area

Thirdly
This entire area is known for flooding especially to insurance companies. I'm sure this isn't going to sit well with them, can't get insurance now so what is going to happen with all this proposed construction. Are we "the residents of Riverside" be able to be compensated by the city if flooding occurs.

Has a Flood Plan report been done along with an Environmental plan? If so please make it available to everyone for viewing.

Fourth
Concerns on the traffic is also a big issue. Banwell is very busy, fast traffic now just image adding 750 units with approximately 2 vehicles per unit. Hard enough to get onto Banwell now either right or left and there's no other way to go. With 2 retirement homes plus a high school Banwell and McHugh very busy corner.
Quote from the representative at the meeting stating only about extra 30-40 cars at busy times. Don't know which planet he came from? Parking isn't sufficient now 3 high rises on Banwell the overflow spills over onto adjacent streets. Who is going to monitor that situation?

Five
Why is East Riverside being pushed for so many residences? Is it to fulfill the mayor's dream of 30,000 places?
How many are being planned for mayor's area or any of our councillors? Would be interesting to find out.
Hopefully this project never gets off the ground just leave some green space for our health and God's creatures instead of a concrete jungle.
Money is the "EVIL" peace and beautiful scenery and fresh air is everyone's dream.
Really praying this project never happens.

Thank You



Sent from my iPad

Neighbourhood, South <southneighbourhood@dillon.ca>

Tue, May 2, 2023 at 9:01 AM

Cc: "mayoro@citywindsor.ca" <mayoro@citywindsor.ca>, "amarignani@citywindsor.ca" <amarignani@citywindsor.ca>

We would like to thank you for taking the time to reach out to us with your concerns.

We acknowledge the many concerns raised regarding the proposed multi-unit residential buildings. Our team will be taking into consideration the thoughts shared during the Public Information Centre as well as the comments received through email and telephone.

The concerns relating to density, wildlife, stormwater management, and traffic have been noted. These concerns will be taken into account during the City's review of the application.

As per City requirements, a Natural Site Features Inventory & Preservation Plan, Stormwater Management Report, and Traffic Impact Study (TIS) have been completed for the sites. Regarding keeping nearby residents informed, we are preparing an engagement summary of all of the comments and concerns raised at the meeting on April 19, 2022 as well as those via telephone and email, which will be available as part of the public record. The supporting materials associated with this proposal will be available on the City's website (*For Residents > Planning > Land Development > Development Applications > Current Development Applications*), at the time the City determines the application submission to be complete.

Future public meetings and information about this proposed development will be the responsibility of the City of Windsor as required by the *Planning Act*. The *Planning Act* requires only that residents within 120 meters of a proposed development site be notified of the proposal.

Please don't hesitate to reach out to our team, your City Councilor, and the City Planner on file with any other comments or concerns.

Thank you,

Theresa

[Quoted text hidden]



Neighbourhood, South <southneighbourhood@dillon.ca>

apartment proposal Banwell and McHugh

2 messages

Fri, Apr 21, 2023 at 4:00 PM

Cc: southneighbourhood@dillon.ca, mayoro@citywindsor.ca, amarignani@citywindsor.ca, 311@citywindsor.ca

My husband and I attended an information meeting on Wednesday, April 19th at WFCU, concerning a proposal for 9 new apartment buildings on the corner of Banwell and McHugh. the properties in question are in a flood zone, we have lived in this area since 2001, and every time there is a storm, this property floods. There was a berm constructed at the back of our property to maintain flood control. The proposal for building these apartments would remove the berm and the people at the meeting had no answer on how the water would be stopped from coming onto our property. When asked about flooding his answer was " Oh you can call 311". There is a purpose for this berm. Was there a floodplain study done? There are many animals living in this area and would be disrupted by this endeavour. Was there an environmental study done? The increase in traffic of 744 plus apartments would put undue stress on our roads and sewers. there are two rest homes on this street, one arena, and one High School. This street can not afford any more traffic. When asked about the increase another person at the meeting said there wouldn't be more than roughly 30 cars increase in traffic. An increase of over 1000 more residents is not doable. After talking to several residents, we were all told the same thing at the time of purchase.

1....No building can be built higher than 2 stories behind us.

2....The building behind us would be a small office-type structure.

We don't want a multi-plex building behind us, we want the greenery saved. We don't want the berm removed.

--
If you forward this, PLEASE REMOVE all email addresses before you send it and use BCC when forwarding to several people at once.

"BE KIND TO OUR EMAIL FRIENDS"

Neighbourhood, South <southneighbourhood@dillon.ca>

Mon, May 1, 2023 at 12:44 PM

Cc: mayoro@citywindsor.ca, amarignani@citywindsor.ca, 311@citywindsor.ca

We would like to thank you for taking the time to reach out to us with your concerns.

We acknowledge the many concerns raised regarding the proposed multi-unit residential buildings. Our team will be taking into consideration the thoughts shared during the Public Information Centre as well as the comments received through email and telephone.

City Council Agenda - Monday, October 30, 2023

The concerns relating to flooding, the berm, wildlife, and traffic have been noted. These concerns will be taken into account during the City's review of the application.

As per City requirements, a Stormwater Management Report, a Natural Features Inventory & Preservation Study, and a Traffic Impact Study (TIS) have been completed for the sites. The supporting materials associated with this proposal will be available on the City's website (*For Residents > Planning > Land Development > Development Applications > Current Development Applications*), at the time the City determines the application submission to be complete.

Future public meetings and information about this proposed development will be the responsibility of the City of Windsor as required by the *Planning Act*.

Please don't hesitate to reach out to our team, your City Councilor, and the City Planner on file with any other comments or concerns.

Thank you,

Theresa

[Quoted text hidden]



Neighbourhood, South <southneighbourhood@dillon.ca>

Banwell & McHugh development

2 messages

Fri, Apr 21, 2023 at 5:09 PM

To: "southneighbourhood@dillon.ca" <southneighbourhood@dillon.ca>

Hi Theresa, I own one of the townhouses on Questa Dr, but I was unable to attend the information centre meeting on Wednesday night at WFCU Centre.

I'm writing to see if there is any new information from that meeting that you can share with me.

Thanks,

Neighbourhood, South <southneighbourhood@dillon.ca>

Mon, May 1, 2023 at 12:56 PM

We would like to thank you for taking the time to reach out to us.

At this time, there is no new information to share. As per City requirements, there are a number of supporting studies (i.e. stormwater management report, Traffic Impact Study, and Shadow Impact Study) that will be completed for the sites. The materials associated with this proposal will be available on the City's website (*For Residents > Planning > Land Development > Development Applications > Current Development Applications*), at the time the City determines the application submission to be complete.

Future public meetings and information about this proposed development will be the responsibility of the City of Windsor as required by the *Planning Act*.

Please don't hesitate to reach out to our team, your City Councilor, and the City Planner on file with any other comments or concerns.

Thank you,

Theresa

[Quoted text hidden]



Neighbourhood, South <southneighbourhood@dillon.ca>

(no subject)

2 messages

Mon, Apr 24, 2023 at 1:08 PM

To: southneighbourhood@dillon.ca

Hello,

I am [REDACTED]

I attended the meeting about the proposed changes to Banwell Road.

This is a very very bad plan in my estimation. I am very much against it.

The three existing apartment buildings on Banwell are already causing problems with parking. Not enough parking spots for the tenants, they are parking on Questa Drive as well as on other parts. I hear now they are even parking right in the parking spots that were installed for the people using the kids playground. So now your plan is to put in 9 more buildings and you have come up with the bright idea of providing 1.3 car space for each apartment. I suspect that the majority of people living in the proposed apartments will not be retirees but working people and many apartments will have two car owners. Can you kindly give us a clue where all those cars are going to park ?

This area of Banwell was not made to cope with so many people and so much traffic. This area of Banwell would need to be renamed Concrete Jungle, because that is how it will look. Twelve apartment buildings in one small area. Give me a break. This plan is probably the worst one Coco has ever come up with.

Also I see on the plans you have listed landscaped open spaces, you have put that in writing but all the spaces I can see are parking spots apart from the tiny landscaped drawing for the Respite Home and the Business space.

The area at the back of my house is the home of geese, ducks, pheasants, wild turkeys, just 3 days ago we had two deer and their fawn, I doubt you give a flying fig as to where these animals move to now as you are taking up more of the green with your horrific building plans, but the residents in this area do care a lot.

Ok, now lets get down to probably the biggest headache for us local residents. **F L O O D I N G.**

Questions were asked at WFCU about how to handle the big problem of the berm and the fact that the apartments and parking spots will all be a higher level than our homes and our lots. I questioned Kyle ? and was astonished to learn, he was there with all the info but when the subject of the berm came up he saide he had not seen it. He then said the berm will be removed ??????? He assured us we would have no flooding and came up with a load of gobbledegook of the parking lot being made to handle the water ?????? Also, why are the 48 garages positioned by the berm, is the plan they protect us from flooding ? I am amazed at how unrealistic this whole plan is.

Just stick in 9 more apartment buildings and ruin the neighbourhood.

Seven years ago, along with a lot of areas of Windsor, we had severe flooding here. I live in a block of four houses and we all flooded, along with many on this road. Coco built these houses so obviously did a poor job of safely preparing the area for construction, so forgive me if I am sceptical of taking the word of Kyle ? on this matter.

Well thats it, I look forward to your reply.

Neighbourhood, South <southneighbourhood@dillon.ca>
 [REDACTED]

Mon, May 1, 2023 at 2:45 PM

[REDACTED]

We would like to thank you for taking the time to reach out to us with your concerns.

We acknowledge the many concerns raised regarding the proposed multi-unit residential buildings. Our team will be taking into consideration the thoughts shared during the Public Information Centre as well as the comments received through email and telephone.

The concerns relating to parking, traffic, wildlife, and stormwater management have been noted. These concerns will be taken into account during the City's review of the application.

As per City requirements, a Traffic Impact Study (TIS), Natural Features Inventory and Preservation Study, and Stormwater Management Report have been completed for the sites. The supporting materials associated with this proposal will be available on the City's website (*For Residents > Planning > Land Development > Development Applications > Current Development Applications*), at the time the City determines the application submission to be complete.

Future public meetings and information about this proposed development will be the responsibility of the City of Windsor as required by the *Planning Act*.

Please don't hesitate to reach out to our team, your City Councilor, and the City Planner on file with any other comments or concerns.

Thank you,

Theresa

[Quoted text hidden]



Neighbourhood, South <southneighbourhood@dillon.ca>

att: Theresa O'Neill

2 messages

Wed, Apr 26, 2023 at 10:25 AM

To: "southneighbourhood@dillon.ca" <southneighbourhood@dillon.ca>

I am concerned about the high rise apartment development proposed for Banwell near McNorton. Please send me a site plan or similar diagram, so I can understand it better. Thank you,

Neighbourhood, South <southneighbourhood@dillon.ca>

Mon, May 1, 2023 at 6:41 PM

We would like to thank you for taking the time to reach out to us with your concern.

We acknowledge the many concerns raised regarding the proposed multi-unit residential buildings. Our team will be taking into consideration the thoughts shared during the Public Information Centre as well as the comments received through email and telephone.

Please find attached to this email a PDF copy of the Notice sent to residents in early April. Included on the second page is the conceptual development plan.

Future public meetings and information about this proposed development will be the responsibility of the City of Windsor as required by the *Planning Act*.

Please don't hesitate to reach out to our team, your City Councilor, and the City Planner on file with any other comments or concerns.

Thank you,

Theresa

[Quoted text hidden]

 **Banwell & McHugh Mixed Use Development - PIC Notice - April 2023.pdf**
1967K



Neighbourhood, South <southneighbourhood@dillon.ca>

Attn:Theresa O'Neill, Planner

2 messages

Wed, Apr 26, 2023 at 9:57 AM

To: southneighbourhood@dillon.ca

Project # 225-5144 and 22-5266

Hello,

I am writing to express my deep concern about the proposed development of 744 new residential units, consisting of 11 buildings that will occupy the west side of Banwell Rd from the tracks all the way to Firgrove.

I believe that the three apartment buildings already on that side of the road by Firgrove as well as adding so many more will cause way too much congestion in the area. Where else in the city has there ever been so many buildings as such proposed been built together? What about normal city services- playgrounds for all the kids that will live there, transportation services, schools, etc.

It sounds like a poor plan at this point in time. What is the plan to make it a safe, vibrant community rather than a haphazard plan for a high density residence?

A concerned resident,

Neighbourhood, South <southneighbourhood@dillon.ca>

Mon, May 1, 2023 at 6:35 PM

We would like to thank you for taking the time to reach out to us with your concerns.

We acknowledge the many concerns raised regarding the proposed multi-unit residential buildings. Our team will be taking into consideration the thoughts shared during the Public Information Centre as well as the comments received through email and telephone.

The concerns relating to density, transportation, and provision of adequate City services have been noted. These concerns will be taken into account during the City's review of the application.

As per City requirements, a Traffic Impact Study (TIS) and other supporting studies have been completed for the sites. The supporting materials associated with this proposal will be available on the City's website (*For Residents > Planning > Land Development > Development Applications > Current Development Applications*), at the time the City determines the application submission to be complete.

Future public meetings and information about this proposed development will be the responsibility of the City of Windsor as required by the *Planning Act*.

Please don't hesitate to reach out to our team, your City Councilor, and the City Planner on file with any other comments or concerns.

Thank you,

Theresa

[Quoted text hidden]



Neighbourhood, South <southneighbourhood@dillon.ca>

BANWELL / MCHUGH PROJECT

2 messages

Mon, Apr 24, 2023 at 12:28 PM

To: southneighbourhood@dillon.ca

Hello, We were told at the first meeting that the builder would put up a privacy white vinyl fence (up to 12 feet high) in the back for the people that live on Questa Drive

As you can see on the drawing our house would be the blue box on the right hand side of the drawing on Questa

From the back of our house, approximately how far back would the fence be built? As you can see the greenery is at it's thinnest just behind our house?

regards,



BANWELL : MCHUGH 2.bmp
198K

Neighbourhood, South <southneighbourhood@dillon.ca>

Mon, May 1, 2023 at 1:16 PM

We would like to thank you for taking the time to reach out to us with your concerns.

The drawings that were at the Public Information Centre show the proposed fence behind the existing dwellings to be a board-on-board privacy fence that is 1.8m (5.9 ft) in height. I am unsure how this miscommunication happened; however, I wanted to provide you with the accurate information for your consideration.

The fence is proposed to be built along the existing rear property line. I am unable to provide you with the distance between the back of your house and your rear property line as I do not have access to this information about your property at this time.

Future public meetings and information about this proposed development will be the responsibility of the City of Windsor as required by the *Planning Act*.

Please don't hesitate to reach out to our team, your City Councilor, and the City Planner on file with any other comments or concerns.

Thank you,

Theresa

[Quoted text hidden]



Neighbourhood, South <southneighbourhood@dillon.ca>

Information request

2 messages

Mon, Apr 24, 2023 at 9:14 PM

To: southneighbourhood@dillon.ca

Hi Theresa,

I attended your Banwell Development meeting.

Can you please send me a copy of the notice you sent to residents? I am mostly interested in the colour image of the development.

Thanks, and let me know if you have any questions or concerns that would prevent you from sending the information.

Regards,

Neighbourhood, South <southneighbourhood@dillon.ca>

Mon, May 1, 2023 at 2:49 PM

Thank you for attending the Public Information Centre!

Please find attached to this email a PDF copy of the Notice sent to residents in early April.

Future public meetings and information about this proposed development will be the responsibility of the City of Windsor as required by the *Planning Act*.

Please don't hesitate to reach out to our team, your City Councilor, and the City Planner on file with any other comments or concerns.

Thank you,

Theresa

[Quoted text hidden]

Banwell & McHugh Mixed Use Development - PIC Notice - April 2023.pdf
1967K



Neighbourhood, South <southneighbourhood@dillon.ca>

North B site Banwell and McHugh

4 messages

Wed, Apr 12, 2023 at 8:24 AM

To: "southneighbourhood@dillon.ca" <southneighbourhood@dillon.ca>

Hello,

Your plans have arrived in my mailbox.

As the planner, I need more details on the area affecting my property line. The back end of the single car garage proposal will be along my backyard. Your orange line is obscuring the amount of green space separating my property from the actual building. This green space poses some questions:

- Will there be a fence along the backside of these garages?
 - What type – open or privacy, how high? What material?
 - Will you have a Shadow Study and Environmental Study for 3 blocks of tree line and the wildlife (birds, turkeys, deer) associated with it that will be disappearing with this entire project available for the Apr 19 meeting? Where can I get access to these online before the meeting.
- I noticed narrow green space between the garages, will people be able to walk between them into my yard?
- How will power be delivered to these garages – underground or overhead with power lines stretching over my property line? With the push for EV, what are the plans for EV charging options in the parking lot and garages?
- There is also a word to small to see under the "RA" in GARAGES, what is this word?
- Where and what type of lighting is planned for the parking lots? These will now be shining into my back windows.

We have seen many overflow people parking on our road (Questa) down by a similar development on Firgrove and Banwell. If there is no fence along this back line, I foresee people cutting through across my property to park on my street or even let pets wander on my property.

Its sad with all the new talk about climate change and Carbon emissions, you elect to decimate 3 blocks of trees that actually recycle Carbon in favour of a concrete parking lot jungle full of carbon emitting vehicles.

Just so you can see the view I won't have anymore.



[Redacted]

Neighbourhood, South <southneighbourhood@dillon.ca>

Mon, Apr 17, 2023 at 4:51 PM

[Redacted]

We would like to thank you for having taken the time to contribute your thoughts to the discussion.

Due to the scale of the concept plan that was shared with you, some of the details are difficult to see clearly. A larger scale copy of the concept plan, with all details being legible, will be available at the Public Information Centre (PIC) on Wednesday from 6:00pm - 8:00pm at the WFCU Centre.

Ahead of the meeting, we wanted to provide you with information about the amount of green space proposed between your backyard property line and the rear of the proposed single car garages. The width of this space is 2.8 meters (9.2 ft). Further, the small word under the "RA" in GARAGES is '204.8m', which is the length of the rear property line.

To answer your initial question, yes, there will be a fence along the backside of the proposed single car garages. The fence is proposed to be board-on-board (providing privacy) and be 1.8 meters in height. The space between the rear of the proposed single car garages and the rear of your property line/proposed fence is not designed to be used as leisure space by future residents. The proposed 1.8 meter high board-on-board fence will further preclude people being able to walk into your yard.

The graphics from the Shadow Impact Study will be available for viewing at the PIC on April 19. The supporting materials for this proposed development application, including the full Shadow Impact Study and the Natural Site Features Inventory & Preservation Study, will be available on the City's website once the application is submitted and deemed complete by the City. The application is not yet deemed complete.

We've noted your other concerns regarding hydro, EV charging stations, and the type of lighting in the parking lots. Along with holding a PIC, we are working on preparing an engagement summary of all of the comments and concerns raised about the proposed development and at the meeting. The engagement summary will be available as part of the public record.

Please don't hesitate to reach out to our team, your City Councilor, and the City Planner on file with any other comments or concerns.

Thank you!

Kindly,

Theresa

[Quoted text hidden]

To: "southneighbourhood@dillon.ca" <southneighbourhood@dillon.ca>

Please check this website that shows the sun position/azimuth.

<https://sun-direction.com/city/13531,windsor/>

For On: 21 Jun, 2023

Time: 07:01

Position: 42.31714267,-82.90237599

Timezone: America/Toronto

Daylight duration: 15h 9m

Shadow length (1m): 5.74 m

Altitude: 9.98°

Azimuth: -112.81°

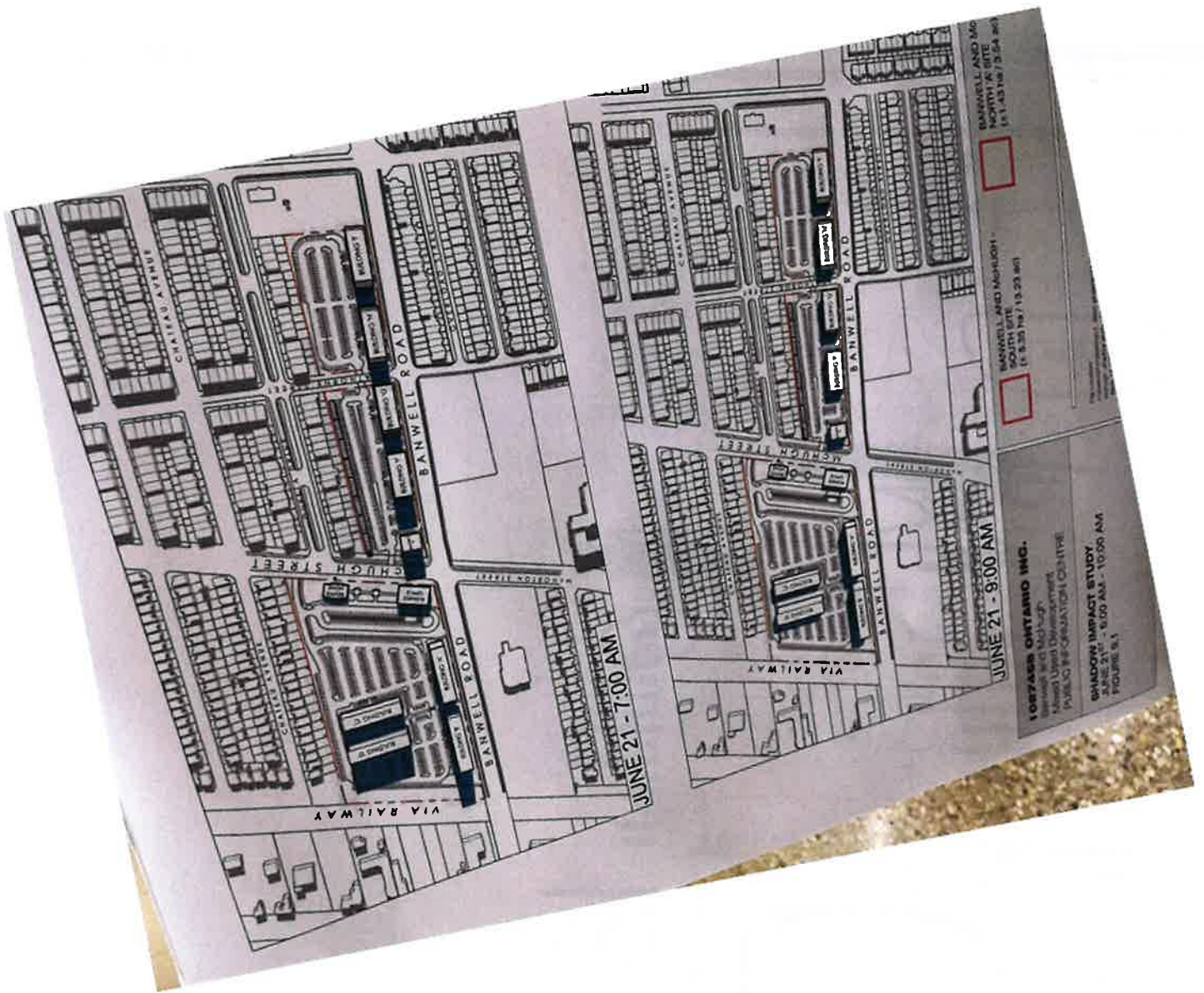
Solar data for Windsor

- 05:21 - Dawn time in Windsor
- 05:56 - Sunrise time in Windsor
- 05:59-21:09 - Daylight time in Windsor
- 13:34 - Solar noon in Windsor
- 21:13 - Sunset time in Windsor
- 21:47 - Dusk time in Windsor

Detailed sun information

- 00:00-03:41 - Night
- 03:41-04:36 - Astronomical twilight
- 04:36-05:21 - Nautical twilight
- 05:21-05:56 - Civil twilight
- 05:21 - Dawn
- 05:56 - Sunrise
- 13:34 - Solar noon
- 21:13 - Sunset
- 21:47 - Dusk
- 21:13-21:47 - Civil twilight
- 21:47-22:32 - Nautical twilight
- 22:32-23:27 - Astronomical twilight
- 23:27-00:00 - Night

In particular, when you insert Jun 21, 2023 @ 0700, the suns position (dark orange line) is absolutely no where near where your shadow charts depict it to be. Your charts show the sun to be well to the North, almost parallel to Banwell road according to the Shadow picture. (I've rotated your chart to a similar orientation to compare maps.



[Quoted text hidden]



01014CD3DB4E41669E5D47B94C1B4369.jpg
1202K

To: "southneighbourhood@dillon.ca" <southneighbourhood@dillon.ca>

Thu, Apr 27, 2023 at 2:23 PM

In addition to the email below. We have additional questions on to the location of Garbage site location for this project.

City Council Agenda - Monday, October 30, 2023

As we are requesting a minimum 10 ft high privacy vinyl fence along the Questa and Chateau homes, we do not want a garbage storage bin right behind our homes either.

This concern is based on the location of garbage storage bins on the existing 3 buildings on Banwell and FirGrove. They are at the back of the parking lot, but no ones lives there, it is just a forested area. (see below)



[Quoted text hidden]



Neighbourhood, South <southneighbourhood@dillon.ca>

East Riverside - South Neighbourhood - Mixed use development

1 message

[Redacted]

Wed, May 3, 2023 at 9:27 PM

To: southneighbourhood@dillon.ca

Attention Theresa O'Neill (Planner),

I am emailing my concerns to express my disagreement with this project. We attended the residents meeting on April 19, 2023 and spoke with multiple employees from Dillon Consulting Ltd. We received information regarding the East Riverside - South Neighbourhood - Mixed use development, and we would like to share our concerns.

We believe that the 2 buildings at the back of the property positioned closer to the houses are too high. We think the 2 buildings along Banwell are fine however, the 2 taller buildings are too much. We feel as though 4 apartment buildings will create way too much traffic and that there will not be enough parking for the residents. We would not appreciate these residents parking on our streets and having access to walk through paths along the tracks. If this project is to move forward we would like a fence that extends the entire perimeter of the property with zero access other than the parking entrance. Our side streets are already over crowded with cars and very busy with the amount of residents living in the area already. More residents in the area would also increase the traffic volume on McHugh which is a school bus route. Vehicles are already going through the school bus lights daily when activated while boarding and deboarding students.

We would like a fence in order to block access and maintain privacy. We would appreciate a nice vinyl fence in order for our house to keep its value and the neighborhood looking nice.

We would like to minimize the parking lot lighting and the lights shining into our back yards and homes. We often have our windows open and excess lighting would not be good for sleeping.

The field in which you are considering building on is constantly flooded. There is no way our properties will not flood due to this project. The land will definitely have to be built up and flooding problems will more than likely occur. The City of Windsor has more than enough flooding issues in the Riverside area. This field is also home to wildlife. Animals like deer, coyote, rabbits, turkey, pheasants and hawks are frequently seen in the area.

We have a pool and the shadow images shown at the meeting were far from accurate. The shade will cause some heat issues for our pool which will in turn will decrease our use of our pool.

Garbage and recycle will be an issue as well. All those buildings means more residents and more garbage. Enclosed garbage areas would be a necessity, preferably inside the building, to prevent rodents and to keep the neighborhood clean. If there are dumpsters in the plan for outside they will be positioned at the rear of the apartment, which means close to our yards and this is not acceptable.

Thank you for listening to our concerns. Please feel free to contact us if you have any questions or would like to address any of these issues with us.

Sincerely,

[Redacted signature]

Appendix F

Record of Attendance and Location Map



1/10

Record of Attendance

East Riverside – South Neighbourhood – Mixed Use Development

Residents Meeting – April 19, 2023

NAME (PLEASE PRINT)	MAILING ADDRESS	POSTAL CODE
[REDACTED]	2325 Luxumy	N8P 1S9
[REDACTED]	2500 Chateau	N8P 1W2
[REDACTED]	1772 Chateau	
[REDACTED]	11325 FIR GROVE DR.	N8P 1L4
[REDACTED]	2612 Chateau	N8P 1N3
[REDACTED]	City 350 city hall	
[REDACTED]	2548 Chateau ave	N8P 1N2
[REDACTED]	2152 QUESTA DR	N8P 1N8.
[REDACTED]	2337 Chateau	
[REDACTED]	2189 Questa Dr.	N8P 1N8




2110

Record of Attendance

East Riverside – South Neighbourhood – Mixed Use Development

Residents Meeting – April 19, 2023

NAME (PLEASE PRINT)	MAILING ADDRESS	POSTAL CODE
	2079 Questa DR.	N8P 1N8
	1812 Questa Dr.	N8P 1M5
	1952 Questa Dr.	N8P 1M5
	2104 QUESTA DR	N8P 1N8
	2152. QUESTA DR.	
	1732 BANWELL RD.	N8P 1P4
	1732 BANWELL RD	N8P 1P4
	2596 Chateau Ave	N8P 1N2
	2164 QUESTA DR	N8P 1N8
	2144 QUESTA DR	N8P 1N8

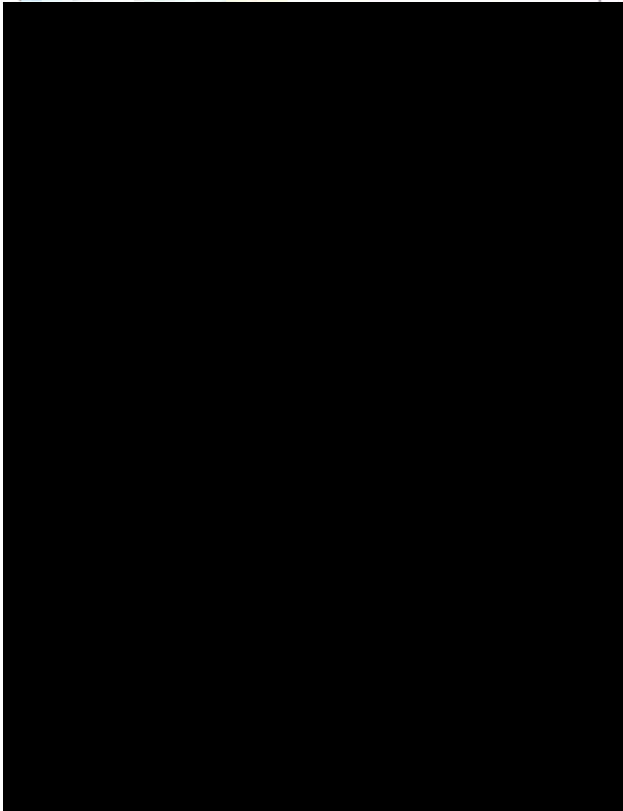


3/10

Record of Attendance

East Riverside – South Neighbourhood – Mixed Use Development

Residents Meeting – April 19, 2023

NAME (PLEASE PRINT)	MAILING ADDRESS	POSTAL CODE
	2564 Chateau, Ave	N8P 1N2
	11273 Firgrove Dr.	N8P 1M5
	2561 CHATEAU AVE	N8P 1N2
	1860 QUESTA DR	N8P 1M5
	2388 chateau Ave.	N8P 1M2
	1338 Blue Heron Crt	N8P 1M8
	2644 CHATEAU AVE	N8P 1N3
	" "	" "
	2401 chateau	N8P 1N2
	1109 Thunderbay	N8P 0C8
	2212 Quseta	



4/10

Record of Attendance

East Riverside – South Neighbourhood – Mixed Use Development

Residents Meeting – April 19, 2023

NAME (PLEASE PRINT)	MAILING ADDRESS	POSTAL CODE
	2072 Chateau	N8P 1N6
	1800 Questa	N8P 1M5
	1836 QUESTA.	N8P 1M5,
	2468 CHATEAU	N8P 1N2
	2188 Questa	N8P 1N8
	2188 Questa	N8P 1N8
	1976 QUESTA	N8P 1M5
	2259 QUESTA DR.	N8P 1N8
	1988 QUESTA DR.	N8P 1M5
	1829 KIRKLAND AVE	N8P 1L9
11229 LEATHORNE ST.	N8P 1N6	

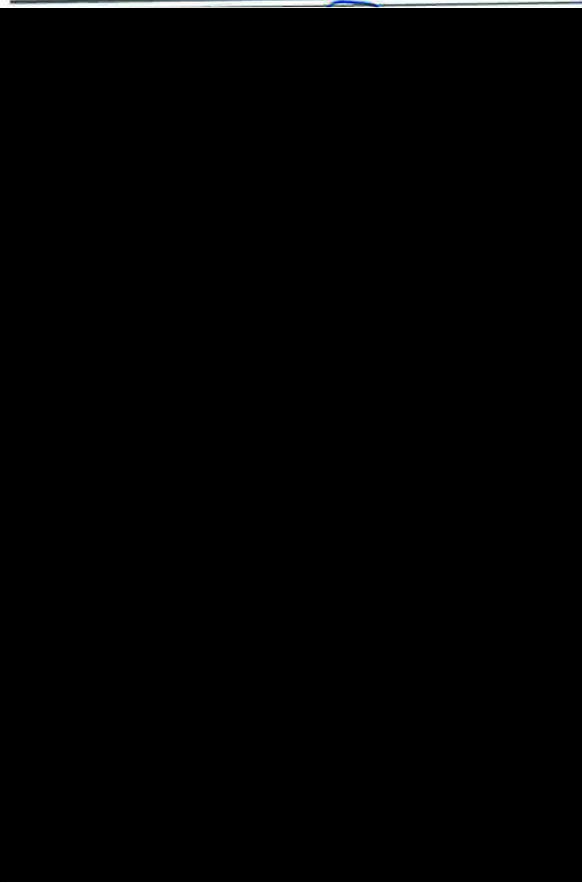


5/10

Record of Attendance

East Riverside – South Neighbourhood – Mixed Use Development

Residents Meeting – April 19, 2023

NAME (PLEASE PRINT)	MAILING ADDRESS	POSTAL CODE
	7-92 Questa Dr. 1824 QUESTA DR	N8P 1N8 N8P 1N8
	2068 QUESTA DR	N8P 1N8
	2128 Questa Dr	N8P 1N8
	2248 Questa DR.	N8P 1N8
	2223 QUESTA	" "
	2576 TRAPPERS	N8P 1N5
	11261 FIRGROVE	N8P 1M5
	11256 LEATHORNE	N8P 1N8
	1805 Kirkland	N8P 1L9
	1805 Kirkland	N8P 1L9



6/10

Record of Attendance

East Riverside – South Neighbourhood – Mixed Use Development

Residents Meeting – April 19, 2023

NAME (PLEASE PRINT)	MAILING ADDRESS	POSTAL CODE
	2085 Chateau Ave, Windsor	N8P 1N2
	2532 CHATEAU AVE WINDSOR	N8P 1N2
	2067 Chateau WIN ON	N8P 1N6
	" "	"
	11949 Boulder	N8P 1Z4
	2212 QUESTA	N8P 1N8
	2420 CHATEAU AVE	N8P 1N2
	2628 Chateau Ave	N8P N3
	2512 TRAPPERS	N8P 1N4
	2512 TRAPPERS	" "
	2032 QUESTA	N8P 1N8

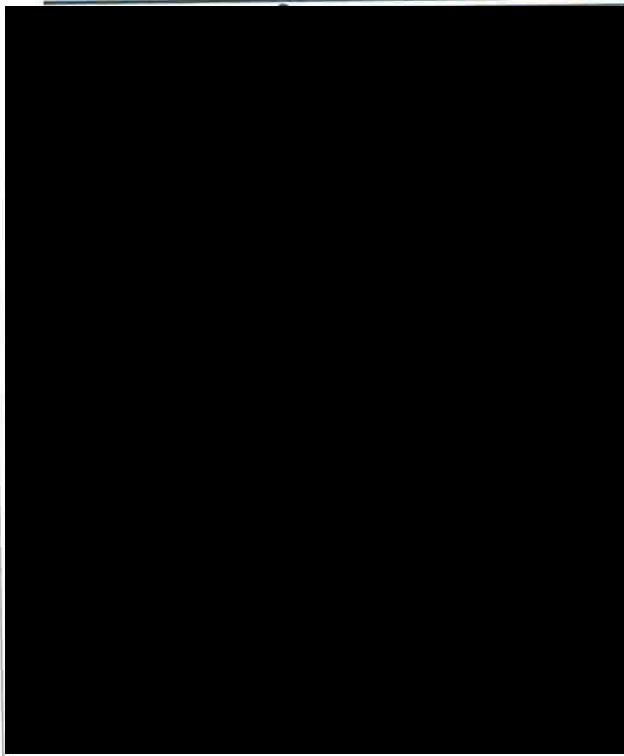


7/10

Record of Attendance

East Riverside – South Neighbourhood – Mixed Use Development

Residents Meeting – April 19, 2023

NAME (PLEASE PRINT)	MAILING ADDRESS	POSTAL CODE
	2272 Questa Dr	②
	2008 Questa Dr	
	2008 Questa Dr	
	11250 Wandaotte St East	N8P 1J9
	1105 Banwell	N8P 1J3
	2260 QUESTA DR.	N8P 1N6
	1928 Questa DR	
	1872 QUESTA DR	N8P 1M5
2545 Chateau Ave.	N8P 1N2	



8/10

Record of Attendance

East Riverside – South Neighbourhood – Mixed Use Development

Residents Meeting – April 19, 2023

NAME (PLEASE PRINT)	MAILING ADDRESS	POSTAL CODE
[REDACTED]	2080 Questa	N8P 1N8
[REDACTED]	2080 Questa	N8P 1N8
[REDACTED]	2090 CHATEAU	N8P 1N6
[REDACTED]	2044 QUESTA	N8P 1N8
[REDACTED]	e 1964 Questa	N8P 1H5
[REDACTED]	11596 Arvilla St	N8P 1L5
[REDACTED]	2036 CHATEAU AVE.	N8P 1N6
[REDACTED]	2020 QUESTA DR.	N8P 1N8.
[REDACTED]	2020 QUESTA DR.	N8P 1N8
[REDACTED]		



9/10

Record of Attendance

East Riverside – South Neighbourhood – Mixed Use Development

Residents Meeting – April 19, 2023

NAME (PLEASE PRINT)	MAILING ADDRESS	POSTAL CODE
[REDACTED]	2525 Trappers Ave	N8P 1N4
[REDACTED]	2525 Trappers Ave	N8P 1N4
[REDACTED]	2270 Chateau	N8P 1N4





1027458 ONTARIO INC.
BANWELL AND McHUGH -
MIXED USE DEVELOPMENTS
PUBLIC INFORMATION CENTRE

LOCATION MAP
FIGURE 1.0

BANWELL AND McHUGH -
 NORTH 'A' SITE
 (±1.43ha / 3.54ac)

BANWELL AND McHUGH -
 NORTH 'B' SITE
 (±1.66ha / 4.1 ac)

BANWELL AND McHUGH -
 SOUTH SITE
 (± 5.35ha / 13.23ac)

PROJECT:
 STATUS: DRAFT
 DATE: 12/04/2023

File Location:
 c:\users\j1\desktop\1_dillon_projects\22-5144 pic figures\addresses\22-5144 &
 22-5266 - banwell sites - addresses.dwg
 April, 12, 2023 10:00 AM

SOURCE: MAPPIVCITY WINDSOR AERIAL (2021)

MAP/DRAWING INFORMATION
 THIS DRAWING IS FOR INFORMATION PURPOSES ONLY. ALL
 DIMENSIONS AND BOUNDARY INFORMATION SHOULD BE
 VERIFIED BY AN O.L.S. PRIOR TO CONSTRUCTION.
 CREATED BY: RR
 CHECKED BY: TJO
 DESIGNED BY: RR



SCALE: N.T.S.

From: [Catherine Zold](#)
To: [Stuart, Kelly](#)
Subject: Re: Zoning comments
Date: September 21, 2023 11:09:39 AM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

This email is to give reasons for my objecting the amendment to allow condos being built on Banwell. Re: Meeting October 3, 2023, at 4:30 p.m.

.. not enough parking. People already parking on Questo from condos already built.
.. traffic will be problematic...too heavy. == congestion, congestion and more congestion
.. flooding is already a problem on Chateau and surrounding streets. This will only get worse.
Sewers won't be able to handle in heavy rain or multiple days of rain. I do not believe promises of better sewers that will accommodate residents. Builders always make promises.
.. the land is just too small to accommodate these buildings.

Catherine Zold-Butcher
2252 Chateau Ave
Windsor, On
N8P 1N6

Sent from my iPad

Development & Heritage Standing Committee
October 3, 2023
Item 7.2 – Written Submission

From: D Dups
Sent: September 19, 2023 2:31 PM
To: clerks <clerks@citywindsor.ca>
Cc: Abbs, James <jabbs@citywindsor.ca>
Subject: File Numbers OPA/7072 and ZNG/7069 - Banwell and McHugh Zoning By-law 8600 Amendments

Hello

I am a resident near the proposed building site of several high-rise buildings at or near the corner of Banwell and McHugh in East Windsor. I received a letter from Anna Ciacelli, Deputy City Clerk of Windsor Ontario outlining that there was a proposed zoning change to accommodate a development of that plot to include a 6-story building, an 8-storey building, two 10-storey buildings and two 2-storey buildings. 408 residential units in total and 603 parking spaces.

This proposed development will result in vastly increased traffic. The expansive parking lots will pave over green-spaces. The tall buildings will disrupt views and cast shadows down upon the homes of resident who have lived in this neighbourhood for decades. The intrusive and stark contrast of these multiple of tall buildings will clash with the suburban nature of this neighbourhood. The out of place, excessively tall buildings will result in current residences being peered down upon, a violation of the current residents' privacy.

These buildings are not in keeping with the character and nature of this established suburban neighbourhood. There is too much paving, too much parking, too many housing units, and at too tall a height to be built in this already established suburban neighbourhood.

Therefore I object to the proposed zoning change, the request to increase the permitted height to 35m, and the reduction in landscaped open space from 35% as required down to 24.5% as found in File Numbers OPA/7072 and ZNG/7069.

I submit this email on record and in opposition to any amendments to Zoning By-Law 8600 pertaining to File Numbers OPA/7072 and ZNG/7069.

We have Zoning By-Laws to govern our city. There should be no allowance, amendments, or exceptions to them in this case to injure this long-established suburban neighbourhood.

Yours,
D. Dupuis
Resident of East Riverside near McHugh and Banwell

**Development & Heritage Standing Committee
October 3, 2023
Item 7.2 – Written Submission**

-----Original Message-----

From: Lorna Barrow

Sent: September 14, 2023 5:32 PM

To: clerks <clerks@citywindsor.ca>

Subject: Amendments to Official Plan and Zoning By-law 8600

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Re: file # OPA/7072 and ZNG/7069

As suggested in the notice we received regarding the above amendments, we wish to voice our objections to these changes. Because of the expected development in other areas of this neighbourhood, we feel these amendments will only make congestion of the area even worse. We realize more housing is needed but this is too much within a condensed area.

We ask that you turn down the request by the developer to amend the bylaw.

Robert and Lorna Barrow

11185 Leathorne St.,

Windsor On

Sent from my iPad

Development & Heritage Standing Committee
Tuesday, October 3, 2023
Item 7.2 – Written Submission

From: Sara Lehoux
Sent: September 29, 2023 12:01 PM
To: clerks <clerks@citywindsor.ca>
Subject: Fwd: Development & Heritage standing committee meeting

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello,

I am a resident in the area where the new condos are proposed, Multiple Dwelling Development Banwell & McHugh (South) Ward 7.

I don't want to attend the meeting to speak, but if someone can bring up one issue that is not mentioned on the documents I received in the mail (Report S 121/2023).

Sara Lehoux

Area resident

My concern is the amount of traffic using the one access point that will be the entrance to the 4 condos, business office and respite home. Given the fact that Banwell and McHugh intersection is busy for the majority of the day and into the night, there are multiple vehicle collisions throughout the year. Being that the traffic in the area will increase, my concern will be vehicles entering and leaving the driveway as it will not have a stop sign, traffic light or any other form of controlled access. This is a concern, considering the amount of traffic in the area already, the closeness to a school bus zone with children 100 metres down from the entrance.

Can someone speak on our behalf about this issue please.

Another neighbour had concerns of where all the new children from these condos will be attending and are there enough schools in the area to house them.

Thank you for your time.

Sara Lehoux

Development & Heritage Standing Committee
Tuesday, October 3, 2023
Item 7.2 – Written Submission

From: Ron Sitarz

Sent: September 30, 2023 2:37 PM

To: clerks <clerks@citywindsor.ca>

Cc: Marignani, Angelo <amarignani@citywindsor.ca>; Menard, Christopher (Cultural Affairs) <cmenard@citywindsor.ca>; mayoro <mayoro@citywindsor.ca>; Sleiman, Ed <esleiman@citywindsor.ca>; Costante, Fabio <fcostante@citywindsor.ca>; Francis, Fred <ffrancis@citywindsor.ca>; Kaschak, Gary <gkaschak@citywindsor.ca>; Morrison, Jim <jmorrison@citywindsor.ca>; Gignac, Jo-Anne (Councillor) <joagignac@citywindsor.ca>; Mckenzie, Kieran <kmckenzie@citywindsor.ca>; McKenzie, Mark <mmckenzie@citywindsor.ca>; Agostino, Renaldo <ragostino@citywindsor.ca>

Subject: Development and Heritage Planning Committee

RE:

Application for Zoning Change

Applicant: 1027458 Ontario Inc.

Location: Banwell and McHugh (South)

While I am not opposed to a development on this site, I am opposed to this particular proposed development for the following reasons:

1. The site is in a residential area and should **remain residential**. As I read the proposed zoning change document sent to me, there is currently a maximum building height restriction of 4 stories.

This height restriction should be maintained for all the buildings contained in this proposal. / or at the very most increased to 6 stories.

The 6 story height restriction would be **consistent with the height of other new residential buildings on Banwell, McHugh (next to WFCU Centre), Tecumseh Road (in Tecumseh) – maintaining the 4 storey restriction is preferable.**

2. The number of parking spaces in the proposal (603) would **add at least 603 cars X 2 = 1206** cars of traffic per day to McHugh “Speedway” which is already overburdened with speeding, “racing”, and running School Bus Stop Signs when activated.

I live on the corner of Luxury and McHugh “Speedway” and I see this every day.

This is the result of **almost zero traffic enforcement** on McHugh “Speedway” – between Banwell and Clover - today as compared to enforcement efforts a few years ago. – this proposal will actually make the problem worse.

3. The additional vehicles will also add to the noise pollution coming from loud mufflers and engines on cars, pick-up trucks and “Yamaha” type motorcycles – again **No Enforcement on McHugh “Speedway”**.
4. The number of proposed parking spaces for the development as currently proposed will be inadequate to accommodate additional vehicles belonging to those residents – which will flow over onto neighboring residential streets creating the same street parking issues that

currently exist on Questa and other neighboring streets for the recently completed 3 residential buildings on Banwell.

5. This is the second such proposal (first 3 buildings are complete on Banwell) and there will be more to follow along Banwell. The residential area (here first) will be adversely burdened by the above concerns even more so in the future.

6. **The planning Committee and City Council needs to take a very hard look at what will happen to this neighborhood with the addition of High-Rise Apartment Buildings (greater than 4 storeys).**

Respectfully

Ron Sitarz
2298 Luxury Ave.
Windsor, N8P 1W9

**City Council Meeting
October 30, 2023
Item 8.5 – Written Submission**

From: Paul Charbachi <Paul_Charbachi@viarail.ca>

Sent: October 5, 2023 10:27 AM

To: clerks <clerks@citywindsor.ca>

Cc: Allan Fisher <Allan_Fisher@viarail.ca>; Shant Demirdjian <Shant_Demirdjian@viarail.ca>; Jefferson Lefranc <Jefferson_Lefranc@viarail.ca>; Michael Mazzilli <Michael_Mazzilli@viarail.ca>

Subject: File number OPA/7072 and ZNG/7069

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello,

The Applicant must submit engineering drawings signed and sealed by a certified professional. The engineering drawings will be reviewed by an engineering firms designated by VIA at the Applicant's expenses.

The Applicant must also submit locates to VIA. The locates must be submitted to VIA electronically and physically. The Applicant must meet the following requirements:

- **Transport Canada:**
- *Railway Safety Act, Part III, Sections 24 and 25.*
- **For Clearance:**
- *Railway Right of Way Access Control Policy;*
- *Wire Crossings and Proximities Regulations – C.R.C., c. 1195;*
- *Standards Respecting Railway Clearances – TC E-05;*
- Notice of Railway Works Regulations, a copy of the notice must be sent to VIA.

- **Traffic control near a railways:**
- *Circular 13 Railway Association of Canada*

- **For Grade Crossings:**
- *Grade Crossings Regulations;*
- The provisions that must be adhered to with respect to the creation of new entrance ways or intersecting roads from the nearest rail. Reference GCR Sub-Section 101(1) and Grade Crossings Standards Article 11.
- *Grade Crossings Standards;*
- *Transport Canada Standard for LED Signals Modules at Highway/Railway Grade Crossings – TC E-14;*
- *Minimum Railway/Road Crossing Sightline Requirements for All Grade Crossings Without Automatic Warning Devices – G4-A.*
- The requirements surrounding sightlines, of which any construction or activities (Duplex development) on the property or new properties must ensure they do not obstruct the required minimum grade crossing sightlines. (reference Section 21 of the GCR).
- All crossings shall be constructed / design with at least 20 feet 0 grade from each side of the rail

- **Canadian Standards Association:**
- CAN/CSA C22.3 No. 1 – Overhead Systems;

- CAN/CSA C22.3 No. 7 - Underground Systems;
- CAN/CSA Z662 – Oil and Pipeline Systems;
- CAN/CSA-B137.4 - Polyethylene Piping Systems for Gas Services.
- **VIA:**
- *Buried Signal and Communication Guidelines;*
- *Guidelines for New Development;*
- *guidance which the Federation of Canadian Municipalities (FCM) has created on this topic specifically, you can find their guidance within the following link: Guidelines for New Development in Proximity to Railway Operations.*
- Adjacent landowners, buildings and overhead structures are not allowed to drain or modify existing drainage ways to divert water onto railway property without a hydraulic study and approval of the VIA Rail Infrastructure Department;
- All loads must be in compliance with Cooper E90;

- **The Federation of Canadian Municipalities and the Railway Association of Canada:**

- *Guidelines for New Development in Proximity to Railway Operations.*

- **Other:**

- Proper fencing must be included or planned to be installed in order to avoid any trespassing or intrusions into the VIA right-of-way;

- All fence maintenance will be done on the Applicant expense.

In addition, the Applicant must comply with the following areas of concern for which VIA request information, reassurances and/or commitments with regards to the application:

- **Utilities:**

- Electrical and Gas Supply

VIA would like assurances from the City and the Applicant that the new development will not negatively impact on the capacity, availability, stability of the supply and future growth capability thereof.

- Communications

VIA would like assurances from the City and the Applicant, that the new development will not impact VIA's operations as a result of potential alterations to the existing cellphone towers or any other fibre-optic infrastructures supplying the VIA station and property.

- **Water & Wastewater:**

- Drainage Sanitary/Storm

VIA would like assurances that the new development will not limit or interfere with its operations, specifically the main sanitary drainage that runs from the Train Yards or station, through VIA's property towards the proposed development.

- Water supply

VIA would like assurances that the new development will not affect the supply and water pressure that is provided for the station.

- **Construction Disturbances:**

- VIA requests a copy of the Pedestrian study (from New Development to LRT).

VIA is concerned by the flow of people that will go through our premises (either interior or exterior) to access the LRT station.

- Station access (vehicle traffic)

Confirmation that the New Development access/exits, and traffic volumes will not affect or interfere VIA traffic circulation. VIA also needs confirmation that Avenue L (yellow dotted line shown on Exhibit A), as well as the access to it, will be kept for our operations and upcoming growth.

- **Neighbour Relationships:**

- VIA requests the Applicant's monitoring and management plan of the impacts of its construction, including but not limited to:
 - Air contaminants / Dust pollution;
 - Noise pollution / Working hours;
 - Existing conditions;
 - and the impacts of vibrations.
- VIA requests the Applicant's communication and management plan for future tenants and or owners of the project with respect to VIA's active train station nearby, that may produce one or more of, but not limited to, the following: emission of noise, dust, vibration, fumes, odours and other gaseous or non-gaseous emissions that may affect the enjoyment of the development for which VIA shall not be held responsible.

VIA requests the Applicant's commitment to making all efforts not to interfere with VIA's operations, VIA's track infrastructure or use of VIA property. When in the vicinity of VIA property or Railway right-of-way, VIA requests the Applicant commitment to comply with and conform to all VIA, Department of Transport and Canadian Transportation Agency rules and regulations, or any other authority having jurisdiction.

When and where the City's or the Applicant's actions, whether direct or indirect, negatively impact any of the above, VIA's operations, and or VIA's property, VIA wants assurances from the City and the Applicant that they will take all necessary and possible steps to mitigate or eliminate those impacts.

In light of our requests, VIA requires the City and the Applicant to indemnify VIA against any and all claims, damages or proceedings (including legal costs and other costs and expenses) that may arise in relation to the non-compliance to any condition contained in this letter.

Should you have any questions or concerns, please feel free to contact the undersigned.

Sincerely,



Paul Charbachi

Infrastructure Engineer

M: 514-607-5833

Paul_Charbachi@viarail.ca

Item No. 8.5



Committee Matters: SCM 269/2023

Subject: Draft Plan of Condominium with Exemption under Section 9(3) of the Condominium Act –CDM 008-23 [CDM-7134] Lankor Horizons Development Inc. 3290, 3320, 3340, 3370 STELLA CRESCENT Ward 7

Moved by: Councillor Kieran McKenzie
Seconded by: Councillor Angelo Marignani

Decision Number: **DHSC 560**

THAT the application of Lankor Horizons Development Inc. for an exemption under Section 9(3) of the *Condominium Act* for approval of a plan of condominium (Standard Condominium), comprised of a total of 199 dwelling units as shown on the attached Map No. CDM-008/23-1 and CDM-008/23-2 on a parcel legally described as; Part of Lot127, Concession 2, City of Windsor, Part 1 to 3 (inclusive), 12R-16010, located at 3290, 3320, 3340, 3370 STELLA CRESCENT **BE APPROVED** for a period of three (3) years.

Carried.

Report Number: S 117/2023
Clerk's File: Z/14663

Clerk's Note:

1. The recommendation of the Development & Heritage Standing Committee and Administration are the same.
2. Please refer to Item 7.3 from the Development & Heritage Standing Committee held on October 3, 2023.
3. To view the stream of this Standing Committee meeting, please refer to:
<http://csg001-harmony.sliq.net/00310/Harmony/en/PowerBrowser/PowerBrowserV2/202320231003/-1/9435>



PLAN OF CONDOMINIUM

APPLICANT: LANKOR HORIZONS DEVELOPMENT INC.
 ADDRESS: STELLA COURT

SCALE: N.T.S.
 DATE: SEPT. 2023
 FILE NO: CDM-008/23-1

CDM-008/23-1

Site Information:

OFFICIAL PLAN	ZONING	CURRENT USE	PREVIOUS USE
Residential	Residential District 3.2 (RD3.2)8	4 Multiple dwelling structures (Under Construction)	Vacant Parcel
LOT WIDTH	LOT DEPTH	AREA	SHAPE
71m	+/-257m	1.775ha	Irregular
All measurements are for the entire parcel and are approximate.			

Neighbourhood Characteristics:

The site is located between a residential neighbourhood to the east, mainly occupied by single detached residential dwelling, Municipal Park to the west, EC Row Expressway to the south, and vacant land and single detached residential to the North.

Discussion:**Planning Analysis:****Statutory Regulations:**

Under Section 9 of the Condominium Act, an owner may request approval of a plan of condominium subject to Section 51 of the Planning Act (subdivisions). As such, the usual approval process for plans of subdivision is invoked, i.e. review by municipal and provincial agencies, draft plan approval, a condominium agreement and final registration.

The Condominium Act also provides that owners can be exempted from the above mentioned Planning Act provision if the approval authority (i.e. the City of Windsor) is of the opinion that "such exemption is appropriate in the circumstances". The reasons for exemptions are not specified, but usually applicants can be exempted if the following conditions are satisfied:

- (i) that all municipal requirements and conditions have been addressed (for example by an approved application for rezoning and/or site plan control); and
- (ii) that the building(s) is suitable for a condominium by virtue of design and amenities.

The proposed condominium complies with the above conditions.

Official Plan:

The subject property is designated Residential on Schedule D of the City of Windsor Official Plan. The designation permits this use. The proposed development conforms to the residential policies in the Official Plan.

The City of Windsor has policies in the Official Plan for approval of a plan of condominium with exemption under Section 9 of the Condominium Act. Applications for exemptions may be considered by Council if:

- (a) a residential building is constructed or a building permit for its construction has been issued;

(b) the development has received site plan control approval; and

(c) the development (new proposed units) do not contain any occupied residential rental units.

The proposed condominium complies with the above conditions.

Zoning By-Law

The property is zoned Residential District (RD) 3.2. Building permits have been issued and construction of the first building is nearing completion.

Risk Analysis:

N/A

Climate Change Risks

Climate Change Mitigation:

N/A

Climate Change Adaptation:

N/A

Financial Matters:

N/A

Consultations:

Municipal and agency requirements have been addressed and implemented through the site plan control approval process, and the registration of a site plan agreement. (File SPC-038/20) registered as instrument CE1026981, on August 18, 2021)

Public Notice:

No public notification is required where exemptions are requested and the development is properly zoned for the permitted use.

Conclusion:

The application has been processed and evaluated with regard to both the Planning Act and the Condominium Act, as well as the City of Windsor Official Plan. It conforms to the City of Windsor Official Plan and complies with the zoning regulations contained in Bylaw 8600. Municipal requirements regarding this development have been addressed in the site plan control agreement. The draft plan of condominium is consistent with the approved site plan (File SPC-038/20) registered as CE1026981, on August 18, 2021)

It is recommended that this application for approval be exempted from Section 51 of the Planning Act (per Section 9(3) of the Condominium Act). This means that the owner can proceed directly to registration following submission of an approved final plan of condominium.

Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Greg Atkinson Manager of Development/Deputy City Planner

Neil Robertson Acting City Planner / Manager of Growth

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP JM

Approvals:

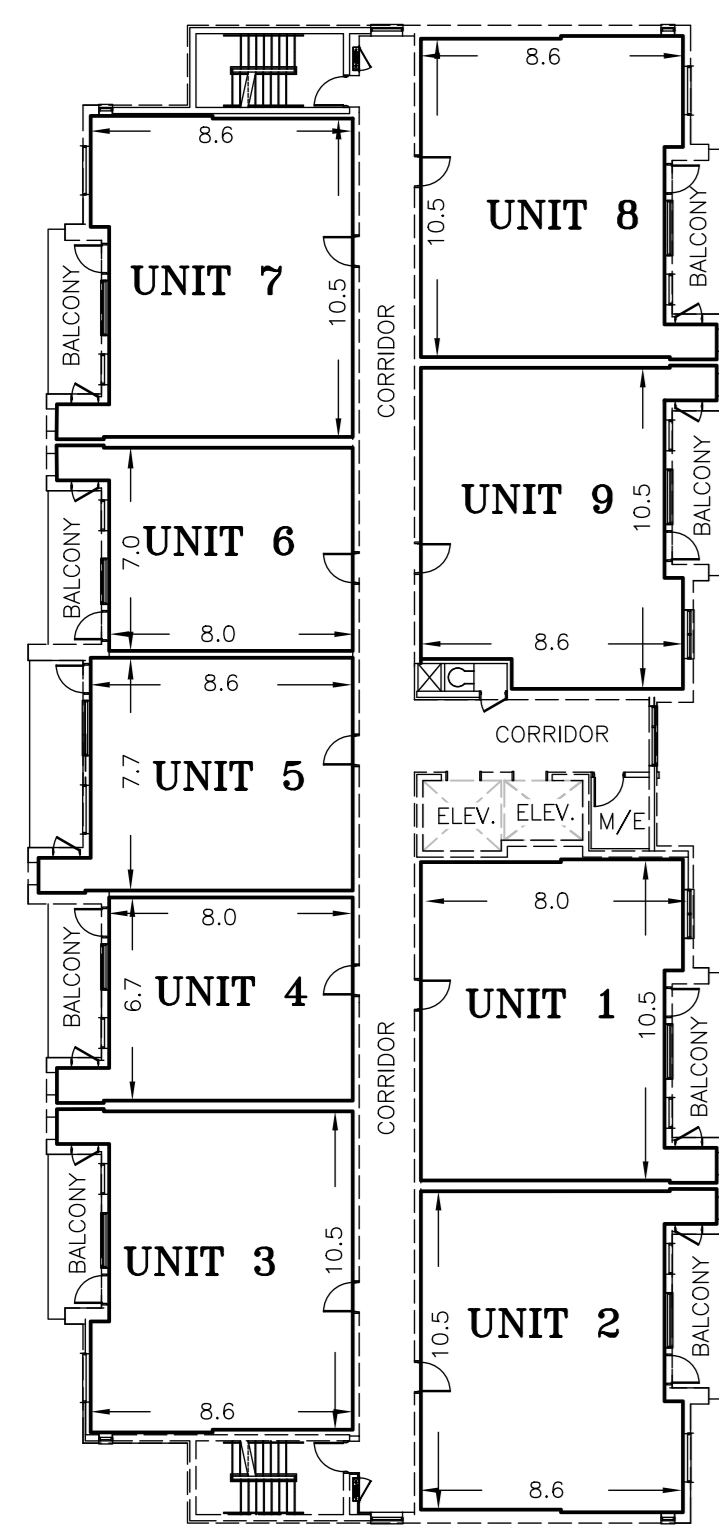
Name	Title
Greg Atkinson	Manager of Development/Deputy City Planner
Neil Robertson	Acting City Planner / Manager of Growth
Wira Vendrasco	Deputy City Solicitor, Legal Services & Real Estate
Jelena Payne	Commissioner of Economic Development and Innovation
Joe Mancina	Chief Administrative Officer

Notifications:

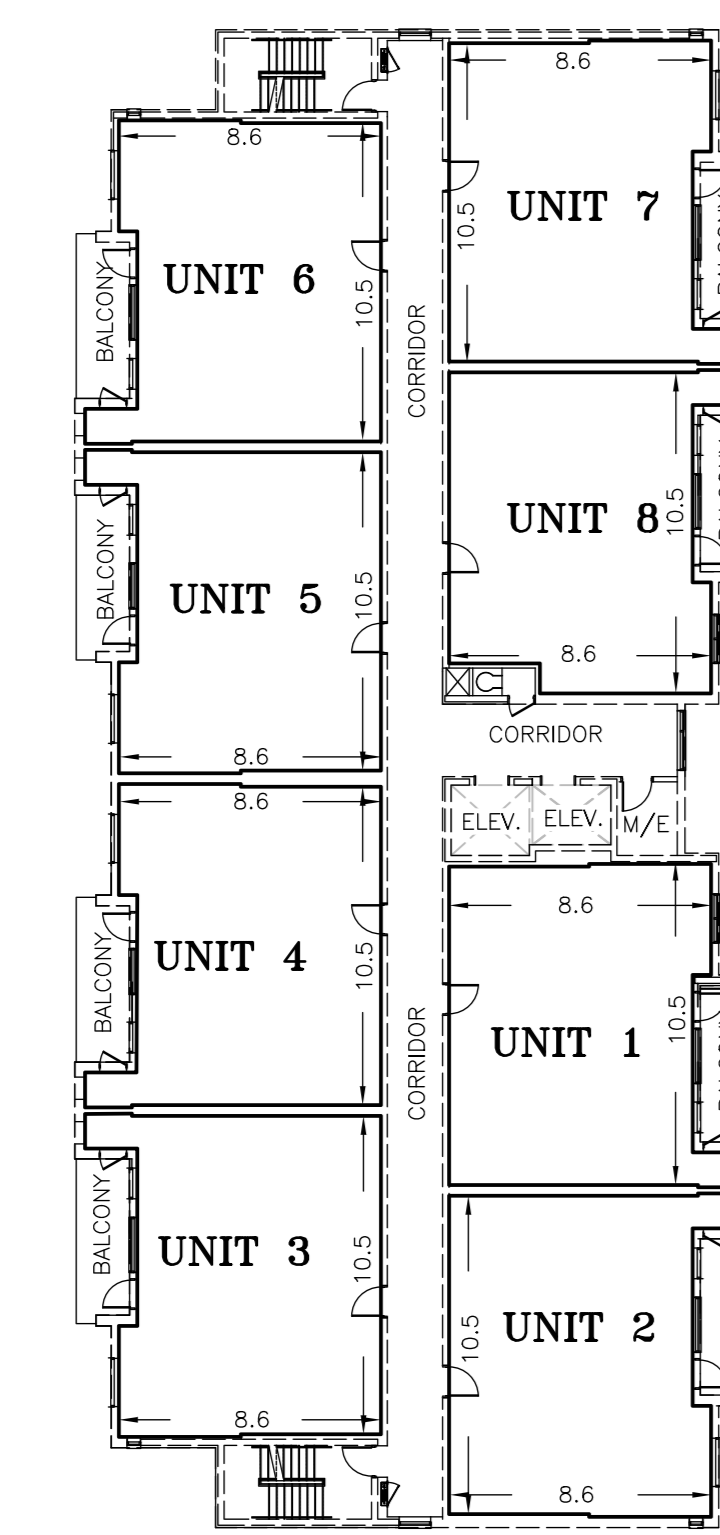
Name	Address	Email
Lankor Horizons Development Inc.	510 Rhodes Drive, Suite 520, Windsor ON	wingon.li@horizoninv.ca
Dillon Consulting Limited (Melanie Muir)	3200 Deziel Drive, Suite 608, Windsor ON	mmuir@dillon.ca

Appendices:

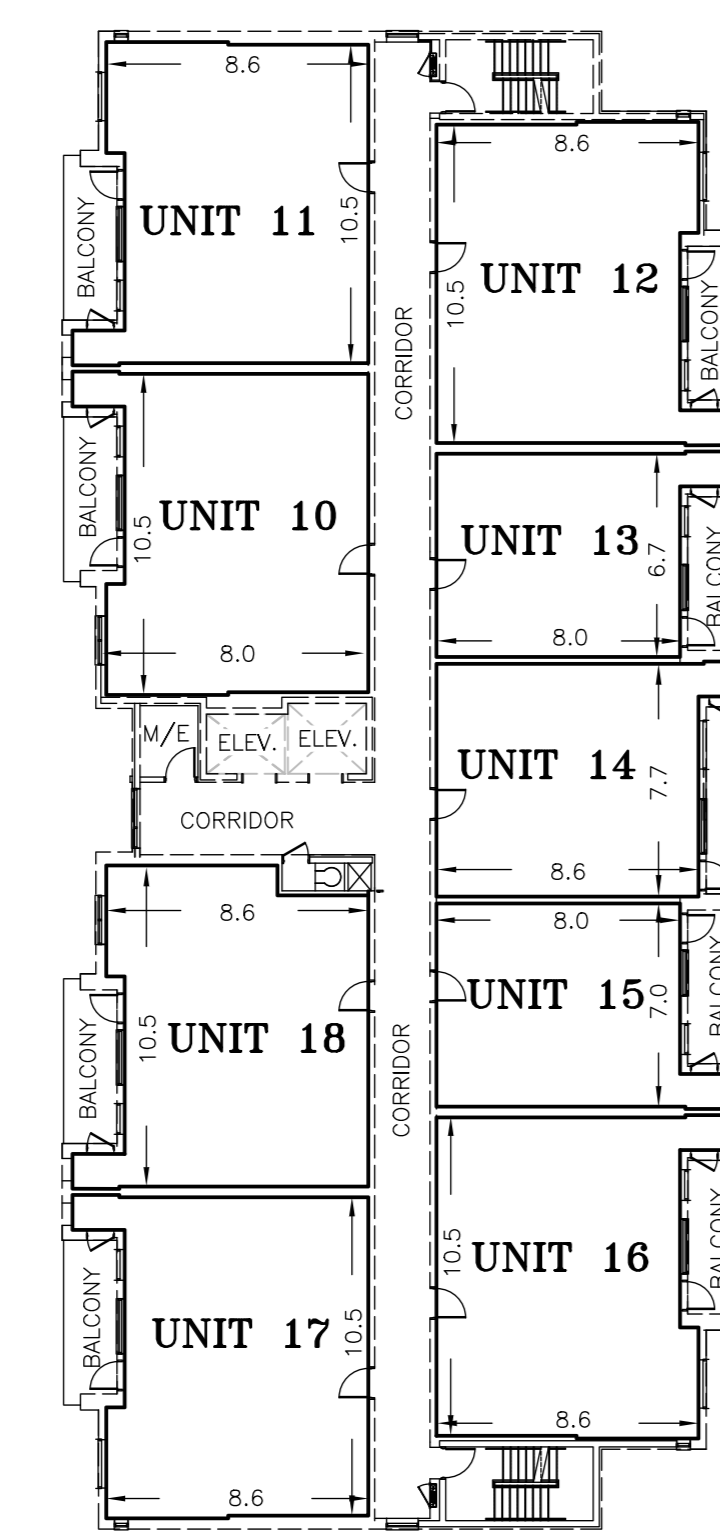
- 1 Proposed Draft Plan of Condominium



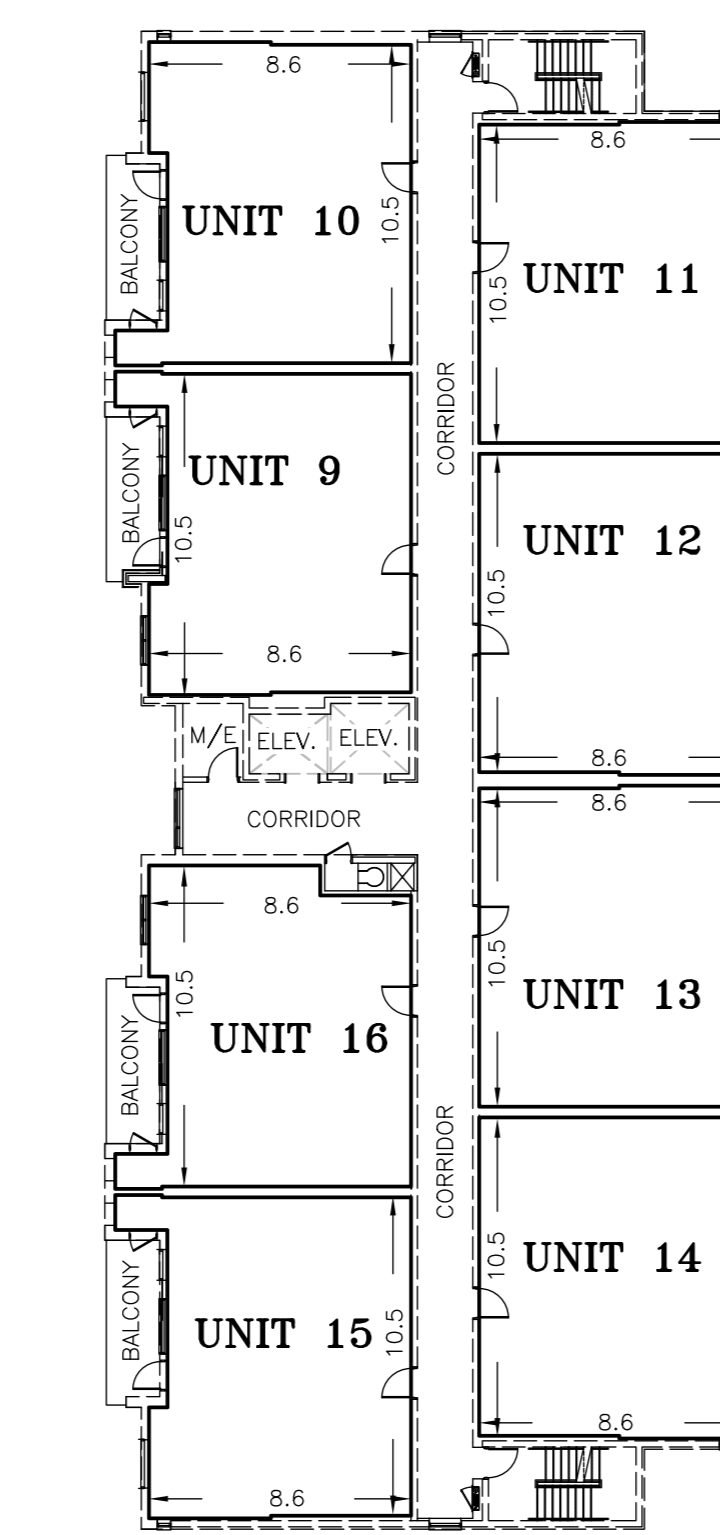
UNITS 1 to 9 (incl.)
LEVEL 2
BUILDING 1
SCALE = 1:250



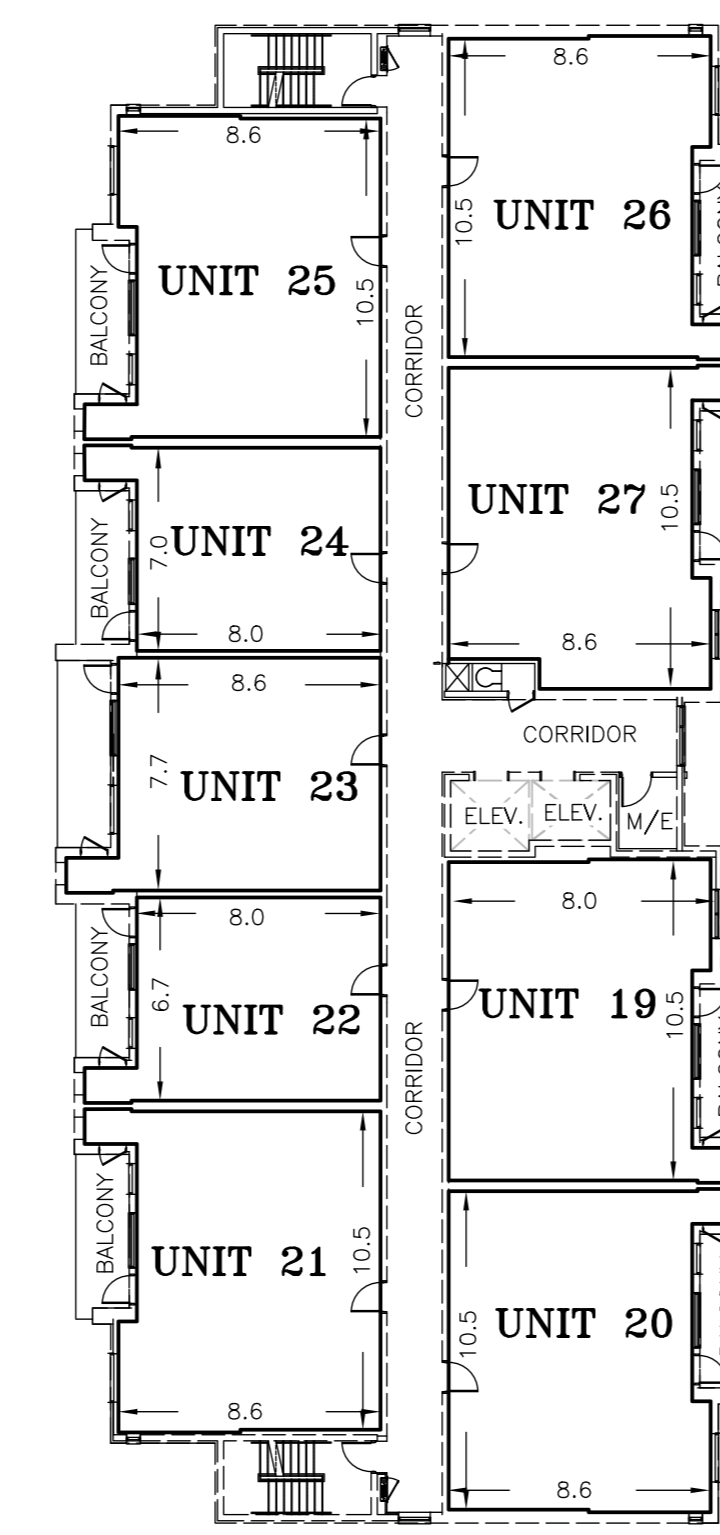
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LEVELS 3 to 6 (incl.)
BUILDING 3
SCALE = 1:250



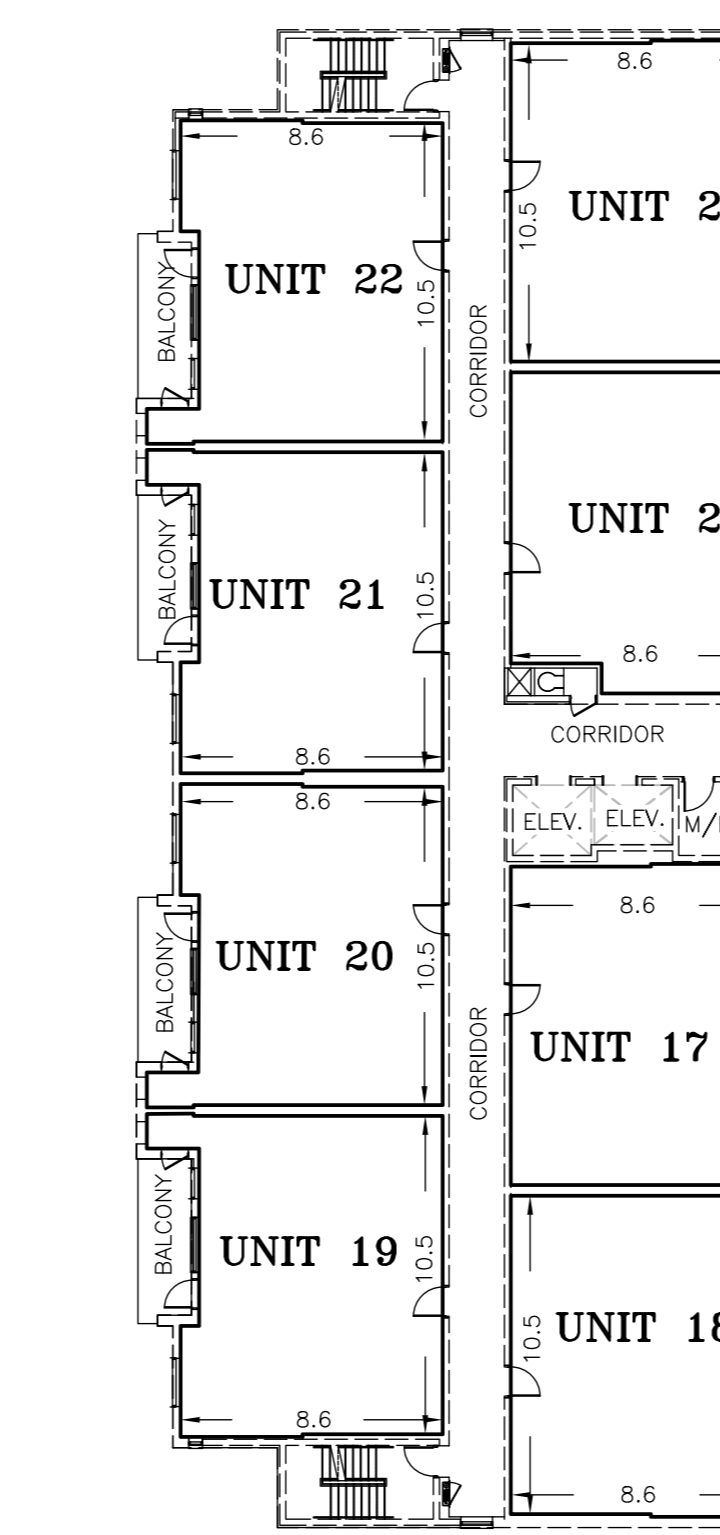
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LEVEL 2
BUILDING 2
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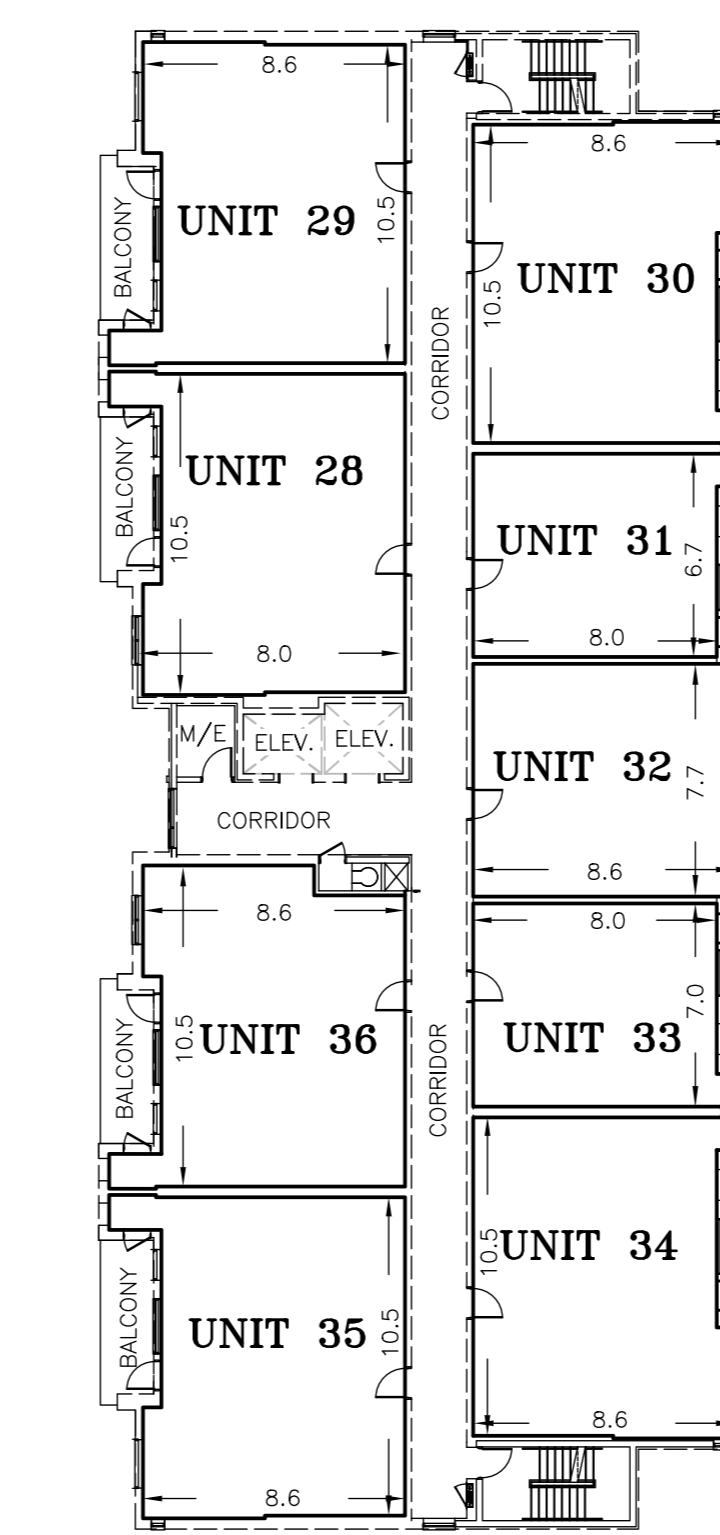
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LEVELS 3 to 6 (incl.)
BUILDING 2
SCALE = 1:250



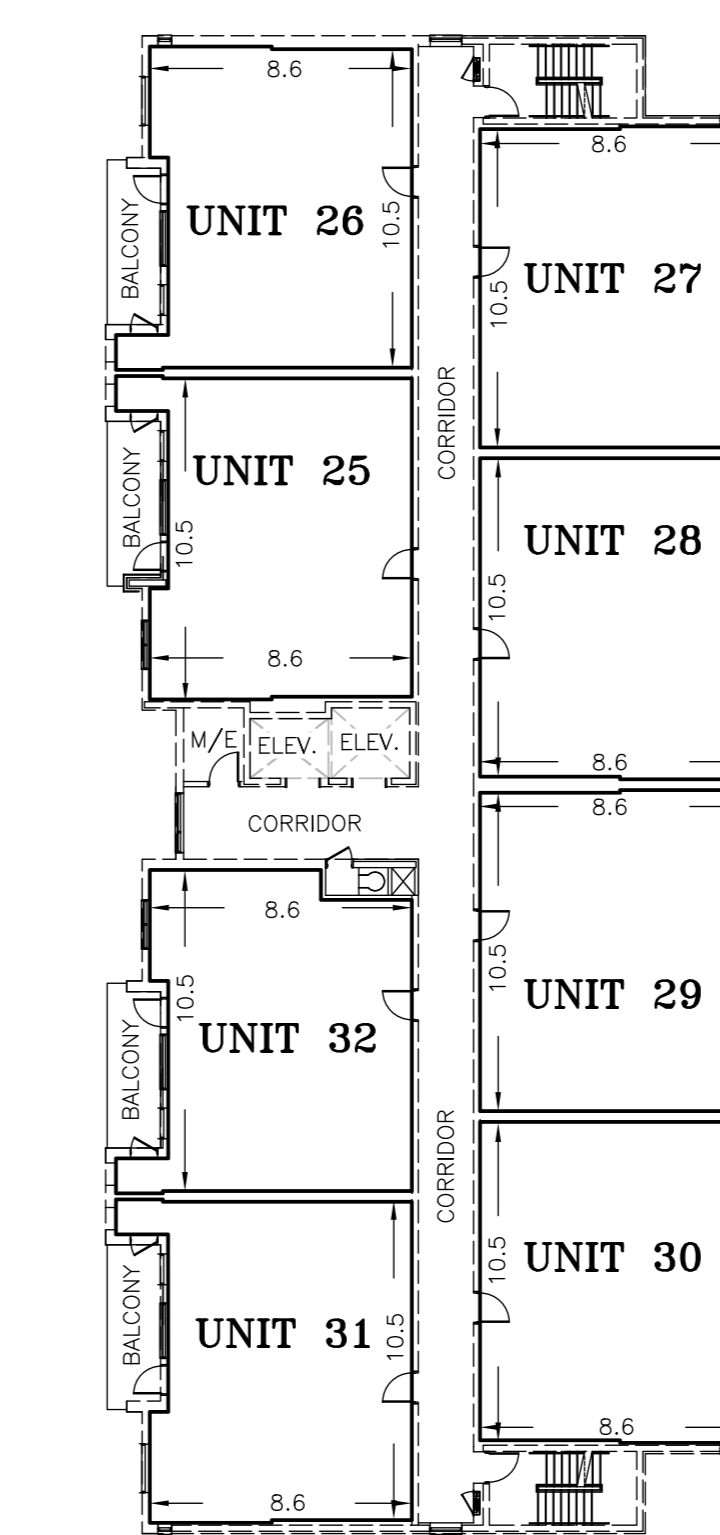
UNITS 19 to 27 (incl.)
LEVEL 2
BUILDING 3
SCALE = 1:250



UNITS 17 to 24 (incl.)
LEVELS 3 to 6 (incl.)
BUILDING 3
SCALE = 1:250



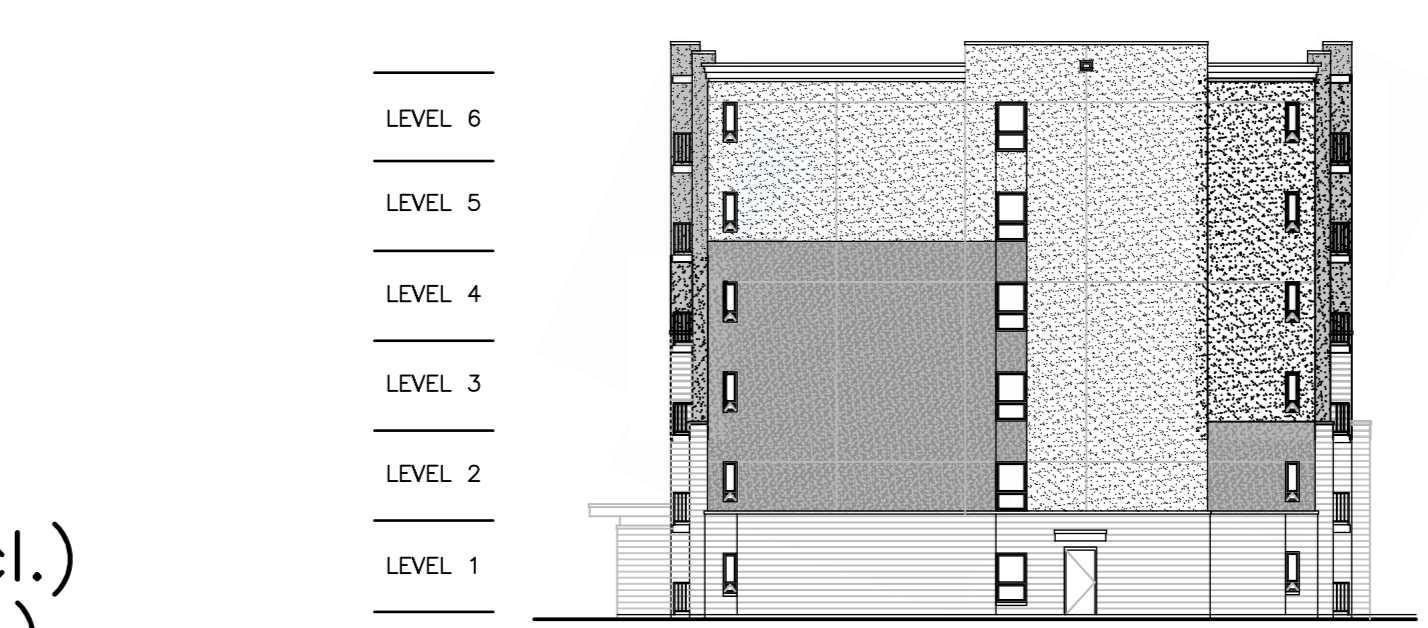
UNITS 28 to 36 (incl.)
LEVEL 2
BUILDING 4
SCALE = 1:250



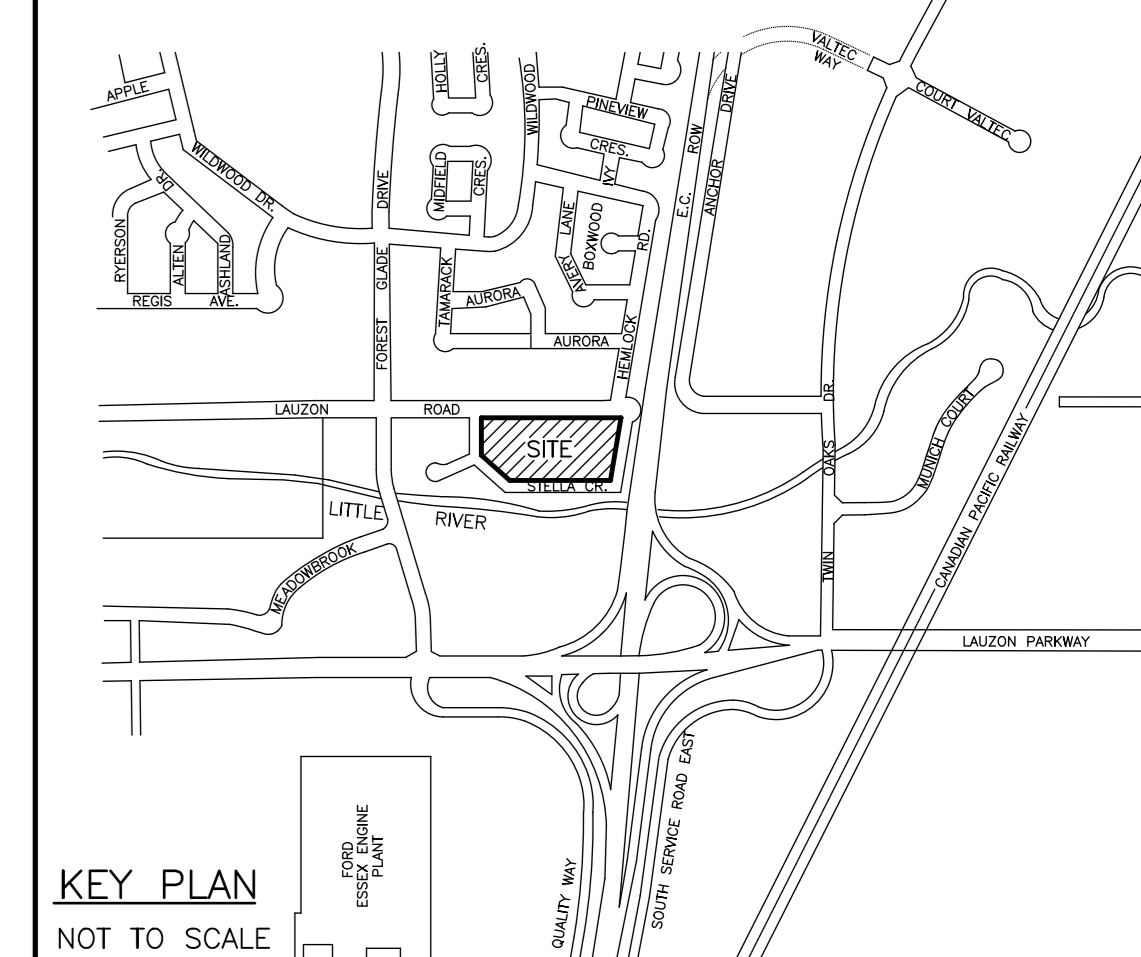
UNITS 25 to 32 (incl.)
LEVELS 3 to 6 (incl.)
BUILDING 4
SCALE = 1:250



FRONT ELEVATION
SCALE = 1:250



SIDE ELEVATION
SCALE = 1:250



DRAFT PLAN OF CONDOMINIUM
OF
PART OF LOT 127,
CONCESSION 2
GEOGRAPHIC TOWNSHIP OF SANDWICH EAST
NOW IN THE
CITY OF WINDSOR
COUNTY OF ESSEX, ONTARIO
© VERHAEGEN LAND SURVEYORS
SCALE = 1:250
5 2.50 5.00 10.00 15.00 METERS
150'

LEGEND AND NOTES
BEARINGS ARE UTM GRID DERIVED FROM OBSERVED REFERENCE POINTS "A" AND "B" BY REAL TIME NETWORK OBSERVATIONS AND ARE REFERRED TO UTM ZONE 17 (81° WEST LONGITUDE) NAD 83 (CSRS) (2010.0).
DISTANCES ON THIS PLAN ARE GROUND AND CAN BE CONVERTED TO GRID BY MULTIPLYING BY THE COMBINED SCALE FACTOR OF 0.99988603.
ALL BEARINGS AND DISTANCES SHOWN HEREON ARE IN AGREEMENT WITH PLAN 12R-28768.
■ DENOTES SURVEY MONUMENT FOUND
□ DENOTES SURVEY MONUMENT SET
SIB DENOTES STANDING IRON BAR
SIB DENOTES SHORT STANDING IRON BAR
B DENOTES IRON BAR
P DENOTES PLASTIC PIPE
WIT DENOTES WITNESS
M DENOTES MEASURED
L DENOTES PERPENDICULAR
I DENOTES INTERSECTION
CRP DENOTES OBSERVED REFERENCE POINT
(CU) DENOTES CURB UNDERGROUND
ALL SET SIB AND PB MONUMENTS WERE USED DUE TO LACK OF OVERBURDEN AND/OR PREVIOUSLY EXISTING UTILITIES IN ACCORDANCE WITH SECTION 11 (4) OF OREG. 525/91.
--- DENOTES UNIT BOUNDARIES
--- DENOTES EXTERIOR FACE OF STRUCTURAL WALLS
V DENOTES VESTIBULAR SPACE
PT DENOTES PATIO AREA
P DENOTES HANDICAP PARKING SPACE

ADDITIONAL INFORMATION REQUIRED UNDER THE
CONDOMINIUM ACT, 1998 AND SECTION 51 (17)
OF THE PLANNING ACT, 1990.
a) on draft Plan g) on draft Plan
b) on draft Plan h) municipal water
c) on draft Plan i) sewer line
d) residential condominium units j) on draft Plan
e) on draft Plan k) all municipal services available
f) on draft Plan l) on draft Plan

SITE AREA
1.775 HECTARES / 4.366 ACRES
199 RESIDENTIAL UNITS
DENSITY
112.7 RESIDENTIAL UNITS PER HECTARE
45.6 RESIDENTIAL UNITS PER ACRE

SITE INFORMATION

LEVEL	BUILDING 1		BUILDING 2		BUILDING 3		BUILDING 4	
	RESIDENTIAL UNITS PER LEVEL	NUMBER OF OPEN PARKING SPACES PER LEVEL	RESIDENTIAL UNITS PER LEVEL	NUMBER OF OPEN PARKING SPACES PER LEVEL	RESIDENTIAL UNITS PER LEVEL	NUMBER OF OPEN PARKING SPACES PER LEVEL	RESIDENTIAL UNITS PER LEVEL	NUMBER OF OPEN PARKING SPACES PER LEVEL
LEVEL 1	8	15	37	3	9			
LEVEL 2	9							
LEVEL 3	8							
LEVEL 4	8							
LEVEL 5	8							
LEVEL 6	8							
TOTAL	49	15	37	3	9			

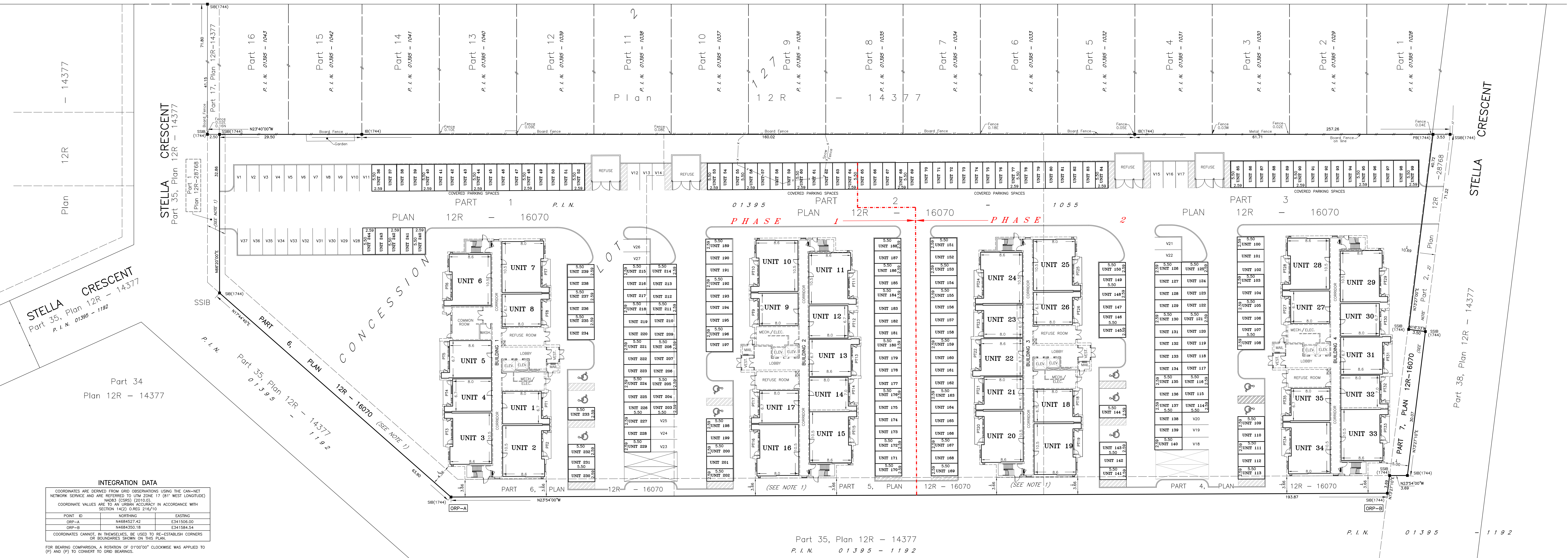
LEVEL	BUILDING 1		BUILDING 2		BUILDING 3		BUILDING 4	
	RESIDENTIAL UNITS PER LEVEL	NUMBER OF OPEN PARKING SPACES PER LEVEL	RESIDENTIAL UNITS PER LEVEL	NUMBER OF OPEN PARKING SPACES PER LEVEL	RESIDENTIAL UNITS PER LEVEL	NUMBER OF OPEN PARKING SPACES PER LEVEL	RESIDENTIAL UNITS PER LEVEL	NUMBER OF OPEN PARKING SPACES PER LEVEL
LEVEL 1	8	15	37	3	9			
LEVEL 2	9							
LEVEL 3	8							
LEVEL 4	8							
LEVEL 5	8							
LEVEL 6	8							
TOTAL	49	15	37	3	9			

OWNER'S AUTHORIZATION
I AUTHORIZE THE FILING OF THIS DRAFT PLAN FOR APPROVAL.
FOREST GLADE HORIZONS INC.
DATE: _____ DANNY TSANG
"I HAVE THE AUTHORITY TO BIND THE CORPORATION"

SURVEYOR'S CERTIFICATE
THE BOUNDARIES OF THE LANDS TO BE SUBDIVIDED AND THEIR RELATIONSHIP TO THE ADJACENT LANDS HAVE BEEN ACCURATELY AND CORRECTLY SHOWN.
DATE: NOVEMBER 8, 2021
Roy Simon
ONTOLEND SURVEYOR

VERHAEGEN
LAND SURVEYORS
A DIVISION OF J.D. BARNES LTD.

DRAWN BY: D.J.	CHECKED BY: R.A.S.	REFERENCE NO.: 20-47-530-04
CAD FILE: 20-47-530-04.dwg	E-WIND-2-127	CAD DATE: November 8, 2021 9:30 AM



INTEGRATION DATA

COORDINATES ARE DERIVED FROM GRID OBSERVATIONS USING THE CAN-NET NETWORK SERVICE AND ARE REFERRED TO UTM ZONE 17 (81° WEST LONGITUDE) NAD 83 (CSRS) (2010.0).
COORDINATE VALUES ARE TO AN URBAN ACCURACY IN ACCORDANCE WITH SECTION 14(2) OREG. 219/10.

POINT ID	NORTHING	EASTING
CRP-A	N4884527.42	E341506.00
CRP-B	N4884530.18	E341508.54

COORDINATES CANNOT, IN THEMSELVES, BE USED TO RE-ESTABLISH CORNERS OR BOUNDARIES SHOWN ON THIS PLAN.

NOTE 1: SUBJECT TO AN EASEMENT AS IN INSTRUMENT No. R1350229 AND (P) TO CONVERT TO GRID BEARINGS.
NOTE 2: SUBJECT TO AN EASEMENT AS IN INSTRUMENT No. R1385405 AND R1384571.

"METRIC" DISTANCES AND COORDINATES SHOWN ON THIS PLAN ARE IN METRES AND CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048.

UNITS 1 to 35 (incl.) - RESIDENTIAL
UNITS 36 to 244 (incl.) - PARKING
EXCLUSIVE USE PATIOS P to P35 (incl.) and

Item No. 8.6



Committee Matters: SCM 270/2023

Subject: Closure of north/south alley located between Joinville Avenue and Haig Avenue, and two intersecting east/west alleys located west of north/south alley, Ward 8, SAA-6822

Moved by: Councillor Fred Francis
Seconded by: Councillor Angelo Marignani

Decision Number: **DHSC 561**

- I. THAT the 4.27-metre-wide north/south alley located between Joinville Avenue and Haig Avenue, and shown on Drawing No. CC-1835 (*attached* hereto as Appendix "A"), and hereinafter referred to as "Alley A", **BE ASSUMED** for subsequent closure;
- II. THAT Alley A **BE CLOSED AND CONVEYED** to the abutting property owners and as necessary, in a manner deemed appropriate by the City Planner, subject to the following:
 - a. 3.0-metre-wide easement, measured 1.50 metres from either side of the following utility infrastructure, subject to there being accepted in the City's standard form and in accordance with the City's standard practice, be granted to:
 - i. Bell Canada to accommodate existing aerial facilities and poles;
 - ii. ENWIN Utilities Ltd. to accommodate existing overhead and underground 120/240V hydro pole distribution, including guy wires and anchors; and
 - iii. MNSi. to accommodate existing aerial facilities.
 - b. Ontario Land Surveyor be directed to use existing encroachments for determining the boundaries of the lands to be conveyed to each abutting property owner, save and except that portion abutting Lots 68 to 71 on Registered Plan 1167, in which case the middle of the alley shall be used.
- III. THAT the 4.27-metre-wide east/west alley located between Alley A and the west limit of the property known municipally as 5065 Joinville Avenue (legally described as Lot 114 & Part of Lot 113, Plan 1107), and shown on Drawing No. CC-1835 (*attached* hereto as Appendix "A"), and hereinafter referred to as the "Alley B", **BE ASSUMED** for subsequent closure;
- IV. THAT Alley B **BE CLOSED AND CONVEYED** to the abutting property owners and as necessary, in a manner deemed appropriate by the City Planner;

- V. THAT the 4.27-metre-wide east/west alley located between the Alley A and the west limit of the property known municipally as 5066 Haig Avenue (legally described as Lot 142 & Part of Lot 143, Plan 1107), and shown on Drawing No. CC-1835 (*attached* hereto as Appendix “A”), and hereinafter referred to as the “Alley C”, **BE ASSUMED** for subsequent closure;
- VI. THAT Alley C **BE CLOSED AND CONVEYED** to the abutting property owners and as necessary, in a manner deemed appropriate by the City Planner;
- VII. THAT Conveyance Cost **BE SET** as follows:
- a. For alley conveyed to abutting lands zoned RD1.4, \$1.00 plus HST (if applicable), deed preparation fee and proportionate share of the survey costs as invoiced to The Corporation of the City of Windsor by an Ontario Land Surveyor.
- VIII. THAT The City Planner **BE REQUESTED** to supply the appropriate legal description, in accordance with Drawing No. CC-1835, *attached* hereto as Appendix “A”;
- IX. THAT The City Solicitor **BE REQUESTED** to prepare the necessary by-law(s);
- X. THAT The Chief Administrative Officer and City Clerk **BE AUTHORIZED** to sign all necessary documents approved as to form and content satisfactory to the City Solicitor;
- XI. THAT the matter **BE COMPLETED** electronically pursuant to By-law Number 366-2003
Carried.

Report Number: S 102/2023
Clerk’s File: SAA2023

Clerk’s Note:

1. The recommendation of the Development & Heritage Standing Committee and Administration are the same.
2. Please refer to Item 11.1 from the Development & Heritage Standing Committee held on October 3, 2023.
3. To view the stream of this Standing Committee meeting, please refer to:
<http://csg001-harmony.sliq.net/00310/Harmony/en/PowerBrowser/PowerBrowserV2/202320231003/-1/9435>

Subject: Closure of north/south alley located between Joinville Avenue and Haig Avenue, and two intersecting east/west alleys located west of north/south alley, Ward 8, SAA-6822

Reference:

Date to Council: October 3, 2023
Author: Brian Nagata, MCIP, RPP
Planner II - Development Review
(519) 255-6543 ext. 6181

Planning & Building Services
Report Date: August 17, 2023
Clerk's File #: SAA2023

To: Mayor and Members of City Council

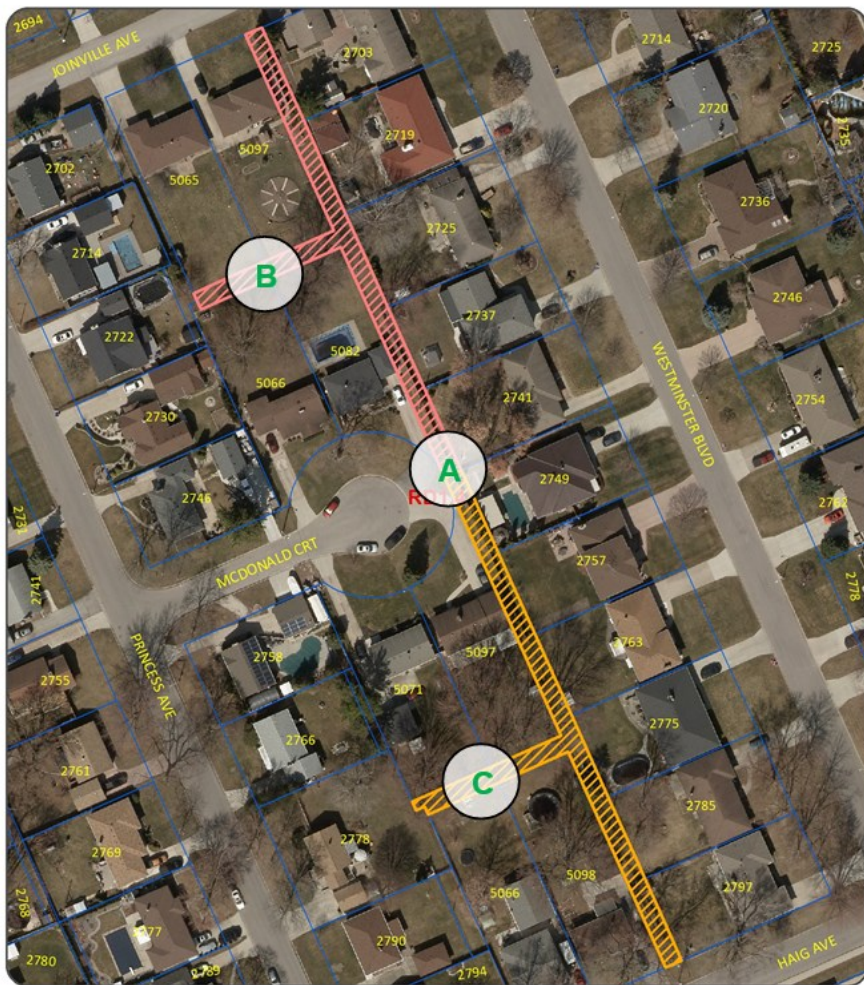
Recommendation:

- I. THAT the 4.27-metre-wide north/south alley located between Joinville Avenue and Haig Avenue, and shown on Drawing No. CC-1835 (*attached* hereto as Appendix "A"), and hereinafter referred to as "Alley A", **BE ASSUMED** for subsequent closure;
- II. THAT Alley A **BE CLOSED AND CONVEYED** to the abutting property owners and as necessary, in a manner deemed appropriate by the City Planner, subject to the following:
 - a. 3.0-metre-wide easement, measured 1.50 metres from either side of the following utility infrastructure, subject to there being accepted in the City's standard form and in accordance with the City's standard practice, be granted to:
 - i. Bell Canada to accommodate existing aerial facilities and poles;
 - ii. ENWIN Utilities Ltd. to accommodate existing overhead and underground 120/240V hydro pole distribution, including guy wires and anchors; and
 - iii. MNSi. to accommodate existing aerial facilities.
 - b. Ontario Land Surveyor be directed to use existing encroachments for determining the boundaries of the lands to be conveyed to each abutting property owner, save and except that portion abutting Lots 68 to 71 on Registered Plan 1167, in which case the middle of the alley shall be used.

- III. THAT the 4.27-metre-wide east/west alley located between Alley A and the west limit of the property known municipally as 5065 Joinville Avenue (legally described as Lot 114 & Part of Lot 113, Plan 1107), and shown on Drawing No. CC-1835 (*attached* hereto as Appendix “A”), and hereinafter referred to as the “Alley B”, **BE ASSUMED** for subsequent closure; THAT Alley B **BE CLOSED AND CONVEYED** to the abutting property owners and as necessary, in a manner deemed appropriate by the City Planner;
- V. THAT the 4.27-metre-wide east/west alley located between the Alley A and the west limit of the property known municipally as 5066 Haig Avenue (legally described as Lot 142 & Part of Lot 143, Plan 1107), and shown on Drawing No. CC-1835 (*attached* hereto as Appendix “A”), and hereinafter referred to as the “Alley C”, **BE ASSUMED** for subsequent closure; THAT Alley C **BE CLOSED AND CONVEYED** to the abutting property owners and as necessary, in a manner deemed appropriate by the City Planner;
- VII. THAT Conveyance Cost **BE SET** as follows:
 - a. For alley conveyed to abutting lands zoned RD1.4, \$1.00 plus HST (if applicable), deed preparation fee and proportionate share of the survey costs as invoiced to The Corporation of the City of Windsor by an Ontario Land Surveyor.
- VIII. THAT The City Planner **BE REQUESTED** to supply the appropriate legal description, in accordance with Drawing No. CC-1835, *attached* hereto as Appendix “A”;
- IX. THAT The City Solicitor **BE REQUESTED** to prepare the necessary by-law(s);
- X. THAT The Chief Administrative Officer and City Clerk **BE AUTHORIZED** to sign all necessary documents approved as to form and content satisfactory to the City Solicitor;
- XI. THAT the matter **BE COMPLETED** electronically pursuant to By-law Number 366-2003

Executive Summary:

N/A



STREET & ALLEY CLOSING (SAA/6822)

1:1,000

APPLICANT : SALEH YAVARI & REYHANEH YAVARI

▨ REQUESTED & TO BE RECOMMENDED FOR CLOSURE
 ▨ TO BE RECOMMENDED FOR CLOSURE



PLANNING DEPARTMENT - DEVELOPMENT DIVISION

DATE: MAY, 2023

Figure 1 - Location Map

Background:

The applicants, Reyhaneh and Saleh Yavari, owners of the property known municipally as 5082 McDonald Court (the subject property), applied to close the portion of the north/south alley and east/west alley abutting the subject property to the east and north respectively, and shown on Drawing No. CC-1835 *attached* hereto as Appendix “A”, and also shown on the aerial photo *attached* hereto as Appendix “B”.

The Planning Department is recommending the full closure of the aforesaid north/south alley located between Joinville Avenue and Haig Avenue (hereinafter referred to as Alley A), aforesaid east/west alley located between Alley A and the west limit of 5065 Joinville Avenue (hereinafter referred to as Alley B), and the east/west alley located

between Alley A and the west limit of 5066 Haig Avenue (hereinafter referred to as Alley C) (refer to Figure 1 above).

Alley A was established by Registered Plan 1167 (Westminster Boulevard Subdivision), registered on May 2, 1925. Alleys B & C were established by Registered Plan 1107, registered on March 7, 1924. Alleys A, B & C were used for agricultural purposes until approximately 1955 when the abutting lands began to be developed as Single Family Dwelling lots (Refer to Figure 2 below).



Figure 2 - 1954 Aerial Photo

(Ontario Department of Lands and Forests)

City of Windsor aerial photography from 1977 to 2021 illustrates that Alleys A, B & C have been encroached on over the years by abutting properties with accessory buildings, fences, landscaping and/or structures.

The following table illustrates that the majority of properties roughly encroached into their abutting half of Alley A. This excludes 2719 Westminster Boulevard, 2725 Westminster Boulevard and the subject property which did not encroach into Alley A, as well as 5097 Joinville Avenue which encroached into 2719 Westminster Boulevard's half of Alley A. Alley A also contains utility poles with guy wires and anchors, as well as underground hydro wires. There are no Encroachment Agreements on record for the use of the alley.

Alley A	Extent of Encroachment		
Abutting Property	Full	Half (Approx.)	None
2703 Westminster Blvd		x	
2719 Westminster Blvd			x
2725 Westminster Blvd			x
2737 Westminster Blvd		x	
2741 Westminster Blvd		x	
2749 Westminster Blvd		x	
2757 Westminster Blvd		x	
2763 Westminster Blvd		x	
2775 Westminster Blvd		x	
2785 Westminster Blvd		x	
2797 Westminster Blvd		x	
5082 McDonald Ct			x
5097 Joinville Ave	x	x	
5097 McDonald Ct		x	
5098 Haig Ave		x	

5065 Joinville Avenue and 5097 Joinville Avenue encroached into their half and 2719 Westminster Boulevard and 2725 Westminster Boulevard’s half of Alley B respectively. There are no Encroachment Agreements on record for the use of the alley.

5071 McDonald Court and 5097 McDonald Court encroached into their half and 5066 Haig Avenue and 5098 Haig Avenue’s half of Alley C respectively. There are no Encroachment Agreements on record for the use of the alley.

The applicant wishes to close the abutting alleys for the purpose of preventing vandalism, reducing litter and improving ease of access for maintenance (i.e. grass cutting, weeding, etc.).

Discussion:

The decision to recommend closure of an alley is derived from the City’s *Classification of Alleys and Suitability for Closure* guideline document (the document), *attached* hereto as Appendix “E”. The document details four classifications of alleys based on their usefulness, and provides corresponding criteria for determining suitability for closure.

Classification of Public Right-of-Ways

The initial step is to determine if the alley is indispensable. This is achieved through the evaluation of the following criteria set forth in Section 1 of the document.

1. *Does the subject alley serve commercial properties?*
 - a. Alleys A, B & C do not serve any commercial properties.

2. *Does the subject alley serve properties fronting on heavily traveled streets i.e. major arterial routes?*
 - a. Alleys A, B & C do not serve any properties fronting on heavily travelled streets.
3. *Does the subject alley contain sewers, and must the alley remain accessible for servicing?*
 - a. Alleys A, B & C do not contain any sewers.
4. *Does the subject alley serve as the only vehicular means of access to rear parking areas and garages where the property has insufficient lot width for a side drive?*
 - a. Alleys A, B & C do not serve as a means of vehicular access to any rear parking areas or garages.
5. *Does the subject alley contain Fire Department connections that are deemed to be necessary for firefighting access?*
 - a. Alleys A, B & C do not contain any Fire Department connections.
6. *Does the subject alley lie within a Holding zone or other similar undeveloped areas where the right-of-way system is clearly obsolete and has never been developed, but where the City needs to keep its options open until new area plans are prepared and development is imminent?*
 - a. Alleys A, B & C do not lie within a Holding zone or other similar undeveloped areas.

Based on the above, the Planning Department deems the Alleys A, B & C “dispensable”, and supports the requested closure.

It is our recommendation that, upon closure, the abutting property owners be given the chance to acquire Alleys A, B & C in the manner described in the Recommendation section herein. Hence the recommendation is to close and convey the alley to the abutting property owners, which is the standard manner of conveyance. This includes 5066 Haig Avenue, 5066 McDonald Court, 5082 McDonald Court and 5098 Haig Avenue in order to be as fair as possible in conveying Alleys B & C which no abutting property owners have permission to encroach upon.

Risk Analysis:

The recommended closure will divest the City of associated liability risks and maintenance costs. The recommended closure poses no known risk to City.

Climate Change Risks

Climate Change Mitigation:

N/A

Climate Change Adaptation:

N/A

Financial Matters:

The rate for an alley conveyed to abutting lands zoned R1.4 is assessed at \$1.00 plus HST (if applicable), deed preparation fee and proportionate share of the survey costs as invoiced to the City by an Ontario Land Surveyor.

Consultations:

Consultations were held with Municipal Departments and Utility Companies, which resulted in the information found in *attached* hereto as Appendix "C".

Notice of this application was issued to property owners abutting Alleys A, B & C by regular mail, with no objections being received as of the date of writing this report.

Notice of Development & Heritage Standing Committee meeting and Council meeting are published in the Windsor Star prior to each of the meetings. In addition, notice of each of the public meetings will be mailed to the abutting/affected property owners prior to the meetings.

Conclusion:

The Planning Department recommends closure of Alley A shown on attached Appendix "A", subject to easements in favour of Bell Canada, ENWIN Utilities Ltd., and MNSi as in Recommendation II of this report.

The Planning Department further recommends closure of the Alleys B & C shown on attached Appendix "A", as in Recommendations IV & VI of this report respectively.

The closed Alleys A, B & C are to be conveyed to the abutting property owners as in Recommendations II, IV & VI of this report, respectively.

Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Greg Atkinson, MCIP, RPP
Manager of Development

Neil Robertson, MCIP, RPP
Acting City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP JM

Approvals:

Name	Title
Greg Atkinson	Manager of Development/Deputy City Planner
Neil Robertson	Acting City Planner / Executive Director, Planning & Development Services
Stephanie Santos	Coordinator of Real Estate Services
Kate Tracey	Senior Legal Counsel, Legal Services & Real Estate
Jelena Payne	Commissioner, Economic Development & Innovation
Joe Mancina	Chief Administration Officer

Notifications:

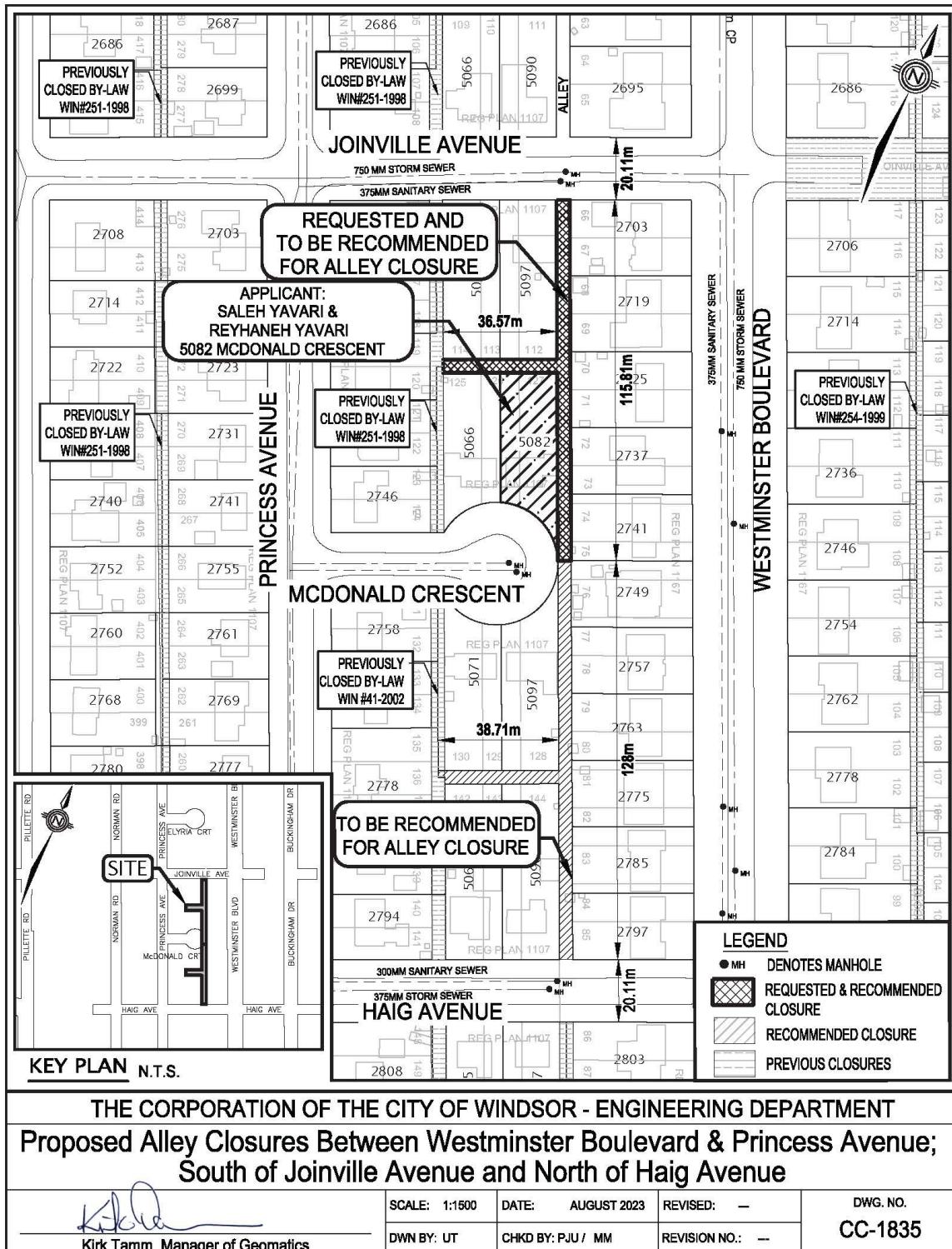
Name	Address	Email
Ward 8 Councillor Gary Kaschak	350 City Hall Square West, Suite 220 Windsor, Ontario N9A 6S1	gkaschak@citywindsor.ca
List of mailing labels for property owners abutting alley issued to Clerks office		

Appendices:

- 1 Appendix A - Drawing No. CC-1835
- 2 Appendix B - EIS Drawing - Aerial Photo
- 3 Appendix C - Consultations with Municipal Departments and Utility Companies
- 4 Appendix D - Site Photos
- 5 Appendix E - Classification of Alleys and Suitability for Closure

APPENDIX "A"

Drawing No. CC-1835



APPENDIX "B" EIS Drawing - Aerial Photo



STREET & ALLEY CLOSING (SAA/6822)

1:1,000

APPLICANT : SALEH YAVARI & REYHANEH YAVARI



REQUESTED & TO BE RECOMMENDED FOR CLOSURE



TO BE RECOMMENDED FOR CLOSURE



PLANNING DEPARTMENT - DEVELOPMENT DIVISION

DATE: MAY, 2023

APPENDIX "C"

Consultations with Municipal Departments and Utility Companies

BELL CANADA WSP

Bell Canada requests easement protection for the length of the N/S alley between Haig and Joinville.

Easement to be 3.0 m wide, 1.5 m on either side of existing aerial facilities, or the width of the alley for the length of the alley.

[Charleyne Hall - Bell Canada External Liaison, Right of Way & Indigenous Relation]



STREET & ALLEY CLOSING (SAA/6822)

1:1,000

APPLICANT : SALEH YAVARI & REYHANEH YAVARI

▨ REQUESTED & TO BE RECOMMENDED FOR CLOSURE ▨ TO BE RECOMMENDED FOR CLOSURE

PLANNING DEPARTMENT - DEVELOPMENT DIVISION

DATE: MAY, 2023



COGECO CABLE SYSTEMS INC.

No comments provided

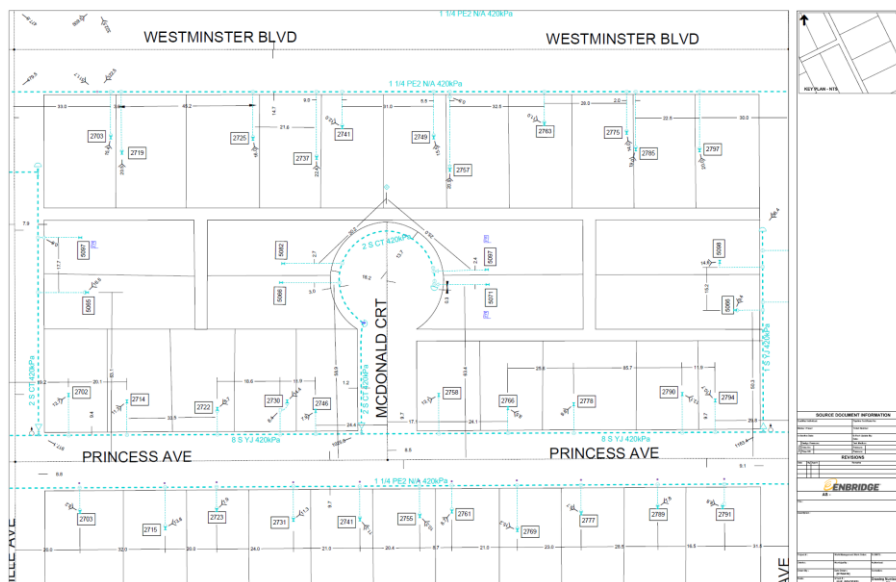
ENBRIDGE GAS

After reviewing the provided drawing of the alley between Princess Ave and Westminster Blvd (south of Joinville Ave & north of Haig Ave) and consulting our mapping system, please note that Enbridge Gas has no active infrastructure in the proposed area. A PDF drawing has been attached for reference.

Also, please note the following should you find any abandoned infrastructure in the area:

- Any pipe that is excavated, please assume that it is live
- If during the course of any job, any pipe is found that is not on the locate sheet and is in conflict with your work, please call our emergency number (1-877-969-0999), and one of our Union Gas representatives will respond to determine if that plant is in fact live or dead
- Please note that our Enbridge Gas representative will respond to the live or dead call within 1-4 hours, so please plan your work accordingly

[Samuel Nguyen - Summer Student Engineer]



ENVIRONMENTAL SERVICES

No concerns from Environmental Services.

[Anne-Marie Albidone - Manager, Environmental Services]

ENWIN UTILITIES - HYDRO

May 30, 2023 (Remainder of north/south alley and southerly east/west alley)

No objection, however, an easement named to ENWIN Utilities Ltd may be required in the N/S alley (if closing) to accommodate existing overhead 120/240-volt hydro distribution.

November 10, 2022

No objection provided clearances are maintained from our distribution plant. However, an easement named to ENWIN Utilities Ltd., will be required for north to the south limit of the property to accommodate for the existing overhead and underground 120/240V hydro pole distribution.

Please see attached for the highlighted area of easement needed.

Please note the following distribution and services:

- Overhead 120/240V triplex secondary distribution pole line and associated down guy wires/ anchors servicing multiple residential houses along the alleyway.
- Underground 120/240V Triplex Secondary service to multiple residential houses along the alleyway.
- Pole #0012311 has the underground 120/240V Triplex secondary distribution servicing 2737 and 2741 Westminster Blvd.
- Pole #0012312 has a down guy and anchor attached in line with the pole line.

Proposed buildings and/or building additions must have adequate clearance requirements from all hydro distribution and services.

We recommend referring to the Occupational Health and Safety Act for minimum safe limits of approach during construction and the Ontario Building Code for adequate clearance requirements for New Buildings and/or Building Additions.

ENWINs easement guidelines:

Overhead

A 3 m (10 ft.) Easement is required for a straight pole line 1.5 m (5 ft.) (on each side). This takes into consideration a 0.3 m (1 ft.) pole diameter, 0.46 m (1.5ft) primary insulator, and 0.9 m (3ft.) clearance from any nearby structure.

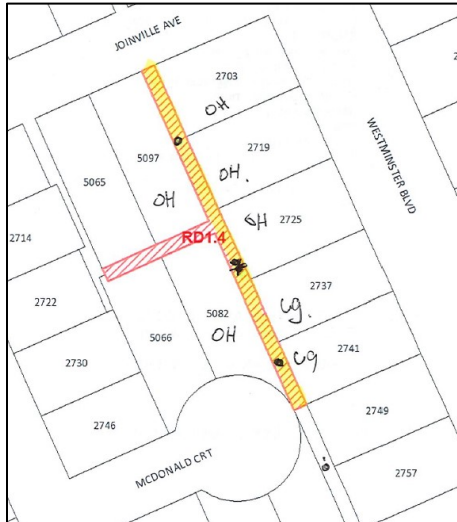
Guy and Anchor

The easement for the guy and anchor is 3.0 m (10 ft.) wide easement is required into private property. This easement should extend 1.0 m (3.3 ft.) into the property.

Underground Cable

A 0.6 m (2.0 ft.) easement on either side of a duct bank is required for underground installations.

[Tia McCloskey - Hydro Engineering Technologist]



ENWIN UTILITIES - WATER

Water Engineering has no objections.

[Bruce Ogg - Water Project Review Officer]

LEGAL DEPARTMENT

For lands abutting RD1.4, \$1.00 plus HST (if applicable), deed preparation fee and proportionate share of the survey cost as invoiced to The Corporation of the City of Windsor by an Ontario Land Surveyor.

[Stephanie Santos - Coordinator of Real Estate Services]

MNSi

MNSi would like an Aerial Easement through all the Alley closures.

[Dave Hartleib - Outside Plant Manager]

PARKS & FACILITIES

No concerns from Natural Areas.

[Karen Alexander - Naturalist & Outreach Coordinator]

No objections from Parks Design & Development.

[Sherif Barsom - Landscape Architect]

No tree relevant concern here.

[Yemi Adeyeye - City Forester / Manager Forestry & Natural Areas]

PLANNING DEPARTMENT

No comments provided

PLANNING DEPARTMENT - LANDSCAPE ARCHITECT

No objections from a landscape architectural perspective.

[Stefan Fediuk - Landscape Architect]

PUBLIC WORKS - ENGINEERING

The proposed alley closure is composed of grass and dirt. There are no sewers, catch basins, or manholes within the alley. There are hydro poles within the alley; an easement will be required for utilities. There are many encroachments within the alley from Joinville Avenue to Haig Avenue. Public Works requests that the entire north-south alley from Joinville Avenue to Haig A be closed as part of this application. Otherwise, the eight (8) properties with encroachments would be required to remove their objects from the alley or enter into an encroachment agreement with the City. This subject alley has no usefulness as per CR146/2005; therefore, we have no objections to the closure, subject to the utility easement.

[Adam Pillon - Manager of Right-of-Way]

PUBLIC WORKS - OPERATIONS

There are no maintenance concerns and the application is supported.

[Roberta Harrison - Coordinator Maintenance]

PUBLIC WORKS - TRAFFIC

The alley systems shown are not required for vehicular or pedestrian access. As such, there are no objections with closing both sections of alley as shown.

Consideration should be given to close the remaining sections of alley to the south to reduce the City's maintenance.

[Mike Spagnuolo - Signal Systems Analyst]

ROGERS COMMUNICATIONS

No comments provided

TELUS COMMUNICATIONS

TELUS has no underground infrastructure in the area of your proposed work

[Meghna Patel - Permit Coordinator]

TRANSPORTATION PLANNING

The alley does not appear to be used for transportation purposes therefore we have no concerns.

[Clare Amicarelli - Transportation Planning Coordinator]

TRANSIT WINDSOR

No concerns from Transit Windsor

[Jason Scott - Supervisor Planning]

WINDSOR FIRE

No comments provided

WINDSOR POLICE

The Windsor Police Service has no concerns or objections with the closure of these sections alley extending from Joinville Avenue to Haig Avenue, between Westminster Boulevard and Princess Avenue to prevent vandalism and illegal dumping. The alley spaces in question are grassed over and largely accessible to primarily pedestrian traffic. In this regard, its closure will not create problems for police to otherwise gain access for emergency incident response or vehicle patrol purposes within the immediate area. The closure will still leave other options available to the police for such purposes. Closure will help facilitate improved physical measures by abutting property owners to restrict/prevent discreet access that may lead to the vandalism and illegal dumping problems identified by the applicant. The end result from this closure will hopefully mitigate problems going forward.

[Barry Horrobin - Director of Planning & Physical Resources]

APPENDIX "D"
Site Photos (April 20, 2023)



Figure 1 - Looking north towards north/south alley from McDonald Court



Figure 2 - North/south alley looking south from 5082 McDonald Court



Figure 3 - North/south alley looking north from 5082 McDonald Court



Figure 4 - North/south alley looking south from east/west alley (5082 McDonald Court on right)



Figure 5 - North/south alley looking north from east/west alley (5097 Joinville Avenue on left)



Figure 6 - Looking east towards north/south alley from east/west alley (5082 McDonald Court on right)



Figure 7 - Looking south towards north/south alley from Joinville Avenue (5097 Joinville Avenue on right)



Figure 8 - Looking west towards east/west alley from north/south alley (5082 McDonald Court on left)



*Figure 9 - Looking north towards north/south alley from Haig Avenue (5098 Haig Avenue on left)
(January 2021 Google Street View)*



*Figure 10 - Looking north towards north/south alley from Haig Avenue (5098 Haig Avenue on left)
(January 2021 Google Street View)*

APPENDIX “E”

Classification of Alleys and Suitability for Closure

Classification of Public Rights-of-Ways:

Currently streets and alleys fall into four classifications on the basis of their usefulness:

- 1) Alleys that are **indispensable**. These would be alleys serving commercial properties and properties fronting on heavily traveled streets i.e. major arterial routes and alleys which contain sewers and must remain accessible for servicing; alleys or streets which serve as the only vehicular means of access to rear parking areas and garages where the property has insufficient lot width for a side drive; and, alleys which contain Fire Department connections that are deemed to be necessary for firefighting access.
- 2) Alleys that, **have some usefulness**, are nevertheless dispensable and may or may not be a complete liability.
- 3) Alleys that appear to serve **no useful purpose**, either now, or anticipated. Such alleys are in residential areas and locations where generally the lots are wide enough for side drives, or those alleys abutting parks and other parcels of land that do not require any servicing from the alley. Remnant or stub-end streets which are dead-ended and do not serve as access to other streets.
- 4) Alleys lying in Holding zones and other similar undeveloped areas where the alley system is **clearly obsolete** and has never been developed, but where the City needs to keep its options open until new area plans are prepared and development is imminent.

Suitability for Closing:

Following are the criteria and suitability for closing alleys in each of the above classifications:

- 1) Indispensable alleys should **not be closed**, conveyed, reduced or otherwise jeopardized through minority interests unless a suitable substitute alley is opened in lieu thereof. They are essential from the viewpoint of fire protection, police protection, emergency services (i.e. ambulance) and loading or unloading of goods, refuse collection, servicing of blocked sewers and utility services. Without such alleys, the above noted services would at least be more costly if not impossible to complete or adequately access; and would noticeably interfere with street traffic, thereby reducing the access capacity of the adjacent arterial, collector, or street for business.
- 2) Alleys having some usefulness should **be considered for closing** only upon request of abutting owners rather than by encouragement of the City.
- 3) Alleys that serve no useful purpose should **be closed** if at all possible, and in fact the owners abutting thereon should be encouraged to accept conveyance.
- 4) Alleys that are clearly obsolete should **not be closed** unless there is a municipal need or specific development proposals acceptable to the City are submitted.



Subject: Closure of north/south alley located between Taylor Avenue and 1124-1224 Campbell Avenue, Ward 2, SAA-6848

Moved by: Councillor Kieran McKenzie

Seconded by: Councillor Angelo Marignani

Decision Number: **DHSC 562**

- I. THAT the 3.66-metre-wide north/south alley located between Taylor Avenue and the property known municipally as 1124-1224 Campbell Avenue (legally described as Part of Lot 1, Plan 64; Lots 27, 28, 45-58, Part of Closed Alley & McEwan, Plan 1367), and shown on Drawing No. CC-1837 (*attached* hereto as Appendix "A"), and hereinafter referred to as the "subject alley", **BE ASSUMED** for subsequent closure.

- II. THAT the subject alley **BE CLOSED AND CONVEYED** to the owner of the property known municipally as 1238 Campbell Avenue (legally described as Lots 1 & 2, Part of Lot 3, Plan 669) and as necessary, in a manner deemed appropriate by the City Planner, subject to the following:
 - a. 3.0-metre-wide easement, measured 1.50 metres from either side of the following utility infrastructure, subject to there being accepted in the City's standard form and in accordance with the City's standard practice, be granted to:
 - i. Bell Canada to accommodate existing aerial infrastructure; and
 - ii. ENWIN Utilities Ltd. to accommodate the poles and existing secondary overhead conductors.

 - b. Easement subject to there being accepted in the City's standard form and in accordance with the City's standard practice, be granted to:
 - i. The owner of the property known municipally as 1660 Taylor Avenue (legally described as Lots 33 to 34 & Part of Closed Alley, Plan 669) for access to repair and maintain the west face of the existing detached garage on the said property.

- III. THAT Conveyance Cost **BE SET** as follows:
 - a. For alley conveyed to abutting lands zoned RD1.3 or RD2.2, \$1.00 plus HST (if applicable), deed preparation fee and proportionate share of the survey costs as invoiced to The Corporation of the City of Windsor by an Ontario Land Surveyor.

- IV. THAT The City Planner **BE REQUESTED** to supply the appropriate legal description, in accordance with Drawing No. CC-1837, *attached* hereto as Appendix "A".
- V. THAT The City Solicitor **BE REQUESTED** to prepare the necessary by-law(s).
- VI. THAT The Chief Administrative Officer and City Clerk **BE AUTHORIZED** to sign all necessary documents approved as to form and content satisfactory to the City Solicitor.
- VII. THAT the matter **BE COMPLETED** electronically pursuant to By-law Number 366-2003
Carried.

Report Number: S 109/2023
Clerk's File: SAA2023

Clerk's Note:

1. The recommendation of the Development & Heritage Standing Committee and Administration are the same.
2. Please refer to Item 11.2 from the Development & Heritage Standing Committee held on October 3, 2023.
3. To view the stream of this Standing Committee meeting, please refer to:
<http://csg001-harmony.slig.net/00310/Harmony/en/PowerBrowser/PowerBrowserV2/202320231003/-1/9435>

Subject: Closure of north/south alley located between Taylor Avenue and 1124-1224 Campbell Avenue, Ward 2, SAA-6848

Reference:

Date to Council: October 3, 2023
Author: Brian Nagata, MCIP, RPP
Planner II - Development Review
(519) 255-6543 ext. 6181

Planning & Building Services
Report Date: August 22, 2023
Clerk's File #: SAA2023

To: Mayor and Members of City Council

Recommendation:

- I. THAT the 3.66-metre-wide north/south alley located between Taylor Avenue and the property known municipally as 1124-1224 Campbell Avenue (legally described as Part of Lot 1, Plan 64; Lots 27, 28, 45-58, Part of Closed Alley & McEwan, Plan 1367), and shown on Drawing No. CC-1837 (*attached* hereto as Appendix "A"), and hereinafter referred to as the "subject alley", **BE ASSUMED** for subsequent closure. THAT the subject alley **BE CLOSED AND CONVEYED** to the owner of the property known municipally as 1238 Campbell Avenue (legally described as Lots 1 & 2, Part of Lot 3, Plan 669) and as necessary, in a manner deemed appropriate by the City Planner, subject to the following:
 - a. 3.0-metre-wide easement, measured 1.50 metres from either side of the following utility infrastructure, subject to there being accepted in the City's standard form and in accordance with the City's standard practice, be granted to:
 - i. Bell Canada to accommodate existing aerial infrastructure; and
 - ii. ENWIN Utilities Ltd. to accommodate the poles and existing secondary overhead conductors.
 - b. Easement subject to there being accepted in the City's standard form and in accordance with the City's standard practice, be granted to:
 - i. The owner of the property known municipally as 1660 Taylor Avenue (legally described as Lots 33 to 34 & Part of Closed Alley, Plan 669) for access to repair and maintain the west face of the existing detached garage on the said property.

- III. THAT Conveyance Cost **BE SET** as follows:
 - a. For alley conveyed to abutting lands zoned RD1.3 or RD2.2, \$1.00 plus HST (if applicable), deed preparation fee and proportionate share of the survey costs as invoiced to The Corporation of the City of Windsor by an Ontario Land Surveyor.
- IV. THAT The City Planner **BE REQUESTED** to supply the appropriate legal description, in accordance with Drawing No. CC-1837, *attached* hereto as Appendix "A".
- V. THAT The City Solicitor **BE REQUESTED** to prepare the necessary by-law(s).
- VI. THAT The Chief Administrative Officer and City Clerk **BE AUTHORIZED** to sign all necessary documents approved as to form and content satisfactory to the City Solicitor.
- VII. THAT the matter **BE COMPLETED** electronically pursuant to By-law Number 366-2003

Executive Summary:

N/A

Background:

The applicant, Avant Group Inc., authorized agent for Mouy Fu and David Tran, owners of the property known municipally as 1238 Campbell Avenue (the subject property), applied to close the portion of the north/south alley located between Taylor Avenue and 1124-1224 Campbell Avenue that abuts the subject property, and shown on Drawing No. CC-1837 *attached* hereto as Appendix “A”, and also shown on the aerial photo *attached* hereto as Appendix “B”.

The applicant wishes to close the aforesaid portion of the alley for the purpose of reconfiguring the rear driveways on the subject property to improve their functionality. The applicant indicated that the existing rear yard depth in combination with the narrow alley containing utility poles and a fence along the majority of its east limit creates maneuverability issues for vehicles using the driveways.

The Planning Department is recommending that the entire north/south alley, hereinafter referred to as the alley, be closed.

The alley was established by Registered Plan 669, registered on July 9, 1913. The alley is unmaintained and composed primarily of gravel and concrete with sporadic patches of grass. The alley contains two (2) utility poles with overhead wires, serves as the only vehicular means of access to the subject property and includes a curb cut off of Taylor Avenue.

The aforesaid concrete along with additional gravel were installed in the alley around July 2023. An Order to Comply was subsequently issued against the owners of the subject property for the removal of the concrete and gravel from the alley. The Order to Comply will remain in place until the concrete and gravel is removed or the alley is closed and purchased by the owners of the subject property.

Discussion:

The decision to recommend closure of an alley is derived from the City’s *Classification of Alleys and Suitability for Closure* guideline document (the document), *attached* hereto as Appendix “E”. The document details four classifications of alleys based on their usefulness, and provides corresponding criteria for determining suitability for closure.

Classification of Public Right-of-Ways

The initial step is to determine if the alley is indispensable. This is achieved through the evaluation of the following criteria set forth in Section 1 of the document.

1. *Does the subject alley serve commercial properties?*
 - a. The alley does not serve any commercial properties.

2. *Does the subject alley serve properties fronting on heavily traveled streets i.e. major arterial routes?*
 - a. The alley serves as the only vehicular means of access to the rear parking area on the subject property which fronts Campbell Avenue, a Class I Collector Road.
3. *Does the subject alley contain sewers, and must the alley remain accessible for servicing?*
 - a. The alley does not contain any sewers.
4. *Does the subject alley serve as the only vehicular means of access to rear parking areas and garages where the property has insufficient lot width for a side drive?*
 - a. The alley serves as the only vehicular means of access to the rear parking area on the subject property.
 - b. The subject property has had vehicular access off of the alley since at least 1963.
 - This assessment is based off of the following factors:
 - Completion of Building Permit 62-B-15537 in 1963 for the construction of a detached garage (demolished along with Single Family Dwelling in 2020 to accommodate the present three (3) unit Townhome Dwelling)
 - City of Windsor aerial photography ranging from 1969 to 2021
 - Building Permits for the construction of the three (3) unit Townhome Dwelling, which approve driveways off of the alley
5. *Does the subject alley contain Fire Department connections that are deemed to be necessary for firefighting access?*
 - a. The alley does not contain any Fire Department connections.
6. *Does the subject alley lie within a Holding zone or other similar undeveloped areas where the right-of-way system is clearly obsolete and has never been developed, but where the City needs to keep its options open until new area plans are prepared and development is imminent?*
 - a. The alley does not lie within a Holding zone or other similar undeveloped areas.

Based on the above, the Planning Department deems the alley “indispensable”.

Notwithstanding the alley being deemed indispensable, the Planning Department is recommending that it be closed and conveyed to the owner of the subject property for the following reasons:

- The aforesaid factors that deem the alley indispensable stem solely from the subject property's use of the alley.
- The conveyance of the alley to either of the other two (2) abutting properties will eliminate the subject property's only means of vehicular access to Taylor Avenue.
- The subject property is not permitted to have a driveway off of Campbell Avenue that would satisfy the parking requirements for the existing three (3) unit Townhome Dwelling.
- A driveway off of an alley and a street is not permitted.

It is our recommendation that, upon closure, the owner of the subject property be given a chance to acquire the alley. Hence the recommendation is to close and convey the alley to the owner of the subject property, which is contrary to the standard manner of conveyance of offering abutting properties first right to acquire their half of the alley.

Risk Analysis:

The recommended closure will divest the City of associated liability risks and maintenance costs. The recommended closure poses no known risk to City.

Climate Change Risks

Climate Change Mitigation:

N/A

Climate Change Adaptation:

N/A

Financial Matters:

The rate for an alley conveyed to abutting lands zoned RD1.3 or RD2.2 is assessed at \$1.00 plus HST (if applicable), deed preparation fee and proportionate share of the survey costs as invoiced to the City by an Ontario Land Surveyor.

Consultations:

Consultations were held with Municipal Departments and Utility Companies, which resulted in the information found in *attached* hereto as Appendix "C".

Notice of this application was issued to property owners abutting the alley by regular mail.

The owner of 1660 Taylor Avenue, David Latouf, submitted a written objection to the alley closure via July 17, 2023 email, *attached* hereto as Appendix "F". The owner

stated the possibility for flooding and elimination of access for maintenance purposes as the primary reasons for his objection.

Property Standards By-law 9-2019 prohibits the release of storm water onto neighbouring properties. The elimination of access for property maintenance is not a valid reason for denying the closure of an alley. Notwithstanding, it is recommended that an easement be granted in favour of the owner of 1660 Taylor Avenue for access to repair and maintain the west face of their existing detached garage which is setback 0.39 metres from the alley. This recommendation is based on the existing setback being less than the minimum setback of 0.60 metres set forth under Zoning By-law 8600.

The owner of 1682 Taylor Avenue, Barb Brown, submitted written objections to the alley closure via June 28, 2023 and July 11, 2023 emails, *attached* hereto as Appendix "F". The owner stated the City's denial of her previous driveway request due to it being located within the Taylor Avenue right-of-way and loss of utility services are the primary reasons for her objection.

There is no record of a driveway permit application being submitted for 1682 Taylor Avenue. The Planning Department is recommending that the alley be closed on the condition that an easement is granted in favour of Bell Canada and ENWIN Utilities Ltd. to accommodate their existing infrastructure within the alley.

Notice of Development & Heritage Standing Committee meeting and Council meeting are published in the Windsor Star prior to each of the meetings. In addition, notice of each of the public meetings will be mailed to the abutting/affected property owners prior to the meetings.

Conclusion:

The Planning Department recommends closure of the alley shown on attached Appendix "A", subject to easements in favour of Bell Canada, ENWIN Utilities Ltd. and the owner of 1660 Taylor Avenue, as in Recommendation II of this report.

The closed alley is to be conveyed to the owner of the subject property as in Recommendation II report.

Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Greg Atkinson, MCIP, RPP
Manager of Development

Neil Robertson, MCIP, RPP
Acting City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP

JM

Approvals:

Name	Title
Greg Atkinson	Manager of Development/Deputy City Planner
Neil Robertson	Acting City Planner / Executive Director, Planning & Development Services
Stephanie Santos	Coordinator of Real Estate Services
Kate Tracey	Senior Legal Council, Legal Services & Real Estate
Jelena Payne	Commissioner, Economic Development & Innovation
Joe Mancina	Chief Administration Officer

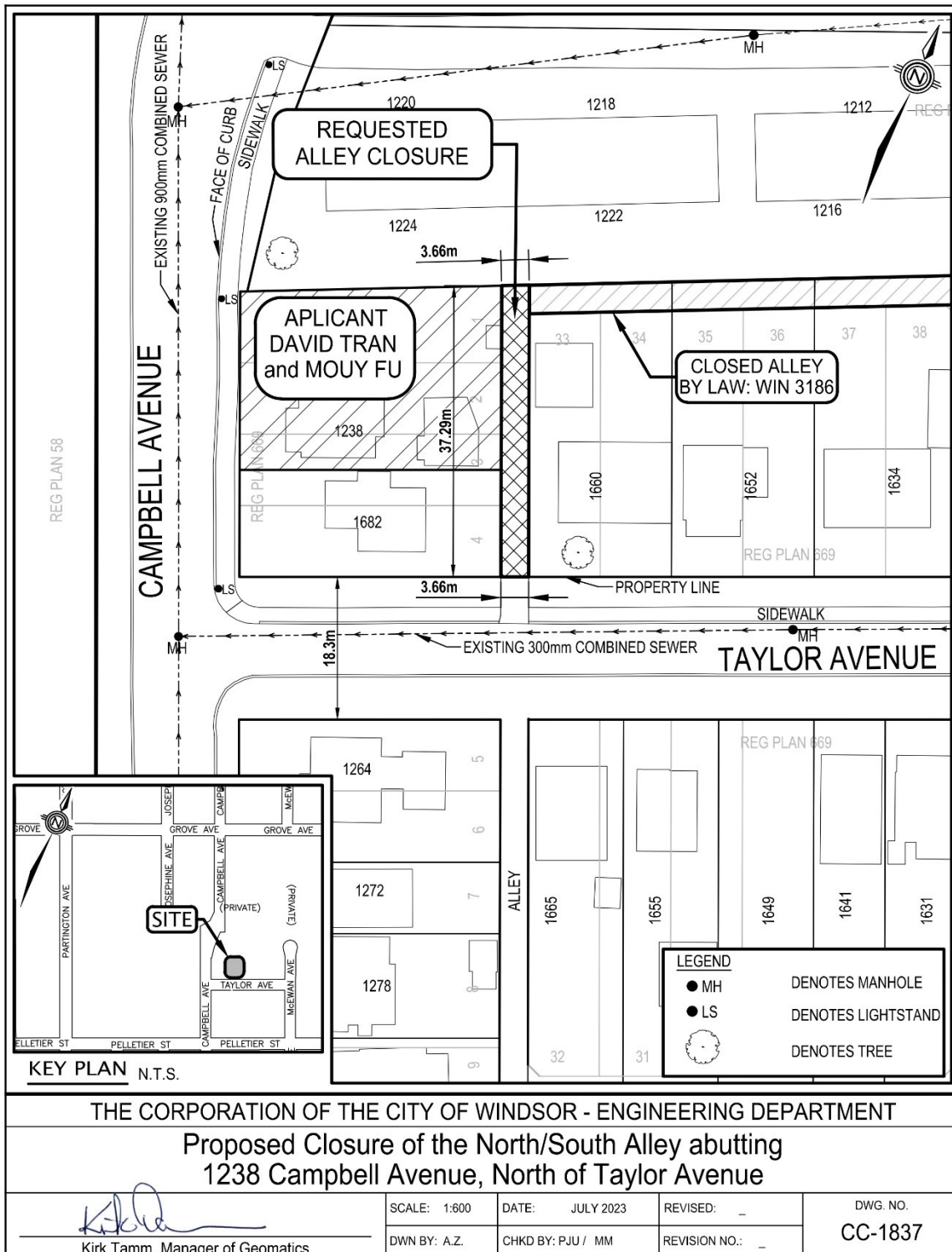
Notifications:

Name	Address	Email
Ward 2 Councillor Fabio Costante	350 City Hall Square West, Suite 220 Windsor, Ontario N9A 6S1	fcostante@citywindsor.ca
List of mailing labels for property owners abutting alley issued to Clerks office		

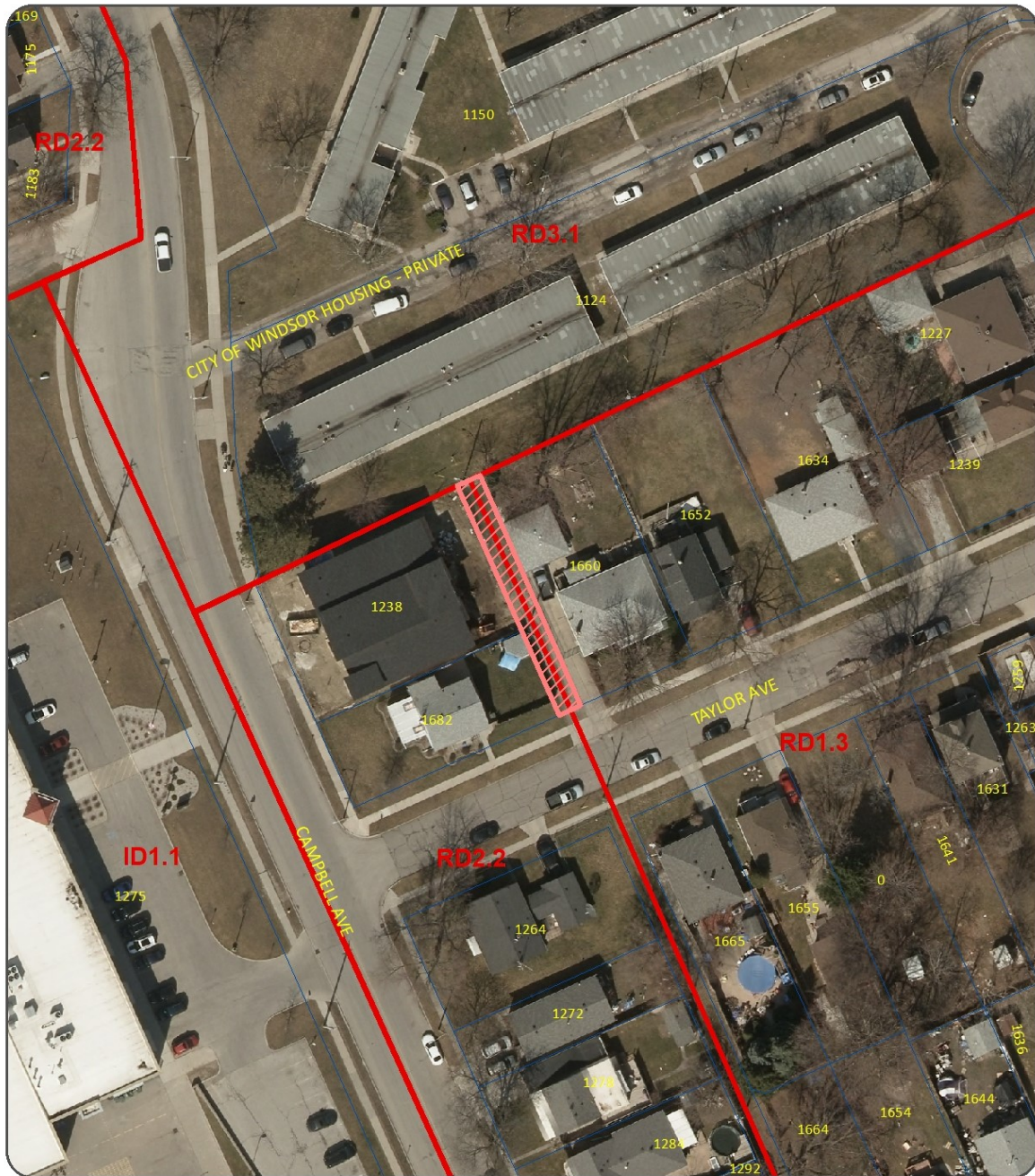
Appendices:

- 1 Appendix A - Drawing No. CC-1837
- 2 Appendix B - EIS Drawing - Aerial Photo
- 3 Appendix C - Consultations with Municipal Departments and Utility Companies
- 4 Appendix D - Site Photos
- 5 Appendix E - Classification of Alleys and Suitability for Closure
- 6 Appendix F - Neighbourhood Concerns

APPENDIX "A" Drawing No. CC-1837




APPENDIX "B" EIS Drawing - Aerial Photo



STREET & ALLEY CLOSING (SAA/6848)

1:750

APPLICANT : DAVID TRAN & KENG FU

 SUBJECT LANDS

PLANNING DEPARTMENT - PLANNING POLICY

DATE: JULY, 2023



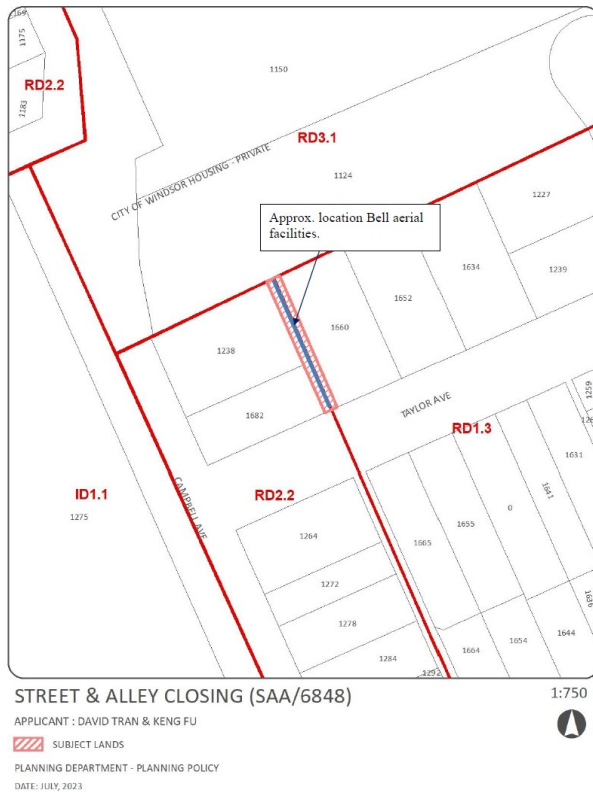
APPENDIX "C"

Consultations with Municipal Departments and Utility Companies

BELL CANADA WSP

Bell Canada requests a 3.0 m wide easement, 1.5 m on either side of the aerial facilities, or over the entire closure area whichever is best suited.

[Charleyne Hall - Bell Canada External Liaison, Right of Way & Indigenous Relation]



COGECO CABLE SYSTEMS INC.

No comments provided

ENBRIDGE GAS

After reviewing the provided drawing of the Alley Behind 1238 Campbell Ave and consulting our mapping system, please note that Enbridge Gas has active infrastructure in the proposed area. A PDF drawing has been attached for reference.

Please Note:

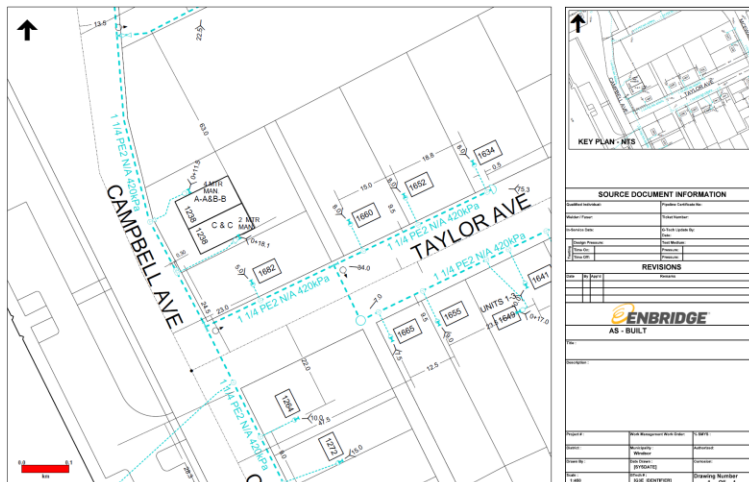
1. The shown piping locations are approximate and for information purposes only
2. The drawings are not to scale
3. This drawing does not replace field locates. Please contact Ontario One Call for onsite locates prior to excavating, digging, etc.

Enbridge Gas requires a minimum separation of 0.6 m horizontal and 0.3 m vertical from all of our plant less than NPS 16 and a minimum separation 1.0 m horizontal and 0.6 m vertical between any CER-regulated and vital pipelines. For all pipelines (including vital pipelines), when drilling parallel to the pipeline, a minimum horizontal clearance measured from the edge of the pipeline to the edge of the final bore hole of 1.0 m (3.3 ft.) is required. Please ensure that this minimum separation requirement is maintained, and that the contractor obtains locates prior to performing any work and utilizes safe excavation practices while performing any work in the vicinity.

Also, please note the following should you find any abandoned infrastructure in the area:

- Any pipe that is excavated, please assume that it is live
- If during the course of any job, any pipe is found that is not on the locate sheet and is in conflict with your work, please call our emergency number (1-877-969-0999), and one of our Union Gas representatives will respond to determine if that plant is in fact live or dead
- Please note that our Enbridge Gas representative will respond to the live or dead call within 1-4 hours, so please plan your work accordingly

[Samuel Nguyen - Summer Student Engineer]



ENVIRONMENTAL SERVICES

No concerns from Environmental Services

[Anne-Marie Albidone - Manager, Environmental Services]

ENWIN UTILITIES - HYDRO

No comments provided

ENWIN UTILITIES - WATER

No comments provided

LEGAL DEPARTMENT

No comments provided

MNSi

No comments provided

[Dave Hartleib - Outside Plant Manager]

PARKS & FACILITIES

No comments or issues with this SAA/6848 Liaison from Parks Design & Development, Natural Areas and Forestry.

[Sherif Barsom - Landscape Architect]

PLANNING DEPARTMENT

No comments provided

PLANNING DEPARTMENT - LANDSCAPE ARCHITECT

No comments provided

PUBLIC WORKS - ENGINEERING

The subject alley closure is approximately 13 ft. wide, appears to be untraveled, and is composed of grass. There are no municipal sewers or manholes located in the alley. There are hydro poles and overhead wires located in the subject alley, a utility easement would be required. If the alley is closed, the owner of the closed portion of the alley must obtain a driveway permit to complete and maintain the driveway approach to City Standard AS-221 or AS-222. If the closure application is rejected, the property owner of 1238 Campbell Avenue will be required to remove the encroaching gravel and concrete from the alley. The subject alley appears to serve no useful purpose under CR146/2005; therefore, we have no objections to the closure of this alley.

[Adam Pillon - Manager of Right-of-Way]

PUBLIC WORKS - TRAFFIC

Conveying the entire alley as shown will cut off access from 1268 Campbell to Taylor Avenue should either 1682 or 1660 Taylor purchase and close their portion of the alley and prevent 1268 Campbell from accessing their rear yard parking.

[Mike Spagnuolo - Signal Systems Analyst]

ROGERS COMMUNICATIONS

No comments provided

TELUS COMMUNICATIONS

TELUS has no underground infrastructure in the area of your proposed work.

[Meghna Patel - Permit Coordinator]

TRANSPORTATION PLANNING

Transportation Planning has no concerns. However, please note, if 1682 or 1660 Taylor Ave purchase and close their portion of the alley, this will prevent 1238 Campbell Ave from accessing their rear yard parking.

[Clare Amicarelli - Transportation Planning Coordinator]

TRANSIT WINDSOR

No issues from Transit Windsor.

[Jason Scott - Supervisor, Planning]

WINDSOR FIRE

Fire has no issue

[Mike Coste - Chief Fire Prevention Officer]

WINDSOR POLICE

No comments provided

[Barry Horrobin - Director of Planning & Physical Resources]

APPENDIX "D"
Site Photos from July 5, 2023 Order to Comply
(File No. VPW 23-31399)



Figure 1 - Looking north towards alley from Taylor Avenue



Figure 2 - Looking south from north end of alley (1238 Campbell Avenue on right)



Figure 3 - Looking south from north end of alley (1238 Campbell Avenue on right)



Figure 4 - Looking south from midpoint of alley (1660 Taylor Avenue on left)

APPENDIX “E”

Classification of Alleys and Suitability for Closure

Classification of Public Rights-of-Ways:

Currently streets and alleys fall into four classifications on the basis of their usefulness:

- 1) Alleys that are **indispensable**. These would be alleys serving commercial properties and properties fronting on heavily traveled streets i.e. major arterial routes and alleys which contain sewers and must remain accessible for servicing; alleys or streets which serve as the only vehicular means of access to rear parking areas and garages where the property has insufficient lot width for a side drive; and, alleys which contain Fire Department connections that are deemed to be necessary for firefighting access.
- 2) Alleys that, **have some usefulness**, are nevertheless dispensable and may or may not be a complete liability.
- 3) Alleys that appear to serve **no useful purpose**, either now, or anticipated. Such alleys are in residential areas and locations where generally the lots are wide enough for side drives, or those alleys abutting parks and other parcels of land that do not require any servicing from the alley. Remnant or stub-end streets which are dead-ended and do not serve as access to other streets.
- 4) Alleys lying in Holding zones and other similar undeveloped areas where the alley system is **clearly obsolete** and has never been developed, but where the City needs to keep its options open until new area plans are prepared and development is imminent.

Suitability for Closing:

Following are the criteria and suitability for closing alleys in each of the above classifications:

- 1) Indispensable alleys should **not be closed**, conveyed, reduced or otherwise jeopardized through minority interests unless a suitable substitute alley is opened in lieu thereof. They are essential from the viewpoint of fire protection, police protection, emergency services (i.e. ambulance) and loading or unloading of goods, refuse collection, servicing of blocked sewers and utility services. Without such alleys, the above noted services would at least be more costly if not impossible to complete or adequately access; and would noticeably interfere with street traffic, thereby reducing the access capacity of the adjacent arterial, collector, or street for business.
- 2) Alleys having some usefulness should **be considered for closing** only upon request of abutting owners rather than by encouragement of the City.
- 3) Alleys that serve no useful purpose should **be closed** if at all possible, and in fact the owners abutting thereon should be encouraged to accept conveyance.
- 4) Alleys that are clearly obsolete should **not be closed** unless there is a municipal need or specific development proposals acceptable to the City are submitted.

APPENDIX "F"

Neighbourhood Concerns

Re: Notice to Close the Alley Abutting 1238 Campbell; North of Taylor; South of Grove - Message (HTML)

File Message BLUEBEAM Tell me what you want to do...

Ignore: X Delete Reply Reply All Forward Meeting IM - More - Move OneNote Mark Unread Categorize Follow Up - Translate Find Related - Select - Zoom Create PDF Change Settings Select Folders

Mon 2023-07-17 6:35 PM

DAVID LATOUF <[REDACTED]>

Re: Notice to Close the Alley Abutting 1238 Campbell; North of Taylor; South of Grove

To: Matthews, Meghan (She,Her)

You replied to this message on 2023-07-18 11:17 AM.

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Meghan,

My name is David Latouf. I'm a resident at 1660 Taylor Ave., a neighbour of 1238 Campbell. Thank you for your letter detailing the proposed changes to the alley between our properties.

While I recognize the need for additional multi-family housing units in the city, the abutting properties have had continued issues with the parking situation at 1238 Campbell. I previously sent an email sharing my concerns about the congestion on an otherwise quiet street.

Aside from congestion, my primary concern with the alley closure is the possibility of flooding. The current parking paved areas of 1238 Campbell are sloped toward my property, and if the alley becomes paved as well, I fear that will have repercussions for my already flood-prone property. In addition, the closure will potentially affect access to my property if I or city workers need to do maintenance.

As per the STREET AND ALLEY CLOSING POLICY - CR 130/2006:

if at least 75% of abutting property owners agree to the closure of the alley, the City will assume responsibility for preparation of Reference Plan by an Ontario Land Surveyor showing the limits of the right-of-way to be closed and the manner in which it will be divided between the abutting owners.

Two thirds of the abutting properties (1660 Taylor and 1682 Taylor) do not agree to this alley closure and have concerns about the parking plan at 1238 Campbell.

I strongly advise you to reconsider approving the application to close the above referenced alley. I believe the alley is best left in its current state as city property.

Please feel free to call me with any questions at [REDACTED]

All the best,
David

David Latouf
1660 Taylor ave. n9b1r6

Gary and Barb Brown
1682 Taylor ave.

RE: SAA/6848 || Alley Closure Application || 1682 Taylor Ave - Message (-HTML)

File Message BLUEBEAM Tell me what you want to do...

Ignore Delete Reply Reply Forward Meeting IM More Move OneNote Mark Unread Categorize Follow Up Translate Find Related Select Zoom Create PDF Change Settings Select Folders

Tue 2023-07-11 2:15 PM

Matthews, Meghan (She/Her)
 RE: SAA/6848 || Alley Closure Application || 1682 Taylor Ave

To: Barb Brown

From: Barb Brown
Sent: July 11, 2023 12:47 PM
To: Matthews, Meghan (She/Her) <MMatthews@citywindsor.ca>
Subject: Re: SAA/6848 || Alley Closure Application || 1682 Taylor Ave

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Yes received letter today what is the difference closing a alley and him owning it he cant close it due to cable edwin etc poles ... No phone calls from no one its ok he will get what he wants Thank You for all your help my neighbour and I will attend the meeting

From: Barb Brown
Sent: July 11, 2023 6:05 AM
To: Matthews, Meghan (She/Her) <MMatthews@citywindsor.ca>
Subject: Re: SAA/6848 || Alley Closure Application || 1682 Taylor Ave

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi still have heard nothing no letter came . It will pass i am sure the inspectors already c=gave this guy a thumbs up to go ahead and do this.He tore up front city sidewalks to make it decoratative to his decor i am sure the city gave him :hat too What bothers me is we tried to get a driveway and the city told us we were impeaching on city property and we were turned down , yet this guy can come in and do all this and the city gives him everything , i guess if i was rich i would get a lousy driveway Thanks Barb brown

From: Barb Brown
Sent: June 28, 2023 1:08 PM
To: Costante, Fabio
Subject: 1238 Campbell

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Fabio anyway to find out if 1238 bought the back alley The owner going around telling people he did ... But i thought you had to have consent from all the neighbours around or in back of alley... He poured a big cement pad back there took the alley cannot fathom the city Inspectors giving approval for this ...
 Thanks Barb Brown

Item No. 8.8



Committee Matters: SCM 272/2023

Subject: Additional information regarding Public Communications

Moved by: Councillor Angelo Marignani
Seconded by: Councillor Fred Francis

Decision Number: **DHSC 558**

That the memo from the Commissioner, Economic Development & Innovation dated September 25, 2023 entitled “Additional Information Regarding Public Communications” **BE RECEIVED** for information.

Carried.

Report Number: SCM 266/2023

Clerk’s Note:

1. Please refer to Item 4.1 from the Development & Heritage Standing Committee held on October 3, 2023.
2. To view the stream of this Standing Committee meeting, please refer to:
<http://csg001-harmony.sliq.net/00310/Harmony/en/PowerBrowser/PowerBrowserV2/202320231003/-1/9435>



Committee Matters: SCM 266/2023

Subject: Additional information regarding Public Communications

TO: Development and Heritage Standing Committee
FROM: Commissioner of Economic Development and Innovation
DATE: September 25, 2023
SUBJECT: Additional information regarding Public Communications

Dear Committee Members,

As follow up to discussions at September's DHSC meeting, we wanted to provide details regarding Administration's multi-pronged approach to public communication and notification about planning applications.

In accordance with the Ontario Regulation 545/06, notice of *Planning Act* applications are placed in the Windsor Star a minimum of 20 days prior to DHSC meetings. Administration keeps a detailed table based on the Standing Committee schedule that highlights the deadlines and timeframes in which to provide the legislated notice. Planning staff send any required notices to the Clerk's Office, who in turn works with the Windsor Star to ensure that the notice is published within the prescribed timelines. Notices placed in the newspaper and meeting the minimum 20-day timeframe fulfill the required statutory notice under the Regulation.

Although not required by the Planning Act, the Planning department has adopted the best practice of mailing a notice of *Planning Act* applications directly to property owners and residents within 120 metres (400 feet) of the subject property to inform the public about a development application in their neighbourhood. Application information is provided to the Clerk's Office for mailing concurrent with the newspaper notice (i.e. more than 20 days prior to the DHSC meeting). The intent is that mailed notices are received 20 days prior to the DHSC meeting.

The notice placed in the Windsor Star and mailed to neighbours indicates that, "*The Council Report is available ten days before the public meeting on the City of Windsor website at <http://www.citywindsor.ca> - search "Development & Heritage Standing Committee Agenda" and click on the meeting date stated above.*"

Planning Act applications, along with supporting reports and studies, are posted on the City's Current Development Applications web page when a complete application is received (i.e. in advance of issuing the notice of public meeting). The information remains on this web page until Council considers the application for final approval.

**COMMISSIONER OF ECONOMIC
DEVELOPMENT AND
INNOVATION
MEMO**

In February 2023, City Council approved a new pre-consultation process to address the impacts of Bills 108 and 109. The new process now requires developers to hold a public open house to introduce the development concept and gather feedback from the neighbourhood. A report summarizing the open house and feedback received must accompany a complete development application. Planning staff provide developers (or their consultants) with a mailing list of all property owners and residents within 120 metres (400 feet) for the purpose of notifying the neighbourhood of the open house. The process improves opportunities for public consultation and is intended to increase the level of awareness in the areas where a development proposal is being considered. Attendees are to have the option to provide an email address to enable ongoing communications, including scheduling details of the DHSC and Council meetings.

Administration strives for continuous improvement and implementation of appropriate best practices and will reach out to other municipalities to determine if any additional best practices can be replicated in Windsor. Further methods to enhance communication and engagement will be forthcoming to Council later this fall.

Approvals:

Name	Title
Jelena Payne	Commissioner, Economic Development and Innovation
Anna Ciacelli on behalf of Steve Vlachodimos	City Clerk
Joe Mancina	Chief Administrative Officer



Subject: Official Plan Amendment and Zoning Bylaw Amendment Site specific regulations for Multiple Dwelling – Z 001-22 [ZNG-6653] Riverside Horizons 3251 Riverside Dr. E & 222 Belleview Ave - Ward 5

Moved by: Councillor Kieran McKenzie

Seconded by: Councillor Angelo Marignani

Decision Number: **DHSC 546**

THAT the City of Windsor Official Plan, Volume II, Part 1 – Special Policy Areas, **BE AMENDED** by adding site specific policies as follows:

1.# South Side of Riverside Drive, between Belleview Avenue and Pratt Street

1.#.1 The property described as of Lots 56 & 57, Plan 597; Lots 1 & 2 Plan 1563; and Part of Lot 101, Concession 1, in the City of Windsor, known municipally as 3251 Riverside Drive E & 222 Belleview Avenue, is designated a special policy area on Schedule A: Planning Districts and Policy Areas in Volume I – The Primary Plan.

1.#.2 Notwithstanding Section 6.3.2.1 of the Official Plan, Volume I, a High Profile Residential Building shall be permitted on the subject property.

THAT an amendment to City of Windsor Zoning By-law 8600 **BE APPROVED** changing the zoning of Lots 56 & 57, Plan 597; Lots 1 & 2 Plan 1563; and Part of Lot 101, Concession 1, in the City of Windsor, known municipally as 3251 Riverside Drive E & 222 Belleview Avenue, from Commercial District CD1.7 and Residential District RD2.2 to Residential District (RD) 3.3; and,

THAT subsection 1 of Section 20 of the City of Windsor Zoning By-law 8600 **BE AMENDED** for Lots 56 & 57, Plan 597; Lots 1 & 2 Plan 1563; and Part of Lot 101, Concession 1, in the City of Windsor, known municipally as 3251 Riverside Drive E & 222 Belleview Avenue by adding site specific regulations as follows:

483. South Side of Riverside Drive, between Belleview Avenue and Pratt Street

For the lands described as for Lots 56 & 57, Plan 597; Lots 1 & 2 Plan 1563; and Part of Lot 101, Concession 1, in the City of Windsor, known municipally as 3251 Riverside Drive E & 222 Belleview Avenue, the following regulations shall apply:

Building setback from an exterior lot line abutting Riverside Drive - 3m

Building setback from an exterior lot line abutting Riverside Drive or Pratt Place for that part of the building having a height of more than 12.5m - 7.5 m

Building setback from a lot line for that part of the building having a height of more than 12.5 m abutting any zone that permits single detached dwellings – 7.5 m

Parking spaces shall be prohibited on the first and second floor of any structure within 7.5 m of an exterior building wall adjacent to Riverside Drive;

Parking spaces shall be prohibited on the first floor of any structure within 7.5 m of an exterior building wall adjacent to Pratt Place

Amenity Area – Minimum - 420 m²;

Lot Area - Minimum - 3,500 m²;

Lot Coverage - Maximum - 66%;

Building Height – Maximum - 45.0m;

Landscaped Open Space - minimum - 20%;

Number of Dwelling units – maximum - 84

(ZDM 6; ZNG/6053)

THAT Lots 56 & 57, Plan 597; Lots 1 & 2 Plan 1563; and Part of Lot 101, Concession 1, in the City of Windsor, known municipally as 3251 Riverside Drive E & 222 Belleview Avenue, **BE CLASSIFIED** as a Class 4 area pursuant to Publication NPC-300 (MOECP Environmental Noise Guideline – Stationary and Transportation Sources – Approval and Planning); and,

THAT the City Planner or their designate **BE DIRECTED** to provide a copy of the Council Resolution approving the Class 4 area classification and a copy of any development agreement or site plan agreement for the subject lands that incorporates noise mitigation measures to East Windsor Cogeneration Plant, Ford Motor Company Windsor Engine Plant, and Greco Aluminum Railings”; and,

THAT the Site Plan Approval Officer **CONSIDER** the following matters in an approved site plan and/or executed and registered site plan agreement:

Incorporating additional, or modify existing, mitigation measures including warning clauses required for a Class 4 designation pursuant to Publication NPC-300 in any future Site Plan Control agreement.

Carried.

Member Daniel Grenier discloses an interest and abstains from voting on this matter.

Report Number: S 93/2023
Clerk's File: Z/14347 & Z/14639

Clerk's Note:

1. The recommendation of the Development & Heritage Standing Committee and Administration are the same.
2. Please refer to Item 7.1 from the Development & Heritage Standing Committee held on September 11, 2023.
3. To view the stream of this Standing Committee meeting, please refer to:
<http://csg001-harmony.sliq.net/00310/Harmony/en/PowerBrowser/PowerBrowserV2/20230911/-1/9432>

**Subject: Official Plan Amendment and Zoning Bylaw Amendment Site specific regulations for Multiple Dwelling – Z 001-22 [ZNG-6653]
Riverside Horizons 3251 Riverside Dr. E & 222 Belleview Ave - Ward 5**

Reference:

Date to Council: September 11, 2023

Author: Jim Abbs,

Senior Planner

255-6543 x6317

jabbs@citywindsor.ca

Planning & Building Services

Report Date: July 13, 2023

Clerk's File #: Z/14347 & Z/14639

To: Mayor and Members of City Council

Recommendation:

THAT the City of Windsor Official Plan, Volume II, Part 1 – Special Policy Areas, **BE AMENDED** by adding site specific policies as follows:

1.# South Side of Riverside Drive, between Belleview Avenue and Pratt Street

1.#.1 The property described as of Lots 56 & 57, Plan 597; Lots 1 & 2 Plan 1563; and Part of Lot 101, Concession 1, in the City of Windsor, known municipally as 3251 Riverside Drive E & 222 Belleview Avenue, is designated a special policy area on Schedule A: Planning Districts and Policy Areas in Volume I – The Primary Plan.

1.#.2 Notwithstanding Section 6.3.2.1 of the Official Plan, Volume I, a High Profile Residential Building shall be permitted on the subject property.

THAT an amendment to City of Windsor Zoning By-law 8600 **BE APPROVED** changing the zoning of Lots 56 & 57, Plan 597; Lots 1 & 2 Plan 1563; and Part of Lot 101, Concession 1, in the City of Windsor, known municipally as 3251 Riverside Drive E & 222 Belleview Avenue, from Commercial District CD1.7 and Residential District RD2.2 to Residential District (RD) 3.3; and,

THAT subsection 1 of Section 20 of the City of Windsor Zoning By-law 8600 **BE AMENDED** for Lots 56 & 57, Plan 597; Lots 1 & 2 Plan 1563; and Part of Lot 101, Concession 1, in the City of Windsor, known municipally as 3251 Riverside Drive E & 222 Belleview Avenue by adding site specific regulations as follows:

483. South Side of Riverside Drive, between Belleview Avenue and Pratt Street

For the lands described as for Lots 56 & 57, Plan 597; Lots 1 & 2 Plan 1563; and Part of Lot 101, Concession 1, in the City of Windsor, known municipally as 3251 Riverside Drive E & 222 Belleview Avenue, the following regulations shall apply:

Building setback from an exterior lot line abutting Riverside Drive - 3m

Building setback from an exterior lot line abutting Riverside Drive or Pratt Place for that part of the building having a height of more than 12.5m - 7.5 m

Building setback from a lot line for that part of the building having a height of more than 12.5 m abutting any zone that permits single detached dwellings – 7.5 m

Parking spaces shall be prohibited on the first and second floor of any structure within 7.5 m of an exterior building wall adjacent to Riverside Drive;

Parking spaces shall be prohibited on the first floor of any structure within 7.5 m of an exterior building wall adjacent to Pratt Place

Amenity Area – Minimum - 420 m²;

Lot Area - Minimum - 3,500 m²;

Lot Coverage - Maximum - 66%;

Building Height – Maximum - 45.0m;

Landscaped Open Space - minimum - 20%;

Number of Dwelling units – maximum - 84

(ZDM 6; ZNG/6053)

THAT Lots 56 & 57, Plan 597; Lots 1 & 2 Plan 1563; and Part of Lot 101, Concession 1, in the City of Windsor, known municipally as 3251 Riverside Drive E & 222 Belleview Avenue, **BE CLASSIFIED** as a Class 4 area pursuant to Publication NPC-300 (MOECP Environmental Noise Guideline – Stationary and Transportation Sources – Approval and Planning); and,

THAT the City Planner or their designate **BE DIRECTED** to provide a copy of the Council Resolution approving the Class 4 area classification and a copy of any development agreement or site plan agreement for the subject lands that

incorporates noise mitigation measures to East Windsor Cogeneration Plant, Ford Motor Company Windsor Engine Plant, and Greco Aluminum Railings”; and,

THAT the Site Plan Approval Officer **CONSIDER** the following matters in an approved site plan and/or executed and registered site plan agreement:

Incorporating additional, or modify existing, mitigation measures including warning clauses required for a Class 4 designation pursuant to Publication NPC-300 in any future Site Plan Control agreement.

Executive Summary:

N/A

Background:

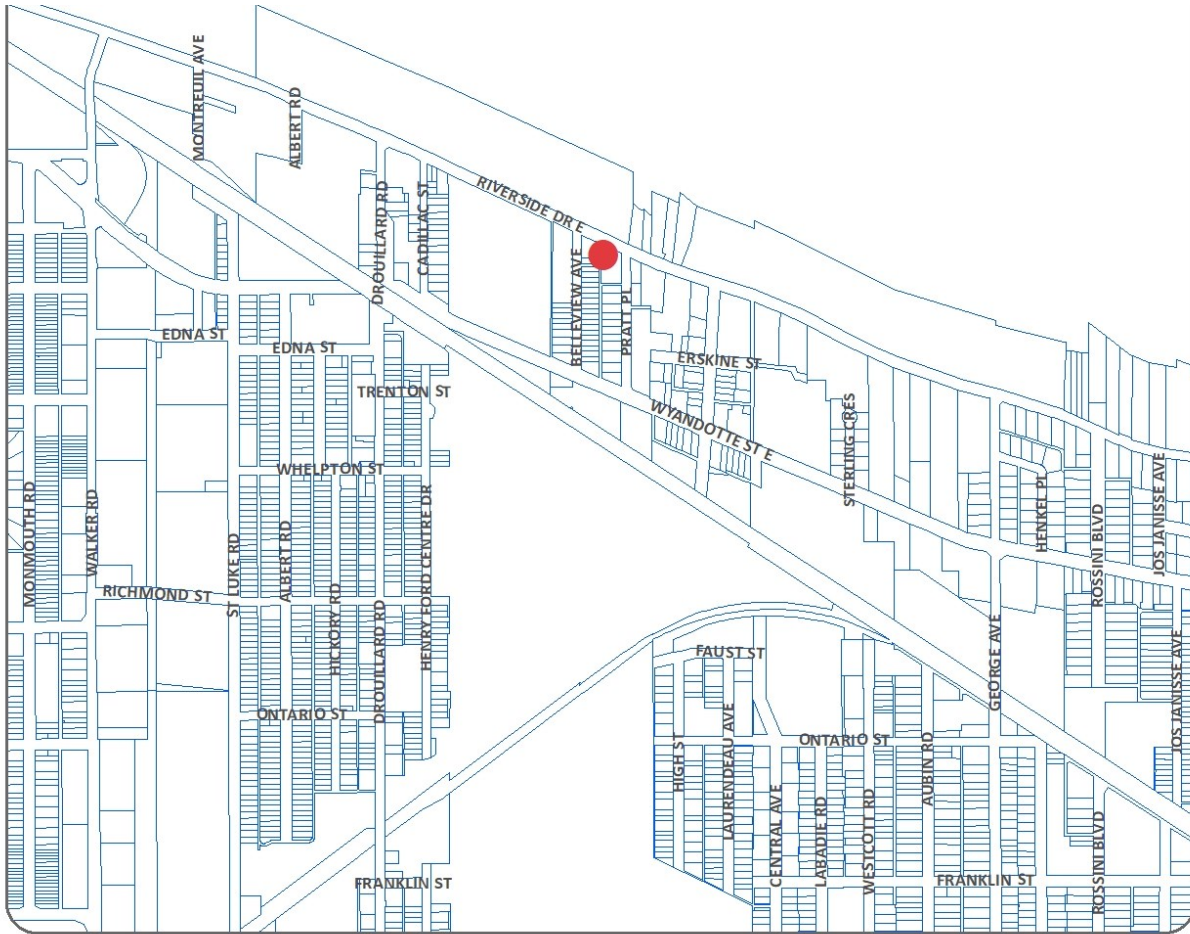
Application Information:

Location: 3251 Riverside Drive E & 222 Belleview Avenue Ward: 5

Planning District: 04 – East Windsor ZDM: 6

Owner: Riverside Horizons

Agent: Dillon Consulting (Melanie Muir)



KEY MAP - Z-001/22, ZNG-6653



● SUBJECT LANDS

Proposal:

The proposed development includes the construction of one, twelve (12) storey residential building, with 84 units and 135 parking spaces between the proposed parking garage and a surface parking lot on site. The proposed building consists of three (3) storeys of parking (Ground Level and Floors 2 and 3) with nine (9) storeys of residential above. The proposed development requires 1.25 parking spaces per dwelling unit, for a total of 105 spaces being required with 135 being provided.

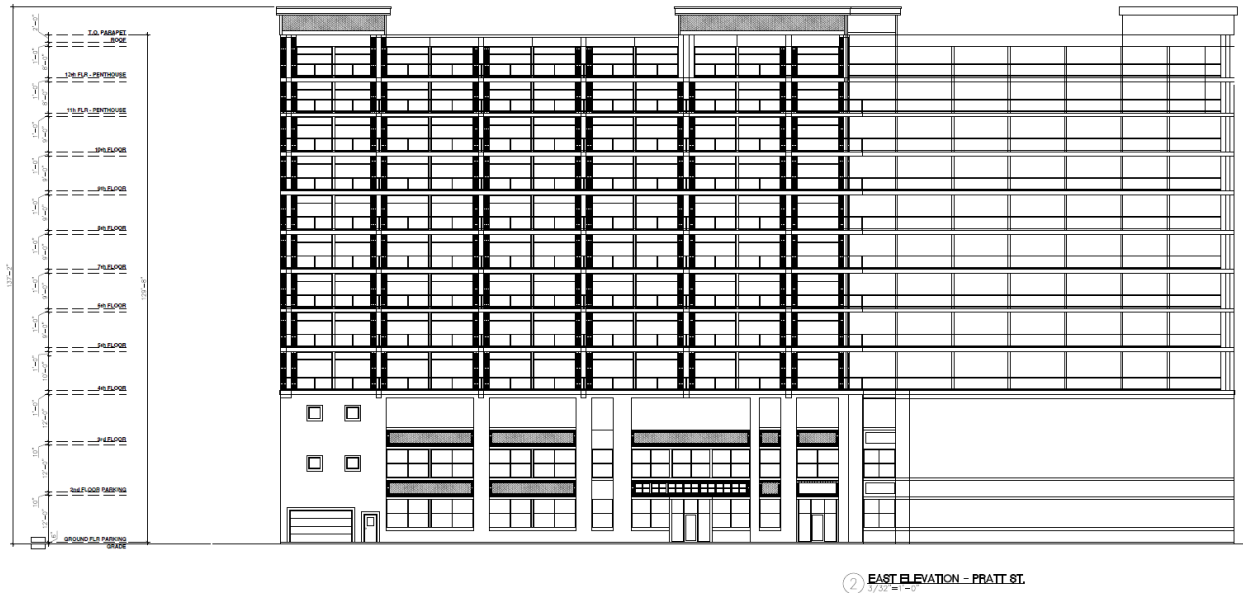


Figure 3: East Elevation (Pratt Place)

SUBMISSIONS BY APPLICANT:

- Storm water Management Report (March 2023)
- Riverside Horizons Planning Justification Report (October 2021 – Revised March 2023)
- Urban Design Brief (October 2021 – Revised March 2023)
- Site Renderings
- Building Floor Plans and Elevations
- Noise Assessment 3251 Riverside Drive
- Shadow Study, March 2023
- Archaeological Assessment Report

The Applicant is requesting a site specific zone that would permit the construction of a Multiple Dwelling with the following site specific regulations:

- The minimum lot area to permit a site area of 3,500m²;
- The maximum lot coverage from 45% (RD2.2) to 66%;
- The maximum building height from 10.0m (RD2.2) to 45.0m;
- The minimum landscaped open space yard from 35% (RD3.1) to 20%;
- The maximum dwelling units per hectare from 225 to 240, and
- That the site be designated a Class 4 area for the purposes of noise mitigation

Site Information:

Official Plan	Zoning	Current Use	Previous Use
Residential	Commercial District CD1.7 and Residential District RD2.2	3251 Riverside Dr. E Vacant/Parking Lot 222 Belleview Ave Residential	3251 Riverside Dr. E Vacant/Parking Lot 222 Belleview Ave Residential
Lot Depth	Lot width	Area	Shape
varies	+/- 75 m (Riverside Drive)	3599 m ²	Irregular
All measurements are for the entire parcel and are approximate.			

Neighbourhood Characteristics:

The proposed development is located on Riverside Drive, Between Belleview Avenue and Pratt Place on an irregularly shaped site with frontage on Riverside Drive East, Belleview Avenue and Pratt Place. The current site is vacant, with a former parking lot on the eastern portion and a recently vacated and demolished single detached dwelling on the south-west side of the site. Several parcels to the east along Riverside Drive and south on Pratt Place are zoned Residential District 3.4 (RD3.4), which permits high rise, multi-unit dwellings similar to the one proposed on the subject site. There does not appear to be any significant natural features on the site.

Surrounding Land Uses:

- North** Riverside Drive East;
Industrial – Ford Motor Company Transportation Shipping (MD1.3)
Single Detached Residential Dwellings (RD1.1)
- East** vacant (former Single Detached Dwelling) (RD2.2)
Multiple Dwelling buildings (RD3.4), including:
 - Pratt Place, Residential;
 - Lifetimes on Riverside (Retirement Residence);
 - Hazelview Properties;
 - Strabane Apartments.
- South** Single Detached, Semi-Detached and Duplex Dwellings (RD2.2)
Rivershore Tower Multiple Dwelling Building (RD3.4)
- West** Belleview Avenue

Multiple Dwelling buildings (ID1.2)
Single Detached Dwellings (RD2.2)
Ford Powerhouse and East Windsor Cogeneration Plants (CD4.5)



NEIGHBOURHOOD MAP - Z-001/22, ZNG/6653



SUBJECT LANDS

Discussion:

Provincial Policy Statement (PPS) 2020:

The Provincial Policy Statement, (PPS) 2020 provides direction on matters of provincial interest related to land use planning and development and sets the policy foundation for regulating the development and use of land in Ontario.

The zoning bylaw amendment would result in a development on a former Industrial site that was previously vacant and underutilized. This is consistent with the Provincial Policy Statement in that the development promotes the efficient use of existing land, promotes cost-effective development patterns and standards to minimize land consumption and servicing costs. Related to this direction, the PPS states:

“1.1.1(b) accommodating an appropriate range and mix of residential (including second units, affordable housing and housing for older persons), employment (including industrial commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs”

e) promoting cost-effective development patterns and standards to minimize land consumption and servicing costs;”

The requested Multiple Dwelling development promotes cost-effective development by redeveloping an under-utilized vacant site. Allowing the proposed zoning bylaw amendment in this location contributes to minimizing land consumption and servicing costs by using a site that already has available infrastructure in the immediate area.

The PPS also states:

“1.1.2 Sufficient land shall be made available to accommodate an appropriate range and mix of land uses to meet projected needs for a time horizon of up to 20 years.”

The PPS requires that land be available to diversify developments to meet the future needs of the community. The zoning by-law amendment is consistent with that requirement by accommodating new residential construction on lands designated for that purpose.

The PPS also states:

“1.4.1 To provide for an appropriate range and mix of housing types and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall:

a. maintain at all times the ability to accommodate residential growth for a minimum of 10 years through residential intensification and redevelopment and, if necessary, lands which are designated and available for residential development; and

b. maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available

through lands suitably zoned to facilitate residential intensification and redevelopment, and land in draft approved and registered plans.”

The requested zoning bylaw amendment is consistent with the PPS in that intensification of the use of the site will provide additional “appropriate range and mix of housing types and densities”.

“1.4.3 Planning authorities shall provide for an appropriate range and mix of housing types and densities to meet projected requirements of current and future residents of the regional market area by:

- a. permitting and facilitating:
 1. all forms of housing required to meet the social, health and well-being requirements of current and future residents, including special needs requirements; and
 2. all forms of residential intensification, including second units, and redevelopment in accordance with policy 1.1.3.3;
- b. directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;
- c. promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed;”

Approving the Official Plan and zoning by-law amendments would support residential development using the infrastructure that is already in place, instead of requiring more expenditure on new infrastructure in a greenfield setting. In terms of supporting active transportation and transit, the site of the proposed zoning amendment is served by Transit Windsor on Wyandotte Street E.

The proposed development is consistent with the PPS in that it promotes compact and transit supportive forms of development. As well, this development will help to support the provision of a range of housing types in this area.

The site is also very near to transit corridors, which provides a range of travel options for the residents. The density of the development may help support the transit options that currently exist in this area.

Official Plan:

Section 3.3.2.2 of the Urban Structure Plan contained in the City of Windsor Official Plan identifies Riverside Drive as a City Corridor. Residential development on a City Corridor may include high profile (26 to 58 metres in height), medium profile (14 to 26 metres in height) and residential over retail at street, as well as row housing and lofts. The proposed development conforms with this policy.

Additionally, the site is subject to the Residential designation of the Official Plan. The Residential designation in the City of Windsor Official Plan permits low and medium profile developments not greater than twenty-six (26) metres in height. The proposed development is 45m (12 storeys) in height which would not conform to Section 6.3.1.2 of the Official Plan.

6.3.2.1 Uses permitted in the Residential land use designation identified on Schedule D: Land Use include Low Profile, and Medium Profile dwelling units.

High Profile Residential Buildings shall be directed to locate in the City Centre, Mixed Use Centres and Mixed Use Corridors.

This policy was implemented through OPA 159 and generally directs high profile development to the City Centre, Mixed Use Centres and Mixed Use Corridors.

To prevent any confusion between the two conflicting sections of the Official Plan, the Planning Department recommends that a Site Specific policy area be applied to the site that will confirm that a High Profile Building be permitted at this location

The applicant is requesting that a Site Specific policy area be applied to the site to permit a high profile building (45m) proposed

Other residential buildings of similar height and form currently exist further east of this site. This indicates that the proposed Multiple Dwelling is compatible within its context.

Discussions with applicant regarding original design resulted in a dramatically improved street presence and street scape by reducing the building footprint and increasing the building height. The revised plan includes townhome style units on the ground floor level that can be accessed from the street, giving the first 3 floors of the development a more pedestrian friendly and a more appropriate streetscape adjacent to Riverside Drive and Pratt Place.

The proposed development is consistent with the following goals and objectives of the City of Windsor Official Plan.

Goal 6.1.1 is to achieve safe, caring and diverse neighbourhoods. Goal 6.1.2 seeks environmentally sustainable urban development. Goal 6.1.3 promotes housing suited to the needs of Windsor's residents. Goal 6.1.10 is to achieve pedestrian oriented clusters of residential, commercial, employment and institutional uses.

Objective 6.3.1.1 supports a complementary range of housing forms and tenures in all neighbourhoods. Objective 6.3.1.2 seeks to promote compact neighbourhoods and balanced transportation systems. Objective 6.3.1.3 seeks to promote selective residential redevelopment, infill and intensification initiatives.

The proposed development will help to support a diverse neighbourhood that represents a sustainable community and will provide housing that is in demand. The proposed development will help to encourage a pedestrian orientated cluster of residential, commercial and employment uses. The proposed residential development represents a

complementary and compact form of housing and intensification that is near sources of transportation.

Zoning By-law

The site is zoned Commercial District CD1.7 and Residential District RD2.2. neither of these zone categories permit the proposed use. The applicant is proposing that the RD3.3 zone be applied, but specific regulations be applied to facilitate the proposed development.

To facilitate the proposed development, the following site-specific regulations are proposed by the applicant.

- Minimum Lot Area - 3,500m²
- The maximum lot coverage from 35% (RD3.3) to 66%;
- The maximum building height from 30.0m (RD3.3) to 45.0m;
- The minimum landscaped open space yard from 35% (RD3.3) to 20%;
- The maximum dwelling units per hectare from 225 to 240.

The RD 3.3 zone would permit the proposed Multiple Dwelling structure as a use, however a number of specific yard regulations will also be required. This site being unique ("L" shaped with Streets on 3 sides) will require its own set of regulations to facilitate the proposed development.

Lot Area

Administration recommends that the minimum lot area be set at the site's current size.

Lot coverage

The proposed increase in lot coverage supports the goal of providing residential units in developments that exhibit compact urban form. Increased lot coverage allows a development to provide more units, using existing infrastructure, on less land within an urban context.

Building height

The building height requested is partly as result of Administration's request to create a development that would address Riverside Drive and Pratt Place with residential units accessible at ground level rather than presenting the facade of a parking garage to those streets.

To ensure a transition in building height on the development site, any structure built on the site will be required to step up in height so as not to present the total height of 45 m immediately adjacent to the low profile development or to Riverside Drive. The Building Height will be stepped as described in the regulation required by Administration.

Landscaped open space yard

The proposed reduction in landscaped open space supports the goal of providing residential units in developments that exhibit compact urban form. The reduction is mitigated by the location of the development, surrounded on 3 sides by municipal roads and by the large amount of amenity area proposed in this development. Allowing the development to be close to Riverside Drive and Pratt Place ensures the development will address and be connected those streets at a pedestrian level, and will not isolate itself from its surroundings. As well, the large amount of amenity area proposed mitigates the need for landscaped open space to serve the residents of the building.

Dwelling units per hectare

While in some cases the practice is to express the lot area as a per unit per unit ratio (41.67 m²/unit) to ensure the maximum number of units does not exceed the applicant's current proposal should the applicant add additional lands to their holdings. Identifying the total number of units permitted on the site will provide better clarity and transparency describing the scope of the proposed development. (84 units)

Additional regulations required by Administration

Amenity Area

As a result of enhanced amenity area used as justification for reduced yards and increased lot coverage, the amenity space supplied in this development should be codified to ensure that the Amenity Area is provided to the extent proposed in the Concept Plan. This is reflected in the minimum Amenity area requirement in administration's recommendation.

Set back

To assist in mitigating the transition in development profile, the applicant's concept plan indicates a 3.0m setback from Riverside Drive and Pratt Place for the base section of the building and an additional setback of the tower section from Riverside and Pratt. Additionally, administration is requesting an additional step back of the tower section adjacent to low profile dwellings south of the development site.

Tower section setbacks

From Riverside Drive:

To ensure the development is constructed in a similar form as shown on the Concept Plan, and to prevent the building from overpowering Riverside Drive, Administration recommends that the tower section of the development be set back 7.5m (4.5m+3m) from Riverside Drive and Pratt Place.

From Low Profile residential

Section 3.4.1.4 (c) of the City of Windsor Intensification Guidelines (June 2022) states that where Medium and High Profile development abuts low profile properties, a

minimum 7.5 metre rear yard setback should be required. To assist the development in providing an appropriate transition from adjacent low profile development to the south, Administration recommends that the tower section of the development be set back the same 7.5m as from Riverside Drive and Pratt Place where the development is adjacent to a zone category where low profile development types are permitted. This will specifically impact the southern limit of the development site.

Parking Space Setback

To ensure the parking spaces in the podium portion of the building are located in accordance with the Concept Plan provided, and do not become the predominate feature of the development along the Riverside Drive and Pratt Place frontages, Administration recommends adding site specific requirements that would prohibit parking spaces located on the first and second floors of the podium within 7.5 m of an exterior building wall adjacent to Riverside Drive and prohibit parking spaces located on the first floor of the podium within 7.5 m of an exterior building wall of adjacent to Pratt Place.

Class 4 area

The applicant has requested that the site be designated a Class 4 area for the purposes of noise mitigation.

The Class 4 Area designation is an acoustical environment area where relaxed (higher) daytime and night time sound level limits from that otherwise permitted in an urban area, for both indoor and outdoor areas, may be considered.

NPC-300 identifies a number of considerations to apply to a proposed Class 4 area and associated new noise sensitive land uses, including, but not limited to:

- Submission of a satisfactory noise impact assessment which includes noise measures as required by NPC-300;
- Appropriate notification to prospective purchasers that the dwelling is located in a Class 4 area, which may include, but is not limited to, agreements for noise mitigation registered on title, and appropriate warning clauses in future agreements of purchase and sale;
- Providing a copy of the approved noise impact assessment and Class 4 area designation confirmation to surrounding owners of the stationary sources;

Once a site is designated a Class 4 area, it remains as such, subject to the continuing presence of the stationary sources.

Where a municipality designates a site a Class 4 area, the relaxed noise levels would apply to the Ministry's Environmental Compliance Approval of the stationary source.

In this case, the noise study submitted by the applicant identified existing stationary noise sources at the East Windsor Cogeneration Plant, Ford Motor Company Windsor Engine Plant, and Greco Aluminum Railings.

To utilize the Class 4 area noise levels, a municipality must provide formal confirmation of the Class 4 area designation, in this case through the approval of the recommendation contained in this report.

Risk Analysis:

N/A

Climate Change Risks

Climate Change Mitigation:

The additional increase in the density of development on the site with access to existing bus routes and being close to commercial and community facilities will encourage the use of transit, walking and cycling as modes of transportation, thereby helping to minimize the City's carbon footprint.

Climate Change Adaptation:

The site will be subject to site plan control and is part of a storm water detention system designed as part of this development.

Financial Matters:

N/A

Consultations:

Comments received from municipal departments and external agencies are attached as Appendix "A" to this report.

Public Notice:

The statutory notice required under the Planning Act was provided in the Windsor Star. In addition, all properties within 120m (400 feet) of the subject parcel received courtesy notice by mail prior to the Development & Heritage Standing Committee Meeting (DHSC) meeting

Conclusion:

Planner's Opinion and Conclusions:

The proposed use of this site as a development containing a Multiple Dwelling structure containing 84 units represents an efficient development that will have no adverse impact on the well-being of the City of Windsor. The proposed development represents an appropriate residential use, adds to the range and mix of uses and will not cause any environmental or public health and safety concerns. This development is consistent with the Provincial Policy Statement.

The proposed Multiple Dwelling represents a housing type and density that meets the requirements of current and future residents, that meets the social, health and well-being of current and future residents, represents a form of residential intensification, is set in a location with access to infrastructure, public service facilities, and is close to commercial land uses.

The proposed Multiple Dwelling is located within an area that contains other Multiple Dwelling buildings of similar form and is compatible within its context.

The proposed zoning by-law amendment is consistent the PPS, with the policy direction of the City of Windsor Official Plan, is compatible with existing and permitted uses in the surrounding neighbourhood and constitutes good planning.

Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Greg Atkinson Manager of Development/Deputy City Planner

Neil Robertson Acting City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP

Approvals:

Name	Title
Greg Atkinson	Manager of Development/Deputy City Planner
Neil Robertson	Acting City Planner
Wira Vendrasco	Deputy City Solicitor, Legal Services & Real Estate
Jelena Payne	Commissioner of Economic Development & Innovation
Joe Mancina	Chief Administrative Officer

Notifications:

Name	Address	Email
Riverside Horizons Inc. (Wing On Li)	4510 Rhodes Dr. Suite 520, Windsor ON N8W 5K5	
Dillon Consulting Limited (Melanie Muir)	3200 Dezeil Drive, Suite 608, Windsor ON N8W 5K8	

Name	Address	Email
Councillor Sleiman		

Appendices:

- 1 Z 001-22 Liaison Comments
- 2 Concept Plan
- 3 Urban Design Brief - March 2023

COMMENTS

Jennifer Nantais – Environmental & Sustainability Coordinator

The Environmental Sustainability and Climate Change Team would like to request an energy strategy.

In response to the application for a zoning amendment there are no objections. Please also note the following comments for consideration:

Energy Conservation, Air Quality and Climate Change:

Please note PPS 2020 energy conservation and efficiency policies as they relate to long-term economic prosperity (1.7.1 (j)), as well as improved air quality and reduced greenhouse gas emissions (1.8.1). In addition, the City of Windsor Community Energy Plan (approved July 17 2017) aims to improve energy efficiency; modifying land use planning; reducing energy consumption and greenhouse gas emissions; and fostering green energy solutions throughout Windsor, while supporting local economic development.

As per these policies the developer should consider energy efficiency in the building design. This may include but not be limited to increased insulation, energy efficient appliances and fixtures, high efficiency windows and doors. In addition, consideration for EV charging infrastructure and opportunities to increase resiliency such as providing strategic back-up power capacity is warranted.

In addition, the large scale paving of natural space will increase the urban heat island in the area. It is recommended that the developer consider shade trees, white colour roofs or green roofs to mitigate this impact. For more suggestions please consult the following resources: LEED, Built Green Canada, and EnerGuide.

To promote the use of active transportation, bike racks should be included.

Stormwater Management:

Consideration should be given, as per PPS 2020 Section 1.6.6.7 to maximize the extent and function of vegetative and pervious surfaces; and promote stormwater management best practices, including stormwater attenuation and reuse, water conservation and efficiency, and low impact development.

Low Impact Design should be considered during Site Plan Review to address quantity and quality of stormwater leaving the site. The addition of Green Infrastructure here would be beneficial. Please see <https://greeninfrastructureontario.org> for examples.

Please note that this area of Windsor has a risk of basement flooding. The applicant should be aware of this risk if developing basements in any buildings.

Landscaping

Consideration for shade trees are recommended to minimize the urban heat island impacts. Consideration of native, drought resistant plants is encouraged to limit watering requirements.

In addition we encourage the developer to consider community gardening space for residents. Local food production is very popular in Windsor and considering the size of this development a space for community garden boxes is warranted.

Via Rail

VIA has no concerns in principle, the only issue would be that the proponent may need to address railway generated noise if the site is within 300m of VIA's right-of-way (not CN's).

Enbridge – Windsor Mapping

After reviewing the provided drawing at 3251 Riverside Dr and consulting our mapping system, please note that Enbridge Gas has active infrastructure in the right-of-way surrounding the proposed area. A PDF drawing has been attached for reference.

Please Note:

1. The shown piping locations are approximate and for information purposes only
2. The drawings are not to scale
3. This drawing does not replace field locates. Please contact Ontario One Call for onsite locates prior to excavating, digging, etc

Enbridge Gas requires a minimum separation of 0.6m horizontal and 0.3m vertical from all of our plant less than NPS 16 and a minimum separation 1.0m horizontal and 0.6m vertical between any CER-regulated and vital pipelines. For all pipelines (including vital pipelines), when drilling parallel to the pipeline, a minimum horizontal clearance measured from the edge of the pipeline to the edge of the final bore hole of 1 m (3.3 ft) is required. Please ensure that this minimum separation requirement is maintained, and that the contractor obtains locates prior to performing any work and utilizes safe excavation practices while performing any work in the vicinity.

Also, please note the following should you find any abandoned infrastructure in the area:

- Any pipe that is excavated, please assume that it is live
- If during the course of any job, any pipe is found that is not on the locate sheet and is in conflict with your work, please call our emergency number (1-877-969-0999), and one of our Union Gas representatives will respond to determine if that plant is in fact live or dead
- Please note that our Enbridge Gas representative will respond to the live or dead call within 1-4 hours, so please plan your work accordingly



Sherif Barsom – Parks D&D

We just want to draw attention to the required tree protection during the construction processes for the existing ROW street boulevard trees which located at the Bellevue Ave. and defined in the EIS as follows:

- 1- Tree Norway Maple
- 2- Tree Hackberry

Otherwise, Parks Development has no comments pertaining the LIAISON: Z-001/22 [ZNG/6653] - Riverside Horizons - 3251 Riverside Dr E & 222 Bellevue Ave.

Jason Scott – Transit Windsor

Transit Windsor has no objections to this development. The closest existing transit route to this property is with the Crosstown 2. The closest existing bus stop to this property is located on Wyandotte at Bellevue Northeast Corner. This bus stop is approximately 270 metres from this property falling within our 400 metre walking distance guideline to a bus stop. This will be maintained with our Council approved Transit Master Plan.

ERCA

The following is provided as a result of our review of Zoning By-Law Amendment Z-001-22 ZNG 6653. The applicant is requesting a Zoning By-law Amendment from the RD2.2 and CD1.7 to a site specific zone similar to the Residential District 3.3 (RD3.3) zone to permit the construction of a 10 storey, 77 unit residential development with 129 parking spaces. Site specific relief is required to facilitate the applicant's desired building design, including requiring relief from:

- The minimum lot area to permit a site area of 3,599m²;
- The maximum lot coverage from 35% to 65%;
- The maximum building height to 40.0m;
- The minimum landscaped open space from 35% to 15%; and
- Class 4 Area Designation (Noise and Vibration) to be applied to the building

DELEGATED RESPONSIBILITY TO REPRESENT THE PROVINCIAL INTEREST IN NATURAL HAZARDS AND REGULATORY RESPONSIBILITIES ASSOCIATED WITH THE CONSERVATION AUTHORITIES ACT

The following comments reflect our role as representing the provincial interest in natural hazards as outlined by Section 3.1 of the Provincial Policy Statement of the *Planning Act* as well as our regulatory role as defined by Section 28 of the *Conservation Authorities Act*.

We have reviewed our floodline mapping for this area and it has been determined this site is not located within a regulated area that is under the jurisdiction of the ERCA (Section 28 of the *Conservation Authorities Act*). As a result, a permit is not required from ERCA for issues related to Section 28 of the *Conservation Authorities Act*, Development, Interference with Wetlands and Alteration to Shorelines and Watercourses Regulation under the *Conservations Authorities Act*, (Ontario Regulation No. 158/06).

WATERSHED BASED RESOURCE MANAGEMENT AGENCY

The following comments are provided in an advisory capacity as a public commenting body on matters related to watershed management.

SECTION 1.6.6.7 Stormwater Management (PPS, 2020)

ERCA has concerns with the potential impact of the quality and quantity of runoff in the downstream watercourse due to the proposed development on this site. ERCA recommends that stormwater quality and stormwater quantity will need to be addressed up to and including the 1:100 year storm event and be in accordance with the guidance provided by the Stormwater Management Planning and Guidance Manual, prepared by the Ministry of the Environment (MOE, March 2003) and the "Windsor-Essex Region Stormwater Management Standards Manual".

We further recommend that the stormwater management analysis, be completed, to the satisfaction of the Municipality, at Site Plan Control. We do not require further consultation on this file with respect to stormwater management.

PLANNING ADVISORY SERVICE TO PLANNING AUTHORITIES - NATURAL HERITAGE POLICIES OF THE PPS, 2020

The following comments are provided from our perspective as an advisory service provider to the Planning Authority on matters related to natural heritage and natural heritage systems as outlined in Section 2.1 of the Provincial Policy Statement of the *Planning Act*. The comments in this section do not necessarily represent the provincial position and are advisory in nature for the consideration of the Planning Authority.

The subject property is not within or adjacent to any natural heritage feature that may meet the criteria for significance as defined by the PPS. Based on our review, we have no objection to the application with respect to the natural heritage policies of the PPS.

FINAL RECOMMENDATION

With the review of background information provided and aerial photograph, ERCA advises that a stormwater management plan be completed to the satisfaction of the City of Windsor, at Site Plan Control.

Barbara Rusan – Building Department

The Building Code Act, Section 8.(1) requires that a building permit be issued by the Chief Building Official for construction or demolition of a building. The building permit review process occurs after a development application receives approval and once a building permit application has been submitted to the Building Department and deemed a complete application.

Due to the limited Ontario Building Code related information received, review of the proposed project for compliance to the Ontario Building Code has not yet been conducted.

It is strongly recommended that the owner and/or applicant contact the Building Department to determine building permit needs for the proposed project prior to building permit submission.

The City of Windsor Building Department can be reach by phoning 519-255-6267 or, through email at buildingdept@citywindsor.ca

Enwin

HYDRO ENGINEERING:

ENWIN has existing overhead pole lines along the south limits with 16,000 volt primary and 120/240 volt secondary hydro distribution.

ENWIN has existing overhead pole lines along the north limits with 120/240 volt secondary streetlight distribution.

ENWIN has existing overhead pole lines along the east limits with 120/240 volt secondary streetlight distribution.

Prior to working in these areas, we would suggest notifying your contractor and referring to the Occupational Health and Safety Act and Regulations for Construction Projects to confirm clearance requirements during construction.

Also, we suggest referring to the Ontario Building Code for permanent required clearances for New Building Construction.

WATER ENGINEERING:

Water Engineering has no objections to the rezoning.

Kristina Tang – Heritage Planner

Built Heritage

The subject property is located in close vicinity or adjacent to a number of identified heritage resource on the Windsor Municipal Heritage Register:

3001 Riverside Dr E / 3150 Wyandotte St E	Ford Powerhouse Ford City	1922	Arch. Albert Kahn
~3150 Riverside Dr E	Ford Powerhouse Screen House Ford City	1923	Arch. Albert Kahn
3336 Riverside Dr E Farm/Foursquare	Damase Pratt House Ford City	c1902	French
3368 Riverside Dr E City	Charles Pratt House	c1910	French Farm Ford
3404 Riverside Dr E Georgian Revival	Cruickshank House Ford City	c1893	Summer Residence
3404 Riverside Dr E	Cruickshank House Garage Ford City	c1893	Summer Residence
243 Pratt Pl City	House	1921	Arts & Crafts Ford

Heritage sensitive considerations should be given to ensure that the proposed large-scale high rise is designed without negative impacts to the heritage resources and so as not to trigger later requests for a Built Heritage Impact Study/Heritage Impact Assessment (Heritage Impact Assessment Guidelines provided for reference). At this point, the Urban Design comments have requested for a Shadow Impact Study, which would be evaluated to determine the impacts on the nearby heritage resources.

The proponent should demonstrate that there would not be negative impacts of vibrations from construction activity on adjacent heritage resources. Depending on the proposal, a Construction Vibration Assessment may be required later as a condition of SPC.

Archaeology

"Stage 1-2 Archaeological Assessment of 3251 Riverside Drive East in part of Lot 101, Concession 1 Petite Cote, Township of Sandwich, Now City of Windsor, Essex County, Ontario", Dated May 30, 2021, Filed with MHSTCI Toronto Office on N/A, MHSTCI Project Information Form Number P1289-0035-2021, MHSTCI File Number 0013878" has been entered into the Ontario Public Register of Archaeological Reports without technical review.

The reports states that "All work met provincial standards and no archaeological sites were identified during the Stage 2 assessment. If construction plans change to incorporate new areas that were not subject to a Stage 2 field survey, these must be assessed prior to the initiation of construction. In keeping with legislative stipulations, all construction and demolition-related impacts (including, for example, machine travel, material storage and stockpiling, earth moving) must be restricted to the areas that were archaeologically assessed and cleared by the Ministry of Heritage, Sport, Tourism, and Culture Industries through acceptance of the assessment report into the provincial register. As no archaeological resources were found on the subject property, no further archaeological assessment of the property is required."

The applicant is to note the following archaeological precautions:

1. Should archaeological resources be found during grading, construction or soil removal activities, all work in the area must stop immediately and the City's Planning & Building Department, the City's Manager of Culture and Events, and the Ontario Ministry of Heritage, Sport, Tourism and Culture Industries must be notified and confirm satisfaction of any archaeological requirements before work can recommence.
2. In the event that human remains are encountered during grading, construction or soil removal activities, all work in that area must be stopped immediately and the site secured. The local police or coroner must be contacted to determine whether or not the skeletal remains are human, and whether the remains constitute a part of a crime scene. The Local police or coroner will then notify the Ontario Ministry of Heritage, Sport, Tourism and Culture Industries and the Registrar at the Ministry of Government and Consumer Services if needed, and notification and satisfactory confirmation be given by the Ministry of Heritage, Sport, Tourism and Culture Industries.

Contacts:

Windsor Planning & Building Department:

519-255-6543 x6179, ktang@citywindsor.ca, planningdept@citywindsor.ca

Windsor Manager of Culture and Events:

Michelle Staadegaard, (O) 519-253-2300x2726, (C) 519-816-0711, mstaadegaard@citywindsor.ca

Ontario Ministry of Heritage, Sport, Tourism and Culture Industries

Archaeology Programs Unit, 1-416-212-8886, Archaeology@ontario.ca

Windsor Police: 911

Ontario Ministry of Government & Consumer Services A/Registrar of Burial Sites, War Graves, Abandoned Cemeteries and Cemetery Closures, 1-416-212-7499, Crystal.Forrest@ontario.ca

Adam Coates – Urban Design

Many of the design decision, site layout and building design choices have taken into account urban design principals that align with the City of Windsor Official Plan. That being said, there are areas of concern that can still be addressed in order to ensure that

the proposed development is designed to function as an integral and complementary part of the areas existing development pattern.

Consideration should be given to:

- Building Height
 - The proposed building height may have an impact on the heritage properties located to the Northeast of the subject property, across Riverside Drive. A shadow study would illustrate the extent of potential impacts on the adjacent properties.
- Main Entrance to the Building
 - The controlled pedestrian entrance along Pratt Place could be designed to be clearly visible from the public R.O.W. This can include elements that intuitively signify an entrance. (eg. Change in building material, canopy or awning, increased glazing, change in exterior building plane, etc.)
- Appealing Street Facades
 - The Riverside elevation along the ground floor has opportunity to be more visually interesting. The use of parking along the ground floor, facing a scenic drive is not encouraged. Further actions can be pursued through the use of Landscape treatments to help soften the interface between the public R.O.W. and the development along Riverside Dr.
 - Riverside Drive is designated as a Civic Way and as such has an impact on the image of the City of Windsor. The building design can address the intersection of Riverside Drive and Pratt Place in a more sensitive manner, with regards to pedestrian scale and architectural proportions. In the absence of being able to utilize permanent landscape elements in the daylight corner, it is encouraged that these concerns be addressed in the building design.

Stefan Fediuk – Landscape Architect

Pursuant to the application for a zoning amendment (**Z-001/22**) to permit site specific RD3.3 zoning on the subject, including requiring relief from:

- The minimum lot area to permit a site area of 3,599m²;
- The maximum lot coverage from 35% to 65%;
- The maximum building height from 30.0m to 40.0m;
- The minimum landscaped open space from 35% to 15%; and
- Class 4 Area Designation (Noise and Vibration) to be applied to the building.

Please note the following comments:

Zoning Provisions for Parking Setback:

The Planning Justification Report (PJR) provided by the applicant cites several documents including the City of Windsor Zoning Bylaw 8600 (ZB).

It is recognized that the site is constrained by the intensity of the proposed development and the desire by the developer to provide additional buffer to the area residences immediate south of the site.

Therefore, the **Maximum Lot Coverage Allowed 35.0 % to a Proposed 65%** and the **Minimum Landscaped Open Space Required 35.0% of lot area to a Proposed 15%** seem to be in conflict with that statement.

The PJR suggests that the landscape open space is minor in nature however, it represents a 57% reduction in the total landscape open space. This is not to be considered a minor reduction as will severely limit the useable landscape open space as amenity areas and to combat climate change or address policies and objectives of the Official Plan as outlined in the Climate Change and Urban Design Sections of these comments.

Based on the parking calculations provided on drawing SPC-2, there are 90 parking spaces required. A total of 114 spaces have been provided within the parking structure for the residents of the building plus 17 additional spaces for visitors external for a total of 131 spaces. This represents a surplus of parking of 24 spaces. Section 3.4.9 of the PJR provided by the applicant, indicates that there is a great opportunity encourage use of public transit through this development. With that, the overall landscape deficiency could be significantly reduced by removing the external parking lot in its entirety and moving it into the parking structure. There would still be a surplus of 7 parking spaces and the impact on the immediately adjacent residences to the south would reduced.

Climate Change:

The Planning Justification Report (PJR) provided by the applicant cites several documents including the Provincial Policy Statement 2020 (PPS). PPS Policy 1.7.1 requires that development minimize negative impacts from changing climate and further emphasizes that climate change be mitigated through vegetation and Green Infrastructure in development (PPS 1.8.1 f)).

The PJR references energy conservation, air quality and climate change in section 3.4.9 by identifying the impact and interconnectivity to existing public amenities, public transportation opportunities and compact form. However, the report is silent on how the proposed site-specific reduction in landscape area will affect the resiliency of the development to the impacts of climate change.

Urban Design:

Both the Urban Design Brief and Planning Justification Report provided by the applicant cite several documents including the City of Windsor Official Plan (OP).

The UDB section 1.1.2 Vision for Area Character references that the OP by stating:

"City Council is committed to enhancing the enjoyment and image of Windsor, along with its people, through urban design principles. It is stated that a memorable, attractive and liveable city is one where people feel comfortable and are inspired by their surroundings. As such, development should use design to protect, maintain and improve the quality of life for present and future generations by integrating the principles of sustainability and place making."

While this reference includes a figure from a distant datum perspective, it does not address the need for human scale at the street level as found in Section 8.3 of the OP, relating to designing for pedestrian scale (8.3.1.2) and policies outlined in 8.3.2.2, by promoting:

- (a) the placement of continuous horizontal features on the first two storeys adjacent to the road;
- (b) the repetition of landscaping elements, such as trees, shrubs or paving modules;

Section 3.2 of the UDB also references proposed Public/Private Amenity Space Recommendations. These are only recommendations being made and not commitments.

The amenity spaces cited in the UDB is minor in nature for the number of residents which will be occupying the development.

- 1) The main amenity space at the NW corner is less than 3 standard parking spaces in size and will be subject to provision of a 4.5m daylight corner cut-off (referenced in section 3.3 of the UDB) that will further reduce the space and limit the vegetation that has been shown in the example photo on page 15. The primary entrance to the underground parking also restricts this space, and as the only open space, facing Riverside Drive will likely be the home of any Pylon or ground sign for the development. All these features would make this space more of a landscaped open space rather than an inviting amenity area.
- 2) The proposed open-air terraces are predominantly private with exception to the NE & SW corners and are all rendered in plan, elevation and renderings as hard surfaces. Overlaying the third floor with the parking deck floors below, it does not appear possible to provide any form of vegetation, and therefore these areas should be considered as building and not part of the landscape area calculations.

Section 3.3 of the UDB references the Landscaping Recommendations. The requested site-specific zoning amendment including relief for the minimum landscaped open space from 35% to 15%. This section of the UDB recognizes the importance of landscaping for aesthetics, shade, buffers and climate change resilience. The setback along Riverside drive has been proposed as 2.5ft (0.76m) and to include planters. Given that the wall of any planter is between 6-12" (0.15-0.30m), that would leave a very tight space that would only be capable of supporting smaller perennial plants, which would provide very little in the way of height or long-term climate change adaptation.

Additionally, Riverside Drive at this location is identified in the OP as part of the Greenway System Linkage (Section 4.2 and Schedule B) as well as a Civic Way (Section 8.7.2 and Schedule G). Both sections require that the development to protect against climate change and human health through sustainable and responsive environmental design. Exterior building designs along Civic Ways (OP 8.7.2.1) are to respect setbacks, scale and context, through reduction of visual dominance. The proposed parking structure has a singular uniform facade with little visual relief for pedestrians that stretches the entire length of the north frontage.

RECOMMENDATION:

It is recommended that an additional site-specific zoning provision be included, in conjunction with the other proposed amendments, specifying a minimum landscape setback from the Riverside Drive to a minimum of 3.0m to allow for the installation of narrower ornamental tree species with year-round interest, while not to exceed a mature height of 30ft (10m). Such species would include: Liquidambar, Columnar Ginkgo, Pyramidal Magnolia, Pyramidal Beech, Ornamental Pear, or Pyramidal Horbeam. This could be accommodated by moving the Garbage Room (8 ft wide) and Loading Space (11.92ft (3.6m)wide) from the southern end of the building internally to the main floor parking structure; allowing for the entire .

Site Plan Review:

The site will be subject to Site Plan Control and it is further recommended that the applicant provide a conceptual landscape plan to be considered as part of that application review process.

Recommendations 4, 5, and 6 of the UDB indicate an acknowledgement of the need for improvement to the landscape considerations for screening of the parking enclosure, pedestrian scale amenities and planting along Riverside Drive and Pratt Place.

Furthermore, to provide privacy for the abutting residences through a mixed planting of tall growing conifers and deciduous trees to act as visual buffers from the third floor southern amenity area and any residential units, that may overlook into the provide yards of the residences along Pratt Place and Belleview Avenue.

Parkland Dedication:

All requirements will be determined at the time a Site Plan application is received.

Transportation Planning – Rania Toufeili:

- Riverside Drive East is classified as a Scenic Drive according to the Official Plan with a required right-of-way width of 24 meters. The Riverside Drive Vista Environmental Assessment does not identify any required land acquisition at this property and therefore no conveyance is required.
- Belleview Avenue is classified as a Local Road according to the Official Plan with a required right-of-way width of 20 meters. The existing right-of-way width is 15.2 meters, however a land conveyance is not being requested at this time.
- Pratt Place is classified as a Local Road according to the Official Plan with a required right-of-way width of 20 meters. The existing right-of-way width is 16.8 meters, however a land conveyance is not being requested at this time.
- A 4.6 meter corner cut-off is required at the corner of Belleview Avenue and Riverside Drive East.
- A 4.6 meter corner cut-off is required at the corner of Pratt Place and Riverside Drive East.
- More details are required on the proposed emergency exit, how it will operate for use during emergencies and if it will be limited to an exit only.
- All accesses shall conform to the TAC Geometric Design Guide for Canadian Roads and the City of Windsor Standard Engineering Drawings.
- All exterior paths of travel must meet the requirements of the Accessibility for Ontarians with Disabilities Act (AODA).

Environmental Services

- no objections.

Pierfrancesco Ruggeri – Engineering & ROW

The subject lands are located at 3251 Riverside Drive East and 222 Belleview Avenue. The applicant is proposing to construct a 10-storey, 77 dwelling unit residential development with 129 parking spaces. The lands are currently designated Residential and Commercial by the City of Windsor Official Plan and zoned Residential District 2.2 (RD2.2) and Commercial District 1.7 (CD1.7) by Zoning By-Law 8600. The applicant is requesting an amendment to Zoning By-Law 8600 to rezone the site to Residential District 3.3 with Site-Specific provisions (RD3.3).

SEWERS – This site may be serviced by a combination of the 600mm vitrified clay combined sewer on Riverside Drive East, 825mm RCP sanitary sewer on Riverside Drive East, 675mm RCP storm sewer on Riverside Drive East, 200mm vitrified clay combined sewer on Belleview Avenue, 375mm vitrified clay combined sewer on Pratt Place and a 600mm RCP storm sewer on Pratt Place. A sewer servicing study is required to demonstrate that there is adequate capacity in the municipal sanitary and storm sewer networks. It must be demonstrated that no negative impacts will be realized by existing areas adjacent to the proposed development. This study must be completed in accordance with the City of Windsor Development Manual and the Windsor/Essex Region Stormwater Management Standards Manual. Approved site servicing drawing(s), lot grading plan(s), and a stormwater management plan are required.

The applicant is currently in discussions with Public Works regarding the relocation of the 900mm concrete pipe storm sewer that runs through the subject lands. A cost sharing agreement between the two parties will be required, subject to Council Approval for the City's share of the costs.

RIGHT-OF-WAY – The Official Plan classifies Riverside Drive East as a Scenic Parkway. An Environmental Assessment for the Riverside Drive Vista Improvement Project does not identify any conveyances required along the frontage of this property. Schedule X of the Official Plan classifies both Belleview Avenue and Pratt Place as Local Roads, requiring a right-of-way width of 20.0m. The current right-of-way widths are 15.20m and 16.80m respectively; requiring a land conveyance of 2.40m and 1.60m, however, a conveyance is not being requested at this time.

A 4.6 meter corner cut-off is required at the north-east corner of Riverside Drive East & Pratt Place, and also at the north-west corner of Riverside Drive East and Belleview Avenue.

Pratt Place is deficient of Curb/gutter. As a condition of approval, the applicant will be construct curb and gutter across the entire Pratt Place frontage to the satisfaction of the City Engineer. The proposed vehicle access to the subject lands is from the abutting alley to the south. The applicant will be required to pave and provide drainage to the entire alley abutting the subject lands. If the surrounding sidewalks or boulevard in the right-of-

way are damaged during construction, then they must be restored to the satisfaction of the City Engineer.

Driveways are to be constructed as per AS-221 or AS-222, complete with straight flares and no raised curbs within the right-of-way. Redundant curb cuts and sidewalks shall be removed and restored in accordance with City Standards to the satisfaction of the City Engineer. More details are required on the proposed emergency exit, how it will operate for use during emergencies and if it will be limited to an exit only. A Street Opening permit will be required for any work in the right-of-way.

In summary we have no objection to the proposed rezoning, subject to the following requirements (requirements will be enforced at the time of Site Plan Control):

Site Plan Control Agreement - The applicant enters into an agreement with the City of Windsor for all requirements under the General Provisions of the Site Plan Control Agreement for the Engineering Department.

Alley Paving – The owner shall agree to drain and pave at the owner's entire expense, the alley abutting the subject lands. The minimum acceptable cross-section will be 230 mm Granular "A" and 75 mm surface course asphalt in accordance with Standard City of Windsor Specifications, Selected Granular Base Course (S4) and Hot Mix, Hot Laid Asphaltic Concrete (S-10). The geometrics of the pavement shall comply with City of Windsor Standard Drawing AS-201. All work shall be to the satisfaction of the City Engineer.

Corner Cut-off – The owner agrees prior to the issuance of a construction permit, to gratuitously convey a 4.6m x 4.6m (15'x15') corner cut-off at the north-east intersection of Riverside Drive East and Pratt Place and also at the north-west intersection of Riverside Drive East and Belleview Avenue in accordance with City of Windsor Standard Drawing AS-230.

Curbs and Gutters – The Owner further agrees, at the discretion of the City Engineer, to construct at their own expense and according to City of Windsor Standard Specifications, a concrete curb and gutter along the entire Pratt Street frontage of the subject lands. All work to be to the satisfaction of the Corporation's City Engineer.

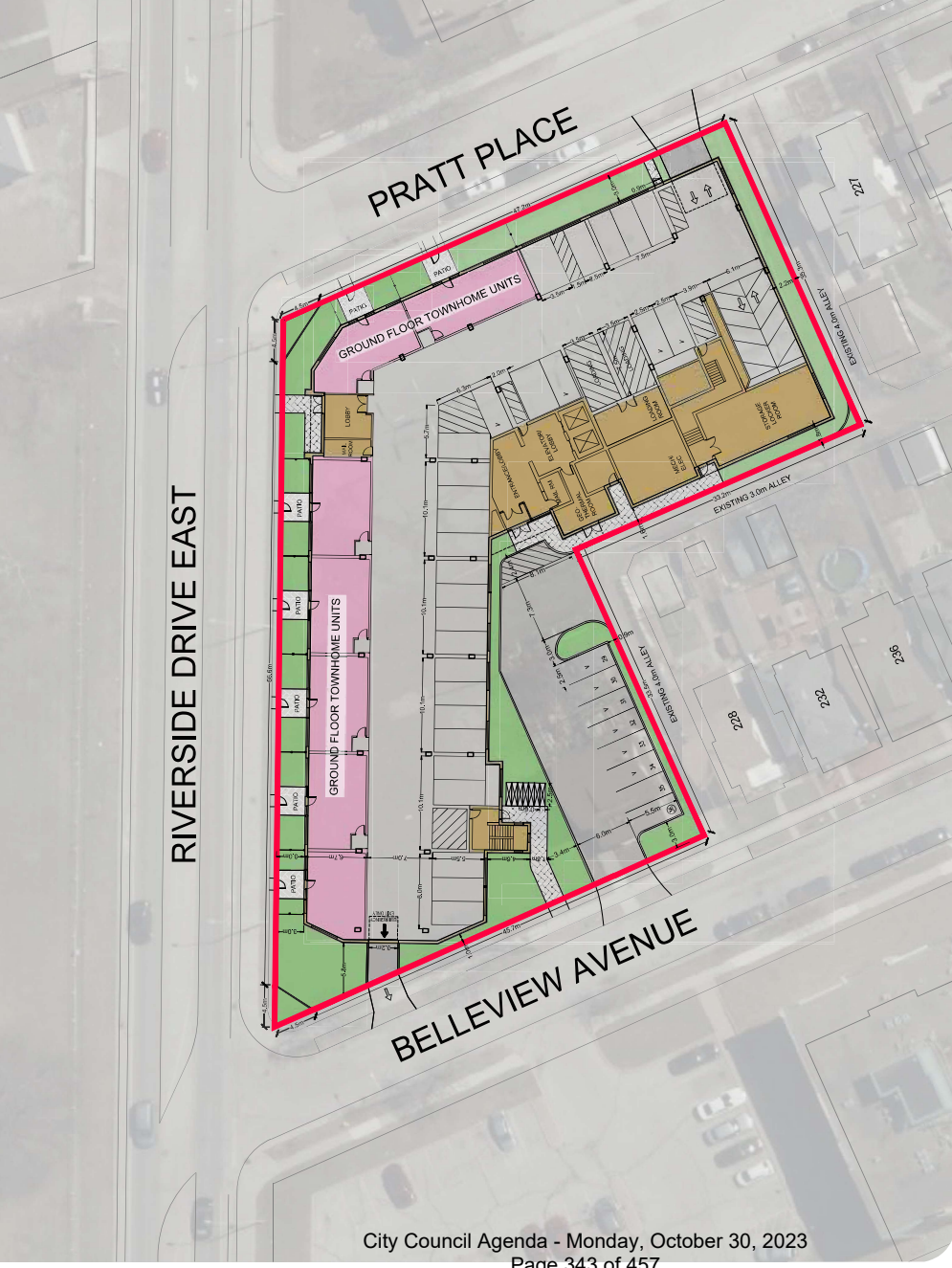
Redundant Curb Cuts – The owner agrees to remove and reinstate any redundant curb cuts on Riverside Drive East and Belleview Avenue with full height curb and properly restore the area to the satisfaction of the City Engineer.

Servicing Study – The owner agrees, at its own expense, to retain a Consulting Engineer to provide a detailed servicing study report on the impact of the increased flow to the existing municipal sewer system, satisfactory in content to the City Engineer and prior to the issuance of a construction permit. The study shall analyze both the sanitary and stormwater flow from the proposed development. The following information shall be provided in the stormwater study:

1. Determine the drainage area for the outlet,

2. Develop a hydrologic model to analyze the drainage area for both the minor and major storm event to determine a conservative release rate,
3. Provide hydraulic grade lines for both storm events as well as a grading plan depicting how the minor and major storm events will be contained.

SITE DATA MATRIX		
	REQUIRED	PROVIDED
1	PROJECT DESCRIPTION	N/A
2	ZONING DESIGNATION	RD3.3 Site Specific*
3	LAND USE	N/A
4	MAJOR OCCUPANCY(S)	N/A
5	MINIMUM SITE AREA	4,082m ²
6	BUILDING AREA (GROUND FLOOR FOOTPRINT)	N/A
7	PROPOSED ATTACHED TOWNHOME STYLE UNITS	-
8	PROPOSED APARTMENT STYLE UNITS	-
9	PROPOSED TOTAL NUMBER OF UNITS	-
10	LOT COVERAGE	35%
11	MINIMUM LOT WIDTH	45m
12	MAXIMUM BUILDING HEIGHT	30.0m
13	MINIMUM FRONT YARD DEPTH	3.0m
14	MINIMUM REAR YARD DEPTH	2.0m
15	MINIMUM EXTERIOR SIDE YARD DEPTH - BELLEVIEW AVENUE	1.0m
16	MINIMUM EXTERIOR SIDE YARD DEPTH - PRATT PLACE	3.0m
17	REQUIRED SPACES - STANDARD	83
18	PROPOSED UNCOVERED PARKING	-
19	PROPOSED FIRST FLOOR PARKING	-
20	PROPOSED SECOND FLOOR PARKING	-
21	PROPOSED THIRD FLOOR PARKING	-
22	REQUIRED SPACES - ACCESSIBLE	6 (3 TYPE A AND 3 TYPE B)
23	REQUIRED SPACES - VISITOR (15%)	16
24	PARKING SPACES - TOTAL	105
25	MINIMUM REQUIRED BICYCLE SPACES	8
26	LOADING SPACES	2
27	LANDSCAPED AREA - SOFT	N/A
28	LANDSCAPED AREA - HARD	615,766m ²
29	LANDSCAPED AREA - TOTAL	158,009m ²
30	DWELLING UNITS PER HECTARE	240*



RIVERSIDE HORIZONS INC.
3251 RIVERSIDE DRIVE EAST

CONCEPTUAL DEVELOPMENT PLAN - GROUND FLOOR LAYOUT
FIGURE 4.0

- PROPOSED SUBJECT SITE (0.35 ha, 0.86 ac)
- PROPOSED MULTI-UNIT BUILDING
- PROPOSED TOWNHOME STYLE UNITS
- PROPOSED PAVEMENT
- PROPOSED SIDEWALK
- PROPOSED LANDSCAPED AREA

MAP/DRAWING INFORMATION
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SOURCE: THE COUNTY OF ESSEX INTERACTIVE MAPPING (2020)

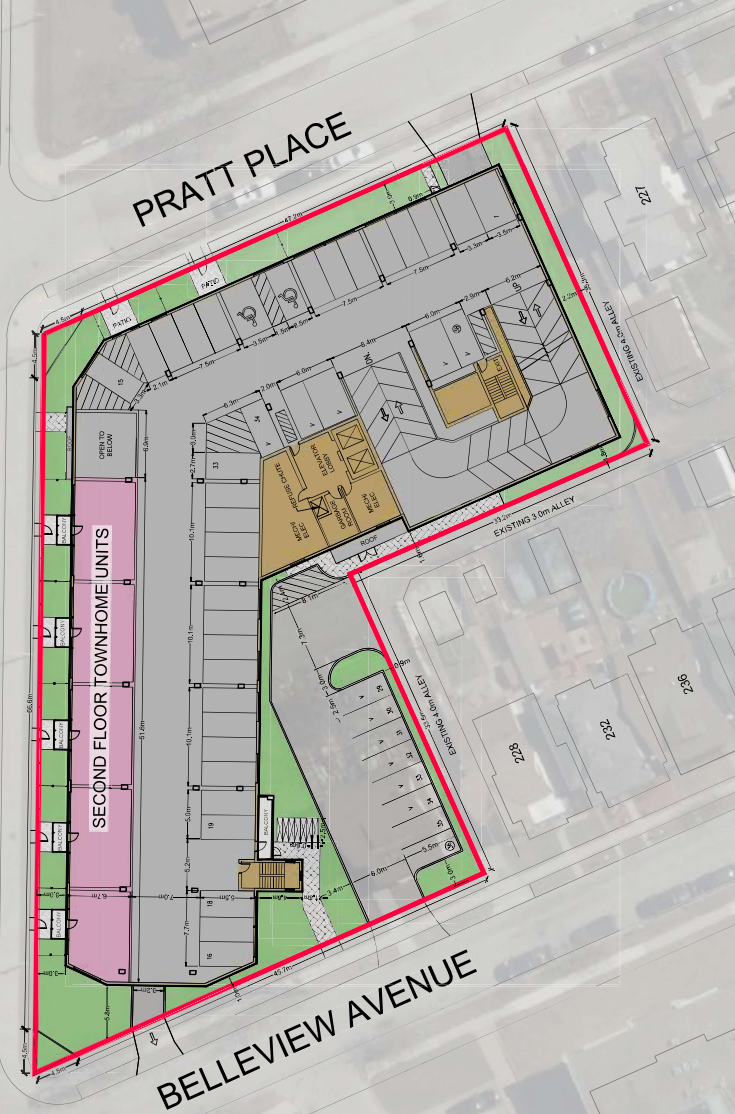
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PROJECT: 20-3740
STATUS: DRAFT
DATE: 01/10/2023

DILLON CONSULTING

SITE DATA MATRIX		
	REQUIRED	PROVIDED
1	PROJECT DESCRIPTION	N/A
2	ZONING DESIGNATION	RD3.3 Site Specific*
3	LAND USE	N/A
4	MAJOR OCCUPANCY(S)	N/A
5	MINIMUM SITE AREA	4,082m ²
6	BUILDING AREA (GROUND FLOOR FOOTPRINT)	N/A
7	PROPOSED ATTACHED TOWNHOME STYLE UNITS	-
8	PROPOSED APARTMENT STYLE UNITS	-
9	PROPOSED TOTAL NUMBER OF UNITS	84
10	LOT COVERAGE	35%
11	MINIMUM LOT WIDTH	45m
12	MAXIMUM BUILDING HEIGHT	30.0m
13	MINIMUM FRONT YARD DEPTH	-
14	MINIMUM REAR YARD DEPTH	3.0m
15	MINIMUM EXTERIOR SIDE YARD DEPTH - BELLEVIEW AVENUE	-
16	MINIMUM EXTERIOR SIDE YARD DEPTH - PRATT PLACE	-
17	REQUIRED SPACES - STANDARD	83
18	PROPOSED UNCOVERED PARKING	-
19	PROPOSED FIRST FLOOR PARKING	-
20	PROPOSED SECOND FLOOR PARKING	-
21	PROPOSED THIRD FLOOR PARKING	-
22	REQUIRED SPACES - ACCESSIBLE	6 (3 TYPE A AND 3 TYPE B)
23	REQUIRED SPACES - VISITOR (15%)	16
24	PARKING SPACES - TOTAL	105
25	MINIMUM REQUIRED BICYCLE SPACES	8
26	LOADING SPACES	2
27	LANDSCAPED AREA - SOFT	N/A
28	LANDSCAPED AREA - HARD	N/A
29	LANDSCAPED AREA - TOTAL	35%
30	DWELLING UNITS PER HECTARE	225
		240*

RIVERSIDE DRIVE EAST



RIVERSIDE HORIZONS INC.
3251 RIVERSIDE DRIVE EAST

CONCEPTUAL DEVELOPMENT PLAN - SECOND FLOOR PARKING LAYOUT
FIGURE 4.1

PROJECT: 20-3740
STATUS: DRAFT
DATE: 01/10/2023

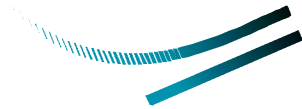
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MAP/DRAWING INFORMATION
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CREATED BY: A/C
CHECKED BY: M/M
DESIGNED BY: A/C

SOURCE: THE COUNTY OF ESSEX INTERACTIVE MAPPING (2020)

LEGEND:

- SUBJECT SITE (0.35 ha/0.86 ac)
- PROPOSED MULTI-UNIT BUILDING
- PROPOSED TOWNHOME STYLE UNITS
- PROPOSED PAVEMENT
- PROPOSED SIDEWALK
- PROPOSED LANDSCAPED AREA



DILLON
CONSULTING

Riverside Horizons Inc.

Official Plan and Zoning By-Law Amendments

Urban Design Brief
3251 Riverside Drive East

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1.0

Background

1.1

Purpose

Dillon Consulting Limited (Dillon) has been retained by Riverside Horizons Inc. (the Developer), to provide an Urban Design Brief to assist in obtaining the necessary planning approvals associated with the proposed 12 storey, 84 unit multiple dwelling residential development with associated parking areas, including three (3) storeys of parking garage located at 3251 Riverside Drive East and 222 Belleview Avenue (the Subject Site) in the City of Windsor. This Urban Design Brief follows the requirements as specified in the Pre-Submission Letter dated February 2, 2021, and provides opportunities and recommendations for the Developer to consider which will help align the proposed development with the goals, objectives and policies found in the City of Windsor Official Plan.

1.1

Description of Site

1.1.1

Existing Area Character

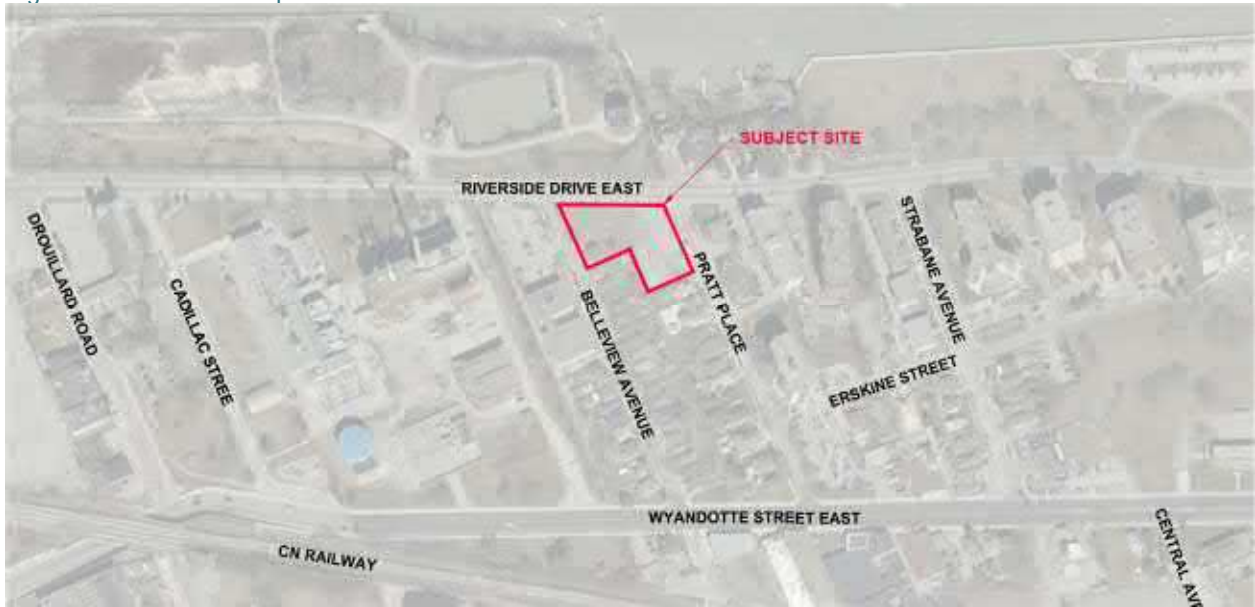
The subject site is located in the East Windsor Planning District on the south side of Riverside Drive East, east of Belleview Avenue and west of Pratt Place. The northern, eastern, and western property lines abut three different streets, while the south lot line is adjacent to east-west alleys with low density dwellings, multi-unit residential and long term care facilities to the east and west. The underutilized site is currently a paved parking area and a vacant single detached dwelling (222 Belleview Avenue), to be demolished. The undeveloped site is 0.35 hectares (0.87 acres), with 75.6m (248ft.) of frontage on Riverside Drive East, 50.2m (164.7ft.) along Belleview Avenue and 51.7m (169.6ft.) along Pratt Place.

The surrounding area is primarily residential, with low rise development consisting of single detached homes, converted dwellings, and a three storey apartment building, as well as high rise development in the form of apartment buildings. In close proximity to the site are some industrial uses, namely the Ford Powerhouse and East Windsor Cogeneration Plant, and open spaces, such as Alexander Park. Beyond the immediate area of the site, there is a range of commercial, residential, and industrial uses.

Pratt Place is a local neighbourhood street cross section with one lane of traffic travelling in either direction. Belleview Avenue is categorized as a one-way local neighbourhood street cross section with only one lane of traffic travelling north and a parking lane. Riverside Drive East is designated as a Scenic Drive in the Official Plan (OP) and has lane of traffic travelling in both directions with a wide painted median. There is no vegetation on the site with three (3) street trees along the western edge of the site.

Figure 1 – Location Map identifies the subject site.

Figure 1 - Location Map



There are sidewalks running along all of the adjacent streets (Riverside Drive, Bellevue Avenue, and Pratt Place), which provide connections into the larger pedestrian sidewalk network running throughout Riverside and the rest of the city. As well, there is a painted bike lane along both sides of Riverside Drive East that provides cyclists with connections throughout the city.

1.1.2 Vision for Area Character

The site, being located within the City of Windsor, is subject to the Urban Design policies detailed in the Official Plan. City Council is committed to enhancing the enjoyment and image of Windsor, along with its people, through urban design principles. It is stated that a memorable, attractive and liveable city is one where people feel comfortable and are inspired by their surroundings. As such, development should use design to protect, maintain and improve the quality of life for present and future generations by integrating the principles of sustainability and place making. As depicted in Figure 2 – Streetscape Surrounding Subject Site, the height of the proposed development is keeping in context with the surrounding area. Along Riverside Drive, there are a number of high rise buildings in close proximity and the height of the proposed development, 45m (12 storeys) is in keeping with what currently exists along much of Windsor's waterfront.

The design aesthetic of the proposed residential development is drawn from The Ford Powerhouse, namely the red brick, fenestration, and architectural details used on the exterior. The intent is to incorporate a similar brick style which is complimentary to The Ford Powerhouse, a neighbouring heritage building to the west.

1.2 Proposed Residential Development

The proposed development includes the construction of one, 12 storey residential building, with 77 apartment style units, five (5) two (2)-storey townhome style units on the ground-floor fronting onto Riverside Drive East, and two (2) one (1)-storey townhome style units on Pratt Place for a total of 84 units. The proposed building consists of three (3) storeys of parking (Floor 1 (Ground Level) to Floor 3) behind the 2 storey townhome units with nine (9) storeys of residential above. On the ground floor of the multiple dwelling residential building, seven (7) townhome style units are proposed along Riverside Drive East and Pratt Place.

The multiple unit residential building will have an entrance from Riverside Drive East with the primary entrance located internal to the site, from the surface parking lot on the southwest portion of the subject site. Secondary entrances to the overall building are to be located along Riverside Drive East and Pratt Place. The main entrance for each ground floor unit is accessed from inside the parking garage, with a patio and amenity space along the right-of-way. The proposed development requires 83 standard parking spaces, 6 accessible parking spaces, and 16 visitor parking spaces for a total of 105 spaces being required with 135 being provided between a three (3) storey parking garage and a small surface parking lot.

There are two (2) proposed site access driveways, one from Pratt Place and one from Belleview Avenue. An additional emergency exit from the parking garage is also proposed out to Belleview Avenue.

The lands are designated Residential in the City of Windsor Official Plan (OP). The units within the building primarily face north, taking advantage of the Detroit River views to the north, and towards the east. The building proposes limited units with south facing windows, all of which limits the habitable windows and balconies compared with the north, west, and east elevations. In order to ensure the privacy of the neighbouring properties, a 6.2 metre minimum separation (setback from the building to the south property line of the alley) between the proposed parking garage and the existing residential dwellings adjacent to the subject site is proposed. The south elevation does not contain any balconies, which protects the privacy for adjacent properties south of the subject site. There is a shared terrace proposed on the fourth floor of the building that includes a portion of the south elevation. The terrace on this portion of the south elevation is set back well over 10.0 metres from the adjacent properties to the south of the subject site, and will be screened with planting and a noise barrier wall.

Seven (7) townhome style units are proposed on the ground floor of the development along Riverside Drive East and Pratt Place. Along Riverside Drive East, the five (5) townhome style units are two (2)-storeys with two (2), one (1)-storey townhome style units proposed along Pratt Place. Each of the townhome style units have a primary entrance from within the parking garage and a secondary entrance from the sidewalks along Riverside Drive East and Pratt Place. Amenity space and patios are provided for each ground floor unit fronting towards the right-of-way. The townhome style units provide for a pedestrian-scale development adjacent to the public realm. In addition, the residential tower is set back above the

third storey, further offsetting the 77 apartment-style units from the existing low rise dwellings surrounding the subject site. This provides an appropriate distance so neighbours can maintain a sense of privacy on their properties. To further ensure the privacy of neighbouring properties, limited habitable windows are located on the south elevation of the proposed development.

The subject site fronts onto Riverside Drive East, which is classified as a Scenic Drive in the City of Windsor Official Plan (OP) Schedule F – Roads and Bikeways. The proposed development provides for parking on-site and makes pedestrian movement safer and more convenient by maintaining the existing sidewalk along the south side of the right-of-way. On Schedule G – Civic Image in the OP, Riverside Drive West is identified as a Civic Way. Civic Ways are intended to promote an attractive image of Windsor and maintain a sense of welcome and arrival for travelers. The existing road pattern in this neighbourhood will be preserved by this proposed development to enhance orientation and maintain the image of Windsor. Landscaping is provided along the adjacent rights-of-ways to visually reinforce the location of the subject site on a Civic Way.

The proposed development enhances the public rights-of-way through the use of design elements such as landscaping to establish a pedestrian-scaled public realm. As shown on Figure 2 – Streetscape Surrounding Subject Site, the proposed development will enhance the prominent vista of Windsor's waterfront and skyline.

Primary vehicular access will be from Pratt Place, via a 2-lane driveway into the enclosed parking garage (one entry lane, and one exit lane to allow for turning in either direction.) The majority of the parking is enclosed within the building, which includes three storeys of parking garage (First Floor, Second Floor, and Third Floor). Under the current proposal, a total of 127 vehicular parking spaces are to be provided within the parking garage. An additional 8 space surface parking lot will be constructed on the southwest side of the building, with access to Belleview Avenue and the open alley to the south for a total of 135 parking spaces on site. Refer to Figure 3 – Conceptual Site Plan – First Floor, Figure 4 – Conceptual Site Plan – Second Floor, and Figure 5 – Conceptual Site Plan – Third Floor.

Figure 3 - Conceptual Site Plan – First Floor



Figure 4 - Conceptual Site Plan – Second Floor



Figure 5 - Conceptual Site Plan – Third Floor



The proposed residential building will be located 1.0 metre from the western property line and will accommodate landscaping on the northwestern corner of the lot, including a 4.5 metre by 4.5 metre daylight corner as required by the City of Windsor. The building is set back 3.0 metres from the northern and eastern property lines abutting Riverside Drive East and Pratt Place with a standard 4.5 metre by 4.5 metre daylight corner being provided at the intersection of the two streets. The building has been configured at this corner to allow for the daylight corner and appropriate sight lines on the three parking garage levels with a cantilever design for the upper residential floors. These setbacks will accommodate landscaping that will enhance the aesthetics of the development and the pedestrian realm along the public sidewalk. Along the southern property line abutting neighbouring homes, a 2.0 metre minimum setback is proposed. The proposed location of the building from the southern lot line ranges from 2.2 metres at ground level up to over 10.0 metres on the upper floors, with the additional 4.0 metres of the existing alley, as a means to enhance the privacy for residents in the low rise residential dwellings by providing an appropriate distance between all residential dwellings.

There is direct access to the residential units via the enclosed parking garage and from outside the building on the north and west sides. The primary building entrance is located on the west side of the building

adjacent to the surface parking area fronting onto Belleview Avenue, with secondary building entrances accessible from Riverside Drive East, Belleview Avenue, and from within the enclosed parking garage. Refer to **Figure 6 – North Elevation**, **Figure 7 – West Elevation (Belleview Avenue)**, Figure 8 – Inner South Elevation, **Figure 9 – Inner West Elevation**, **Figure 10 – South Elevation**, and **Figure 11 – East Elevation (Pratt Street)**.

Figure 6 - North Elevation



Figure 7 - West Elevation (Belleview Avenue)



Figure 8 - Inner South Elevation

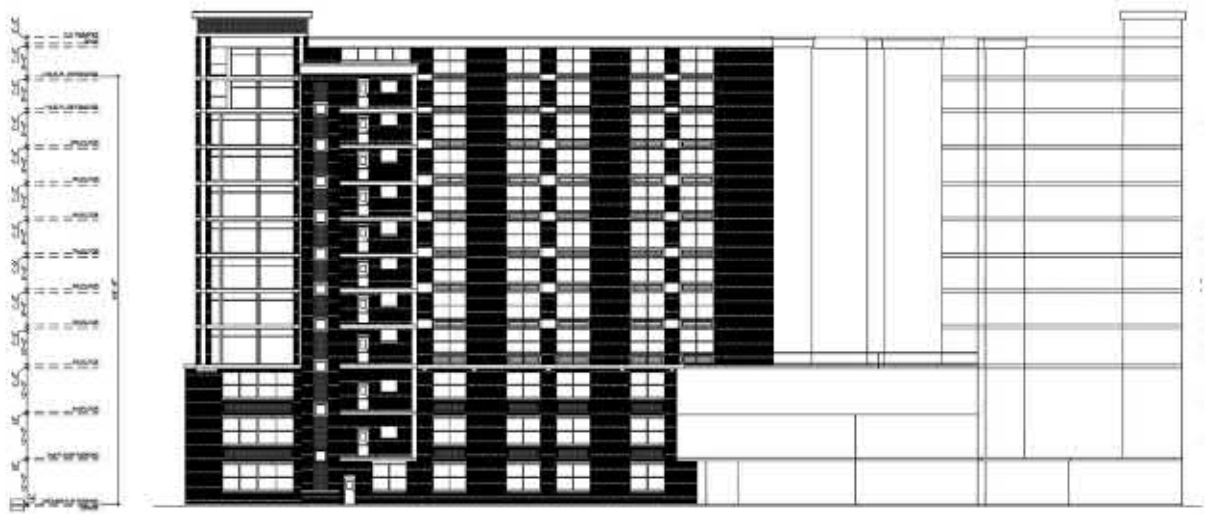


Figure 9 - Inner West Elevation



Figure 10 - South Elevation



Figure 11 - East Elevation (Pratt Street)



Private unit amenity spaces, in the form of balconies, facing Riverside Drive East to the north and Pratt Place to the east will be provided. In addition a clubhouse, internal to the building, as well as common terraces for the residents on the north side are provided on the fourth floor (first residential floor) of the building as additional amenity areas. There is a large fourth floor terrace proposed for provide private communal space on the southeast side of the building near Pratt Place. The fourth floor terrace will include a community garden area, adding to the amenity space available for future residents.

The floor plan for the first residential floor is shown in **Figure 12 – Floor Plan for Residential Floor 4**. Refer to **Figure 13 – Floor Plan for Residential Floors 5 to 9** for the remaining floor plans of the proposed residential development.

Figure 12 - Floor Plan for Residential Floor 4

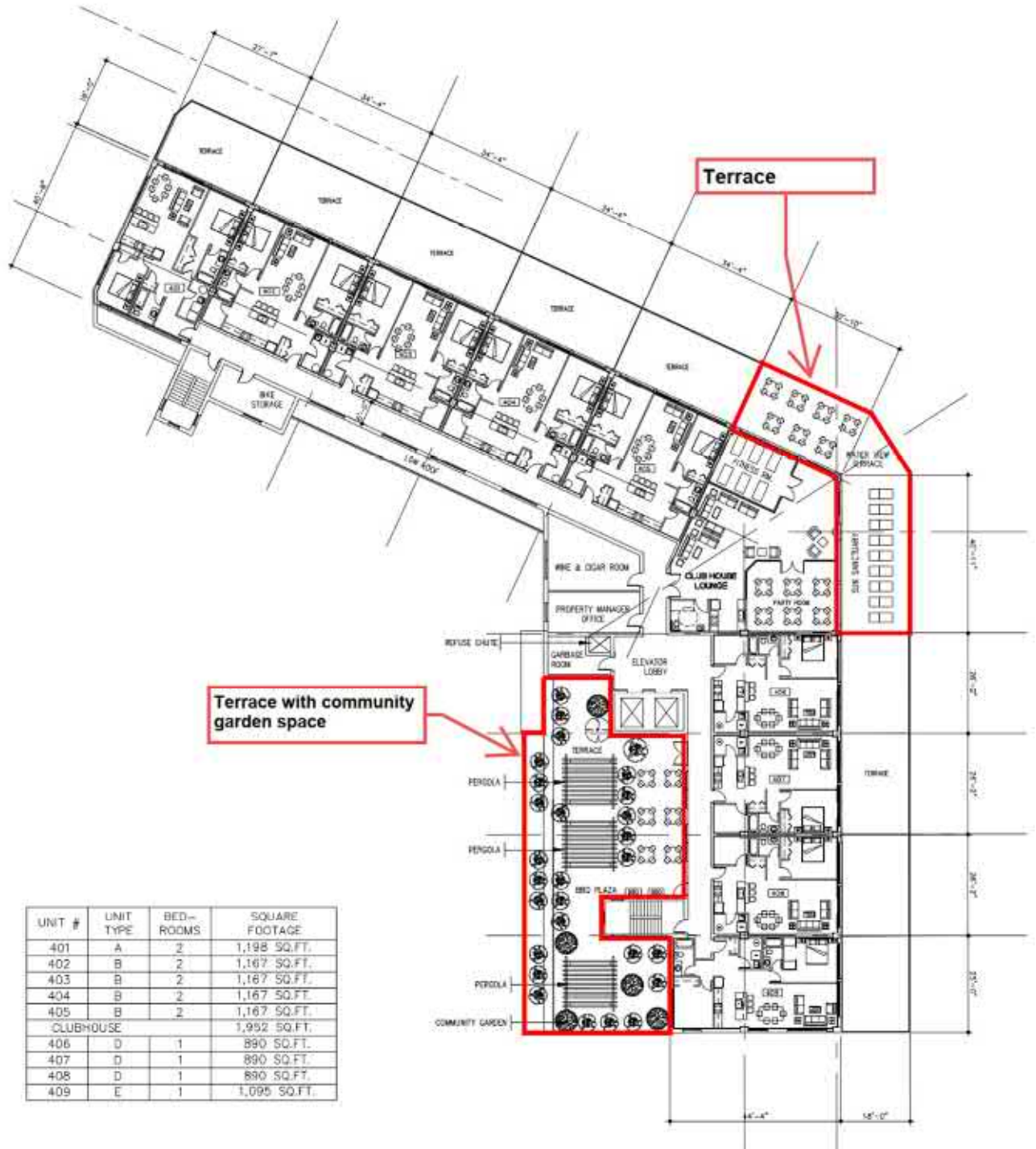
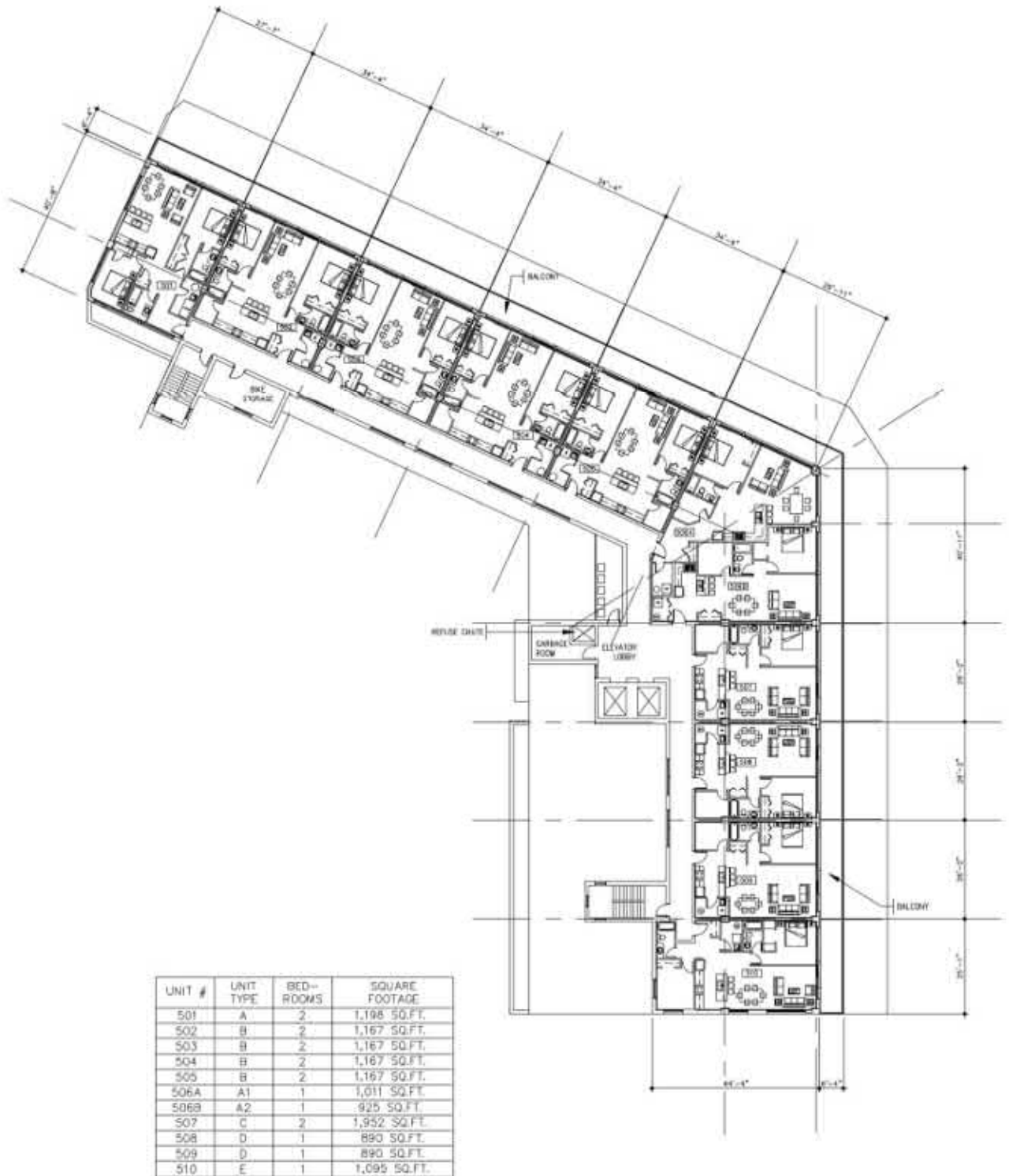


Figure 13 - Floor Plan for Residential Floors 5 to 9



To soften the façade along Riverside Drive, there is a 3.0 metre setback that includes an area for landscaping. Despite the limited landscaping space provided on the ground floor of the subject site, there is a patio proposed along the north elevation of the building and large 291.10 m² (3,133.43 sq. ft.) terrace is proposed on the fourth floor facing south and west, which will provide space for a community garden area, a patio, and seating area to be provided. In addition, balconies will provide amenity space for each dwelling unit. The landscaping buffer provided along the development's frontage and the garden space provided on the terrace will help augment the decreased landscaping on the ground floor.

2.0 Urban Design Evaluation

To determine appropriateness of the proposed development, an evaluation of the site context, building massing, and site/landscape treatment has been undertaken. The planning rationale for the project addresses the various policies and criteria in the Official Plan and identifies and evaluates the potential issues associated with the proposed development. The urban design evaluation assesses how well the proposal integrates with the context and whether the design is appropriate within its setting.

2.1 Context and Orientation

The City of Windsor Official Plan and the Provincial Policy Statement (PPS) promote the development of 'Strong, Healthy Communities' which include a provision of a variety of types and densities of housing forms. This diversity in the residential housing stock allows for a wider range of opportunities for residents of Windsor to live and to allow them to age in place. Both documents also outline the importance of providing pedestrian scale neighbourhoods. This particular site is currently vacant and sits within the East Windsor Planning District.

The subject site, which is located along Riverside Drive East between Bellevue Avenue and Pratt Place, is nestled within an existing residential area in close proximity to the Detroit River. Adjacent land uses are mainly residential, comprised of high-rise apartment buildings, a low-rise apartment building, single-detached dwellings, and converted dwellings, with an industrial use located on the opposite side of Riverside Drive East. The height, massing and scale of the existing high-rise residential developments provide a context for the proposed development. See Figure 2 – Streetscape Surrounding Subject Site for a comparison between existing high-rise developments in the area and the proposed development. Connected townhome style units are proposed along the Riverside Drive East and Pratt Place frontages to enhance the public-realm with a pedestrian-scaled design. Other nearby land uses include additional residential and industrial developments, as well as commercial and institutional uses within walking distance from the site. A large open space area along the Detroit River is in close proximity to the site, along with more open space areas to the east.

Site Specific Official Plan and Zoning By-Law Amendments are being sought for the subject site to permit the development. The Residential District 3.3. Zone will be used as the base for the amendment. The proposed development is in keeping with the neighbouring heights and densities found along Riverside Drive. Many of the multi-unit residential dwellings along this stretch of Riverside Drive have similar setbacks and heights. The townhome style units along Riverside Drive East and Pratt Place provide for a transition between the high-rise multiple dwelling building and the low-rise single and semi-detached dwellings along Pratt Place. All of which maintains the existing context of the surrounding neighbourhood.

2.2 Public Realm and Private Amenity Area

The proposed development has provided outdoor amenity spaces for residents to utilize. Every unit has access to a private balcony or a patio (ground floor townhomes). There is also a common terrace proposed on the fourth floor along the north portion of the building as part of the clubhouse, and a larger terrace on the southwest portion of the building on the fourth floor. The southwest amenity area is 291.10 m² (3,133.43 sq. ft.) and includes a community garden, patio space, and a seating area.

Residents will also have access to a landscaped outdoor area which is located at ground level at the northwest corner of the site at the corner of Riverside Drive East and Belleview Avenue. Alexander Park is located approximately 700 meters from the subject site, which provides additional outdoor recreational space for residents. Seating, a shade structure, tree planting and decorative gardens will help to improve the aesthetic appearances of the exterior of the building. There are no apartment-style dwelling units proposed for the ground floor up to the third storey as these floors are proposed as a covered parking area for future residents. The ground level townhome style units are setback 3.0 metres along Riverside Drive East and Pratt Place with fencing for each unit's private patio space. As a result, providing visual and physical separation for any private amenity spaces on the ground floor level is not a concern.

There is a proposed sidewalk provided from the existing pedestrian network along Belleview Avenue, connecting to the west entrance of the proposed residential building. The front entrance along Riverside Drive East is proposed with a connection from the existing sidewalk. In addition, pedestrian sidewalks are provided from the existing sidewalk network along Pratt Place leading to the entrances on the east side of the building. The garbage and recycling room is located on the ground floor in the rear of the building interior to the site to minimize exposure to the residents. A garbage pickup area is proposed on the southwest side of the building, fronting directly towards the existing alley that runs along the southern property line of the subject site, with access provided between Pratt Place, Belleview Avenue, and south to Wyandotte Street East. Waste and recycling receptacles will be wheeled out to the drop off area outside of the garbage room on pick up day.

Currently there are little to no street tree plantings along the development site. The proposed development looks to provide vegetation to soften the interface of the façade along Riverside Drive and the public realm. Vegetation and plantings can enhance the pedestrian realm along the public right of way and to improve microclimatic conditions including protection from wind, sun etc. Due to the required site visibility triangles on the northeast and northwest corners of the subject property, vegetation cannot be planted within these areas.

2.3 Built Form and Massing

The residential apartment building is twelve (12) storeys in height, which is similar to the context of the surrounding area to the east, which is comprised of other high-rise residential buildings. Within the Residential land use designation, high profile developments are not currently permitted. Given that the

proposed development is twelve (12) storeys in height, a site specific Official Plan Amendment is required as per Section 6.3 of the City's Official Plan to permit High Profile buildings. The proposed building is expected to be 45 metres in height, and will be located in an area of Windsor with several existing multi-unit residential buildings that range in height between 20 and 36 metres. A Shadow Study, prepared by Dillon Consulting, demonstrates that the proposed development will have no major impacts on the surrounding properties.

The orientation of the building and the restricting of the residential units facing the south side of the building is intended to minimize the effect on the neighbouring low rise on Pratt Place and Belleview Avenue, reducing any concerns regarding privacy. Vehicular access to the enclosed parking garage is from Pratt Place and is located towards the rear of the development, away from Riverside Drive East, which aims to provide an improved pedestrian realm. The townhome style units screen the first two floors of the enclosed parking area from Riverside Drive East and a single floor along Pratt Place further contributing to a more pedestrian friendly streetscape. On the third floor, above the townhome style units, the enclosed parking area provides a visual transition to the apartment style units setback above.

There are pedestrian sidewalks that provide access to the enclosed parking area, one from Riverside Drive East near the intersection with Pratt Place and the other from the main driveway off of Pratt Place as well as to each of the townhome style units. The main pedestrian access is located at the corner of Riverside Drive East and Pratt Place, which has a unique setback design that leads into the parking garage. A sidewalk connection to the parking area from Belleview Avenue to the secondary building entrance will be provided as well. An emergency access for vehicles from the parking garage to Belleview Avenue is located on the west elevation at the corner of Riverside Drive East and Belleview Avenue.

The materials proposed for the façades are complementary to the existing neighbourhood which consists of more neutral brick and mortar building faces. Design inspiration for this development is drawn primarily from the architectural style of The Ford Powerhouse, located to the west of the site. The community surrounding the subject site has a long history influenced significantly by the Ford Motor Company. In fact, this part of the City of Windsor was formerly part the municipality of Ford City. Ford City emerged in the early 20th century due to the success of the Ford Motor Company and attracted workers and residents to the area. Given the nature of Ford City, the area contained many buildings constructed in a style specific to early 20th century industrial design. A hallmark of this design style is the use of red brick. The primary design inspiration for the proposed development is The Ford Powerhouse. It was designed by renowned architect Albert Kahn and constructed in 1923. The Ford Powerhouse, along with the former Our Lady of the Rosary Church (Waters Edge Event Centre), are two of the remaining structures in the area from the early 20th century and are identified on the City of Windsor Heritage Properties Inventory.

The proposed development incorporates masonry into the built form through the use of red brick, which has a long-standing history in the community. Similar to The Ford Powerhouse, the red brick is visually divided by the use of concrete details throughout the exterior of the proposed development. The

proposed development will also incorporate black mullions into the design, which is similar to the fenestration found at The Ford Powerhouse and other long-standing structures in the community. The details found on the façades of The Ford Powerhouse can be seen in Figure 14 – Exterior Façade of the Ford Powerhouse Building.

The mix of bricks and glazing proposed for the townhome style units help to break up the look of the façade and the neutral colours are consistent with the surrounding community. The façade of the upper floors have some variation in the materials and the setback of the building to break up the look of a continuous wall. The south walls, and some of the west walls, are comprised largely of brick with some windows, limited balconies, and masonry detailing to provide variation.

Figure 14 - Exterior Façade of the Ford Powerhouse Building



3.0

Recommendations

Through this urban design evaluation, a number of recommendations have been developed to help align the proposed development with the vision for the area. The recommendations pertain to the building materiality and massing, public connections, landscape and visual aspects of the proposed development. The aim should be to lessen the impact of the higher density development on the adjacent lower density neighbourhood as well as complement and enhance the public realm right of way along Riverside Drive East, Belleview Avenue, and Pratt Place. The recommendations are outlined below.

3.1

Building Massing and Materiality Recommendations

The main entrances should be clearly defined to avoid confusion, especially for visitors. The Developer should consider adding more visually prominent and welcoming features to the entrances on both sides of the building. Elements could include a change in material such as glazing/glass or architectural elements to provide distinction.

The proposed development incorporates prominent features to the entrances of the building. The main pedestrian entrance is located at the corner of Riverside Drive East and Pratt Place. This entrance includes glazing details and is proposed in a location that clearly defines the entrance for future users. Above the ground level units, there is a change in materials that visually distinguishes the townhome style units from the upper floors with apartment style units.



Example of glass/glazing entrance



Example of elements used to indicate an entrance

To better integrate into the surrounding area and to create a more aesthetically interesting development, the Developer should consider interventions pertaining to the massing of the building to be in harmony with the neighbouring development. The development, as it stands, does not appear to have a major impact on the adjacent properties and fits with the character of the Riverside Drive East streetscape. The introduction of ground level units along Riverside Drive East and Pratt Place break up the appearance of a monotonous wall.

The side walls of the building, especially the façades on the ends of development which face the adjacent residential neighbourhoods and a portion of Belleview Avenue should have brickwork or other features that provide interest and harmonize with the balance of the neighbourhood. By having similar treatments as the north façade, the entirety of the building maintains visual interest. To respect the privacy and scale of the neighbouring dwellings, the windows on the south façade of the building are limited in number and size, and are primarily hallway access corridors.



Example of architectural articulations and window fenestrations



Example of colour and material variation on building façade

The fenestration for the proposed development incorporates similar details found at the Ford Powerhouse Building. Namely, the vertical and horizontal detailing of each window. In addition, the predominantly brick exterior of the proposed development is broken up with concrete detailing in a similar fashion to the Ford Powerhouse Building and neighbouring high rise developments east of the subject site. To best fit with the existing character of the neighbourhood, the proposed development incorporates a modern interpretation of the design aesthetic and desire to play homage to the important history of the area.

3.2 Public / Private Amenity Space Recommendations

The main outdoor amenity space proposed is the landscaped area at the northwest corner of the building, adjacent to the intersection of Riverside Drive East and Belleview Avenue, and around the surface parking area closest to Belleview Avenue. In addition, open air terraces are proposed on the fourth floor, one along Riverside Drive, as part of the club house lounge area and one on the west side of the proposed development. A community garden space is proposed on the west side of the fourth floor terrace. This space adds to the amenity space available for future residents. These open air terraces are proposed above a portion of the garage, where the residential tower is setback. The west-facing open air terrace is separated from the existing residential dwellings via a rear-yard setback providing over 6.0 metres of

separation from the neighbouring dwellings to the south, and is at a height which precludes residents from imposing on the privacy of neighbours. In addition, a noise barrier wall will be installed along the outside walls of the terrace further screening the terrace from the existing residents. Other amenity space is available for future residents via the club house lounge space located within the building on the fourth floor.

This is an important amenity for residents of the development as well as the surrounding community as a gathering space and rest area. The amenity area should be appealing and provide a connection from the public right-of-way. Internal circulation, such as sidewalks and pedestrian thoroughfares, are also important outdoor amenities.



Example of using landscaping to delineate outdoor amenity space.

Amenity spaces should be visually or physically accessible from the public right-of-way for safety purposes and to maximize use. This can be done with changes in materials, pedestrian paths, landscaping, lighting and other public realm features which would invite users into the space. We would recommend that seating, landscaping and lighting be provided throughout the site in the final design, creating multiple viewpoints in both shade and sunny locations for four season use.

Efforts should be made to incorporate aspects of sustainability and climate change proactivity within the development. Along with designing a more energy efficient building, the outdoor amenity spaces can become sustainable as well. This can be accomplished by efforts such as choosing sustainable materials and incorporating stormwater management into the design (i.e. permeable pavers, rain gardens, stormwater collection, etc.), as well as incorporating only local materials and planting.



Example of sustainable design, which incorporates permeable pavement to help with stormwater management¹

Private amenity space has been considered and is important for the residents of the proposed development. Each apartment style unit includes a private balcony. The units on the north side of the building have balconies that face north towards Riverside Drive East with the units on the east side of the building all facing towards Pratt Place. On the ground floor, each townhome style unit has a private patio fronting onto the public right-of-way. The proposed private balconies and patios offer residents their own outdoor amenity space, which is an extension of their homes.

¹ "Permeable interlocking concrete pavement," Paving the Future, accessed October 6, 2021, <https://landscapeontario.com/paving-the-future>.

The community garden space on the fourth floor contributes to the available amenity space for future residents. In addition, plantings are proposed on the fourth floor terrace that will enhance the amenity space provided for future residents. Other private amenity space will be provided within the building, allowing residents to meet and interact when necessary.

3.3 Landscaping Recommendations

The site has potential to better incorporate trees and vegetation within the property's boundaries, contributing to the existing urban tree canopy in the vicinity. The concept plan showcases some areas which can be treated with strategic landscaping which would improve the aesthetic for the amenity area for residents and to soften the transition for residents and pedestrians in the adjacent neighbourhood. Areas for planting are provided along the patios of each townhome style unit adjacent to the public realm.

A landscaped buffer as well as fencing will be installed between the south-facing exterior of the building and the alley, as well as on the west-facing exterior of the building and the neighbours. Due to the 4.5m by 4.5m daylight corners required at both intersections - Pratt Place and Riverside Drive as well as Belleview Avenue and Riverside Drive, the area for planting trees or vegetation on the subject property is limited. Planting is not permitted within the required site line setbacks; however, landscaping can be provided along some of the west portions of the site as well as the possibility for planters included along the sidewalk or between the daylight corner and the building on the east portion of the site. Incorporated within the proposed development, planters will be added on the fourth floor terrace space, enhancing the green space within the subject site. Landscaping features can be incorporated along the frontage of the development facing Riverside Drive East and Pratt Place. This will improve the pedestrian environment for future residents as well as neighbouring properties that use the sidewalks.

The Developer should consider adding screening vegetation to lessen the visual impact of the surface parking lot from the adjacent parcel as well as from the street. In addition to views from outside the site, there is also opportunity to provide additional planting within the parking areas for improved aesthetics. The Developer should also consider organized landscape plantings to add aesthetic quality the space in between the building and property lines where the informal and formal amenity spaces are being proposed.

A Landscape Plan should be completed for the subject site as part of Site Plan Control approval.

3.4 Surrounding Context

Given the location of the subject site, there is an opportunity to incorporate design elements that enhance both the proposed development as well as benefit the surrounding area.

3.4.1 Ford City Community Improvement Plan

The site is located directly outside of a Community Plan Area boundary; however, the building design still incorporates elements from the Ford City Community Improvements Urban Design Guidelines and Policies. As per the Guidelines and Policies, the scale of the building is consistent with heights, mass, widths, and proportions (Ford City CIP, 4.3.2.4.1). This is evidenced in Figure 2 – Streetscape Surrounding Subject Site, which shows the proposed residential development compared with other existing high-rise developments in the area. The building is orientated towards the right-of-way and along the exterior property line as a means to frame and strengthen the street edge (Ford City CIP, 4.3.2.4.2). Further, the at-grade entrances of the residential building should be prominent and facing the public street (Ford City CIP, 4.3.2.4.3). The materials and colours are proposed to be in a palette and of a quality that is compatible with the existing buildings in the neighbourhood (Ford City CIP, 4.3.2.4.6).

3.4.2 Pedestrian Accessible Amenities

There is an excellent opportunity to utilize the parkland which is located within 700m of the proposed development to the northeast. Alexander Park is located on the north side of Riverside Drive East and provides users with access to the riverfront. The park provides for additional amenity space as it includes an expansive grassy area with a number of mature trees that provide shade during the spring and summer months. Throughout Alexander Park there are paved trails and seating available.

Looking to the Greenway System (Schedule B) via the Official Plan, the subject site is located amidst multiple 'Recreation-ways' that connect all parkland across the City of Windsor. Further, Alexander Park includes linkages to additional parks elsewhere in Windsor. The proposed residential development would help to maintain the use of the continuous trail and parkland network and would be a definite benefit and commodity for residents in the future units.

As noted, the proposed residential development will provide pedestrian connections from the site directly into the existing sidewalk networks along Riverside Drive East, Belleview Avenue, and Pratt Place. This public sidewalk network provides direct pedestrian connection between the proposed development and Alexander Park. Providing these connections will achieve the goal for the area of providing a pedestrian focused environment. It will allow the public as well as residents to access the parkland easily. The Developer should ensure that the walkway connections from the site to the existing sidewalk network facilitate easy and safe pedestrian navigation towards Alexander Park. Providing benches and pedestrian scale lighting along the interior pathways would improve the experience for users.

Conclusions

Based on our review of the proposed development if done in consideration of urban design opportunities and recommendations which have been presented we have concluded:

1. The proposed development aligns with the overall vision for the area as outlined in the Official Plan, and with adjustments to some design elements the site can further align with the character as outlined by the City of Windsor and the Ford City CIP recommendations.
2. The proposed development does not adversely affect the neighbouring properties as it is similar in scale, mass and design to many of the high rise buildings in the area. The existing low rise development in the area is already surrounded by higher profile residential, industrial and institutional uses and exist in harmony with these structures.
3. The proposed development provides a transition from the surrounding low rise dwellings through the addition of townhome style units along the ground floor of the development. The one and two storey townhome style units are similar in scale and height to the existing single and semi-detached dwellings along Pratt Place. Townhome style units will enhance the public realm along Riverside Drive East and Pratt Place.
4. The proposed development respects the close proximity to the lower profile buildings and has been positioned on the site as far to the north as possible and has limited the potential for privacy issues by reducing the number of habitable windows along the south side of the building. In addition, any units located on the southernmost elevation have been set back from the edge of the enclosed parking garage and the southwest limit of the property to provide further separation.
5. The proposed development provides visual screening for the ground level parking enclosure through the inclusion of one and two-storey townhome style units.
6. The proposed development should acknowledge, through design, the nearby park. There would be benefits for future residents if connections to the parkland system, located northeast of the site, were easily navigable.
7. The proposed development has opportunities to provide landscaping and planting of vegetation along Riverside Drive East and Pratt Place, softening the interface along the property lines and to provide internal planting to improve the aesthetic appeal of the site.
8. The proposed development should implement the design considerations as presented in the Recommendations section for this development (including pathways, seating, and pedestrian-scale lighting).

July 28, 2023

Attention: The City of Windsor Development and Heritage Standing Committee

Re: File #ZNG/6653 Z-001/22
Proposed Zoning Bylaw Relief for the Multi-Residential Building Development
3251 Riverside Dr E and 222 Belleview

Dear Members of the Development and Heritage Standing Committee:

We are writing this letter as long-time residents of Riverside Drive East who share the vision of a vibrant and flourishing Riverside Drive. While the efforts towards development and progress in our community are appreciated, this document is respectfully submitted to express opposition to the proposed zoning bylaw relief that would allow a significant increase in building height, reduced landscaped space, and expanded lot coverage for the proposed multi-residential building at 3251 Riverside Dr E and 222 Belleview.

First and foremost, the importance of development in our city to accommodate growth and address the needs of our growing population should be acknowledged. However, that development must be carried out responsibly and thoughtfully, with a keen understanding of the existing zoning bylaws and the impact it may have on the neighborhood's character, infrastructure, and environment.

- 1) **Preserving the Current Zoning Bylaw Parameters:** The proposed development is asking the committee to amend the zoning to go from RD2.2 (which allows a 20m maximum height for any building) and CD1.7 (parking lot designation), both to RD3.3. There is no opposition to this change to RD3.3. However, the proposed development is then asking to far overreach the parameters of a newly designated RD3.3 to build *to heights more than double the current zoning, to eliminate most of the green space requirements, and to drastically increase the lot coverage rules*. The subject property is simply too small to accommodate the structure that is being proposed. Current zoning bylaws were established to strike a balance between development and preservation of the existing character of the area. It is essential to adhere to these parameters to maintain a harmonious and cohesive environment that respects the needs of both residents and businesses. Granting excessive height and lot coverage exceptions as well as reducing minimum landscaped space would not only disrupt the existing urban fabric but will set a precedent for further deviations from the original vision.
- 2) **Consideration for Setback from the Road:** The proposed multi-residential building's proximity to Riverside Drive East is of significant concern. The current proposal is only set back a few feet from an extremely busy roadway and is asking permission to rise higher than any surrounding buildings. Combining these factors creates a looming structure that overshadows all else around it and detracts from the visual attractiveness of Riverside Drive East. By setting the building further back from the road, we can create

a more pedestrian-friendly environment, reduce traffic congestion, and improve safety for residents and visitors. Moreover, this setback will ensure the preservation of open spaces and views, contributing to the overall aesthetic appeal and enhancing the streetscape quality.

- 3) Alignment with Riverside Drive Vista Improvement Project Goals: The developer's submitted plan by Dillon Consulting makes reference to attempting to adhere to the Ford City Urban Design but clearly acknowledge that this proposed development is *outside* the area of Ford City. It would be more fitting for this project to align with the Riverside Drive Vista Improvement Project, which was designed with a comprehensive vision for enhancing transportation, aesthetics, civic, and residential functions along the corridor. To achieve these goals, any development should be carefully aligned with this project's principles and contribute positively to the community's well-being. Deviating from the established guidelines and parameters would undermine the project's vision and could potentially lead to missed opportunities for creating a thriving and cohesive neighborhood.
- 4) Sustainable and Responsible Development: As responsible citizens, sustainable development practices that minimize the environmental impact and consider the long-term consequences on our community's well-being must be prioritized. By changing to *but still maintaining the zoning bylaw parameters of RD3.3*, developers are encouraged to design projects that utilize eco-friendly technologies and support a sustainable lifestyle for future generations, while maintaining the aesthetic appeal and functionality of the area that is in line with Windsor's current zoning bylaws.

In conclusion, it should be reiterated that the opposition is **not** against development itself, nor the zoning change to RD3.3, but **opposition is firmly against the request for relief from RD3.3 zoning bylaws**. We all support responsible and well-planned development that aligns with the existing zoning bylaws and maintains the aesthetic and functional appeal of Riverside Drive East. The Development and Heritage Standing Committee is requested to carefully consider these concerns and promote a collaborative approach that respects the aspirations of the community while fostering growth and prosperity.

Thank you for your attention to this matter. We hope these factors will be taken into close consideration in order to create a future that all residents can be proud of.

Sincerely,

Robert Woodall
Linda Tietze
Bill (Vito) Maggio
Michelle Maggio
Dan Mazur
Betty Mazur

From: Stephen Ducharme
Sent: August 21, 2023 3:46 PM
To: Stuart, Kelly <kstuart@citywindsor.ca>; Abbs, James <jabbs@citywindsor.ca>; clerks <clerks@citywindsor.ca>
Subject: Re: FILE NUMBER ZNG/6653 Z-001/22

Hi Kelly,

Thank you for your email. The problem is that I am scheduled to be in Central America that week of September 11. Even though all neighbours that I have spoken to are against this proposed development, the crucial challenge would need to be addressed by me as I am the closest property owner that has already been impacted by the increased traffic congestion on Pratt and Riverside. It is the historical Damase Pratt House, 3336 Riverside Drive East that I own where it has now been multiple times that I have had to re-build my historical fence on Riverside Drive from cars hitting this from increased car traffic in just the past few years alone. It is my property that would be most severely impacted by this proposed development.

I am strongly opposed to this proposed zoning change after having invested so heavily in this specific area. Not only I am opposed of the change of the historical character of this very unique neighbourhood that I have invested so heavily in, but most importantly, the proposed building proximity to Riverside Drive is of grave concern to all residents. This specific intersection is already one of the busiest in the city with extreme traffic congestion. I have witnessed severe traffic accidents including head-on collisions resulting in severe injury. There is also a senior living retirement home with over 135 senior Windsor residents that live only a few feet from this proposed development site that I watch many of them carefully with limited mobility as well as now possibly an unsafe proposed pedestrian set back from Riverside Drive. I strongly believe someone should speak on their behalf as well as those in the community have the obligation to look after their interest.

Both increased density as well as unsafe setback from Riverside Drive as proposed, to all pedestrian traffic is dangerous. This specific issue is important enough that if I am able to, I would be willing to fly all the way from Central America to be able to be addressed. It will be very difficult for me to be given such short notice of only August 29th for me to change my schedule and cancel my other meetings to make arrangements to fly all the way back. It would be very helpful if this file could be added to the following standing committee meeting after September 11th to ensure that I can attend as September 11th is already extremely short notice for me and will be very difficult for me to attend. Please do let me know if this could be added to the next meeting after September 11th, along with as far in advance notice as possible so that I can have a voice in a community that I have invested in and love so much.

Thank you,
Stephen P. Ducharme

September 11, 2023
Development & Heritage Standing Committee
Item 7.1 – Written Submission

From: Tara Rabie
Sent: August 29, 2023 9:19 AM
To: clerks <clerks@citywindsor.ca>
Subject: Written Submission RE: OPA/171 and ZNG/6653

I'm writing in regards to OPA/171 and ZNG/6653 - Application Riverside Horizons - 3251 Riverside Drive E and 222 Belleview. I'm strongly opposed to the amendment to the zoning by-law 8600.

This original plan was for a 4 storey building. 12 storeys would overshadow our neighbourhood and the traffic would be a nuisance to residents on Pratt and Belleview. The proposed building would be larger than the Ford Powerhouse building, which is a beautiful landmark in our area, as well as the Holy Rosary Church / Water's Edge Event Centre building. Our neighbourhood block consists of 2 storey homes built mostly between 1910 - 1921.

We have had issues with nearby Lifetimes staff and guests and other nearby apartment residents taking up street parking and parking on the boulevard in front of our homes (no curbs), despite having adequate parking, which now causes flooding in some areas on our street and sidewalks. Belleview residents also have issues with street parking and the apartment on their street. Should the building's parking not be used by tenants or guests, it would cause more stress and I am concerned about the added traffic when leaving our driveways. If cars properly park, the street is narrow and our driveways are small, offering little warning when cars are driving or speeding down the street. Other apartment buildings in the area have driveways from Riverside Drive / Erskine / Wyandotte, separating them and offering traffic relief. This building would only have access from Belleview and Pratt. I'm also concerned about the future changes to our street to accommodate this building (possible one way). The proposed driveways for the building are located on Pratt are closely located where the emergency vehicles (fire trucks) park when called for the nursing home. I've seen as many as 4 fire trucks parked on that corner. Turning left onto Riverside or Wyandotte would be difficult during busy times, potentially backing up traffic. Had this issue when Strabane was being worked on and traffic was detoured to our street. It made coming and going very difficult, and I would not want this on a daily basis. The nursing home frequently has a large Sysco truck that makes deliveries, and garbage / recycling trucks for them and the Skyline apartment.

The apartment buildings in the neighbourhood are currently surrounded by landscaped open space, this building would have a reduced area offering little separation from the residential homes. Green space and trees currently offer a nice buffer between our homes and buildings. I often sit in my living room where my north facing windows show the bright blue sky, and the sun is reflected from the Lifetimes windows / building towards my home, this would negatively affect this building according to the shadow report. We would be almost completely surrounded by oversized apartment buildings, decreasing the enjoyment / views from our homes. No one wants to feel like they're being watched all the time while trying to enjoy their own space.

Also the lack of green space for pet owners. I don't want my yard used as a replacement. I've had people in apartments using my garbage to dispose waste, and renters without yards using our front yards, killing some of my garden plants and not cleaning up frequently down the block. This would be multiplied. <https://www.theglobeandmail.com/real-estate/toronto/article-torontos-condo-communities-have-a-dog-poo-problem/>

My home was built in 1921, I spent a significant amount of money repointing my bricks and improving my home and am very concerned about the proximity, and the negative effects the construction may have on our properties.

I'm not opposed to the development, but I'm strongly opposed to the size and negative impact it would have in our neighbourhood. Buildings of this size, in this close proximity negatively impact our homes. This would be better suited for Drouillard and Riverside. Our neighbourhood needs something complementary to our neighbourhood.

Thank you,

Tara R.



Subject: Gouin Drain Provisional By-Law for Repair and Improvement - Ward 9

Reference:

Date to Council: October 30, 2023
Author: Tom Graziano
Engineer III/Drainage Superintendent
(519) 255-6257 Ext. 6490
tgraziano@citywindsor.ca
Development – Engineering
Report Date: October 12, 2023
Clerk's File #: APM/14378 & SPL/14449 & SW2023

To: Mayor and Members of City Council

Recommendation:

- I. That Council **ADOPT** the Engineer's Drainage Report completed by Dillon Consulting Ltd. dated September 14, 2023 (attached), for the Repair and Improvement to the Gouin Drain, and give Provisional By-law ____-2023 a first and second reading in accordance with Section 45(1) of the Drainage Act , R.S.O 1990;
- II. Council **DIRECT** the Clerk to set a date for the first sitting of the Court of Revision and distribute the Provisional By-law and the Notice of the Court of Revision, in accordance with Sections 46(2) and 46(3) of the Drainage Act, R.S.O. 1990;

Executive Summary:

N/A

Background:

The Gouin Drain is a municipal drain servicing both the City of Windsor and the Town of Tecumseh. The drainage area within the City of Windsor consists of the portion of the EC Row Expressway between Little River and Banwell Road, and properties within the Twin Oaks Business Park, including a portion of the NextStar Energy Electric Vehicle Battery Plant project site (the Nextstar Site). The Gouin drain outlets to the Little River.

At the November 28, 2022, meeting, City Council received Report S120/2022, which recommended approval of a request from NextStar Energy Inc., to enclose portions of

the Gouin Drain on the NextStar Site in order to facilitate development of the property. City Council approved the request as CR478/2022 and appointed Dillon Consulting Ltd. as the Drainage Engineer of record for the work.

A virtual “site-meeting” was hosted by Dillon Consulting Ltd. on March 21, 2023, and was attended by representatives of the City of Windsor, NextStar Energy Inc. and property owners residing within the Town of Tecumseh.

Discussion:

The Engineer’s Report addresses the request of NextStar Energy Inc. to install two culverts over the Gouin Drain adjacent to the NextStar Site. Council Resolutions CR388/2007 and CR64/2015 confirm that the municipality would absorb all municipal drain maintenance costs through the general tax levy, except for private access structures and “special benefit” works benefiting individual properties, as authorized by the Province of Ontario through the City of Windsor Act, 1968. The portions of the costs associated with the private access structure(s) proposed for the NextStar Site are for the sole benefit of the NextStar Energy Inc. and will not be funded through the general levy.

In accordance with Section 45 of the Drainage Act, at the meeting to consider the engineer’s report (Council Meeting), the report may be adopted by by-law when such by-law is given two readings by Council. The report shall then be deemed to be adopted and the by-law shall be known as a provisional by-law. Council shall then within 30 days after adoption of the drainage report, send a copy of the provisional by-law and Notice of the Court of Revision meeting to all affected landowners listed in the schedule of assessment. The notice shall inform each landowner of their right to appeal their drainage assessment and/or allowances to a Court of Revision by giving notice to the City Clerk not later than 10 days prior to the first sitting of the Court of Revision. Following the Court of Revision and the expiration of the appeal period, the by-law may be passed by giving third and final reading of the by-law by Council.

NextStar Energy Inc. and the City have agreed to pay their proportionate share of the assessment as established in the Drainage Report, and therefore no costs shall be borne to the upstream property owners. A copy of the engineer’s report entitled, “Drainage Report for the Gouin Drain (NextStar Energy Access Bridges)”, dated September 14, 2023 is attached in the appendices.

Risk Analysis:

There are no identified risks to the Corporation in adopting the recommendation at this time. A lack of adoption of the report, however, would risk the construction timelines for the NextStar Site project and limit the ability for it to proceed according to the expected schedule.

Climate Change Risks

Climate Change Mitigation:

The recommendation does not materially impact upon Climate Change Mitigation.

Climate Change Adaptation:

The recommendation does not materially impact upon Climate Change Adaptation.

Financial Matters:

Capital expenses for the City’s share of construction associated with the repair and improvements to the Gouin Drain will be funded through the capital budget (project 7221006 – Point East Site Development Site Servicing) in the estimated amount of \$258,420 (plus non-recoverable HST). There are sufficient funds in this project to carry out the work.

The Drainage Report notes minor changes to the Maintenance Assessment Schedule. The changes are not substantial and inconsequential for property owners within the City of Windsor, as CR388/2007 assigns maintenance costs to the general rate. For the properties located within the Town of Tecumseh, the City would recover those costs at the time of maintenance activities. Those costs would be recovered to Municipal Drains Maintenance (project 7086004).

Consultations:

Wira Vendrasco, Deputy City Solicitor, Legal & Real Estate

Kathy Buis, Financial Planning Administrator

Patrick Winters, Manager of Development

Conclusion:

Administration recommends that City Council approve the Provisional By-law to adopt the Drainage Report for repairs and improvements to be made to a portion of the Gouin Drain in the City of Windsor, dated September 14, 2023, in accordance with Section 45 of the *Drainage Act*.

Planning Act Matters:

N/A

Approvals:

Name	Title
Kathy Buis	Financial Planning Administrator
Patrick Winters	Manager of Development
Stacey McGuire	Executive Director of Engineering / Deputy City Engineer
Chris Nepszy	Commissioner of Infrastructure Services

Name	Title
Shelby Askin Hager	Commissioner of Legal & Legislative Services
Janice Guthrie	Commissioner of Corporate Services and Chief Financial Officer
Joe Mancina	Chief Administrative Officer

Notifications:

Name	Address	Email
Mark Hernandez, Dillon Consulting Ltd.	3200 Deziel Drive, Suite 608, Windsor, ON N8W 5K8	mhernandez@dillon.ca
Alessia Mussio, Drainage Superintendent, Town of Tecumseh	917 Lesperance Rd, Tecumseh, ON N8N 1W9	amussio@tecumseh.ca
NextStar Energy Inc. Attn: Ryan Oswald	800 Chrysler Drive, Auburn Hills, MI, 48326	ryan.oswald@stellantis.com
Essex Region Conservation Authority, Attn: Ashley Gyori	360 Fairview Ave W, Essex, ON N8M 1Y6	agyori@erca.org

Appendices:

- 1 Drainage Report for the Gouin Drain (NextStar Energy Access Bridges)
- 2 Draft Provisional By-Law for the Gouin Drain

**DRAINAGE REPORT
FOR THE**

**GOUIN DRAIN
(NEXTSTAR ENERGY
ACCESS BRIDGES)**

**IN THE
CITY OF WINDSOR**



14 SEPTEMBER 2023
MARK HERNANDEZ, P.ENG.
FILE No. 22-5108

File No. 22-5108

Corporation of the City of Windsor
Engineering – Design and Development
350 City Hall Square, Suite 310
Windsor, Ontario
N9A 6S1



**Drainage Report for the
GOUIN DRAIN
(NEXTSTAR ENERGY ACCESS BRIDGES)
In the City of Windsor**

Mayor and Council:

Instructions

A request was made by the City of Windsor Road Authority to repair and improve the Gouin Drain. Council accepted the request under Section 78 of the Drainage Act and on 28 November 2022 appointed Dillon Consulting Limited to prepare a report. The proposed works involves two (2) new access bridges to accommodate the NextStar battery production plant development proposed within property Roll Nos. 090-040-03403 and 090-040-00103.

Watershed Description

The Gouin Drain commences on the line between Lot 149 and Lot 150, Concession 1 in the Town of Tecumseh. It flows in a westerly direction, turning north at the west limit of Lot 149 toward the north side of Gouin Street. Here, it turns west for 124 metres before turning north for 75 metres where it again then flows westerly. The Gouin Drain continues northerly from the south side of E.C. Row Avenue East/Anchor Drive, crossing the E.C. Row Expressway and then turning westerly again to its outlet into the Little River Drain.

The total length of the drain is approximately 3,286 metres. The watershed area is 211.00 hectares (521.30 acres) which consists of 114.32 hectares (282.47 acres) within the Town of Tecumseh and 96.68 hectares (238.83 acres) within the City of Windsor.

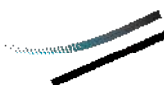
The lands comprising the watershed are under mixed agricultural, and residential use. There is little topographic relief. From the Ontario Soil Survey, the principle surficial soil in the study area is described as Brookston Clay. Brookston Clay is characterized as having very poor drainage.

Drain History

The recent history of Engineers' reports for the Gouin Drain follows:

- **28 January 2023 by Mark D. Hernandez, P.Eng.:** This report recommended brushing and removal of sediment along the drain. The report also recommended the extension of the drain from its previous outlet at E.C. Row Expressway downstream to the Little River Drain and established the Gouin Drain Branches North & South in the upstream residential area. Bridge work included removals, replacements, and providing future maintenance provisions. Bridge removals included Bridge Nos. 8, 9, and 10.

3200 Deziel Drive
Suite 608
Windsor, Ontario
Canada
N8W 5K8
Telephone
519.948.5000
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519.948.5054

- 
- **28 January 1987 by Wm. J. Settington, P.Eng.:** This reconsidered report recommended brushing and removal of sediment along the entirety of the drain, along with deepening/widening and relocation of the drain in select locations along its length. This report also recommended the removal and replacement of access bridges along the drain.
 - **2 October 1981 by C. G. Russell Armstrong, P.Eng.:** This report found the drain to be “badly out of repair”. The recommended work included the repair and improvement of the Gouin Drain, including a thorough brushing and cleanout. It also recommended that the drain be deepened from E.C. Row Expressway to Gouin Street. The report recommends that the most upstream 28 metres of drain be enclosed with a 450 mm (18”) diameter plastic pipe, and that the existing culverts with the exception of the Shawnee Road culvert, be replaced and enlarged to accommodate for flows from a proposed development.

On-Site Meeting

We conducted an on-site meeting on 21st March 2023. A record of the meeting is provided in Schedule 'A', which is appended hereto.

Survey

Our survey and examination of the Gouin Drain was carried out in April 2018 as part of the work completed from the 28th January 2023 report.

Design Considerations

The requested improvements include the construction of an additional bridge access to a future Enbridge Gas Inc. facility that will provide service to the NextStar site. The bridge will also facilitate associated road improvements. This access is to be installed immediately upstream of, and attached to the existing access bridge to property Roll No. 070-650-01351 (Jamieson Laboratories Ltd.) referred to as Bridge No. 7, thus shall be considered an extension of Bridge No. 7. This access currently provides capacity for the peak flows expected in the 2-year design storm according to the previous drainage report (Dillon, 2023). Therefore, the Bridge No. 7 extension shall provide the same level of service at minimum.

The requested improvements also include the construction of a lawn enclosure to facilitate the proposed plan for the NextStar site. This enclosure is proposed to be installed directly downstream of and attached to the Banwell Road bridge, referred to as Bridge No. 13, thus the enclosure is referred to as Bridge No. 13A. Bridge No. 13 currently provides capacity for the peak flows expected in the 10-year design storm according to the previous drainage report (Dillon, 2023). Since Bridge No. 13A is to be directly connected to Bridge No. 13 on the downstream end, it shall also provide the same level of service.

Bridge No. 13A will affect the current level of service of Bridge No. 13. However, we do not recommend replacing Bridge No. 13 at this time because improvement to the E.C. Row Expressway/Banwell Road interchange are expected in the near future and will include the replacement of Bridge No. 13. Since the design requirements of Bridge No. 13 to facilitate the future improvements to Banwell Road cannot be known at this time, for the purposes of this report, the future replacement of Bridge No. 13 is based on the existing road configuration.

A separate technical memorandum was completed at the request of the Essex Region Conservation Authority (ERCA) to outline the hydrologic and hydraulic analysis completed for the recommended drainage works.

This memo included an assessment of the hydraulic impacts of the enclosure using the recently completed two-dimensional hydrologic/hydraulic model in the PCSWMM software for the Little River Drain watershed as part of the Little River Floodplain Mapping Study (Dillon, 2022).

The design criteria required by ERCA for Bridge No. 13 and Bridge No. 13A is that proposed improvements are not to worsen the expected water levels within the drain during the 100-year design storm to within one (1) centimeter of water level increase, regardless of design capacity of the bridge or condition of the drain. The modelling/assessment and physical improvements required to achieve this threshold and level of service are above and beyond what is typically required for a municipal drain, particularly as municipal drains are not designed for that level of service. Bridge No. 7 was not included in the PCSWMM model and was assessed separately in the culvert design software, HY-8.

To meet the design criteria set by ERCA for Bridge No. 13/13A, recommendations were assessed in PCSWMM according to the following scenarios:

- An existing conditions model that was developed through the Little River Floodplain Mapping study (Dillon, 2022) to estimate the existing 100-year flood water elevation upstream of Bridge No. 13.
- An interim conditions model that was developed based on the temporary condition where Bridge No. 13A is constructed and Bridge No. 13 remains in its existing state, prior to the interchange improvements.
- An ultimate condition model that was developed based on the full build-out of Bridge No. 13A and Bridge No. 13 on the basis that Banwell Road is reconstructed to its existing configuration.

Existing Conditions and Recommendations

The 28 January 2023 report for the Gouin Drain & Branches included recommendations along the same section of the Gouin Drain as is considered in this report. These recommendations included drain bottom cleanout, brushing of the banks, establishment of a grass buffer, surface inlet repairs, and bridge removals. The recommendations within this report assume the work described in the previous report have already been completed.

We recommend the drain be widened from a 1 metre bottom width to a 2 metre bottom width from Station 1+032 to Station 1+930. Excavated material from the drain widening shall be trucked to, or remain within the lands of property Roll No. 090-040-00130 for the use of the remaining drainage works and/or use by the landowner. Any unused soils not required for the purposes of the drainage works shall remain on the said property for the landowner to manage.

We further recommend the bridges on the Gouin Drain be either removed or improved as described below. Specific structure numbers have been designated for ease of reference between the specifications and drawings. The locations, dimensions and use of each structure are as follows:

Bridge No. 7: Jamieson Laboratories Ltd. (Roll No. 070-650-01351), City of Windsor (Roll No. 090-040-03403) and City of Windsor Road Authority

A 40.5 m long, 2440 mm x 1600 mm corrugated steel pipe arch with rip rap end protection and asphalt driveable top width provides secondary access to property Roll No. 070-650-01351 through property Roll No. 070-650-01303.

The owner of property Roll No. 090-040-03403 requested a new access over the Gouin Drain to facilitate the development of a new Enbridge Gas facility for the purpose of servicing the NextStar battery production plant. The access is proposed immediately upstream of the existing Bridge No. 7 which currently only provides access to a commercial property (outside of the Gouin Drain watershed) through a vacant lot. Thus, the new bridge shall be considered an upstream extension of Bridge No. 7. Further, the City of Windsor plans to terminate E.C. Row Avenue in a cul-de-sac, which requires additional length of drain to be enclosed.

We recommend Bridge No. 7 be extended with an 1800 mm diameter reinforced concrete pipe, attached to the existing corrugated steel pipe arch with a poured in place concrete collar. We further recommend the existing sloping stone end wall on the upstream end of Bridge No. 7 be removed to accommodate the extension and replaced on the new upstream end with a concrete block headwall, all in accordance with the City of Windsor engineering detail AS-209A. The bridge is recommended to have a minimum 6.0 m wide gravel driveway.

An existing swale that services the Jamieson Laboratories Ltd. access road currently enters the drain on the upstream end of the bridge. The swale shall be intercepted with a new ditch inlet catch basin that is connected to the new culvert pipe with a prefabricated 'tee' section and a 300 mm diameter PVC DR35 pipe.

Bridge No. 8: City of Windsor (Roll No. 090-040-03403)

A 9.7 m long, 2240 mm x 1630 mm corrugated steel pipe arch with bagged concrete end protection and a gravel driveway provides access to this property. This bridge has been recommended to be removed under a separate report. No recommendations are being made under this report.

Bridge No. 9: City of Windsor (Roll No. 090-040-00103)

An 8.2 m long, 2240 mm x 1630 mm corrugated steel pipe arch with bagged concrete end protection and a gravel driveway provides secondary access to this property. This bridge has been recommended to be removed under a separate report. No recommendations are being made under this report.

Bridge No. 10: City of Windsor (Roll No. 090-040-00103)

A 7.8 m long, 2240 mm x 1630 mm corrugated steel pipe arch with bagged concrete end protection and a gravel driveway provides secondary access to this property. This bridge has been recommended to be removed under a separate report. No recommendations are being made under this report.

Bridge No. 11: City of Windsor (Roll No. 090-040-00103)

A 7.9 m long, 2240 mm x 1630 mm corrugated steel pipe arch with bagged concrete end protection and a gravel driveway provides access to this property. We recommend this culvert be removed as the site plans indicate, they are no longer required.

Bridge No. 12: City of Windsor (Roll No. 090-040-00103)

A 7.6 m long, 2240 mm x 1630 mm corrugated steel pipe arch with bagged concrete end protection and a gravel driveway provides secondary access to this property. We recommend this culvert be removed as the site plans indicate, they are no longer required.

Bridge No. 13A: City of Windsor (Roll No. 090-040-00103)

The owner of property Roll No. 090-040-00103 requested a portion of the drain be enclosed to facilitate the proposed NextStar battery production plant site plan. This enclosure shall be on the downstream end of the existing Bridge No. 13 (Banwell Road).

To facilitate the development of the NextStar battery production plant site, we recommend a 178 m long, 2250 mm diameter reinforced concrete pipe be connected to the downstream end of Bridge No. 13 with a prefabricated bend and a poured in place concrete collar to enclose the drain. The downstream end treatment is recommended to be a concrete block headwall, all in accordance with the City of Windsor engineering detail AS-209A. The enclosure shall intercept the existing roadside ditch on the west side of Banwell Road.

Bridge No. 13: City of Windsor Road Authority (Banwell Road)

A 15.6 m long, 2240 mm x 1630 mm corrugated steel arch pipe with bagged concrete end protection and asphalt roadway provides a road crossing.

We anticipate that the existing road portion of culvert will require replacement within the next 10 years or sooner if conditions warrant. We recommend that in the future the culvert be replaced with a new 16 m long, 2250 mm diameter reinforced concrete pipe complete with concrete block end wall and a 12.0 m asphalt driveable top width. The recommended pipe length assumes future road improvements maintain the same road configuration as currently exists.

The future replacement of this bridge shall require the removal of the proposed concrete collar and prefabricated bend of Bridge No. 13A. When Bridge No. 13 is replaced, a new prefabricated bend will be required, complete with a new concrete collar to facilitate the works.

Allowances

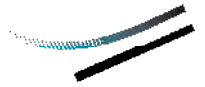
In accordance with Section 29 of the Drainage Act, we have determined allowances be given for the land used in widening the drain between Station 1+032 and Station 1+930. Schedule 'B' shows the distribution of the allowances in the total amount of \$2,000.00 to property Roll No. 070-650-01303.

In accordance with Section 30 of the Drainage Act, we do not anticipate any agricultural lands being damaged or taken as a result of the proposed drainage works. Any damage to existing grassed areas shall be restored to original conditions as part of the work.

Cost Estimate

Based on our review of the history, the information obtained during the site meeting and our examination and analysis of the survey data, we recommend that the Gouin Drain be repaired and improved as described below:

Item	Description	Amount
1.	Bridge works, as follows:	
	a) <u>Bridge No. 7 Extension</u> – Jamieson Laboratories Ltd. (Roll No. 070-650-01351), City of Windsor (Roll No. 090-040-03403) & City of Windsor Road Authority (Anchor Drive) – Remove existing stone erosion protection from east end wall of existing bridge and dispose of off-site. Supply and installation of a new	\$248,000.00



Item	Description	Amount
	<p>45.5 m long, 1800 mm diameter reinforced concrete pipe, Class 65-D complete with full compacted Granular 'A' bedding and backfill up to 300 mm above the top of pipe (approximately 1,320 tonnes) providing a 6 m wide driveable top width. Supply and install a concrete block headwall consisting of 1200 mm x 600 mm x 600 mm interlocking concrete blocks (approximately 20 full blocks and 4 half blocks), designed and sealed by a professional engineer licensed in the province of Ontario and 1 m wide stone erosion protection on drain banks (approximately 25 m²). Connect new culvert to existing 2440 mm x 1600 mm corrugated steel pipe arch with a poured in place concrete collar. Supply and placement of topsoil (100 mm thickness). Restore all disturbed areas with fine grading and seeding. The work is to include drain bottom cleanout in close proximity to the bridge as necessary, site cleanup and restoration within the working area. This work also includes the supply and installation of a 600 mm x 600 mm ditch inlet catch basin with 2H:1V grate complete with 300 mm diameter PVC DR 35 leader pipe connected to culvert pipe and a 600 x 600 mm concrete catch basin complete with cast iron frame and grate. The Contractor shall extend the private roadside swale as shown on the drawings attached herein. This work includes the supply and placement of a stone erosion protection apron around the ditch inlet catch basin with stone erosion protection as well as placement of topsoil over newly excavated and disturbed banks (minimum 50 mm thickness), fine graded and seeded.</p>	
b)	<p><u>Bridge No. 11</u> – City of Windsor (Roll No. 090-040-00103) – Removal and disposal off-site of existing 7.9 m long, 2240 x 1630 mm CSPA with bagged concrete end walls.</p>	\$3,000.00
c)	<p><u>Bridge No. 12</u> – City of Windsor (Roll No. 090-040-00103) – Removal and disposal off-site of existing 7.6 m long, 2240 x 1630 mm CSPA with bagged concrete end walls.</p>	\$3,000.00
d)	<p><u>Bridge No. 13A</u> – City of Windsor (Roll No. 090-040-00103) – Remove and dispose of existing west side bagged concrete end wall on Bridge No. 13 at Banwell Road. Supply and installation of a new 178 m long, 2250 mm diameter reinforced concrete pipe, Class 65-D complete with prefabricated 16 degree bend and fully compacted Granular 'A' backfill up to minimum 300 mm above the top of pipe (approximately 4,900 tonnes) with placement of topsoil over top (minimum</p>	\$1,209,000.00

Item	Description	Amount
	<p>100 mm thickness), fine graded and seeded. Connect the new prefabricated 16 degree bend to the existing 2240 mm x 1630 mm corrugated steel pipe arch with a poured in place concrete collar. Supply and installation of a concrete block headwall consisting of 1200 mm x 600 mm x 600 mm interlocking concrete blocks (approximately 19 full blocks and 6 half blocks), designed and sealed by a professional engineer licensed in the province of Ontario and 1 m wide stone erosion protection on drain banks (approximately 25 m²). The work is to include drain bottom cleanout in close proximity to the bridge, site cleanup and restoration within the working area. Replace existing 450 mm diameter CSP from south with new 450 mm diameter HDPE pipe, 12 m long complete with 60 degree bend (Boss 2000 as manufactured by Armtec or approved equal) and connect to the new 2250 mm diameter pipe with a pre-fabricated 'tee'. Extend roadside ditch to new pipe inlet. This work also includes the supply and installation of 600 mm x 600 mm concrete catch basin complete with cast iron frames and grates along the enclosure (approximately 15 required). Work includes salvage and replacement of stone erosion protection, as well as restoration. Restore all disturbed areas with fine grading and seeding.</p>	
2.	<p>Widening of the drain from the south side between Station 1+032 and Station 1+930 (approximately 2200 m³), complete with placement of topsoil over new and disturbed drain banks (minimum 50 mm thickness), fine graded and seeded (bonded fibre matrix). Excavated material shall be trucked to, or remain on the lands owned by the City of Windsor (property Roll No. 090-040-00103 & 090-040-03403) for use by the landowner. This work includes re-establishment of swale inlets with salvaged or new stone erosion protection, as encountered on the south bank (approximately 4 inlets), as well as re-seeding of 1 metre wide grassed buffer.</p>	\$64,300.00
3.	<p>Supply and installation of rock check dam as per OPSD 219.211 and refuge stilling pool as per OPSD 219.220 lined with stone erosion protection on filter fabric underlay directly upstream of Bridge No. 5 (Anchor Drive). The work shall include trucking excavated materials off-site following completion of the work.</p>	\$2,000.00
4.	<p>Temporary Silt Control Measures During Construction</p>	<u>\$1,500.00</u>
	SUB-TOTAL	\$1,530,800.00

Item	Description	Amount
5.	Allowances (Section 29)	\$2,000.00
6.	Report, Coordination with DFO, Technical Memorandum, Assistance during construction and Final Inspection (cost portion)	\$64,500.00
7.	Expenses and incidentals (cost portion)	\$1,500.00
8.	ERCA application review and permit fee	<u>\$800.00</u>
	TOTAL ESTIMATE	\$1,599,600.00

The estimate provided in this report was prepared according to current materials and installation prices as of the date of this report. In the event of delays from the time of filing of the report by the Engineer to the time of tendering the work, it is understood that the estimate of cost is subject to inflation. The rate of inflation shall be calculated using the Consumer Price Index applied to the cost of construction from the date of the report to the date of tendering.

Assessment of Costs

The individual assessments are comprised of three (3) assessment components:

- i. Benefit (*advantages relating to the betterment of lands, roads, buildings, or other structures resulting from the improvement to the drain*).
- ii. Outlet Liability (*part of cost required to provide outlet for lands and roads*).
- iii. Special Benefit (*additional work or feature that may not affect function of the drain*).

Assessment Rationale for Special Benefit Assessments (Bridges)

Special Benefit assessment shown in Schedule 'C' and detailed in Schedule 'D' were derived as follows:

1. Since the sole purpose of the Bridge No. 7 extension is for the use by the owner, associated costs shall be assessed 100% to the landowner being the City of Windsor (Roll No. 090-040-03403).
2. Since the sole purpose of the Bridge No. 13A enclosure is for the use by the owner, associated costs shall be assessed 100% to the landowner being the City of Windsor (Roll No. 090-040-00103).
3. Costs associated with the widening of the drain between Station 1+032 and Station 1+930 shall be assessed 100% to the City of Windsor (Roll No. 090-040-00103) because the widening was necessitated by the enclosure works.
4. A future replacement detail was recently completed for Bridge No. 13 as part of the 28 January 2023 report and was assessed to the City of Windsor Road Authority. The Bridge No. 13A enclosure under this report requires a new future maintenance detail for Bridge No. 13 be developed, superseding the previous detail. Therefore, the costs associated with the engineering for the new future replacement of Bridge No. 13 shall be assessed 100% to the City of Windsor (Roll No. 090-040-00103).

Utilities

It may become necessary to temporarily or permanently relocate utilities that may conflict with the construction recommended under this report. In accordance with Section 26 of the Drainage Act, we assess any relocation cost against the public utility having jurisdiction. Under Section 69 of the Drainage Act, the public utility is at liberty to do the work with its own forces, but if it should not exercise this option within a reasonable time, the Municipality will arrange to have this work completed and the costs will be charged to the appropriate public utility.

Future Maintenance (Bridges over the Gouin Drain) (City of Windsor)

We recommend that future work of repair and maintenance of the Gouin Drain private access Bridge No. 7 and Bridge No. 13A shown herein be carried out by the City of Windsor at the expense of the properties. The assessments under Section 26 of the Drainage Act shall be non-proratable assessments.

Enbridge Gas Inc. has been identified to be responsible for the future maintenance costs for a portion of Bridge No. 7 because they are expected to be the future owner of property Roll No. 090-040-03403.

The division for each bridge shall be as follows:

Bridge No.	Type	Owner(s)	Special Benefit	Outlet
7	Private Access Existing Bridge	Roll No. 070-650-01351 Jamieson Laboratories Ltd.	45%	0%
7	Private Access Bridge	Roll No. 090-040-03403 Enbridge Gas Inc. (Section 26)	45%	0%
7	Road Bridge	City of Windsor Road Authority (E.C.Row Ave. East) (Section 26)	10%	0%
13A	Enclosure (178 m)	Roll No. 090-040-00103 City of Windsor (Future NextStar Energy Inc.)	100%	0%
13	Road Bridge (16 m)	City of Windsor Road Authority (Section 26)	100%	0%

Future maintenance costs associated with the new ditch inlet catch basin connected to Bridge No. 7 shall be at the cost of property Roll No. 070-650-03403.

Future Maintenance (Open Drain from Station 1+032 to Station 1+930)

Future maintenance costs associated with the open portion of drain between Station 1+032 and Station 1+930 shall continue to be as directed under the 28 January 2023 Gouin Drain & Branches (Little River Outlet) report.

Drawings and Specifications

Attached to this report is Schedule 'F', which are Specifications setting out the details of the recommended works and Schedule 'G' which represent the drawings that are attached to this report.

Page 1 of 7 – Watershed Plan

Page 2 of 7 – Profile

Page 3 of 7 – Bridge Extension Details

Page 4 of 7 – Bridge Details

Page 5 of 7 – Bridge No. 13 Enclosure Details

Page 5 of 7 – Future Bridge No. 13 Replacement Details

Page 6 of 7 – Miscellaneous Details

Approvals

The construction and/or improvement to a drainage works, including repair and maintenance activities, and all operations connected therewith are subject to the approval, inspection, by-laws and regulations of all Municipal, Provincial, Federal and other authorities having jurisdiction in respect to any matters embraced by the proposed works. Prior to any construction or maintenance works, the Municipality or proponent designated on the Municipality's behalf shall obtain all required approvals/permits and confirm any construction limitations including timing windows, mitigation/off-setting measures, standard practices or any other limitations related to in-stream works. No work shall be permitted during the Restricted Activity timing windows for the protection of spawning fish and developing eggs and fry as determined by Fisheries and Oceans Canada, which is from March 15 to July 15.

Respectfully submitted,

DILLON CONSULTING LIMITED

Mark D. Hernandez, P.Eng.
MDH:wlb:lld



MEETING SUMMARY



Subject: Gouin Drain (NextStar Culverts)
 Date: March 21, 2023, 2:30pm
 Location: Virtual Call
 Our File: 22-5108

Attendees

Patrick Winter	City of Windsor
Tom Graziano	City of Windsor
Mark Hernandez	Dillon Consulting Limited
Oliver Moir	Dillon Consulting Limited
Kyle Savoie	Stellantis
Ryan Oswald	Stellantis
Concetta Cafueri	Landowner
Troy Meloche	Landowner

Notes

Item	Discussion	Action by
1.	Dillon introduced the staff from Dillon and the City.	INFO.
2.	<p>An overview of the project was provided:</p> <ul style="list-style-type: none"> - The City of Windsor submitted a request under Section 78 of the Drainage Act for the installation of two new culverts over the Gouin Drain. - These culverts are to support the development of the battery plant which is currently under construction, located west of Banwell Road and south of EC Row Avenue East. - One culvert is to be located immediately upstream of the Jamieson Inc. access culvert and will provide access to a future Enbridge Gas station as well as allow for EC Row Avenue to be terminated as a cul-de-sac. - The other culvert is to be located immediately downstream of the Banwell Road culvert and is considered an enclosure that will allow for the better use of the battery plant land. - The culverts will be designed to maintain the level of service the drain is designed for, considering the type of access. - All costs of the project are expected to be assessed to the City of Windsor, and no costs will be assessed to upstream landowners 	INFO.
3.	It was identified that the on-site meeting is for discussing the NextStar battery plant culverts under the current City of Windsor appointment, and this differs from the Town of Tecumseh appointment for the repair and improvement of the Gouin Drain.	INFO.

Item	Discussion	Action by
4.	<p>The anticipated schedule of the report was discussed:</p> <ul style="list-style-type: none"> - Approval from the Department of Fisheries and Oceans (DFO) for the approval of the proposed works is anticipated to be approximately two months from submission of the DFO application. DFO is anticipated to have concerns over the length of the enclosure. - Once DFO approval is obtained, the finalized report will be circulated for comments before being submitted to the City of Windsor Council. - Statutory board meetings for approval of a drainage report (Meeting to Consider & Court of Revision) requires a minimum of two months. - Stellantis indicated that the Enbridge access culvert was critical to the timeline. 	INFO.

Errors and/or Omissions

These minutes were prepared by Oliver Moir, P.Eng who should be notified of any errors and/or omissions.

"SCHEDULE B"
SCHEDULE OF ALLOWANCES
GOUIN DRAIN (NextStar Energy Access Bridges)
CITY OF WINDSOR

Roll No.	Con.	Description	Owner	Section 30 Damages	Section 29 Land	Total Allowances
070-650-01303	3	Pt. Lot 138 & 139	2823351 Ontario Ltd	\$0.00	\$2,000.00	\$2,000.00
TOTAL ALLOWANCES				\$0.00	\$2,000.00	\$2,000.00

"SCHEDULE C"
SCHEDULE OF ASSESSMENT
GOUIN DRAIN (NextStar Energy Access Bridges)
CITY OF WINDSOR

MUNICIPAL LANDS:

Description	Area Affected		Owner	Special Benefit	Benefit	Outlet	Total Assessment
	(Acres)	(Ha.)					
090-040-00103 3 Pt. Lots 139-141	102.46	41.46	City of Windsor	\$1,341,180.00	\$0.00	\$0.00	\$1,341,180.00
090-040-03403 3 Pt. Lot 139	7.55	3.06	City of Windsor	\$258,420.00	\$0.00	\$0.00	\$258,420.00
Total on Municipal Lands.....				\$1,599,600.00	\$0.00	\$0.00	\$1,599,600.00
TOTAL ASSESSMENT (City of Windsor)				\$1,599,600.00	\$0.00	\$0.00	\$1,599,600.00

	(Acres)	(Ha.)
Total Area	-----	-----
City of Windsor:	110.01	44.52

"SCHEDULE D"
DETAILS OF SPECIAL BENEFIT
GOUIN DRAIN (NextStar Energy Access Bridges)
CITY OF WINDSOR

SPECIAL BENEFIT ASSESSMENT
(MUNICIPAL LANDS)

Roll No.	Owner	Item Description	Estimated Cost	Cost of Report	Special Benefit
090-040-00103	City of Windsor	Cost of Bridge No. 13A and Bridge No. 13 (future replacement) as well as drain widening and associated works (100%)	\$1,287,100.00	\$54,080.00	\$1,341,180.00
090-040-03403	City of Windsor	<u>Bridge No. 7 Extension</u> (100%)	\$248,000.00	\$10,420.00	\$258,420.00
Total Special Benefit Assessment (Non - Agricultural Lands).....			\$1,535,100.00	\$64,500.00	\$1,599,600.00
OVERALL TOTAL SPECIAL BENEFIT ASSESSMENT					\$1,599,600.00

”SCHEDULE F”
DRAINAGE REPORT FOR THE
GOUIN DRAIN (NEXTSTAR ENERGY ACCESS BRIDGES)
IN THE CITY OF WINDSOR
SPECIAL PROVISIONS - GENERAL

1.0 GENERAL SPECIFICATIONS

The General Specifications attached hereto is part of “Schedule F.” It also forms part of this specification and is to be read with it, but where there is a difference between the requirements of the General Specifications and those of the Special Provisions which follow, the Special Provisions will take precedence.

2.0 DESCRIPTION OF WORK

The work to be carried out under this Contract includes, but is not limited to, the supply of all **labour, equipment, and materials** to complete the following items:

- Bridge works, as follows:
 - Bridge No. 7 Extension – Jamieson Laboratories Ltd. (Roll No. 070-650-01351), City of Windsor (Roll No. 090-040-03403) & City of Windsor Road Authority (Anchor Drive) – Remove existing stone erosion protection from east end wall of existing bridge and dispose of off-site. Supply and installation of a new 45.5 m long, 1800 mm diameter reinforced concrete pipe, Class 65-D complete with full compacted Granular ‘A’ bedding and backfill up to 300 mm above the top of pipe (approximately 1,320 tonnes) providing a 6 m wide driveable top width. Supply and install a concrete block headwall consisting of 1200 mm x 600 mm x 600 mm interlocking concrete blocks (approximately 20 full blocks and 4 half blocks), designed and sealed by a professional engineer licensed in the province of Ontario and 1 m wide stone erosion protection on drain banks (approximately 25 m²). Connect new culvert to existing 2440 mm x 1600 mm corrugated steel pipe arch with a poured in place concrete collar. Supply and placement of topsoil (100 mm thickness). Restore all disturbed areas with fine grading and seeding. The work is to include drain bottom cleanout in close proximity to the bridge as necessary, site cleanup and restoration within the working area. This work also includes the supply and installation of a 600 mm x 600 mm ditch inlet catch basin with 2H:1V grate complete with 300 mm diameter PVC DR 35 leader pipe connected to culvert pipe and a 600 x 600 mm concrete catch basin complete with cast iron frame and grate. The Contractor shall extend the private roadside swale as shown on the drawings attached herein. This work includes the supply and placement of a stone erosion protection apron around the ditch inlet catch basin with stone erosion protection as well as placement of topsoil over newly excavated and disturbed banks (minimum 50 mm thickness), fine graded and seeded.
 - Bridge No. 11 – City of Windsor (Roll No. 090-040-00103) – Removal and disposal off-site of existing 7.9 m long, 2240 x 1630 mm CSPA with bagged concrete end walls.

- Bridge No. 12 – City of Windsor (Roll No. 090-040-00103) – Removal and disposal off-site of existing 7.6 m long, 2240 x 1630 mm CSPA with bagged concrete end walls.
- Bridge No. 13A – City of Windsor (Roll No. 090-040-00103) – Remove and dispose of existing west side bagged concrete end wall on Bridge No. 13 at Banwell Road. Supply and installation of a new 178 m long, 2250 mm diameter reinforced concrete pipe, Class 65-D complete with prefabricated 16 degree bend and fully compacted Granular ‘A’ backfill up to minimum 300 mm above the top of pipe (approximately 4,900 tonnes) with placement of topsoil over top (minimum 100 mm thickness), fine graded and seeded. Connect the new prefabricated 16 degree bend to the existing 2240 mm x 1630 mm corrugated steel pipe arch with a poured in place concrete collar. Supply and installation of a concrete block headwall consisting of 1200 mm x 600 mm x 600 mm interlocking concrete blocks (approximately 19 full blocks and 6 half blocks), designed and sealed by a professional engineer licensed in the province of Ontario and 1 m wide stone erosion protection on drain banks (approximately 25 m²). The work is to include drain bottom cleanout in close proximity to the bridge, site cleanup and restoration within the working area. Replace existing 450 mm diameter CSP from south with new 450 mm diameter HDPE pipe, 12 m long complete with 60 degree bend (Boss 2000 as manufactured by Armtec or approved equal) and connect to the new 2250 mm diameter pipe with a pre-fabricated ‘tee’. Extend roadside ditch to new pipe inlet. This work also includes the supply and installation of 600 mm x 600 mm concrete catch basin complete with cast iron frames and grates along the enclosure (approximately 15 required). Work includes salvage and replacement of stone erosion protection, as well as restoration. Restore all disturbed areas with fine grading and seeding.
- Widening of the drain from the south side between Station 1+032 and Station 1+930 (approximately 2200 m³), complete with placement of topsoil over new and disturbed drain banks (minimum 50 mm thickness), fine graded and seeded (bonded fibre matrix). Excavated material shall be trucked to, or remain on the lands owned by the City of Windsor (property Roll No. 090-040-00103 & 090-040-03403) for use by the landowner. This work includes re-establishment of swale inlets with salvaged or new stone erosion protection, as encountered on the south bank (approximately 4 inlets), as well as re-seeding of 1 metre wide grassed buffer.
- Supply and installation of rock check dam as per OPSD 219.211 and refuge stilling pool as per OPSD 219.220 lined with stone erosion protection on filter fabric underlay directly upstream of Bridge No. 5 (Anchor Drive). The work shall include trucking excavated materials off-site following completion of the work.
- Temporary Silt Control Measures During Construction

3.0 ACCESS TO THE WORK

Access to the drain shall be from E.C. Row Avenue East. Through traffic must be maintained at all times along municipal roads with the required traffic control as per Section 13.0 in the General Specifications. All construction materials for the bridge are to be placed on the field side of the road side drains. Any damage resulting from the Contractor's access to the bridge site shall be rectified to pre-existing conditions at the Contractor's expense.

All road areas, grass lawn areas and fence lines disturbed shall be restored to original conditions at the Contractor's expense. The Contractor shall make his/her own arrangements for any additional access for his/her convenience.

4.0 WORKING AREA

The working area at the bridge sites shall be restricted to the E.C. Row Avenue East right-of-way and a 10.0 m wide corridor centred over the proposed centreline of the new culvert.

Any damages to lands and/or roads from the Contractor's work shall be rectified to pre-existing conditions at his/her expense.

5.0 BRIDGE CONSTRUCTION

5.1 Location of New Bridges

Bridge No. 7 extension shall be constructed immediately upstream of, and attached to the existing Bridge No. 7. Bridge No. 13A shall be constructed immediately downstream of, and attached to the existing Bridge No. 13 (Banwell Road). Both bridges shall be constructed in accordance with the specifications and drawings attached hereto.

5.2 Removal of Existing Bridges

The Contractor shall exercise caution when removing Bridge No. 11 and Bridge No. 12 as to minimize damage to the drain banks. Any damage to the drain shall be restored to original conditions at the expense of the Contractor. The removed materials (existing culvert debris and end wall materials) shall be hauled away off-site.

5.3 Materials for New Bridges

Materials shall be as follows:

<i>Culvert Pipe</i>	Bridge No. 7: <i>New 45.5 m long, 1800 mm diameter reinforced concrete pipe Class 65-D manufactured in accordance with CSA-A257.2.</i>
	Bridge No. 13A: <i>New 178 m long, 2250 mm diameter reinforced concrete pipe Class 65-D manufactured in accordance with CSA-A257.2.</i>
	Bridge No. 13 (Future Maintenance): <i>New 16 m long, 2250 mm diameter reinforced concrete pipe Class 65-D manufactured in accordance with CSA-A257.2.</i>
<i>Pipe Bedding Below Pipe</i>	<i>Granular 'A' conforming to OPSS Division 10.</i>
<i>Backfill 300 mm min. above Pipe</i>	<i>Granular 'A' conforming to OPSS Division 10.</i>

<i>Driveway Surface</i>	<i>Granular 'A' conforming to OPSS Division 10.</i>
<i>Enclosure Surface Materials</i>	<i>100 mm minimum top soil, fine graded and seeded</i>
<i>Vertical End Wall</i>	<i>Vertical interlocking concrete blocks (600 mm x 600 mm x 1200 mm size) specified herein.</i>
<i>Erosion Stone</i>	<i>All stone to be used for erosion protection shall be 125 - 250 mm clear quarried rock or OPSS.Muni 1004, minimum 300 mm thickness.</i>
<i>Filter Fabric</i>	<i>"Non-Woven" geotextile filter fabric with a minimum strength equal to or greater than Terrafix 270R, Amoco 4546, Mirafi 140NC or approved equivalent.</i>

5.4 Culvert Installation

Suitable dykes shall be constructed in the drain so that the installation of the pipe can be accomplished in the dry. The drain bottom shall be cleaned, prepared, shaped and compacted to suit the new culvert configuration, as shown on the drawings. Granular materials shall be compacted to 100% of their maximum dry density; imported clean native materials shall be supplied, placed and compacted to 95% of their maximum dry density.

5.5 Interlocking Concrete Block End Walls

End walls shall be constructed of interlocking concrete blocks, as shown on the drawings. The size of the blocks shall measure 600 mm wide, 600 mm high and 1200 mm long, and shall be plain chamfered concrete style blocks, with a minimum 20 MPa strength, with 6% air entrainment, as manufactured by Lock-Block Ltd. or approved equal. The concrete block end walls shall be set on a minimum 10:1 batter and shall be keyed into the existing drain banks and provided with a filter fabric backing to separate the backfill materials from the concrete blocks. The void space between the pipe culvert and the concrete blocks shall be formed and filled with minimum 30 MPa concrete.

Concrete block end walls shall be designed by a Professional Engineer licensed in the Province of Ontario and submitted to the Engineer for review for approval.

5.6 Granular 'A' Driveway

The Contractor shall construct the driveway with a longitudinal grade 2% – 10% approach over the new culvert providing a minimum cover as specified on the drawings. This work includes the installation of a minimum 200 mm thickness of compacted Granular 'A' (crushed limestone) surface. The minimum top width of the driveway shall be as shown on the drawings.

5.7 Asphalt Road Restoration

The Contractor shall restore the road providing a minimum cover as per OPSD 805.020. This work includes 100 mm asphalt surface (40 mm HL4 surface asphalt and 60 mm HL8 base asphalt) over a compacted Granular 'A' surface.

5.8 Lateral Tile Drains

Should the Contractor encounter any lateral tiles within the proposed culvert limits not shown on attached drawings, the Contractor shall connect the outlet tile drain(s) to the new drain pipe in consultation with the Drainage Superintendent, as required.

Known lateral tile drains identified on the drawings shall be connected to the new drain through a pre-fabricated 'Tee'

Care must be taken in handling plastic drain pipe in cold weather to avoid causing damage. Plastic drain pipe shall be held in position on planned grade immediately after installation by careful placement of backfill material.

5.9 Culvert Connection

The Contractor shall connect the existing pipes to the new reinforced concrete pipe using a concrete collar. The Contractor shall install a poured in place concrete collar to join the two (2) pipe ends as shown on the Drawings. The pipes shall be butted together and wrapped with filter fabric (Terrafix 270R or approved equal) a minimum 1.0 m wide prior to pouring the concrete collar. All concrete used to construct the concrete collar shall have a minimum compressive strength of 30 MPa in 28 days. The Contractor shall prepare the form work and false work within the drain as required that facilitates the placement of the concrete within the minimum coverage thicknesses surrounding the pipe joint as specified on the Drawings. The concrete needs to be vibrated to fill in all voids. The Contractor shall avoid backfilling the new concrete collar until the concrete has had a minimum three (3) days to cure.

5.10 Site Cleanup and Restoration

As part of the work and upon completion, the Contractor shall remove and dispose of, off-site any loose timber, logs, stumps, large stones, rubber tires, cinder blocks or other debris from the drain bottom and from the side slopes. Where the construction works cross a lawn, the Contractor shall take extreme care to avoid damaging the lawn, shrubs and trees encountered. Upon completion of the work, the Contractor shall completely restore the area by the placement and fine grading of topsoil and seeding or sodding the area as specified by the Engineer or Drainage Superintendent.

6.0 NEW CATCH BASINS

The Contractor shall arrange for the supply and installation of pre-cast concrete catch basins as specified on the drawings shown. Grate elevations of catch basins shall be set to allow proper grading and drainage of surrounding lands and to the satisfaction of the Drainage Superintendent and/or Engineer.

The Contractor shall install all precast structures plumb and true to line and grade. Precast bases shall be set to the specified grade, shall be level, and shall have uniform overall contact with the underlying soil.

All catch basins installed shall meet the dimensions and locations outlined in the drawings and in accordance with City of Windsor Drawing AS-309A and conform to the requirements of Ontario Provincial Standard Specification (OPSS 1351). Frame and grates installed shall be in accordance with City of Windsor Drawing AS-301.

Leader pipes of the catch basins shall be connected to the new concrete pipe drain centered about the drain pipe springline, with a minimum pipe grade of 1%. New catch basin leads shall be PVC DR 35. Approved native material may be used in areas outside of the roadways.

Leader pipes shall connect to the drain pipe with pre-fabricated 'tees'.

All parging material shall be hydraulic cement. Portland cement mixes will not be allowed.

Pipe placed in the catch basin walls for inlet and outlet connections shall extend through the wall a sufficient distance to allow for connections. The pipes shall be trimmed flush with the inside wall and shall be securely sealed into place using grout.

All catch basins shall be manufactured to allow for a minimum 150 mm height of riser adjustment rings to permit elevation modifications if required.

Catch basins shall be backfilled with clean native material in maximum 300 mm lifts and compacted to 98% of the maximum standard proctor density.

6.1 Ditch Inlet Catch Basin (Bridge No. 7 Extension)

The ditch inlet catch basin shall be as per OPSD 705.03 and be supplied with a steel honeycomb grate as per OPSD 403.01 with a 2:1 side slope. The inlet elevation and location shall be as specified on the Drawings.

7.0 HYDRAULIC SEEDING OF GRASS BUFFER & ENCLOSURE

All new drain banks and existing grassed areas disturbed by construction shall be hydraulic mulch seeded as specified herein. The existing ground surface to be seeded shall be loosened to a depth of 26 mm and shall be rendered uniformly loose for that 25 mm depth. The surface shall be predominantly fine and free from weeds and other unwanted vegetation. All other loose surface litter shall be removed and disposed of.

Hydraulic mulch shall consist of finely ground cellulose pulp derived from recycled newsprint and shall be dyed green. Its fiber consistency shall be approximately 60% fine fiber with the balance being paper particles, 40% of which shall be a diameter of 3 mm minimum and 6 mm maximum. Hydraulic mulch shall be applied at 2,000 kg per 10,000 m². Clean water shall be applied at 42,700 liters per 10,000 m².

Seeding and mulching shall be a one step process in which the seed, fertilizer and hydraulic mulch are applied simultaneously in a water slurry via the hydraulic seeder/mulcher. The materials shall be added to the supply tank while it is being loaded with water. The materials shall be thoroughly mixed into a homogeneous water slurry and shall be distributed uniformly over the prepared surface. The materials shall be measured by mass or by a mass-calibrated volume measurement, acceptable to the Drainage Superintendent.

The hydraulic seeder/mulcher shall be equipped with mechanical agitation equipment capable of mixing the materials into a homogenous state until applied. The discharge pumps and gun nozzles shall be capable of applying the material uniformly.

Grass seed shall be Canada No. 1 grass seed mixture meeting the requirements of a Waterway Slough Mixture as supplied by Growmark or approved equal, as follows:

<i>Creeping Red Fescue</i>	20%
<i>Meadow Fescue</i>	30%
<i>Tall Fescue</i>	30%
<i>Timothy</i>	10%
<i>White Clover</i>	10%

Bags shall bear the label of the supplier indicating the content by species, grade and mass. Seed shall be applied at a rate of 200 kg per 10,000 m².

Fertilizer shall be 8-32-16 applied at 350 kg per 10,000 m². It shall be in granular form, dry, free from lumps and in bags bearing the label of the manufacturer, indicating mass and analysis.

The hydraulic seeding shall be deemed "Completed by the Contractor" when the seed has established in all areas to the satisfaction of the Engineer. Re-seeding and/or other methods required to establish the grass will be given consideration to achieve the end result and the costs shall be incidental to the works.

8.0 HYDRAULIC SEEDING OF DRAIN BANKS

The newly established drain banks and all existing grassed areas disturbed by construction shall be hydraulic mulch seeded as specified herein. The surface shall be predominantly fine and free from weeds and other unwanted vegetation. All other loose surface litter shall be removed and disposed of.

Bonded Fibre Matrix shall consist of thermally refined wood fibers and 10% cross-linked hydro-colloidal tackifiers. It should be 100% biodegradable. The curing period shall be not more than 48 hours. Bonded Fibre Matrix shall be hydraulically applied and after application be capable of adhering to the soil. In a dry state, shall be comprised of not less than 70% by weight of long, stranded wood fibres held together by organic or mineral bonding agents or both.

Bonded Fibre Matrix shall be applied at a minimum rate of 3,700 kg of dry product per 10,000 m². It shall be thoroughly mixed with water in a hydraulic seeder and mulcher at a rate of 20-30 kg of dry product to 500-600 litres of water to form a homogeneous slurry. Refer to OPSS.PROV 804 for specifications.

Seeding and mulching shall be a one step process in which the seed, fertilizer and hydraulic mulch are applied simultaneously in a water slurry via the hydraulic seeder/mulcher. The materials shall be added to the supply tank while it is being loaded with water. The materials shall be thoroughly mixed into a homogeneous water slurry and shall be distributed uniform, cohesive mat over the prepared surface. The materials shall be measured by mass or by a mass-calibrated volume measurement, acceptable to the Drainage Superintendent.

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The hydraulic seeding shall be deemed "Completed by the Contractor" when the seed has established in all areas to the satisfaction of the Engineer. Re-seeding and/or other methods required to establish the grass will be given consideration to achieve the end result and the costs shall be incidental to the works.

9.0 ROCK CHECK DAM

Rock check dam shall be installed upstream of Bridge No. 5 prior to commencing construction. The location and exact dimensions of the rock check dams will be confirmed with the Drainage Superintendent prior to their installation. Installation shall be in accordance with OPSD 219.211 with the modifications to size as discussed with the Drainage Superintendent.

The rock check dam will not be removed until as directed by the Drainage Superintendent.

10.0 REFUGE STILLING POOL

The Contractor shall construct a refuge stilling pool in the bottom of the open drain directly upstream of Bridge No. 5 in accordance with OPSD 219.220. The contractor shall excavate the pool in the drain bottom to enhance fish habitat. The pool shall have a length of 4 metres, a bottom width of 1.0 metres with 1:1 side slopes and a depth below design grade of 300 mm. A stone rip-rap lining, countersunk and 200 mm thick with filter fabric underlay, shall be placed in the bottom. The pool shall be centred on the finished bottom width of the drain as specified herein. Material excavated from the pool shall be disposed of in the same manner as all other material excavated from the channel bottom.

11.0 ROADSIDE DITCH WORK

The Contractor shall replace the existing corrugated steel pipe located on the west side of Banwell Road with a new 450 mm diameter high density polyethylene (HDPE) Boss 2000 pipe (as manufactured by Armtex, or approved equal) complete with a prefabricated 60 degree bend connected to prefabricated 'tee' as specified on the drawings attached herein. The Contractor shall also extend the existing roadside ditch to the new pipe inlet and reinstall salvaged stone erosion protection.

GENERAL SPECIFICATIONS

1.0 AGREEMENT AND GENERAL CONDITIONS

The part of the Specifications headed "Special Provisions" which is attached hereto forms part of this Specification and is to be read with it. Where there is any difference between the requirements of this General Specification and those of the Special Provisions, the Special Provisions shall govern.

Where the word "Drainage Superintendent" is used in this specification, it shall mean the person or persons appointed by the Council of the Municipality having jurisdiction to superintend the work.

Tenders will be received and contracts awarded only in the form of a lump sum contract for the completion of the whole work or of specified sections thereof. The Tenderer agrees to enter into a formal contract with the Municipality upon acceptance of the tender. The General Conditions of the contract and Form of Agreement shall be those of the Stipulated Price Contract CCDC2-Engineers, 1994 or the most recent revision of this document.

2.0 EXAMINATION OF SITE, PLANS AND SPECIFICATIONS

Each tenderer must visit the site and review the plans and specifications before submitting his/her tender and must satisfy himself/herself as to the extent of the work and local conditions to be met during the construction. Claims made at any time after submission of his/her tender that there was any misunderstanding of the terms and conditions of the contract relating to site conditions, will not be allowed. The Contractor will be at liberty, before bidding to examine any data in the possession of the Municipality or of the Engineer.

The quantities shown or indicated on the drawings or in the report are estimates only and are for the sole purpose of indicating to the tenderers the general magnitude of the work. The tenderer is responsible for checking the quantities for accuracy prior to submitting his/her tender.

3.0 MAINTENANCE PERIOD

The successful Tenderer shall guarantee the work for a period of one (1) year from the date of acceptance thereof from deficiencies that, in the opinion of the Engineer, were caused by faulty workmanship or materials. The successful Tenderer shall, at his/her own expense, make good and repair deficiencies and every part thereof, all to the satisfaction of the Engineer. Should the successful Tenderer for any cause, fail to do so, then the Municipality may do so and employ such other person or persons as the Engineer may deem proper to make such repairs or do such work, and the whole costs, charges and expense so incurred may be deducted from any amount due to the Tenderer or may be collected otherwise by the Municipality from the Tenderer.

4.0 GENERAL CO-ORDINATION

The Contractor shall be responsible for the coordination between the working forces of other organizations and utility companies in connection with this work. The Contractor shall have no cause of action against the Municipality or the Engineer for delays based on the allegation that the site of the work was not made available to him by the Municipality or the Engineer by reason of the acts, omissions, misfeasance or non-feasance of other organizations or utility companies engaged in other work.

5.0 RESPONSIBILITY FOR DAMAGES TO UTILITIES

The Contractor shall note that overhead and underground utilities such as hydro, gas, telephone and water are not necessarily shown on the drawings. It is the Contractor's responsibility to contact utility companies for information regarding utilities, to exercise the necessary care in construction operations and to take other precautions to safeguard the utilities from damage. All work on or adjacent to any utility, pipeline, railway, etc., is to be carried out in accordance with the requirements of the utility, pipeline, railway, or other, as the case may be, and its specifications for such work are to be followed as if they were part of this specification. The Contractor will be liable for any damage to utilities.

6.0 CONTRACTOR'S LIABILITY

The Contractor, his/her agents and all workmen or persons under his/her control including sub-contractors, shall use due care that no person or property is injured and that no rights are infringed in the prosecution of the work. The Contractor shall be solely responsible for all damages, by whomsoever claimable, in respect to any injury to persons or property of whatever description and in respect of any infringement of any right, privilege or easement whatever, occasioned in the carrying on of the work, or by any neglect on the Contractor's part.

The Contractor, shall indemnify and hold harmless the Municipality and the Engineer, their agents and employees from and against claims, demands, losses, costs, damages, actions, suits, or proceedings arising out of or attributable to the Contractor's performance of the contract.

7.0 PROPERTY BARS AND SURVEY MONUMENTS

The Contractor shall be responsible for marking and protecting all property bars and survey monuments during construction. All missing, disturbed or damaged property bars and survey monuments shall be replaced at the Contractor's expense, by an Ontario Land Surveyor.

8.0 MAINTENANCE OF FLOW

The Contractor shall, at his/her own cost and expense, permanently provide for and maintain the flow of all drains, ditches and water courses that may be encountered during the progress of the work.

9.0 ONTARIO PROVINCIAL STANDARDS

Ontario Provincial Standard Specifications (OPSS) and Ontario Provincial Standard Drawings (OPSD) shall apply and govern at all times unless otherwise amended or extended in these Specifications or on the Drawing. Access to the electronic version of the Ontario Provincial Standards is available online through the MTO website, free of charge to all users. To access the electronic standards on the Web go to <http://www.mto.gov.on.ca/english/transrd/>. Under the title Technical Manuals is a link to the Ontario Provincial Standards. Users require Adobe Acrobat to view all pdf files.

10.0 APPROVALS, PERMITS AND NOTICES

The construction of the works and all operations connected therewith are subject to the approval, inspection, by-laws and regulations of all Municipal, Provincial, Federal and other authorities having jurisdiction in respect to any matters embraced in this Contract. The Contractor shall obtain all approvals and permits and notify the affected authorities when carrying out work in the vicinity of any public utility, power, underground cables, railways, etc.

11.0 SUBLETTING

The Contractor shall keep the work under his/her personal control, and shall not assign, transfer, or sublet any portion without first obtaining the written consent of the Municipality.

12.0 TIME OF COMPLETION

The Contractor shall complete all work on or before the date fixed at the time of tendering. The Contractor will be held liable for any damages or expenses occasioned by his/her failure to complete the work on time and for any expenses of inspection, superintending, re-tendering or re-surveying, due to their neglect or failure to carry out the work in a timely manner.

13.0 TRAFFIC CONTROL

The Contractor will be required to control vehicular and pedestrian traffic along roads at all times and shall, at his/her own expense, provide for placing and maintaining such barricades, signs, flags, lights and flag persons as may be required to ensure public safety. The Contractor will be solely responsible for controlling traffic and shall appoint a representative to maintain the signs and warning lights at night, on weekends and holidays and at all other times that work is not in progress. All traffic control during construction shall be strictly in accordance with the **Occupational Health and Safety Act** and the current version of the **Ontario Traffic Manuals**. Access to the electronic version of the **Ontario Traffic Manual** is available online through the MTO website, free of charge to all users. To access the electronic standards on the Web go to <http://www.mto.gov.on.ca/english/transrd/>, click on "Library Catalogue," under the "Title," enter "Ontario Traffic Manual" as the search. Open the applicable "Manual(s)" by choosing the "Access Key," once open look for the "Attachment," click the pdf file. Users require Adobe Acrobat to view all pdf files.

Contractors are reminded of the requirements of the Occupational Health and Safety Act pertaining to Traffic Protection Plans for workers and Traffic Control Plan for Public Safety.

14.0 SITE CLEANUP AND RESTORATION

As part of the work and upon completion, the Contractor shall remove and dispose of, off-site any loose timber, logs, stumps, large stones, rubber tires, cinder blocks or other debris from the drain bottom and from the side slopes. Where the construction works cross a lawn, the Contractor shall take extreme care to avoid damaging the lawn, shrubs and trees encountered. Upon completion of the work, the Contractor shall completely restore the area by the placement and fine grading of topsoil and seeding or sodding the area as specified by the Engineer or Drainage Superintendent.

15.0 UTILITY RELOCATION WORKS

In accordance with Section 26 of the Drainage Act, if utilities are encountered during the installation of the drainage works that conflict with the placement of the new culvert, the operating utility company shall relocate the utility at their own costs. The Contractor however will be responsible to co-ordinate these required relocations (if any) and their co-ordination work shall be considered incidental to the drainage works.

16.0 FINAL INSPECTION

All work shall be carried out to the satisfaction of the Drainage Superintendent for the Municipality, in compliance with the specifications, drawings and the Drainage Act. Upon completion of the project, the work will be inspected by the Engineer and the Drainage Superintendent.

Any deficiencies noted during the final inspection shall be immediately rectified by the Contractor. Final inspection will be made by the Engineer within 20 days after the Drainage Superintendent has received notice in writing from the Contractor that the work is completed, or as soon thereafter as weather conditions permit.

17.0 FISHERIES CONCERNS

Standard practices to be followed to minimize disruption to fish habitat include embedment of the culvert a minimum 10% below grade, constructing the work 'in the dry' and cutting only trees necessary to do the work (no clear-cutting). No in-water work is to occur during the timing window unless otherwise approved by the appropriate authorities.



PLAN NORTH

LEGEND

- MUNICIPAL BOUNDARY
- GOUIN DRAIN DRAINAGE AREA BOUNDARY
- GOUIN DRAIN
- OTHER DRAINS
- BRIDGE EXTENSION
- EXISTING BRIDGE
- BRIDGE REMOVAL (BY OTHERS)
- NEW ENCLOSURE
- LOCAL BENCHMARK
- WOODLOT

SITE BENCHMARKS

BM1 – TOP OF SOUTH END OF CONCRETE BOX CULVERT UNDER EC ROW EXPRESSWAY AT APPROX. STA. 0+980.

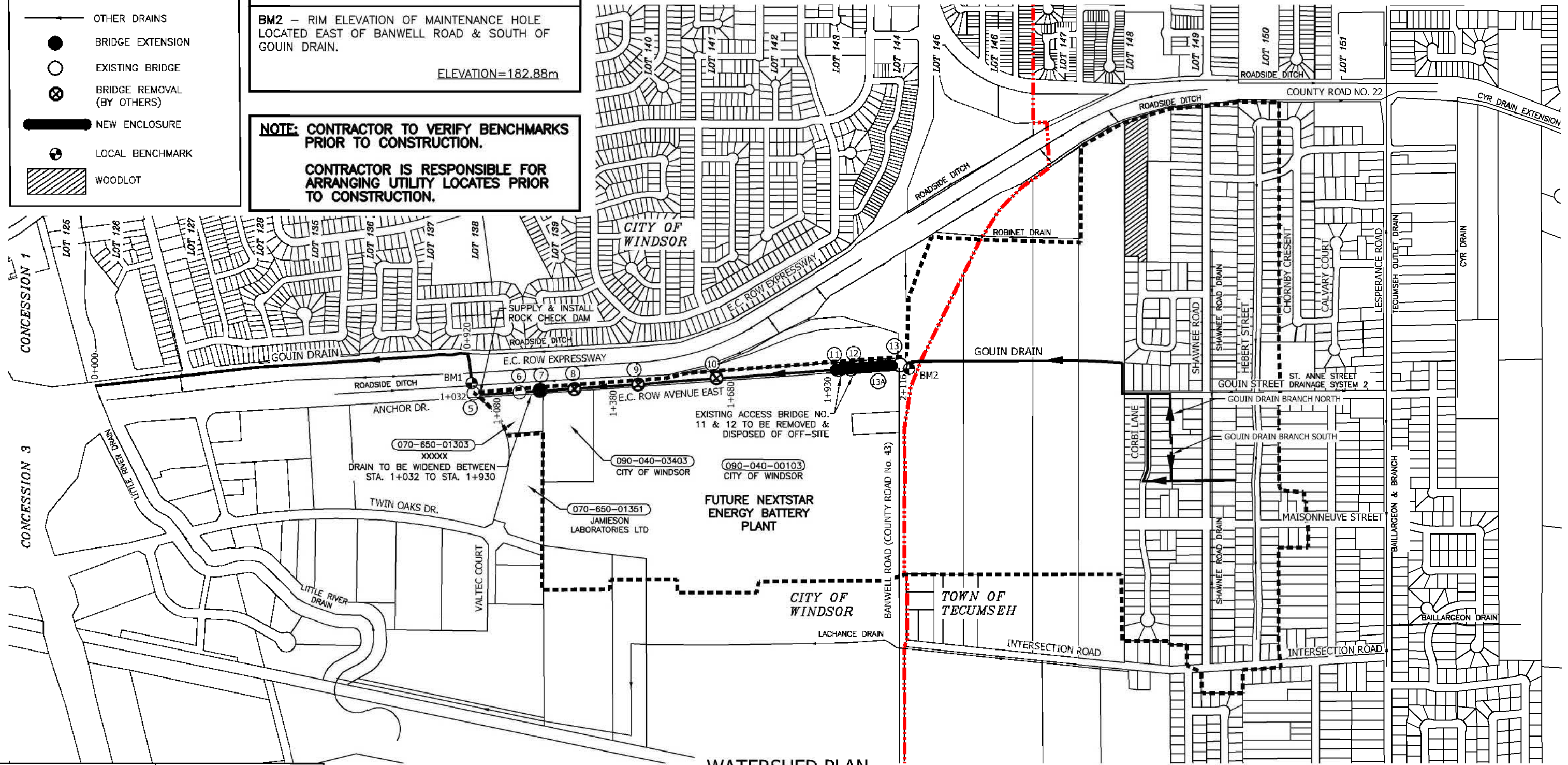
ELEVATION=181.18m

BM2 – RIM ELEVATION OF MAINTENANCE HOLE LOCATED EAST OF BANWELL ROAD & SOUTH OF GOUIN DRAIN.

ELEVATION=182.88m

NOTE: CONTRACTOR TO VERIFY BENCHMARKS PRIOR TO CONSTRUCTION.

CONTRACTOR IS RESPONSIBLE FOR ARRANGING UTILITY LOCATES PRIOR TO CONSTRUCTION.



WATERSHED PLAN
SCALE=1:10,000

'SCHEDULE G'

Drainage Report for the
GOUIN DRAIN
(NEXTSTAR ENERGY BRIDGES)
City of Windsor

WATERSHED PLAN

1 of 7

Conditions of Use

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No.	City Council Agenda - Monday, October 30, 2023	DATE	BY
3	FINAL REPORT SUBMISSION	SEP. 14/23	MDH
2	CLIENT REVIEW	AUG. 16/23	MDH
1	ERCA REVIEW	JULY 21/23	MDH

DESIGN	REVISED BY
OEM	TRO
DRAWN	CHECKED BY
WLB	LRO
DATE	September 11, 2023
SCALE	AS SHOWN

DILLON CONSULTING

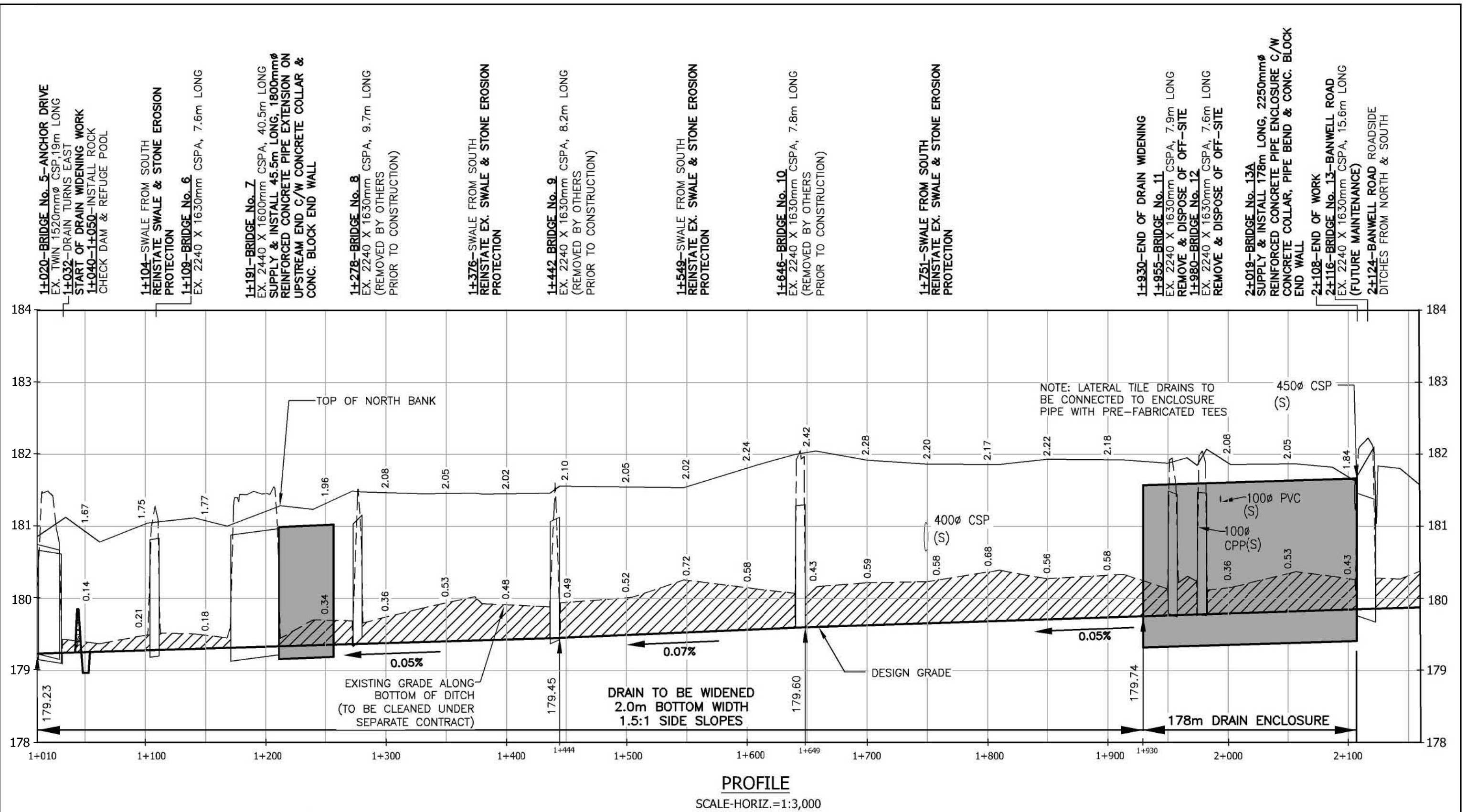
PROJECT NO. 22-5108

DRAWING SCALES BASED ON A 11" X 17" SHEET

SHEET TITLE	WATERSHED PLAN
PAGE NO.	1 of 7



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1+020-BRIDGE No. 5-ANCHOR DRIVE
EX. TWIN 1520mm CSP, 19m LONG
1+032-DRAIN TURNS EAST
START OF DRAIN WIDENING WORK
1+040-1+050-INSTALL ROCK
CHECK DAM & REFUGE POOL

1+104-SWALE FROM SOUTH
REINSTATE SWALE & STONE EROSION
PROTECTION
1+108-BRIDGE No. 6
EX. 2240 X 1630mm CSP, 7.6m LONG

1+191-BRIDGE No. 7
EX. 2440 X 1600mm CSP, 40.5m LONG
SUPPLY & INSTALL 45.5m LONG, 1800mmØ
REINFORCED CONCRETE PIPE EXTENSION ON
UPSTREAM END C/W CONCRETE COLLAR &
CONC. BLOCK END WALL

1+275-BRIDGE No. 8
EX. 2240 X 1630mm CSP, 9.7m LONG
(REMOVED BY OTHERS
PRIOR TO CONSTRUCTION)

1+376-SWALE FROM SOUTH
REINSTATE EX. SWALE & STONE EROSION
PROTECTION

1+442 BRIDGE No. 9
EX. 2240 X 1630mm CSP, 8.2m LONG
(REMOVED BY OTHERS
PRIOR TO CONSTRUCTION)

1+549-SWALE FROM SOUTH
REINSTATE EX. SWALE & STONE EROSION
PROTECTION

1+646-BRIDGE No. 10
EX. 2240 X 1630mm CSP, 7.8m LONG
(REMOVED BY OTHERS
PRIOR TO CONSTRUCTION)

1+751-SWALE FROM SOUTH
REINSTATE EX. SWALE & STONE EROSION
PROTECTION

1+930-END OF DRAIN WIDENING
1+955-BRIDGE No. 11
EX. 2240 X 1630mm CSP, 7.9m LONG
REMOVE & DISPOSE OF OFF-SITE
1+980-BRIDGE No. 12
EX. 2240 X 1630mm CSP, 7.6m LONG
REMOVE & DISPOSE OF OFF-SITE

2+019-BRIDGE No. 13A
SUPPLY & INSTALL 178m LONG, 2250mmØ
REINFORCED CONCRETE PIPE ENCLOSURE C/W
CONCRETE COLLAR, PIPE BEND & CONC. BLOCK
END WALL
2+108-END OF WORK
2+116-BRIDGE No. 13-BANWELL ROAD
EX. 2240 X 1630mm CSP, 15.6m LONG
(FUTURE MAINTENANCE)
2+124-BANWELL ROAD ROADSIDE
DITCHES FROM NORTH & SOUTH



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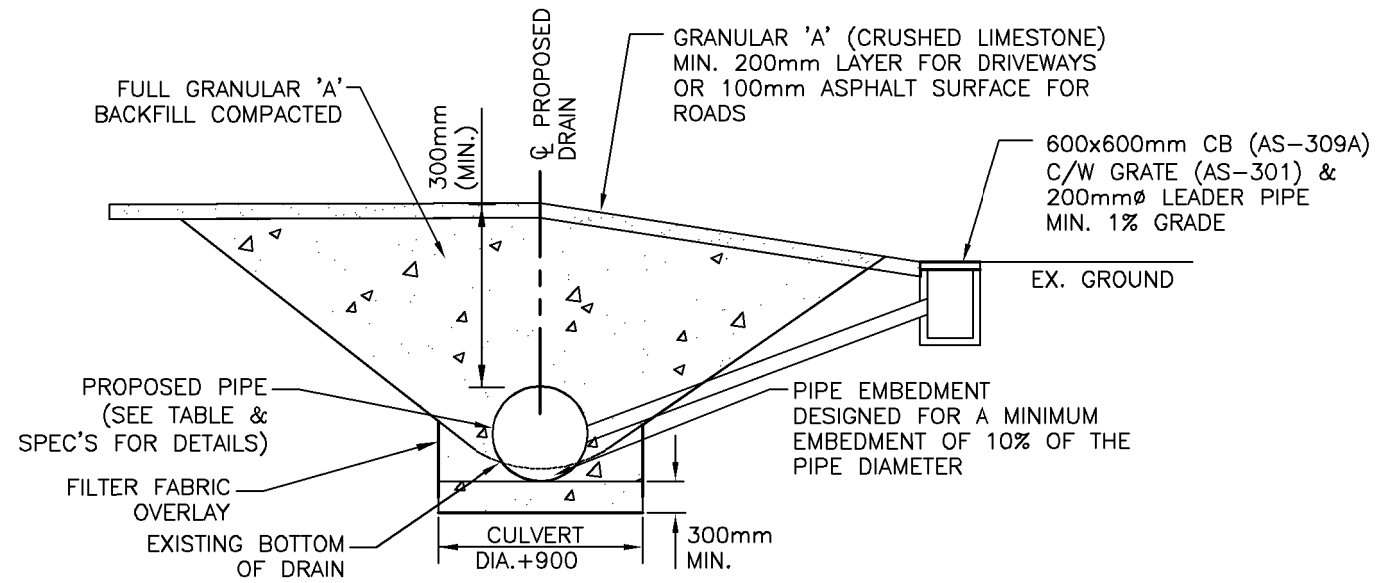
No.	City Council Agenda - Monday, October 30, 2023	DATE	BY
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1	ERCA REVIEW	JULY 21/23	MDH

DESIGN	OEM	REVIEWED BY	TRO
DRAWN	WLB	CHECKED BY	LRO
DATE	September 14, 2023		
SCALE	AS SHOWN		

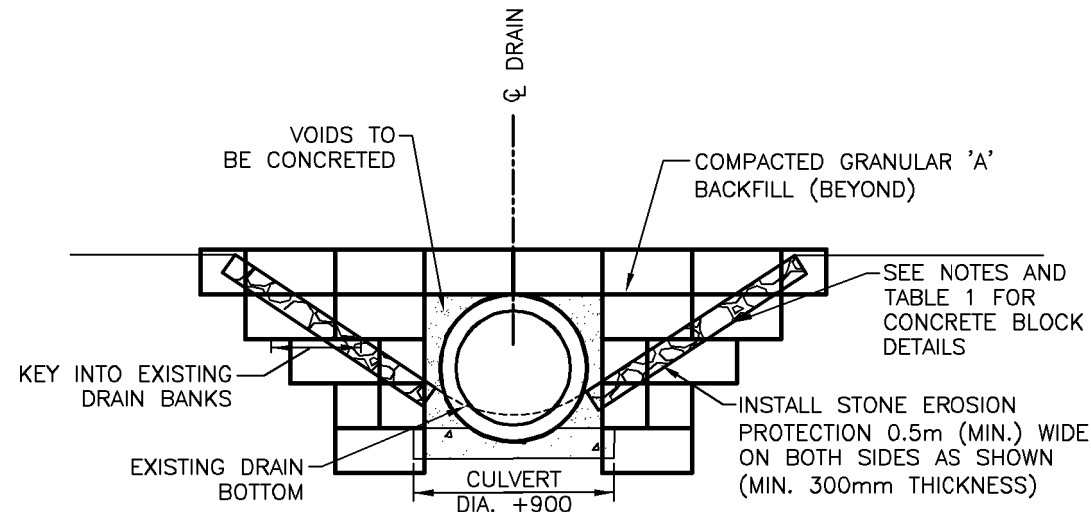
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PROJECT NO. 22-5108
DRAWING SCALES BASED ON A 11" X 17" SHEET

'SCHEDULE G' Drainage Report for the GOUIN DRAIN (NEXTSTAR ENERGY BRIDGES) City of Windsor	
SHEET TITLE PROFILE	
PAGE NO. 2 of 7	

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**CROSS SECTION
(BRIDGE NO. 7 EXTENSION)
NOT TO SCALE**



**TYPICAL CONCRETE BLOCK END WALL SECTION
(BRIDGE NO. 7 & 13A)
NOT TO SCALE**

NOTES:

- BLOCK END WALLS TO BE DESIGNED AND SEALED BY A PROFESSIONAL ENGINEER LICENSED IN THE PROVINCE OF ONTARIO
- THE CULVERT PIPE TO PROTRUDE A MINIMUM 150mm BEYOND THE LOWEST BLOCK ROW.
- FULL CONCRETE BLOCKS ARE 600x600x1200mm

TABLE 1 - ACCESS CULVERT DESIGN INFORMATION		
DESCRIPTION	BRIDGE No. 7 EXT.	BRIDGE No. 13A
PIPE INVERT ELEV. U/S SIDE(m)	179.15	179.41
PIPE INVERT ELEV. D/S SIDE(m)	179.12	179.32
TOP OF ϕ DRIVEWAY SURFACE ELEV. (m)	181.48	182.25
DRAIN BOTTOM (m) (DESIGN) (AT CENTRELINE OF CULVERT)	179.35	179.80
MIN. TOP WIDTH OF DRIVEWAY (m)	6.0	N/A
MIN. CULVERT GRADE (%)	0.05	0.05
CULVERT TYPE	CONCRETE	CONCRETE
CULVERT LENGTH (m)	45.5	178.0
PIPE SIZE (mm)	1800	2250
CULVERT ENDWALL TYPE	CONC. BLOCK	CONC. BLOCK
TOP ELEV. OF CONCRETE BLOCK ENDWALL	181.76	182.42
APPROX. NO. OF CONCRETE BLOCKS	20 FULL BLKS 4 HALF BLKS	19 FULL BLKS 6 HALF BLKS



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DRAWN	WLB	CHECKED BY	LRO
DATE	September 14, 2023		
SCALE	AS SHOWN		

DILLON CONSULTING

PROJECT NO. 22-5108

DRAWING SCALES BASED ON A 11" X 17" SHEET

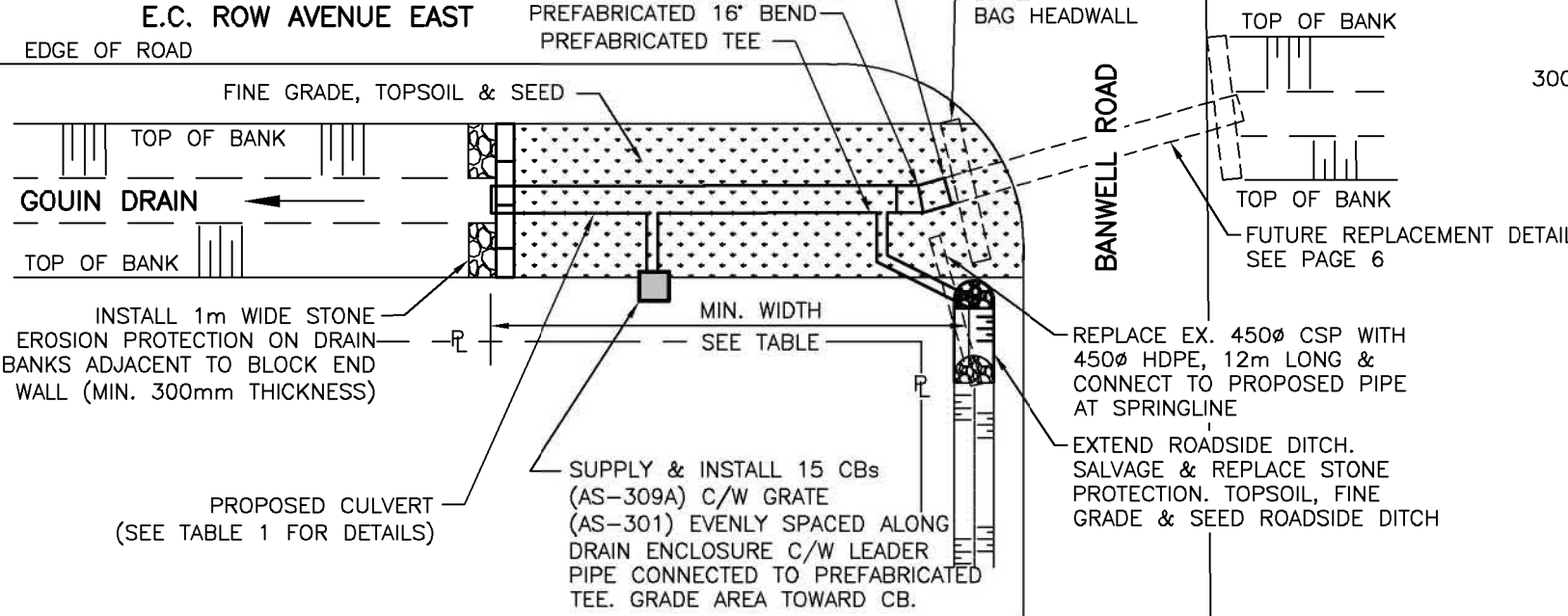
'SCHEDULE G'

Drainage Report for the
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City of Windsor

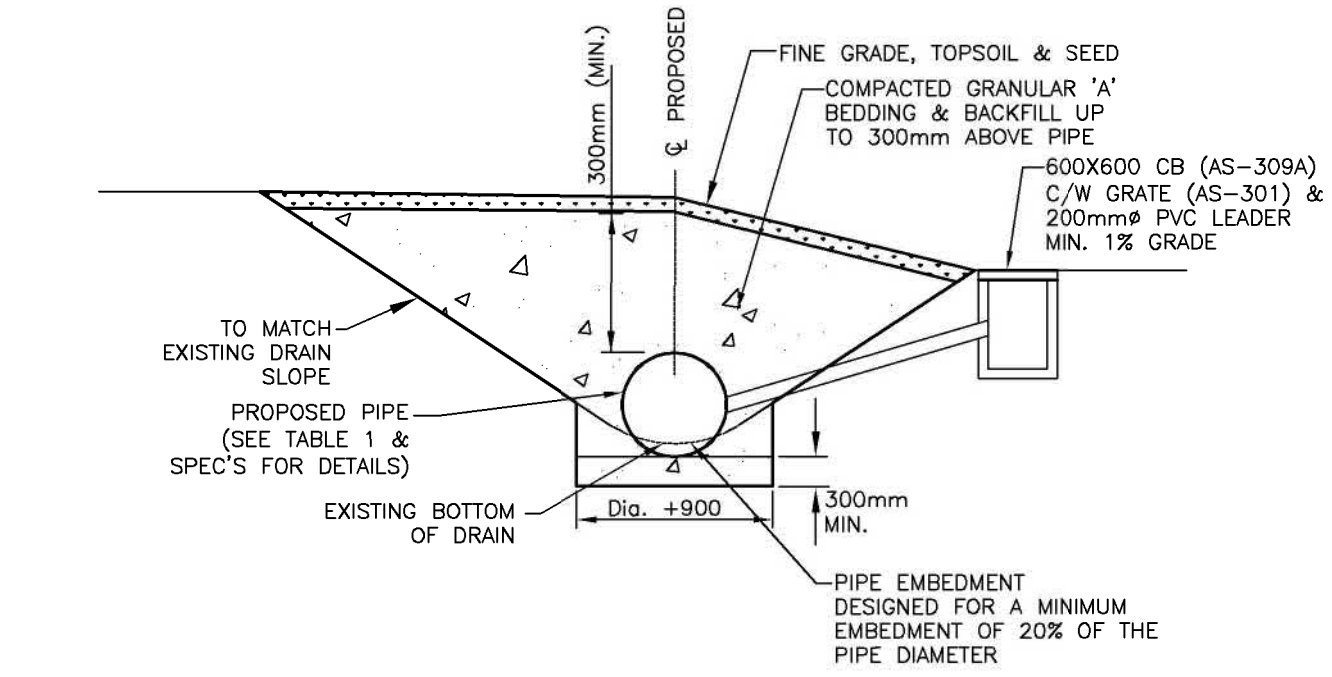
SHEET TITLE
BRIDGE DETAILS

PAGE NO.
4 of 7

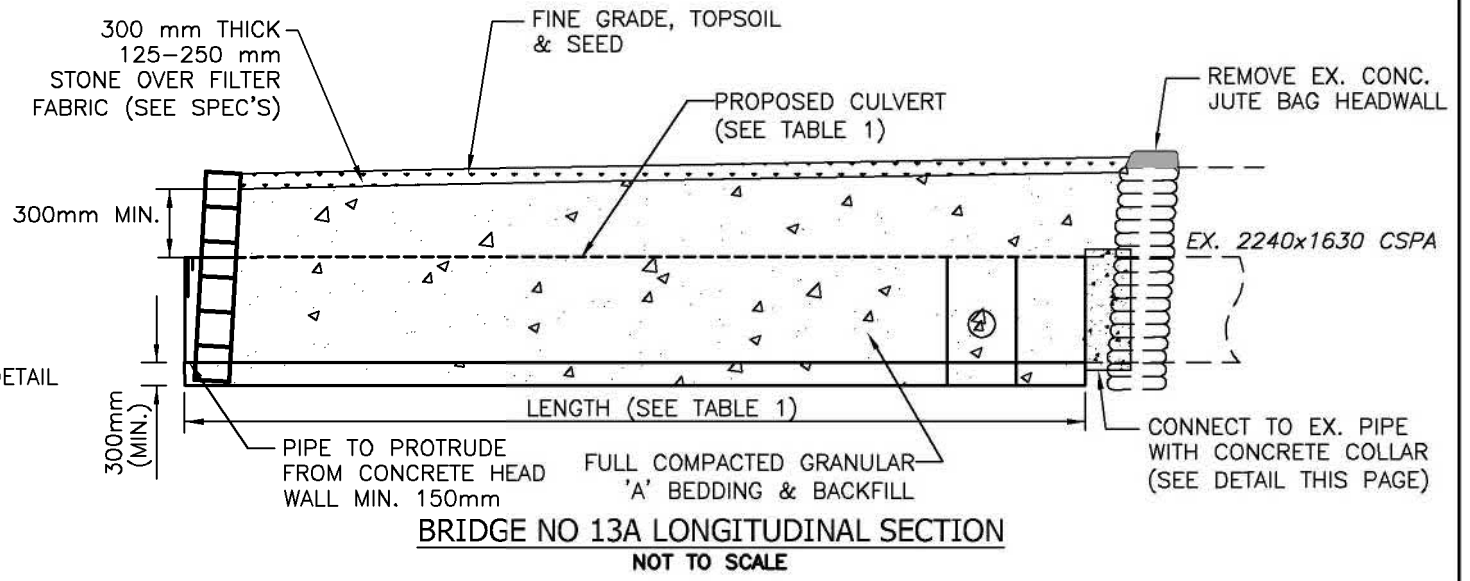
NOTE: THE PREFABRICATED BEND & CONCRETE COLLAR SHALL BE INSTALLED SUCH THAT IT MAY BE DETACHED FROM THE PREFABRICATED TEE IN THE FUTURE



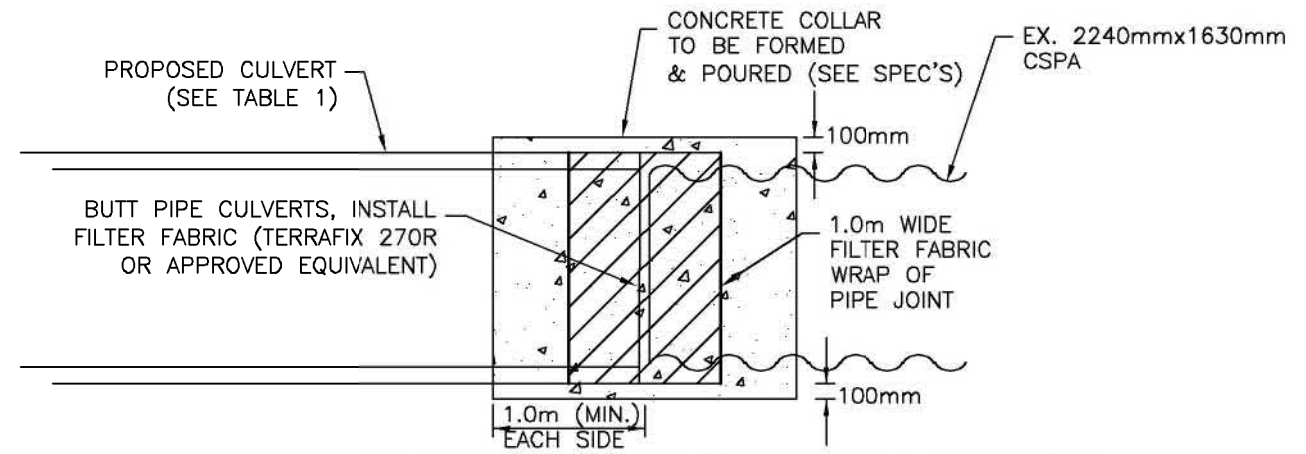
BRIDGE NO. 13A ENCLOSURE PLAN
NOT TO SCALE



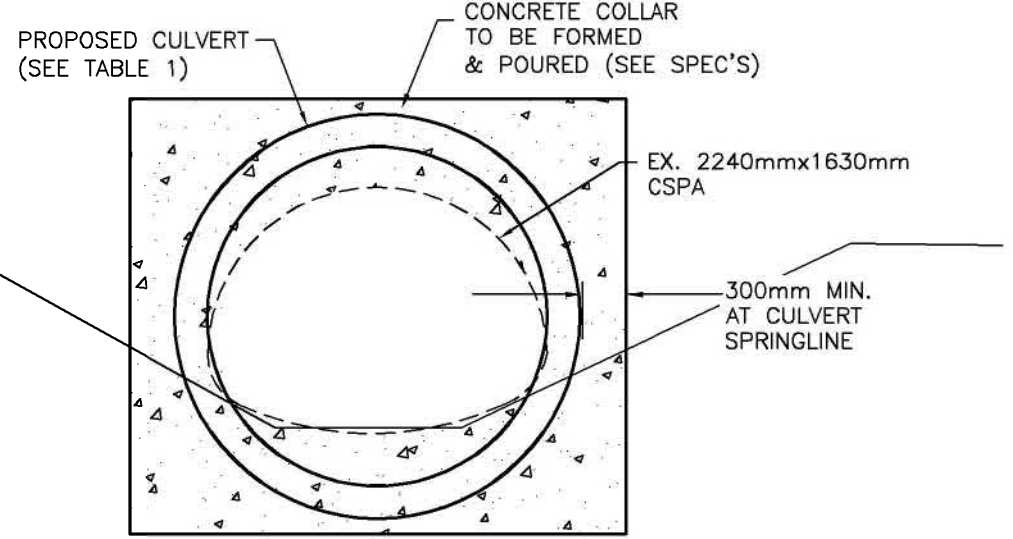
BRIDGE NO. 13A CROSS SECTION
NOT TO SCALE



BRIDGE NO 13A LONGITUDINAL SECTION
NOT TO SCALE



BRIDGE NO. 13/13A PIPE CONNECTION DETAIL - SIDE VIEW
NOT TO SCALE



BRIDGE NO. 13/13A CONCRETE COLLAR DETAIL
NOT TO SCALE

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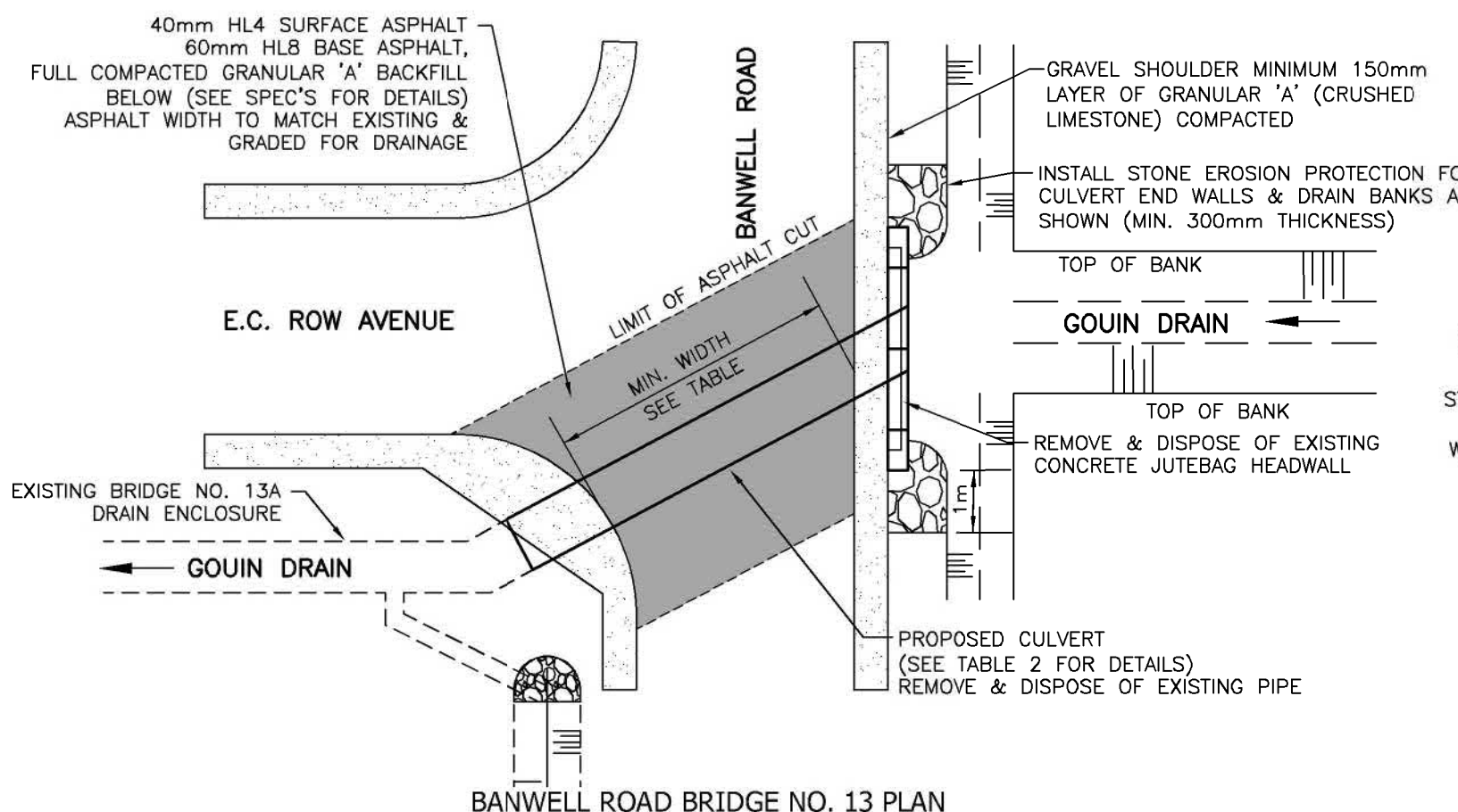
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SCALE	AS SHOWN		

DILLON CONSULTING

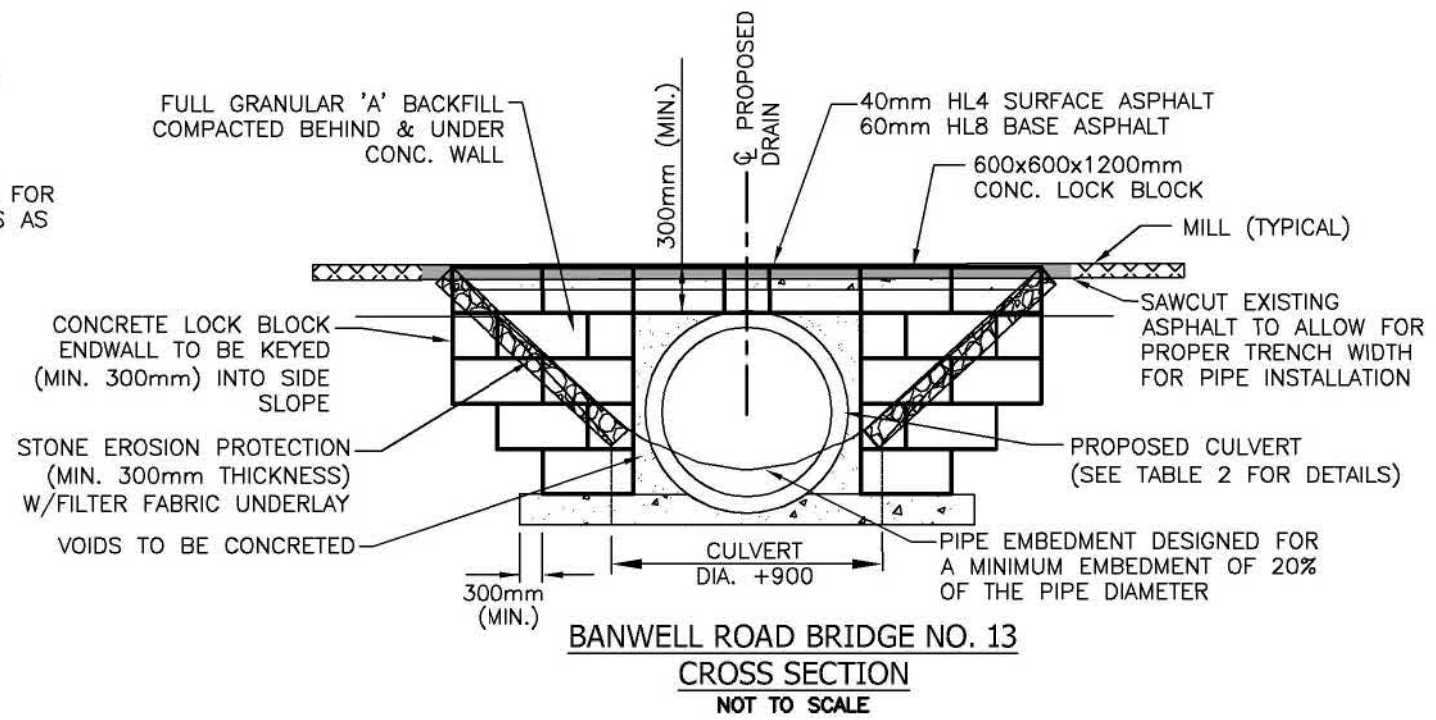
PROJECT NO. 22-5108

DRAWING SCALES BASED ON A 11" X 17" SHEET

<p>'SCHEDULE G'</p> <p>Drainage Report for the GOUIN DRAIN (NEXTSTAR ENERGY BRIDGES) City of Windsor</p>	
<p>SHEET TITLE BRIDGE NO. 13A ENCLOSURE DETAILS</p>	
<p>PAGE NO. 5 of 7</p>	



BANWELL ROAD BRIDGE NO. 13 PLAN
NOT TO SCALE

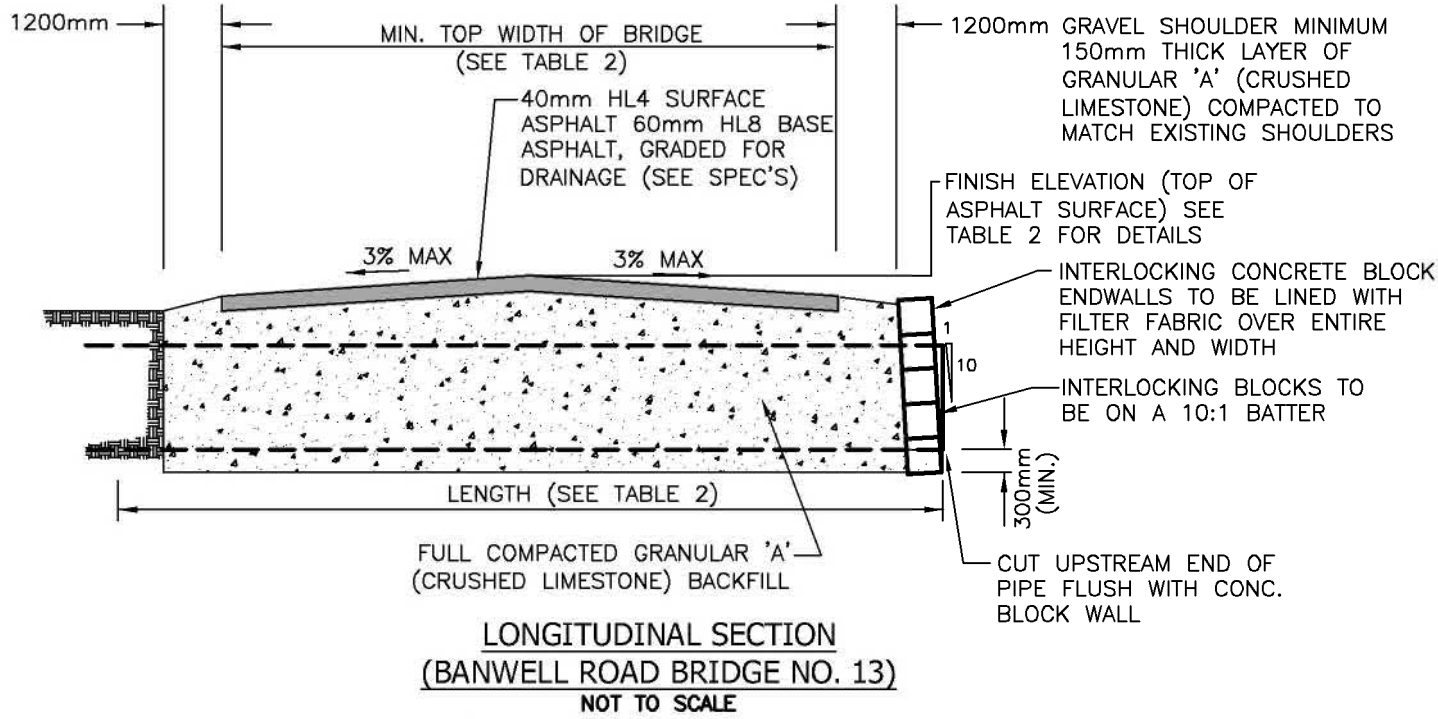


BANWELL ROAD BRIDGE NO. 13
CROSS SECTION
NOT TO SCALE

- NOTES:**
- BLOCK END WALLS TO BE DESIGNED AND SEALED BY A PROFESSIONAL ENGINEER LICENSED IN THE PROVINCE OF ONTARIO
 - FULL CONCRETE BLOCKS ARE 600x600x1200mm
 - SUPPLY & INSTALL OBJECT MARKER SIGNS WA-33L/R AS PER THE ONTARIO TRAFFIC MANUAL

TABLE 2 - ACCESS CULVERT DESIGN INFORMATION	
DESCRIPTION	BRIDGE No. 13
PIPE INVERT ELEV. U/S SIDE(m)	179.42
PIPE INVERT ELEV. D/S SIDE(m)	179.41
TOP OF ϕ DRIVEWAY SURFACE ELEV. (m)	182.23
DRAIN BOTTOM (m) (DESIGN) (AT CENTRELINE OF CULVERT)	179.86
MIN. TOP WIDTH OF DRIVEWAY (m)	12.0
MIN. CULVERT GRADE (%)	0.05
CULVERT TYPE	CONCRETE
CULVERT MATERIAL	CLASS 65-D
CULVERT LENGTH (m)	16.0
PIPE SIZE (mm)	2250
CULVERT ENDWALL TYPE	CONC. BLOCK
TOP ELEV. OF CONCRETE BLOCK END WALL	182.22
APPROX. NO. OF CONCRETE BLOCKS	20 FULL BLOCKS 6 HALF BLOCKS

NOTE:
THIS DRAWING ASSUMES BRIDGE NO. 13A HAS BEEN CONSTRUCTED ACCORDING TO PAGE NO. 4 & 5 & THE SPECIFICATIONS.



LONGITUDINAL SECTION
(BANWELL ROAD BRIDGE NO. 13)
NOT TO SCALE



Conditions of Use

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Do not scale dimensions from drawing.
Do not modify drawing, re-use it, or use it for purposes other than those intended at the time of its preparation without prior written permission from Dillon Consulting Limited.

No.	City Council Agenda	Monday, October 30, 2023	DATE	BY
3	FINAL REPORT SUBMISSION	SEP. 14/23	MDH	
2	CLIENT REVIEW	AUG. 16/23	MDH	
1	ERCA REVIEW	JULY 21/23	MDH	

DESIGN	REVIEWED BY
OEM	TRO
DRAWN	CHECKED BY
WLB	LRO
DATE	September 4, 2023
SCALE	AS SHOWN

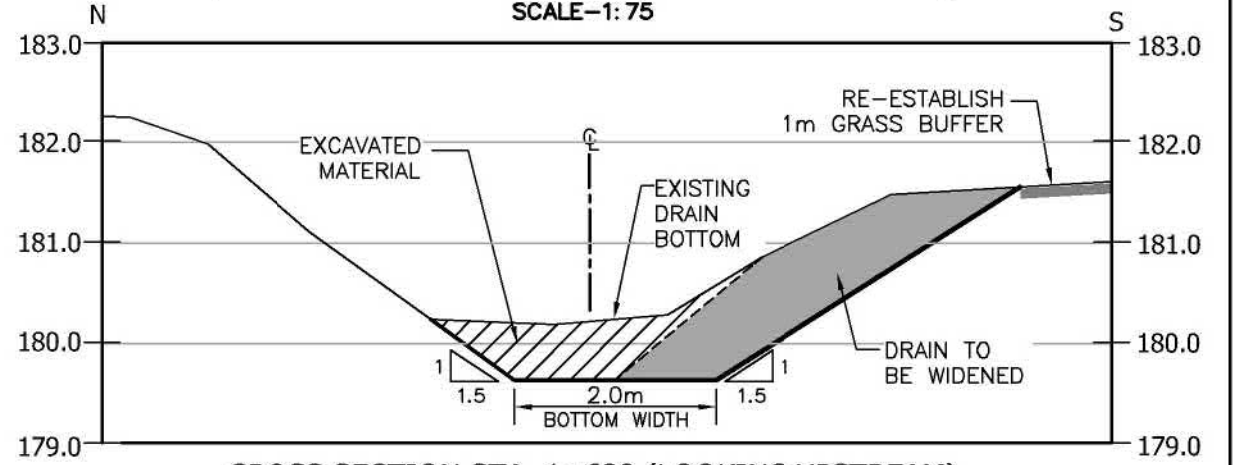
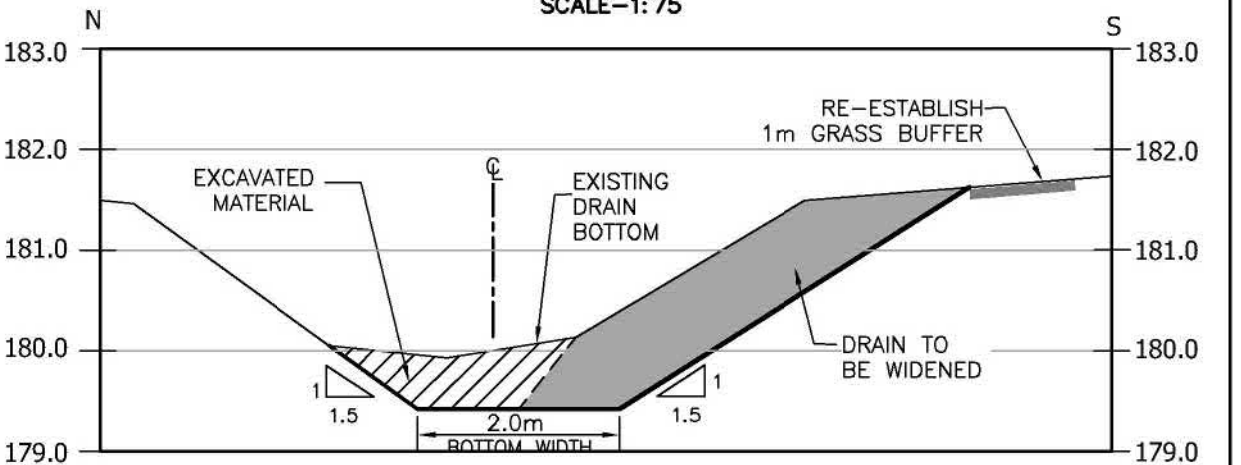
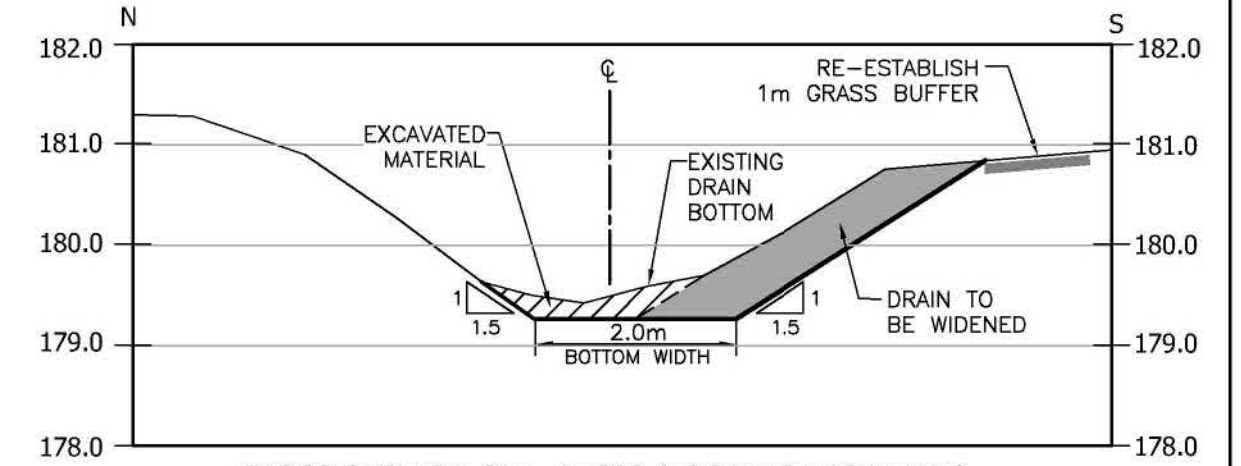
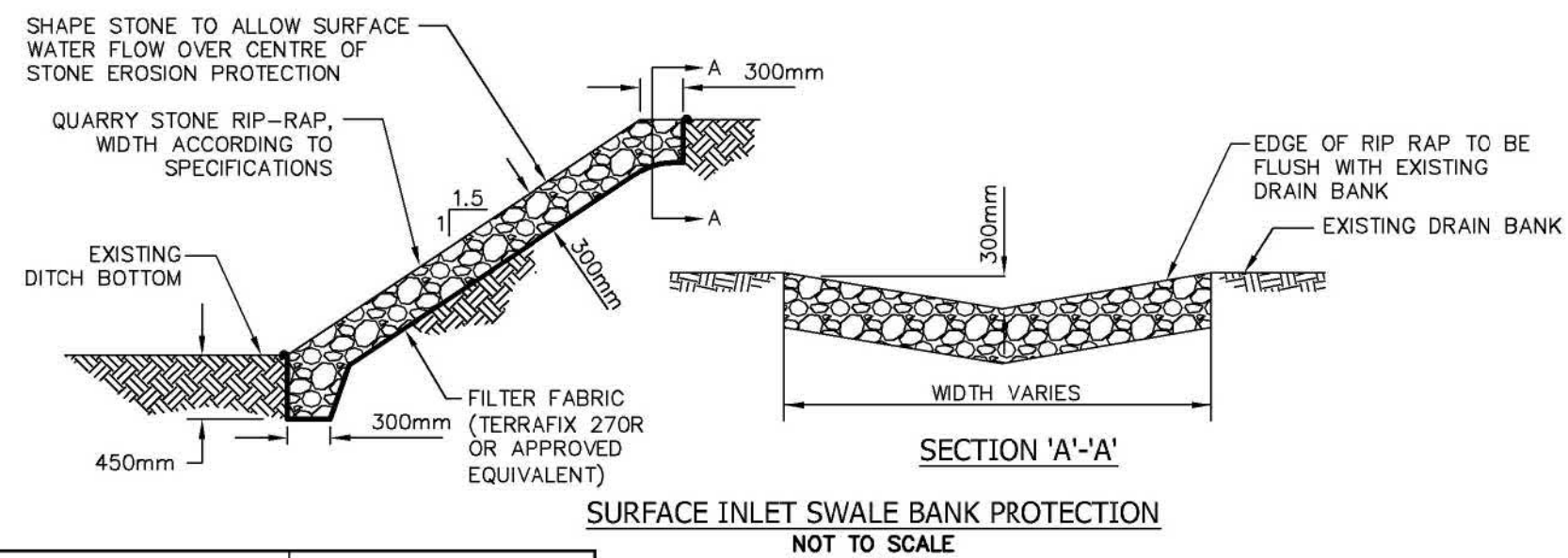
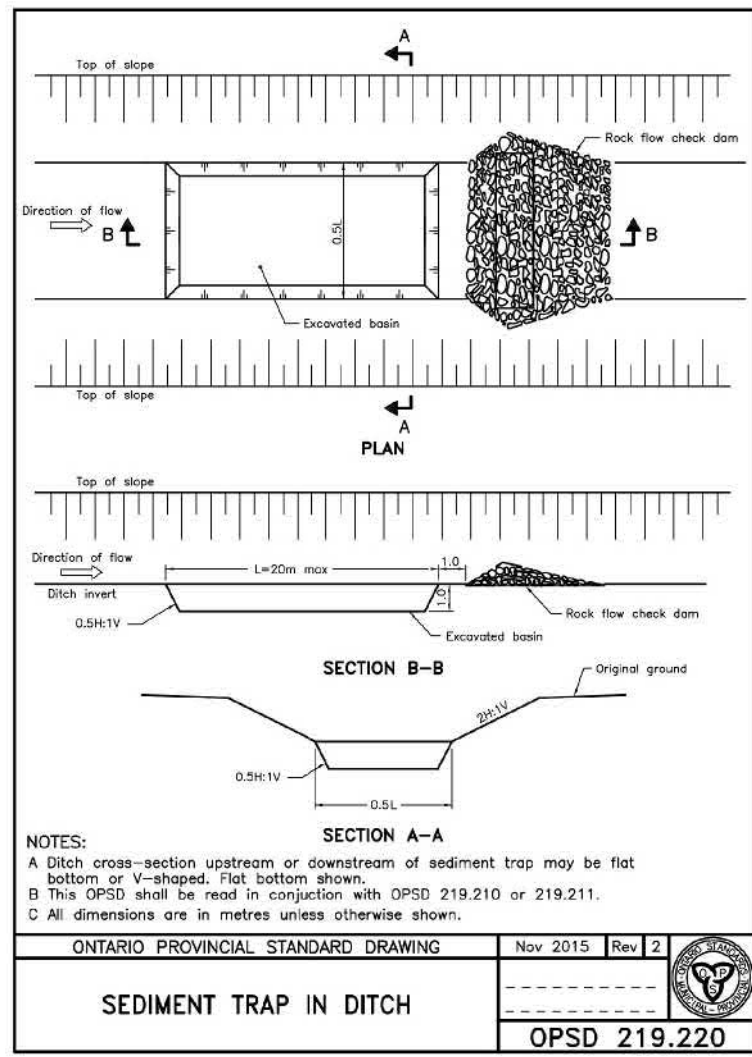
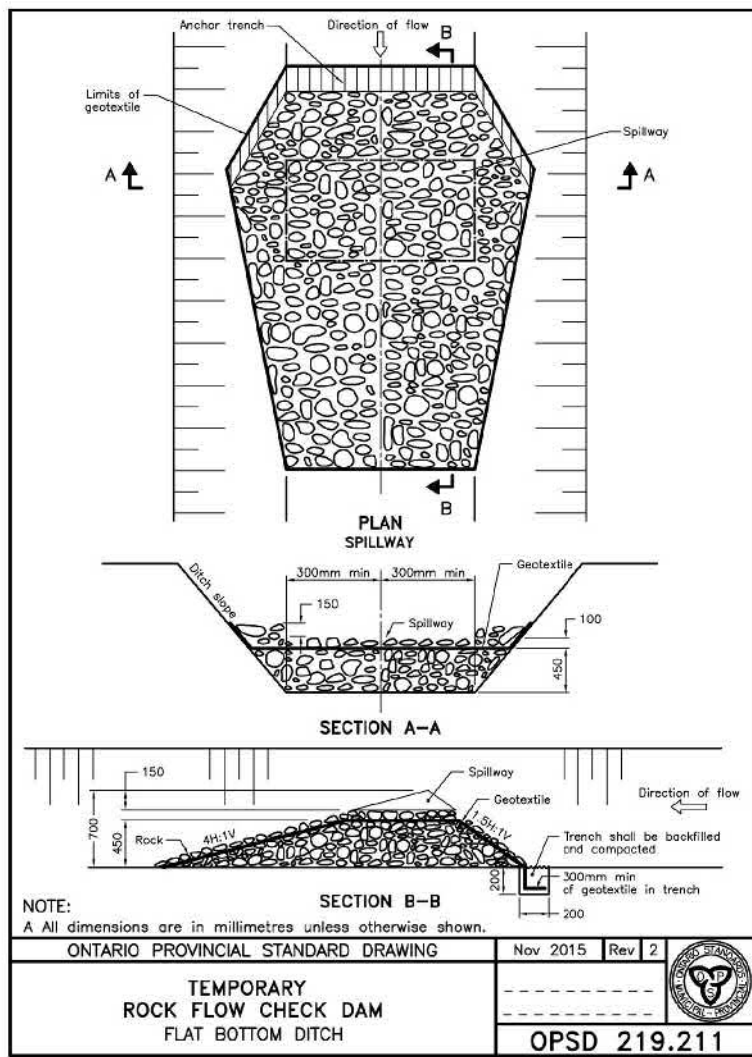
PROJECT NO. 22-5108
DRAWING SCALES BASED ON A 11" X 17" SHEET

'SCHEDULE G'
Drainage Report for the
GOUIN DRAIN
(NEXTSTAR ENERGY BRIDGES)
City of Windsor

SHEET TITLE: **FUTURE BRIDGE NO. 13 REPLACEMENT DETAILS**

PAGE NO.: **6 of 7**

Sep 12, 2023 10:58am C:\Users\330EM\appdata\local\temp\AcPublish_3666\225108-03-DRN-CON.dwg



- NOTES:**
- CONTRACTOR REQUIRED TO CHECK DESIGN GRADE OF DRAIN & CLEAN DRAIN AS REQUIRED.
 - DRAIN BOTTOM TO BE WIDENED BY 1.0m FROM STA. 1+032 TO 1+930.
 - CLEANING OF EXISTING DRAIN SCHEDULED UNDER SEPARATE CONTRACT.



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DESIGN	OEM	REVIEWED BY	TRO
DRAWN	WLB	CHECKED BY	LRO
DATE	September 14, 2023		
SCALE	AS SHOWN		

DILLON CONSULTING

PROJECT NO. 22-5108

DRAWING SCALES BASED ON A 11" X 17" SHEET

'SCHEDULE G'

Drainage Report for the
GOUIN DRAIN
(NEXTSTAR ENERGY BRIDGES)
City of Windsor

SHEET TITLE
MISCELLANEOUS DETAILS

PAGE NO.
7 of 7

Sep 12, 2023 10:58am C:\Users\330EM\appdata\local\temp\AcPublish_3666\225108-03-DRN-CON.dwg

BY – L A W N U M B E R XX-2023

A PROVISIONAL BY-LAW TO PROVIDE FOR THE REPAIR AND IMPROVEMENT OF THE GOUIN DRAIN

Passed the __ day of _____, 2023.

WHEREAS the Council of The Corporation of the City of Windsor has procured a Drainage Report for the Gouin Drain as prepared by Dillon Consulting, dated September 14, 2023 (“Engineer’s Report”) which report is attached hereto and forms part of this by-law;

AND WHEREAS the estimated total cost of the drainage works is \$1,599,600.00 which will be borne by the property owners within the City of Windsor in accordance with the Schedule of Assessment in the Drainage Report;

AND WHEREAS the Council of The Corporation of the City of Windsor is of the opinion that the repair and improvement of the Gouin Drain is desirable;

THEREFORE the Council of the Corporation of the City of Windsor, pursuant to the provisions of Section 45 of the Drainage Act, R.S.O. 1990, hereby enacts as follows:

1. **THAT** the Engineer’s Report for the Repair and Improvement of the Gouin Drain as prepared by Dillon Consulting, dated September 14, 2023, which is attached hereto as Schedule “A”, is hereby adopted, and the drainage works as therein indicated and set forth are hereby authorized and shall be completed in accordance therewith.
2. **THAT** this by-law shall come into force and take effect on the day of the final passing thereof.

DREW DILKENS, MAYOR

CITY CLERK

First Reading - October 30, 2023
Second Reading- October 30, 2023
Third Reading - , 2023



Subject: Update Regarding The Divestment of the East Riverside Shorewall; Supplementing Report C77/2023 - Ward 7

Reference:

Date to Council: October 30, 2023

Author: Isak Quakenbush

Pollution Control Project Engineer

(519) 253-7111 Ext. 3226

iquakenbush@citywindsor.ca

Engineering

Report Date: October 14, 2023

Clerk's File #: SL2023

To: Mayor and Members of City Council

Recommendation:

I. THAT City Council **RECEIVE** this report to supplement Council Report C77-2023 with respect to Council Decision B14/2019, the City-owned shorewall along Lake St. Clair; and,

II. THAT **APPROVAL BE GIVEN** to divest the East Riverside shorewall of The Corporation of the City of Windsor (the "**City**") on the following terms:

1. **BUYERS:** Each of the owners of 10870 to 11906 Riverside Drive East, Windsor who do not currently own their portion of the subject lands (hereinafter the "**Buyers**", and each a "**Buyer**")
2. **PROPERTY:** A 1ft. wide section of land upon which the concrete shorewall is situated, between the south shore of Lake St. Clair and the lots municipally known as 10870 to 11906 Riverside Drive East, Windsor to the south, as set out in Letters Patent registered as Instruments No. R6939 Riverside, to be further described on a Reference Plan to be registered (hereinafter the "**Subject Lands**")
3. **SALE PRICE:** \$1, plus HST, if applicable. The City shall also be responsible for all legal fees and disbursements of the Buyers incurred in connection with the transfer of the Subject Lands; and,

- III. THAT the transaction identified in Recommendation II **BE COMPLETED** electronically, for the property where it is available, pursuant to By-Law 366-2003 and that the Chief Administrative Officer and the City Clerk **BE AUTHORIZED** to execute all documents necessary to complete the transaction, as required, and that the City Solicitor **BE AUTHORIZED** to execute documents standard to the real estate transaction; and,
- IV. THAT City Council **APPROVE** the transfer of \$350,146 from projects 7191024 (Wellesley Rehab – Terminal to Ypres \$240,000) and 7145005 (Enhanced Interim Financing Fund \$110,146) to project 7192007 (Riverside Breakwall Study) to bear the costs associated with the above-referenced transactions as described in Recommendation II.

Executive Summary:

N/A

Background:

The East Riverside Shorewall (“the Shorewall”) is a 1ft. wide section of land upon which the concrete shorewall is situated, between the south shore of Lake St. Clair and the lots municipally known as 10870 to 11906 Riverside Drive East.

On March 31, 1931 the Lieutenant-Governor in Council signed a Letters Patent document restricting the sale of the Shorewall without prior approval by the Lieutenant-Governor. The Letters Patent site the reasoning behind this restriction is to protect the rights of the public to access and enjoy the adjacent navigable waters. As the access to the water is now restricted by private property these restrictions are no longer applicable to the public.

At its meeting on April 1, 2019, City Council adopted resolution B14/2019 directing Administration to request permission from the Lieutenant Governor of Ontario to divest of the Shorewall and report back. Council Report C77/2023 outlines the request that was made and the steps taken to address the Council Direction. This report provides current information regarding the status of the restrictions and proposes a process to legally divest the Shorewall.

Discussion:

On July 25, 2023, Administration received correspondence from the Ministry of Natural Resources and Forestry (the “Ministry”) stating that the Ministry will approve the release of the Letters Patent restrictions. Subsequently, the Ministry encountered title issues with the Subject Lands, namely that they could not be legally described, and that not all of the Subject Lands appeared to be owned by the City.

Administration, in coordination with the Ministry, determined that the following steps are required to resolve these issues so that the Ministry can issue the formal Release Order.

1. New Reference Plans are required, in order to appropriately describe the individual parts of the Shorewall. It is anticipated to take 23 weeks from notice to proceed to prepare and register the required Reference Plans.
2. The Ministry will then register a Release Order on title to the Subject Lands, releasing the restrictions in the Letters Patent.
3. If approved by City Council, Administration will offer the divestment of the Subject Lands to the Buyers as described in Recommendation II.

A title search revealed that 29 of the 70 segments of the Subject Lands already belong to the abutting owners. Investigations revealed these segments were originally vested in accordance with a tax certificate dated June 1, 1934 and registered on title on July 9, 1934 as Instrument No. 7477. Subsequent to the vesting, the 29 segments were inadvertently conveyed by the former municipality of Riverside along with the abutting properties. Although contrary to the restrictions set out in the Letters Patent, the conveyances were valid and effectively transferred ownership of the Shorewall to the abutting owners. A list of the properties showing the current Shorewall owners by municipal lot number is attached as **Appendix A**. Pending Council Approval and the Release Order, the owners of these properties will receive correspondence confirming that the restrictions in the Letters Patent have been lifted and that they are full owners of their portion of the Subject lands.

The owners of the remaining 41 abutting properties, pending Council approval, will be offered the proposed divestment to acquire ownership of their individual segments of the Shorewall. If the owners do not accept the offer of divestment the City will continue its ownership of those segments of the Shorewall. Because it is anticipated that some property owners will decline the proposed divestment, Administration previously recommended that Council pass the Local Improvement Policy Amendment as proposed in Council Report C77/2023 to provide property owners with options to improve the Shorewall.

As the wall cannot be considered a single entity, engineered terminations or transitions would need to be provided at the boundaries at the ends of any proposed Local Improvement Projects (LIPs). This will complicate and likely increase the average cost of design and construction if LIPs occur. However, reducing the number of owners involved in each LIP could allow for a more tailored approach to design improvements to individual segments of the Shorewall. Pending Council approval, Shorewall owners may still elect to participate in future LIPs but this will need to be addressed on a case by case basis.

Risk Analysis:

Divesting the Shorewall poses minimal risk to the City as the standard throughout the province is that waterfront owners are responsible for their properties, including

Shorewalls. Divestment may also reduce the overall cost of future maintenance projects, thus reducing the impacts to City funding. Individual owners would be responsible for shore protection designs and permitting responsibilities would be passed to Essex Region Conservation Authority.

Property owners are under no obligation to accept the divestment and as such it is unlikely that all property owners will participate. This risk is mitigated by the recommendations of report C77/2023 which proposes a local improvement strategy for shorewall rehabilitation.

Climate Change Mitigation:

There are no mitigation risks associated with divesting the Shorewall.

Climate Change Adaptation:

More frequent and intense rain events, higher lake levels and wave action can accelerate erosion and increase the need for enhanced shoreline protection. Future shorewall improvements should consider any increasing risks associated with climate change, including taking any action that may be required by permitting agencies.

Financial Matters:

The divestment of the Shorewall should reduce the financial impacts of future LIPs as it is anticipated that most privately owned properties would not participate in a LIP. In order to encourage Owner acceptance of the divestment, Administration proposes that the City bear the costs associated with the divestment of the wall, and offer the land to owners for \$1 as stated in Recommendation II.

The following outlines the costs associated with this process. Administration has already paid the processing fee of \$830 to the Ministry and \$8,136 for a title search. Verhaegen Land Surveyors has provided an estimate of \$65,000 to prepare and deposit the new required Reference Plans. Buyers will need a legal representative to acquire the Subject Lands which may cost as much as \$1,500 per transaction. Buyers also typically incur other minor costs, including document preparation and deed registration fees (\$250 and \$80 respectively).

Administration recommends waiving the abovementioned fees and reimbursing Buyers for their legal costs. This will ensure the true cost to Buyers is the \$1 sale price and will remove cost as a barrier to their decision.

The costs (excluding HST) and proposed funding sources are detailed in the financial summary table below.

EXPENSES			
Item Description	Unit Cost	QNTY	Total Cost
MNRF Processing Fee (Paid)	830	1	\$830
Title Search (Paid)	\$8,136	1	\$8,136

Survey And Registration of 12R Plans	\$65,000	1	\$65,000
Waiving Deed Document Preparation Fee	\$250	41	\$10,250
Waiving Deed Registration	\$80	41	\$3,280
Reimburse Owners Individual Legal Fees	\$1,500	41	\$61,500
Sale Price (Cost to Owners)	-\$1	41	-\$41
Internal Costs / Contingency / Taxes	\$211,045	1	\$211,045
Total Expenses			\$360,000
REVENUES			
Project / Fund	Amount of Funds		
7192007 - Riverside Break Wall Study (Remaining)	\$9,854		
7191024 – Wellesley Rehab (Terminal to Ypres)	\$240,000		
7145005 – Enhanced Interim Financing Fund	\$110,146		
Total Revenues			\$360,000

If approved by council a total of \$350,146 will be transferred to Project 7192007, Riverside Breakwall Study, to cover the costs of offering the divestment of the Shorewall to the owners of each abutting property. The transfer of funding has been taken from projects that are considered near or at completion and as such previously allocated funding is no longer required.

Consultations:

Kathy Buis Financial Planning Administrator

Karina Richters Supervisor Environmental Sustainability and
Climate Change

Kate Tracey Senior Legal Council – Real Estate and Risk Management

Denise Wright Manager of Real Estate Services

Conclusion:

Administration recommends divesting the Shorewall to the interested abutting property owners at the minimal cost of \$1. To do this, additional funding is required to cover the costs of the legal documents and processes needed to lift the restrictions and properly coordinate the deeds for each lot.

Planning Act Matters:

N/A

Approvals:

Name	Title

Name	Title
Fahd Mikhael	Manager of Design
Stacey McGuire	Executive Director of Engineering/Deputy City Engineer
Chris Nepszy	Commissioner of Infrastructure Services
Shelby Askin Hager	Commissioner of Legal & Legislative Services
Janice Guthrie	Commissioner of Corporate Services – CFO/City Treasurer
Joe Mancina	Chief Administrative Officer

Notifications:

Name	Address	Email
Scott Kaldeway	Lands and Business Services Section Ministry of Natural Resources and Forestry	Scott.Kaldeway@ontario.ca
Additional list provided to the Clerks Office		

Appendices:

Appendix A East Riverside Shorewall Owner List

APPENDIX A
RIVERSIDE SHOREWALL OWNERS BY PROPERTY

Parcel	Municipal Address	PIN number	Breakwall Owner
1	10870	01054-0826	Homeowner
2	10896	01054-0827	Homeowner
3	10910	01054-0290	Homeowner
4	10930	01054-0291	Homeowner
5	10950	01054-0292	Homeowner
6	10960	01054-0331	Homeowner
7	10976	01054-0212	City
8	10988	01054-0213	City
9	11000	01054-0214	City
10	11014	01054-0215	Homeowner
11	11030	01054-0216	City
12	11044	01054-0217	Homeowner
13	11060	01054-0218	Homeowner
14	11080	01054-0219	Homeowner
15	11120	01054-0220	Homeowner
16	11134	01054-0272	City
17*	11150	01054-0260	City
18	11166	01054-0261	City
19	11178	01054-0262	City
20	11194	01054-0263	City
21*	11200	01054-0415	City
		01054-0834	Homeowner
22	11218	01054-0835	Homeowner
23	11228	01054-0265	City
24	11246	01054-0814	City
25	11262	01054-0813	City
26	11280	01054-0275	City
27	11310	01054-0310	City
28	11336	01054-0311	City
29	11350	01054-0312	City
30	11360	01054-0313	City
31	11370	01054-0323	Homeowner
32	11382	01054-0002	City
33	11394	01054-0327	Homeowner
34	11414	01054-0005	City
35	11420	01054-0006	City
36	11430	01054-0007	City
37	11438	01054-0334	Homeowner

38	11450	01054-0335	Homeowner
39	11460	01054-0296	Homeowner
40	11474	01054-0336	City
41	11488	01054-0565	City
42	11506	01054-0338 (R)	City
43	11520	01054-0561	City
44	11530	01054-0472	Homeowner
45	11550	01054-0563	City
46	11566	01054-0559	Homeowner
47	11584	01054-0362	City
48	11600	01054-0371	City
49	11622	01054-0372	Homeowner
50	11636	01054-0373	City
51	11646	01054-0374	Homeowner
52	11660	01054-0375	Homeowner
53	11676	01054-0376	City
54	11694	01054-0377	Homeowner
55	11710	01054-0378	City
56	11722	01054-0379	City
57	11740	01054-0386	City
58	11750	01054-0356	City
59	11766	01054-0355	City
60	11770	01054-0833	City
61	11810	05054-0352	City
62	11820	01054-0351	City
63	11830	01054-0350	City
64 *	11850	01054-0349	City
65	11860	01054-0348	Homeowner
66	11870	01054-0347	Homeowner
67	11880	01054-0346	Homeowner
68	11890	01054-0345	Homeowner
69	11906	01054-0411	Homeowner

*** NOTE:** **Parcel 17 – City owns 13’3” therefore an additional part must be added to 12R**
Parcel 21 – Part of the shorewall is owned by the City, part is owned by
Homeowner
Parcel 64 – City owns 14” therefore an additional part must be added to 12R

Subject: Update Regarding Council Decision B14/2019 and Proposed Local Improvement Policy Amendment for Shoreline Structures - City-Wide

Reference:

Date to Council: August 8, 2023

Author: Isak Quakenbush

Engineer II

519-255-6257 ext. 6407

iquakenbush@citywindsor.ca

Engineering

Report Date: May 29, 2023

Clerk's File #: SL2023

To: Mayor and Members of City Council

Recommendation:

- I. That City Council **RECEIVE** this report in response to Council Decision B14/2019 with respect to the City-owned shorewall along Lake St. Clair; and,
- II. That City Council **APPROVE** the addition of Section 4.6 Shoreline Structures to the existing Consolidated Local Improvement Policy (Appendix B).

Executive Summary:

N/A

Background:

At its meeting on April 1, 2019, City Council adopted resolution B14/2019 which reads:

- I. That Council **FORMALLY REQUEST** the Lieutenant Governor of Ontario for permission to divest of the City-owned Shorewall along Lake St. Clair; and
- II. That Administration **FURTHER REPORT BACK** with respect to the questions regarding possible transfer ownership of shorewalls and associated land; and
- III. That City Council **DIRECT** Administration to establish a Public Shorewall Local Improvement Policy in accordance with the parameters of this report and bring forward for approval of City Council; and
- IV. That Administration conduct a condition assessment of the 1.5 km. Riverside breakwall situated between the South Shore of Lake St. Clair and the north side

of the lots located on the north side of Riverside Drive from Sand Point Beach to the Rendezvous Shores Subdivision; and

- V. That \$ 75,000.00 **BE FUNDED** as a Council priority project in 2019 from the Pay-as-you-go Reserve Fund 160 funding source; and
- VI. That Administration be requested to report back with recommendations with those results including the number of affected property owners in Southwood Lakes and on the East end; and
- VII. That Administration **BE REQUESTED** to organize a public meeting with the Ward Councillors and affected residents to be scheduled before the report is finalized; and
- VIII. That ERCA **BE REQUESTED** to take on the project and fund the condition assessment.

This report is intended to respond to and provide status updates to the above noted Council Decision items.

Discussion:

Divesture of the Riverside Drive East Shorewall

The request of the City for permission to divest was acknowledged by the Ministry of Natural Resources and Forestry (MNRF) on November 15, 2019. An investigation was initiated by the MNRF in early 2020 and was determined to have carriage of the request for divestiture and accompanying recommendation to the Lieutenant Governor.

In late 2021, Bill 13 received royal ascent authorizing the Minister of the MNRF to make some public land decisions including release from restrictions in letter patents, as is the case here. Administration has confirmed that the City's initial request is still valid and have paid the required administration fee to the MNRF to initiate the Minister's review. Administration is still awaiting a decision at this time but continues to follow up.

Regardless of the Minister's decision, divestiture of the entire wall may be difficult as not all property owners may accept ownership. Administration will develop viable plans on how to divest any portion of the shorewall, should that become an option, and report back.

Local Improvement Policy (LIP) Amendment

Ontario Regulation 586/06 Local Improvement Charges allows maintenance and work on shoreline structures to be assessed as a Local Improvement. The attached Shoreline Structures Local Improvement Policy Amendment (Appendix B) was drafted for consideration. A municipal structure that solely benefits private property is not common, however, the amendment provides a method to address shoreline improvements through the LIP process.

Since these structures solely benefit the adjacent properties, the full cost of the work would be assessed to the abutting property owners through one of the following:

- A one-time payment
- 10- year special charges period*
- 20-year special charges period*

*At an interest rate deemed appropriate by Administration for carrying costs.

Local Improvement works can be petitioned for when one or more abutting property owners formally requests the work and delivers a sufficiently signed petition.

In cases where improvements are determined to be necessary to protect the ongoing integrity and sustainability of the infrastructure, but absent a request from an abutting resident, the City can initiate those improvements. If the affected residents are opposed to the work, they will have 30 days to submit a sufficiently signed petition against the works. If a sufficiently signed petition is provided, the City may not proceed and may not initiate the same project again for two years. If deemed necessary, the City could apply to the Ontario Land Tribunal for approval to undertake the work.

Riverside Drive East Shorewall Condition Assessment

Administration engaged Landmark Engineers Inc. (Landmark) for the completion of a structural assessment and functional design for the 1.5 km East Riverside Shorewall. This assignment was completed through the Engineering Roster and the report was delivered by Landmark on January 16, 2020 (Appendix A). The contents of this report were released at the public meeting on March 31, 2023 and indicates the wall is structurally in fair to good condition; however, the wall has been undermined along a significant portion of the length allowing the retained fill materials to be eroded and undermine the concrete splash decks.

Landmark provided a functional design that places a rip-rap and geotextile toe in front of the wall to re-establish the base of the wall to the lakebed to prevent further erosion. The wall would then be backfilled and the concrete splash decks replaced or rehabilitated as needed.

The improvements are recommended to extend the life of the existing structure. The estimated costs noted within the report are between \$3.5 to 5.5 million depending on final design and timing of work.

Eligibility for Policy

The total number of abutting properties affected by this proposed policy amendment in East Riverside is 70 properties.

Public Meetings East Riverside:

The public meeting for residents abutting the East Riverside Shorewall was held on Friday, March 31, 2023 at the Riverside Sportsman's Club from 4-7 p.m. The boards presented at the meeting are attached as Appendix C. Approximately 36 properties

were represented according to the sign-in sheet and administration received 17 comment sheets. In addition, 14 properties provided comments during the comment period for a total of 31 received comment sheets. The resident comments and inquiries received are summarized below:

- 1) Abutting residents should not have to pay 100% of the construction costs because:
 - a) This is a City owned shorewall.
 - b) The shorewall protects the community to the south from flooding.
 - c) Some owners have already paid for shoreline related improvements.
- 2) Landmark's Functional Design will:
 - a) Affect lake access for swimming, fishing and boat usage.
 - b) Cause boat lifts and other structures to no longer be functional.
 - c) Trap trash and debris causing odor issues and poor visual aesthetics.
- 3) Will residents be able to purchase their portion of the shorewall?
- 4) Has the City pursued other funding sources, such as, federal and provincial?

As presented during the Public meeting, Ontario Regulation 586/06, Local Improvement Charges, grants municipalities the power to pass by-laws to perform work on a retaining wall and recover the cost from the abutting residents.

The shorewall does not protect against flooding. The Sewer and Coastal Flood Protection Master Plan has identified the instantaneous 1:100 year flood elevation to be 176.5 meters, while the highest elevation of the shorewall is 176.2 meters. In addition, the properties to the west have generally lower elevations than the lots abutting the shorewall. During coastal flooding events water infiltrates onto Riverside Drive from these western properties, bypassing the shorewall.

The City continues to implement the Sewer and Coastal Flood Protection Master Plan by constructing continuous inland landform barriers at the 176.5 meter elevation to protect the community south of Riverside Drive. In the area of the municipally owned shorewall, the Ganatcho Trail is currently acting as this landform barrier.

It is acknowledged that some residents have repaired or replaced the splash decks behind the shorewall (some multiple times). This supports Landmark's findings that material is being pulled from behind the shorewall through the gap between the shorewall and the lakebed. Re-establishing the shorewall's connection to the lakebed would prevent the undermining of the splash decks and prevent the need to regrade and replace them. Therefore, Administration has revised the amendment from that presented at the public meeting to clarify that required work on private property would be handled similar to other LIP projects. Each individual property would be assessed for

the work required on that property. With the revised LIP amendment, the property owners who have previously completed splash deck related improvements may not have to be burdened with further costs if no splash deck repairs or improvements are required on their property.

Under the proposed amendment, each abutting property would be assessed 100% of the work required to maintain the shorewall along only its frontage.

The Landmark report presented the most cost effective shorewall functional design and recognizes that there are other design options that could be explored.

It is currently not possible for property owners to purchase their portion of the shorewall due to restrictions in the letters patent. The City has requested from MNRF that the restrictions be lifted and if approved, transfer of ownership would become a possibility.

Administration has investigated possible shorewall funding programs and continues to monitor opportunities as new grants and funding sources become available regularly. However, application processes are generally competitive and there is no guarantee of success with any future applications.

Southwood Lakes:

The Sewer and Coastal Flood Protection Master Plan recommends operational improvements to the ponds. Administration is currently working with a consultant to investigate options to alter the ponds hydrology and the ramifications of doing so. The results are expected in November of this year, after which a public meeting may be planned with the Southwood Lakes residents.

ERCA Response

ERCA was requested to take on the project and contribute funds to the Condition Assessment. ERCA advised that they do not have the resources to take on such an asset nor to fund the assessment.

In ERCA's opinion, having the City maintain ownership of the structure enables a consistent approach to hazard protection, rather than individualized solutions that would vary property-to-property. ERCA also advised, that if municipal ownership is maintained, there may be federal level funding sources for breakwall remediation addressing fish habitat issues.

Risk Analysis:

There is significant financial risk involved with not approving the proposed amendment to the Local Improvement Policy to include Shoreline Structures. If not approved, the City would have to fund the repairs through the general tax levy for works that solely benefit private individuals.

Climate Change Mitigation:

There are no mitigation risks associated with the proposed shorewall policy.

Climate Change Adaptation:

Though there are no adaptation risks associated with the proposed shorewall policy, it should be noted that climate change may impact shorelines.

More frequent and intense rain events, higher lake levels and wave action can accelerate erosion and increase the need for enhanced shoreline protection. Future shorewall improvements should address any increasing risks associated with climate change.

Financial Matters:

There are a number of financial impacts that have been analyzed should the recommendation within this report be approved. This included an assessment of the impact to City funding, the impact on available cash flows, and costs associated with the administration of the program, etc. Currently, the cost for such work is either funded as part of a municipal capital project or cost shared between the City and the property owner(s). The portion that remains to be funded over time by the taxpayer is in essence a loan with the City, in which the municipality becomes the financial lending authority on a long-term basis to the property owner and this is not necessarily aligned with service delivery. A 20-year term requires administrative resources that could be utilized for other priorities.

Furthermore, in situations where interest rates are initially higher and then fall over the repayment period, a property owner can choose a payout based upon the remaining balance outstanding. The same does not hold true for the city, as interest rates rise over the 20-year period, the City would be essentially forgoing interest revenue until the full balance owing is collected. There is no ability for the City to renegotiate the terms of the loan.

Under the current policy, property owners will be provided with the choice to pay their share of the cost in full. Those property owners who require financial assistance with the full payment can choose to access alternative financing through various lending institutions, which would likely have provisions for longer-term repayment periods and more attractive interest rates.

An illustrative example is shown below with today's 20 year lending institution average rate of 6.85% versus the City's rate offering of 8.38%.

Lending Institution		City of Windsor	
Average Loan Amount of \$50,000		Average Loan Amount of \$50,000	
Loan Term Fixed Rate	20 Yrs	Loan Term Fixed Rate	20 Yrs
Interest Rate	6.85%	Interest Rate	8.38%
Monthly Payment	\$383	Monthly Payment	\$430
Total at end of Term	\$91,959	Total at end of Term	\$103,229
Total Interest	\$41,959	Total Interest	\$53,229

As illustrated above, it is financially beneficial to seek external financing rather than to rely on the city program.

Consultations:

Wira Vendrasco,	Deputy City Solicitor – Legal and Real Estate
Karina Richters,	Supervisor Environmental Sustainability and Climate Change
Linda Mancina,	Financial Planning Administrator
Adam Pillon,	Manager of Right-of-Way
Adam Mourad,	Engineer II
Stacey McGuire,	Manager of Development
Tom Graziano,	Drainage Superintendent
Ryan Langlois,	Water and Waste Water Engineer
Natasha Gabbana,	Senior Manager of Asset Planning
Luigi Congi,	Asset Coordinator
Kristina Savi-Mascaro,	Deputy Solicitor – Purchasing, Risk Management and POA
Public Meeting	March 31, 2023

Conclusion:

This report is intended to provide an update to Council Decision B14/2019. Administration recommends adopting the proposed amendment to the Local Improvement Policy to include Section 4.6 Shoreline Structures. Should the option to divest the City of this asset be confirmed, Administration will report back.

Planning Act Matters:

N/A

Approvals:

Name	Title
Fahd Mikhael	Manager of Design
France Isabelle-Tunks	Executive Director of Engineering/Deputy City Engineer

Name	Title
Chris Nepszy	Commissioner of Infrastructure Services
Shelby Askin Hager	Commissioner of Legal & Legislative Services
Janice Guthrie	(A) Commissioner of Corporate Services – CFO/City Treasurer
Joe Mancina	Chief Administrative Officer

Notifications:

Name	Address	Email
Craig Kondruk	11060 Riverside Drive East	
James Bryant	Essex Region Conservation Authority 360 Fairview Ave W Suite 311 Essex, Ontario, N8M 1Y6	JBryant@erca.org
Additional list provided to the Clerks Office		

Appendices:

- A Landmark Shorewall Assessment and Functional Design Report
(January 16 2020)
- B Proposed Local Improvement Policy Amendment - 4.6 Shoreline Structures
- C March 31 2023 Public Meeting Boards



January 16, 2020

Project No. 19-007

City of Windsor Engineering Dept.
350 City Hall Square West
Windsor, Ontario
N9A 6S1

Attention: Mr. Andrew Dowie, P.Eng.
Executive Initiatives Coordinator

**Re: East Riverside Shorewall Assessment & Functional Design
Final Report**

Dear Mr. Dowie:

In accordance with the terms of our proposal dated 21 May 2019, please be advised that we have completed our investigation and assessment of the City's shorewall structure along Lake St. Clair in East Riverside. The findings of our investigation and our design recommendations for the restoration of the shorewall are presented in the following report.

1.0 Background

It is our understanding that in 1931, the former Town of Riverside acquired approximately 30 acres of waterlots along the Lake St. Clair shoreline from the Province of Ontario and constructed a concrete breakwall on the property in cooperation with the Federal government. We understand that the subject waterlots were subsequently conveyed to the owners of the abutting shoreline properties – with the specific exception of the 1-foot wide strip of property upon which the concrete breakwall was built.

When the City of Windsor annexed the Town of Riverside in 1965, it also inherited the subject breakwall. This situation was brought to the attention of Windsor City Council in response to Council Question 25-2017, which noted the concerns of residents along Riverside Drive East with regard to both the ownership and the condition of the existing breakwall. It is our understanding that abutting property owners have reported the existence of several significant cracks and holes in the 85+ year old concrete

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Professional Engineers
Ontario

structure, resulting in active erosion occurring along portions of the privately-owned Lake St. Clair shoreline north of Riverside Drive East.

In response to the above, the City of Windsor retained Landmark Engineers in the summer of 2019 to undertake a condition assessment of the existing concrete breakwall structure located on the Lake St. Clair shoreline between the East Marsh Pumping Station (10864 Riverside Drive East) and the Rendezvous Shores subdivision.

A key plan, depicting the location of the study area, is provided here as Figure 1.

2.0 Purpose and Scope

The principal purpose of this study was to document and assess the condition of the City-owned concrete breakwall along the Lake St. Clair Shoreline between the East Marsh Pumping Station (10864 Riverside Drive East) and the Rendezvous Shores subdivision in East Riverside. The assignment also included the development of a functional design for a scope of shorewall repairs (including the adjacent privately-owned deck slabs), complete with cost estimates for budgeting purposes.

In carrying out this assignment, we executed the following scope of work:

- Visual and tactile inspections (both above and below the waterline) of the existing shoreline structure(s) within the study area;
- Documenting the condition of the existing shoreline structures (both above and below the waterline) via photographs and inspection notes;
- Identifying and quantifying observed deficiencies in the shoreline structure(s); and,
- Preparing functional design recommendations for repairs to the shoreline structures – including order-of-magnitude cost estimates for budgeting purposes.

Our findings are presented below.

3.0 Inspection Methodology

3.1 Underwater Inspections by Watech

For the underwater inspection component of this assignment, Landmark retained Watech Services Inc., a specialized marine engineering and inspection company based out of London, Ontario. We understand that on three separate occasions in October and November 2019, Watech mobilized a workboat to the site and carried out underwater inspections of the subject shorewall using a three-person inspection crew. Their crew included 2 commercial divers equipped with surface-supplied-air diving helmets, complete with inbuilt voice communication and recording devices.

Over the course of their inspections, Watech maintained location control by marking stationing in metres along the shorewall at regular intervals. Watech's stationing began with Station 0+000 at the steel sheet pile section of shorewall along the Rendezvous Shores subdivision, and

ran westerly as far as Station 1+330 at 10870 Riverside Drive East. Watech utilized this stationing to reference regular water depth measurements across the study area – as well as their observations of any deficiencies in the shorewall. We understand that underwater conditions were generally poor over the course of Watech’s inspections, with a maximum visibility noted of approximately 0.3m.

A copy of Watech’s full report, complete with above- and below-water photographs detailing the findings of their underwater inspections, is attached to this report as Appendix A.

3.2 Surface Inspections by Landmark

On 20 November 2019, Landmark staff mobilized a small watercraft and carried out a visual and tactile inspection of the subject shoreline from the water surface on Lake St. Clair. Photographs of the shoreline condition were taken at each individual property within the study area, starting with Property #1 at 10870 Riverside Drive East and working easterly as far as Property #69 at 11906 Riverside Drive East. Notes were also compiled to document the conditions observed at each property. A plan depicting the location of each property inspected by Landmark (cross-referenced with Watech’s stationing) is attached as Figures 2A and 2B.

A photographic inventory of the shoreline condition at each property within the study area is attached as Appendix B. A summary of Landmark’s field notes from the inspection (noting the general configuration and condition of the wall, deck slab, and other onshore features at each property) is attached as Appendix C.

4.0 Summary of Findings

A cross-section depicting the typical configuration of the existing shorewall and shoreline deck slab is attached as Figure 3. This figure was prepared based on the field observations and measurements documented by Watech and Landmark.

4.1 Shorewall Configuration & Condition

As depicted in Figure 3, the existing shorewall structure along the Lake St. Clair shoreline generally consists of a 1.8m-tall vertical concrete wall, cast in-place over timber pilings. The cast-in-place concrete shorewall appears to have been constructed with a slightly concave vertical curvature along its lakeward face, and was constructed with 100mm-diameter drain holes located near mid-height at intermittent locations along its length. The shorewall is generally approximately 0.3m wide along its top edge.

Over the course of Watech’s underwater inspections, it was noted that the concrete wall itself is generally in fair to good physical condition, with some spalling and cracking noted at isolated locations – generally at or near the waterline. This is consistent with the observations made in the field by Landmark. Watech also noted that the timber support piles appear to be in fair to good condition at the locations where they were visible. There is no evidence of any significant settlement or misalignment (vertical or horizontal) anywhere along the length of the shorewall within the study area.

The primary concern regarding the condition of the shorewall is that it appears to have been undermined by wave action along a significant proportion of its length, exposing the supporting timber piles and allowing the retained backfill to erode through the gap at the base of the wall. The intermittent drainage holes through the concrete wall could also be providing a pathway for wave action to erode the retained fill materials behind the shorewall.

4.2 Deck Slab Configuration & Condition

For most of the properties within the study area, a concrete deck slab runs along the shoreline, immediately inland of the concrete wall. The configuration of the deck slab varies considerably from property to property, with deck widths ranging between 2.5m and 5.0m, varying deck heights, and varying support configurations for the slab. Generally, it appears that the original deck slabs were supported directly on the backfill materials, while most of the newer-looking slabs rest directly on top of the concrete shorewall.

The condition of the existing deck slab also varies widely from property to property along the length of the subject shoreline, with several properties having what appears to be fairly new and intact slabs (e.g., Appendix B, Property #4 & #5), while others consist of severely broken slabs and/or rubble (e.g., Appendix B, Property #22 & #23). While some of the deterioration of the older deck slabs can be attributed to age and weathering, it appears that most of the damage exhibited in the shoreline deck slabs results from undermining and erosion of the underlying backfill.

Based on our inspection notes (see Appendix C), we estimate that approximately 40 of the 69 properties within the study area exhibit cracking and/or settlement in the deck slab to the extent that would warrant significant repair or replacement in the short- to medium-term.

4.3 Onshore Flood Control Features

As part of Landmark's inspection of the subject shoreline, we noted the presence and condition of any secondary walls and/or berms behind the shoreline deck slabs at each property within the study area (see Appendix C). We understand that many of these features were built in the late 1980s based on the City's 1986 *Shoreline Management Plan* (by N.K. Becker and Associates Ltd.) in an effort to mitigate inland flooding from Lake St. Clair.

The type, condition, and approximate top elevation of these features are listed in Appendix C for the information purposes. No assessment of the integrity and/or effectiveness of these features was carried out at this time, as these features are located on private property and this was beyond the scope of our current assignment.

5.0 Analysis and Discussion

As noted above and as indicated in the Watech report (see Appendix A), the existing concrete shorewall within the study area appears to be generally stable and in fair condition. Despite the structure's age and its exposure to harsh shoreline conditions, most of the concrete below the observed water level on Lake St. Clair appears to be generally sound, and the supporting timber

piles (where they are exposed) appear to be generally intact. Furthermore, there is no evidence of any significant settlement or misalignment (vertical or horizontal) anywhere along the length of the shorewall within the study area.

Based on the field observations documented by Landmark and Watech, it appears that the primary cause for structural damage and instability along the shoreline within the study area is the undermining of the concrete shorewall and the resulting loss of retained backfill. This ongoing erosion has led to the formation of significant voids below the shoreline deck slabs at several properties, and has resulted in the settlement and/or failure of many of the deck slabs.

Given the above, it is our opinion that any viable strategy for restoration of the shoreline along Lake St. Clair in East Riverside must include measures to fill the existing gaps along the base of the shorewall and protect it from further undermining in the future. Measures to plug and/or fill the existing drainage holes in the shorewall would also be necessary.

6.0 Recommended Works

6.1 Shorewall Repair and Toe Protection Works

To address the undermining and erosion issues noted above, we recommend that an engineered toe protection structure be constructed along the base of the existing shorewall along its entire length. Although toe protection could be achieved by a variety of means (including new steel sheet piling, tremie concrete wall extensions, etc.), we believe that the most practical and economical method would be to install appropriately-sized and bedded rip-rap along the base of the wall, as depicted in Figure 4. This would allow the toe protection to be constructed from a barge with a minimum of disruption to the lake bed and the existing shoreline properties. Based on our initial assessment, any other structural solution would be at least twice as expensive.

In implementing the rip-rap toe protection depicted in Figure 4, it is essential that the clear stone used to infill the gap at the base of the existing shorewall be covered with a heavy-duty non-woven geotextile, as shown. This will serve to prevent the migration of fine backfill materials through the layered rock protection and thus maintain the integrity of the shoreline structures. A similar method (or other non-pervious material) should also be used to plug and/or cap the existing drain holes through the concrete shorewall.

6.2 Slope Stabilization & Deck Restoration Options

With the toe protection installed as described above, the existing voids behind the shorewall and the deficiencies in the deck slab can then be properly addressed. Where the existing deck slab is exhibiting significant cracking and/or settlement, we would recommend that the slab be removed and the underlying eroded backslope covered with a heavy-duty non-woven geotextile. The void can then be filled with an appropriate granular material and a structurally-reinforced deck slab reconstructed over top. In designing the slab, the north edge should rest directly over the shorewall, while the south edge should be supported by a cast-in-place grade beam, founded to frost depth. This scope of slope stabilization and deck restoration works is presented as Option #1 in Figure 5.

Where the existing deck slab is in good condition, it may be possible to stabilize the underlying slope by drilling holes through the deck surface and filling the underlying voids with unshrinkable lean concrete fill. This option (i.e., Option #2 – see Figure 6) would serve to address the eroded backslope while minimizing the disruption to the existing shoreline.

6.3 Cost Estimate

Based on the scope of repairs outlined in Figures 4 through 6 and described above, we estimate that the construction cost to repair and restore the shoreline structures within the study area will be in the order of **\$3.5 to \$4.5 Million** (excluding HST). A breakdown of this cost estimate is provided in Table 1, based roughly on unit rates that we have observed on similar barge-based construction projects in recent years.

Please note that the breakdown provided in Table 1 represents our most optimistic projections for the scope of work outlined herein. Given the relative inaccessibility of the site (backing entirely on private property) and potential for difficulties to arise during construction due to strong winds, high waves, and inclement weather, we have added a \$1 Million estimating contingency to our base estimate to provide the range of costs noted above. Please also note that our cost estimate does not include allowances for engineering, administration, or financing.

Thank you for the opportunity to work on this assignment. We trust that the above will be sufficient for your purposes. If you have any questions or concerns, please do not hesitate to call.

Yours truly,

Landmark Engineers Inc.

Daniel M. Krutsch, P.Eng.



David T. Killen, P.Eng.





LAKE ST. CLAIR

STUDY AREA

FLORENCE AVE.

RIVERSIDE DRIVE EAST

WYANDOTTE STREET EAST

GREENPARK BLVD.

BANWELL RD.

LESPERANCE RD.

LITTLE RIVER BLVD



Project: City Council Agenda - Monday, October 30, 2020 EAST RIVERSIDE SHOREWALL ASSESSMENT Page 441 of 457	Title: KEY PLAN – STUDY AREA	Date: 14 JAN. 2020	FIGURE 1
		Scale: NTS	
		Project No.: 19-007	



LEGEND

- ① PROPERTY IDENTIFICATION #
- PROPERTY LIMITS
- 0+500 | STATIONING (AS PER WATECH REPORT)



Title	PROPERTIES INSPECTED (10870 TO 11382 RIVERSIDE DR. E.)		Date	15 DEC 19	FIGURE 2A
	Project	EAST RIVERSIDE SHOREWALL ASSESSMENT		Scale	
		Project No.	19-007		

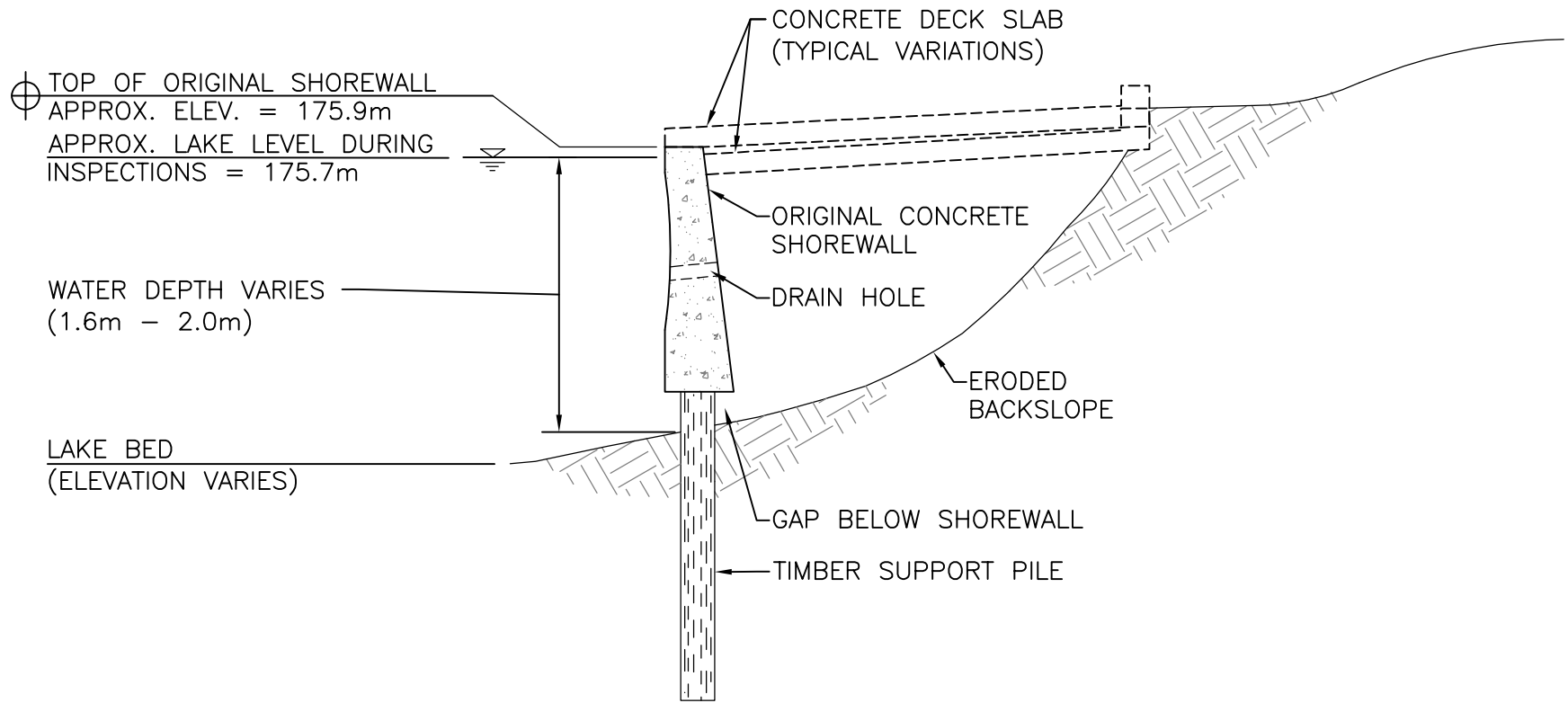


LEGEND

- ① PROPERTY IDENTIFICATION #
- PROPERTY LIMITS
- 0+500
| STATIONING (AS PER WATECH REPORT)

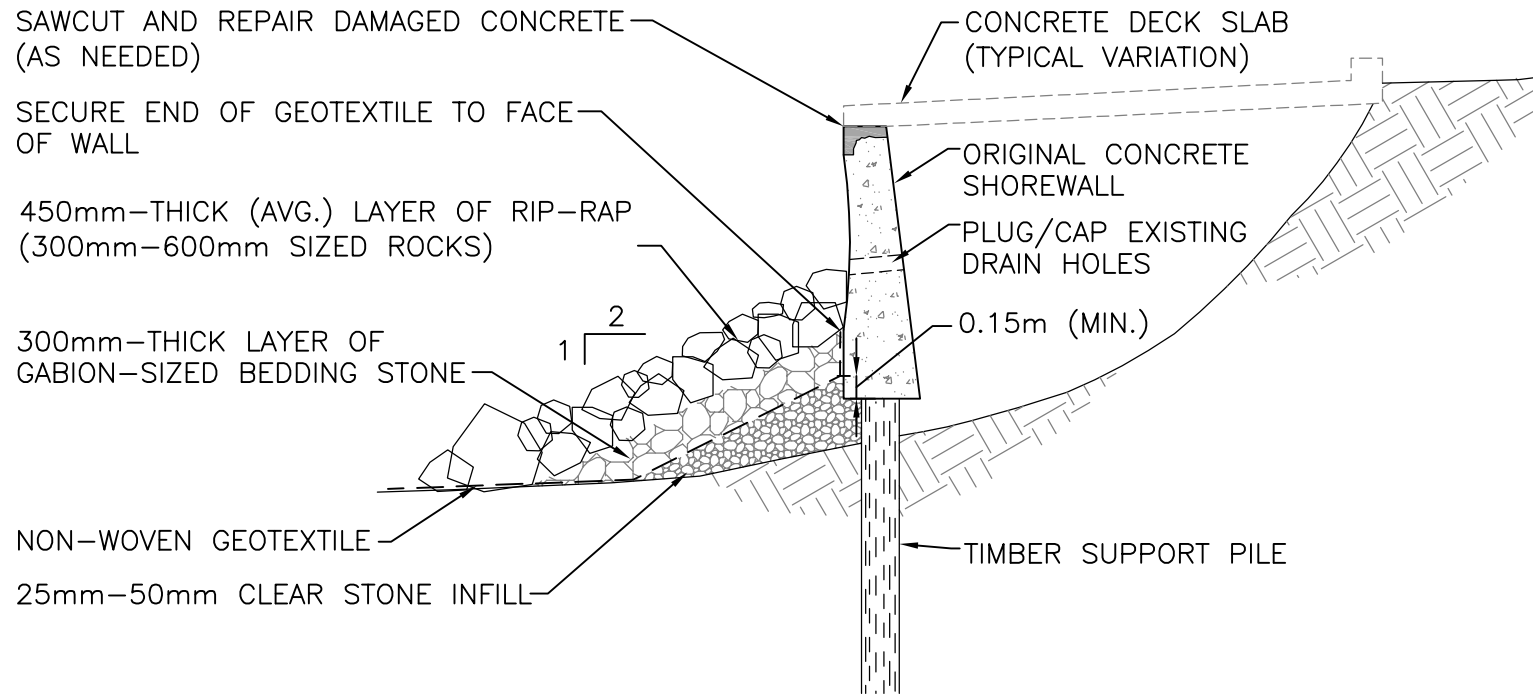


Title	PROPERTIES INSPECTED (11382 TO 11906 RIVERSIDE DR. E.)		Date	15 DEC 19	FIGURE 2B
	Project	EAST RIVERSIDE SHOREWALL ASSESSMENT		Scale	
		Project No.	15-007		



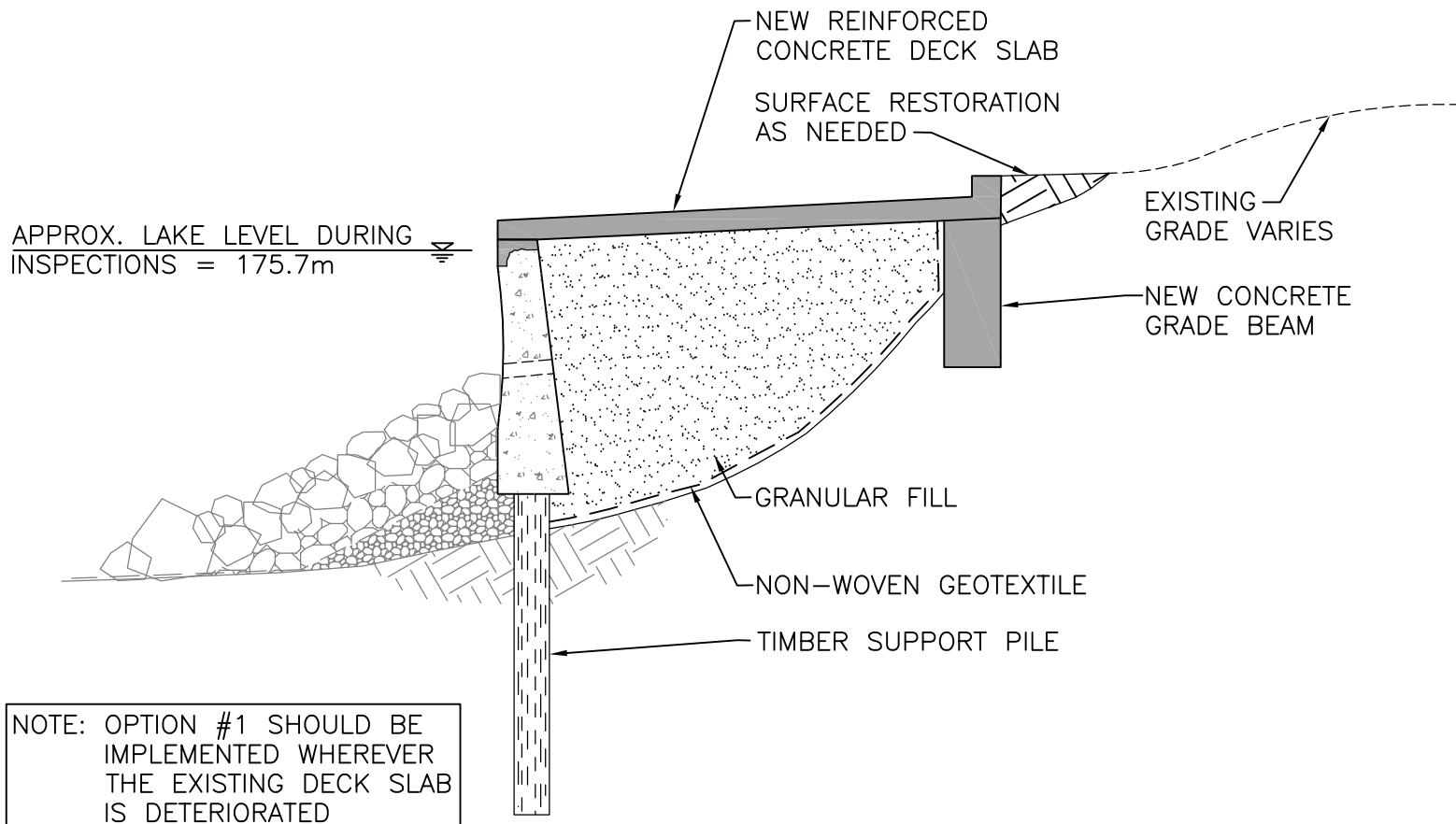
Title	EXISTING CONDITIONS (TYPICAL)	Date	14 JAN. 2020	FIGURE 3
	Project	EAST RIVERSIDE SHOREWALL	Scale	
ASSESSMENT & FUNCTIONAL DESIGN		Project No.	19-007	

City Council Agenda - Monday, December 30, 2023
Page 444 of 457



Project	Title	PROPOSED WORKS: SHOREWALL REPAIR AND TOE PROTECTION EAST RIVERSIDE SHOREWALL	Date	14 JAN. 2020
	ASSESSMENT & FUNCTIONAL DESIGN		Scale	1:50
	City Council Agenda Monday, October 30, 2023		Project No.	19-007

FIGURE
4



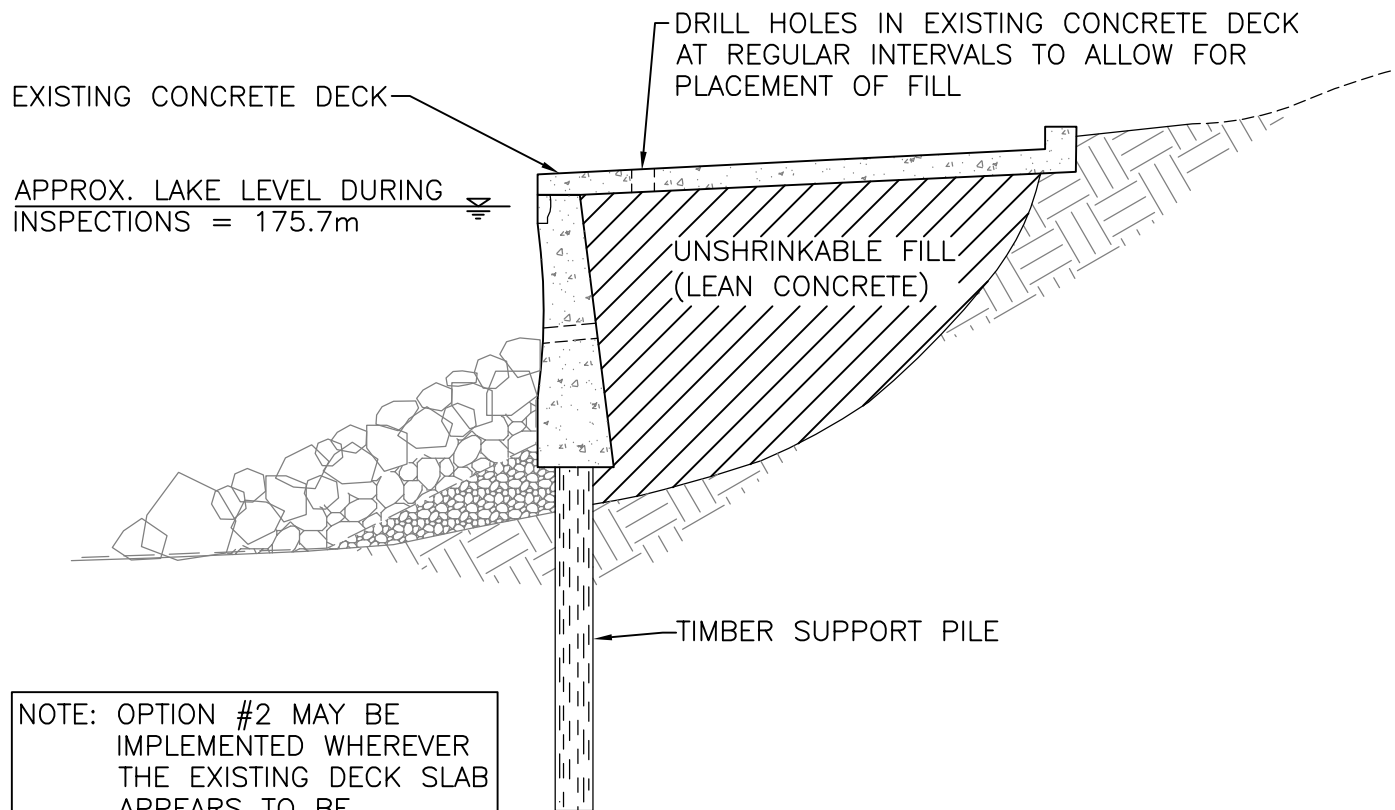
NOTE: OPTION #1 SHOULD BE IMPLEMENTED WHEREVER THE EXISTING DECK SLAB IS DETERIORATED



Title	PROPOSED WORKS: SLOPE STABILIZATION & DECK RESTORATION (OPTION #1)
	Project
EAST RIVERSIDE SHOREWALL	
ASSESSMENT & FUNCTIONAL DESIGN	

Date	14 JAN. 2020
Scale	1:50
Project No.	19-007

FIGURE
5



NOTE: OPTION #2 MAY BE IMPLEMENTED WHEREVER THE EXISTING DECK SLAB APPEARS TO BE DETERIORATED



Title	PROPOSED WORKS: SLOPE STABILIZATION (OPTION #2)	Date	14 JAN. 2020
Project	EAST RIVERSIDE SHOREWALL	Scale	1:50
	ASSESSMENT & FUNCTIONAL DESIGN	Project No.	19-007

FIGURE
6

**Table 1 - East Riverside Shoreline Restoration Works
Budget Estimate**

ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	UNIT PRICE	TOTAL PRICE
<u>SECTION A - SHOREWALL REPAIR & TOE PROTECTION WORKS</u>					
1	Patch repairs to existing concrete shorewall	Cu. M.	5	\$20,000.00	\$100,000.00
2	Supply and place clear stone infill	Tonnes	1,200	\$90.00	\$108,000.00
3	Supply and install geotextile	Sq. M.	5,000	\$5.00	\$25,000.00
4	Supply and place gabion-sized bedding stone	Tonnes	1,400	\$90.00	\$126,000.00
5	Supply and place rip-rap	Tonnes	2,800	\$110.00	\$308,000.00
TOTAL SECTION A					<u>\$667,000.00</u>
<u>SECTION B - SLOPE STABILIZATION & DECK RESTORATION WORKS (Option #1)</u>					
1	Break up, remove, and dispose of existing concrete deck slab and underlying debris	Sq. M.	3000	\$25.00	\$75,000.00
2	Rough grade backslope	Sq. M.	3000	\$10.00	\$30,000.00
3	Supply and install geotextile	Sq. M.	3000	\$5.00	\$15,000.00
4	Supply, place, and consolidate granular fill	Tonnes	7500	\$80.00	\$600,000.00
5	Construct new reinforced concrete grade beam	Lin. M.	760	\$500.00	\$380,000.00
6	Construct new reinforced concrete deck slab	Sq. M.	3000	\$200.00	\$600,000.00
TOTAL SECTION B					<u>\$1,700,000.00</u>
<u>SECTION C - SLOPE STABILIZATION WORKS (Option #2)</u>					
1	Drill holes in existing concrete deck and infill void with unshrinkable lean concrete fill	Cu. M.	2000	\$500.00	\$1,000,000.00
TOTAL SECTION C					<u>\$1,000,000.00</u>
<u>SECTION D - MISCELLANEOUS COSTS</u>					
1	Mobilization & Demobilization	Lump Sum			\$80,000.00
2	Yard Restoration Works	Allowance			\$28,000.00
3	Contract Costs	Allowance			\$25,000.00
TOTAL SECTION D					<u>\$133,000.00</u>
TOTAL ESTIMATED COST					<u>\$3,500,000.00</u>

APPENDIX B

Amendment to: Local Improvement Policy Consolidation

4.6 Shoreline Structures

a) Where:

- A municipally owned shoreline structure abuts one or more privately owned properties; and
- The abutting property owner(s) have requested in writing that repair and/or replacement of the shoreline structure be carried out; or
- The City initiates repair or replacement of the shoreline structure as a Local Improvement.

The abutting property owner(s) shall be assessed for:

- 100% of the cost per metre frontage for the repair and/or replacement of the shoreline structure abutting their property, including restoration.
- 100% of work required in the backshore of the benefitting property.

The City will:

- Pay 100% of the cost per metre frontage for the repair and/or replacement of the shoreline structure abutting municipally owned property.
- Assess the costs plus rate of interest determined by the City Treasurer to the property owner, to be repayable as a one time payment or as a Local Improvement Charge assessed to the property owner for the desired repayment period of no longer than 20 years.

WELCOME

to the
MARCH 31, 2023

PUBLIC MEETING

PROPOSED LOCAL IMPROVEMENT POLICY AMENDMENT

(SHORELINE STRUCTURES)

&

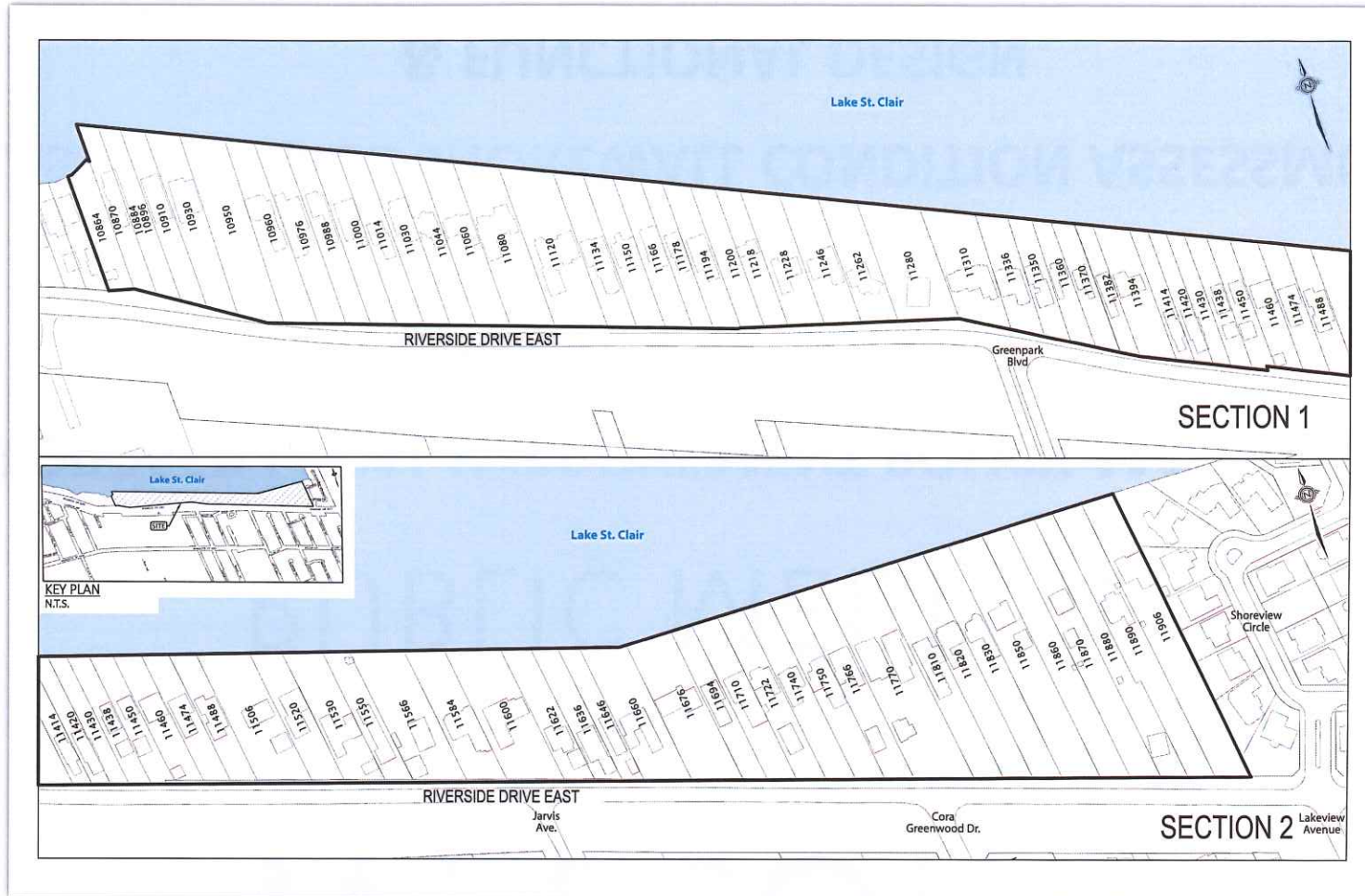
EAST RIVERSIDE SHOREWALL CONDITION ASSESSMENT

& FUNCTIONAL DESIGN

(PREPARED BY LANDMARK ENGINEERS INC.)



EAST RIVERSIDE AFFECTED AREAS



HISTORY OF EAST RIVERSIDE SHOREWALL

- 1931 – Concrete breakwall constructed by Town of Riverside.
- 1935+/- – Lots were conveyed to owners with specific exception of the breakwall by Riverside.
- 1966 – City of Windsor Annexes Town of Riverside and inherits the breakwall.
- 2017 – Windsor City Council hears concerns on condition and ownership of breakwall (CQ25-2017) from councilor Kusmierczyk.
- 2018 – City Council Report C188/2018 provides background to City Council.
- 2019 – Council Decision B14/2019 funds shorewall assessment report and directs development of an LIP policy amendment.
- 2020 – Landmark Engineers Inc. finalizes their East Riverside Shorewall Assessment and Functional Design Report.
- 21/22 – City of Windsor coordinates with government agencies and develops LIP amendment.
- 2023 – This public meeting to discuss report and policy.

EXISTING LOCAL IMPROVEMENT POLICY (LIP)

Ontario Regulation 586-06 Local Improvement Charges – Priority Lien Status

- Grants the power to Municipalities to construct infrastructure work requested by the residents or proposed by the Municipality by passing a by-law to undertake the work as a local improvement for the purpose of raising all or part of the cost of the work by imposing special charges on,
 - (a) lots that abut on the work;
 - (b) lots that do not abut on the work but will be immediately benefited by it; or
 - (c) a combination of the lots described in clauses (a) and (b). O. Reg. 586/06, s. 5 (1).

Definition of work includes many types of infrastructure (a-q) including:

- (i) Constructing a retaining wall, dyke, breakwater, groyne, crib or other shore protection work along a body of water;

City of Windsor – Local Improvement Policy Consolidation – S 60-2020

- **Purpose:** To present a cost-sharing policy setting forth special assessments for municipal infrastructure such as storm and sanitary sewers, street lighting, sidewalks, pavements, curbs and gutters and private drain connections constructed under the provisions of the Local Improvement Regulation, O. Reg. 586/06.
- **Does not** currently include shoreline protection works, see LIP Amendment – Shoreline Structures

PROPOSED SHORELINE STRUCTURES AMENDMENT

Where:

- A municipally owned shoreline structure abuts one or more privately owned properties; and
- The abutting property owner(s) have requested in writing that repair and/or replacement of the shoreline structure be carried out; or
- The City initiates repair or replacement of the shoreline structure as a Local Improvement.

The abutting property owner(s) shall be assessed for:

- 100% of the cost per metre frontage for the repair and/or replacement of the shoreline structure abutting their property, including restoration.

The City will:

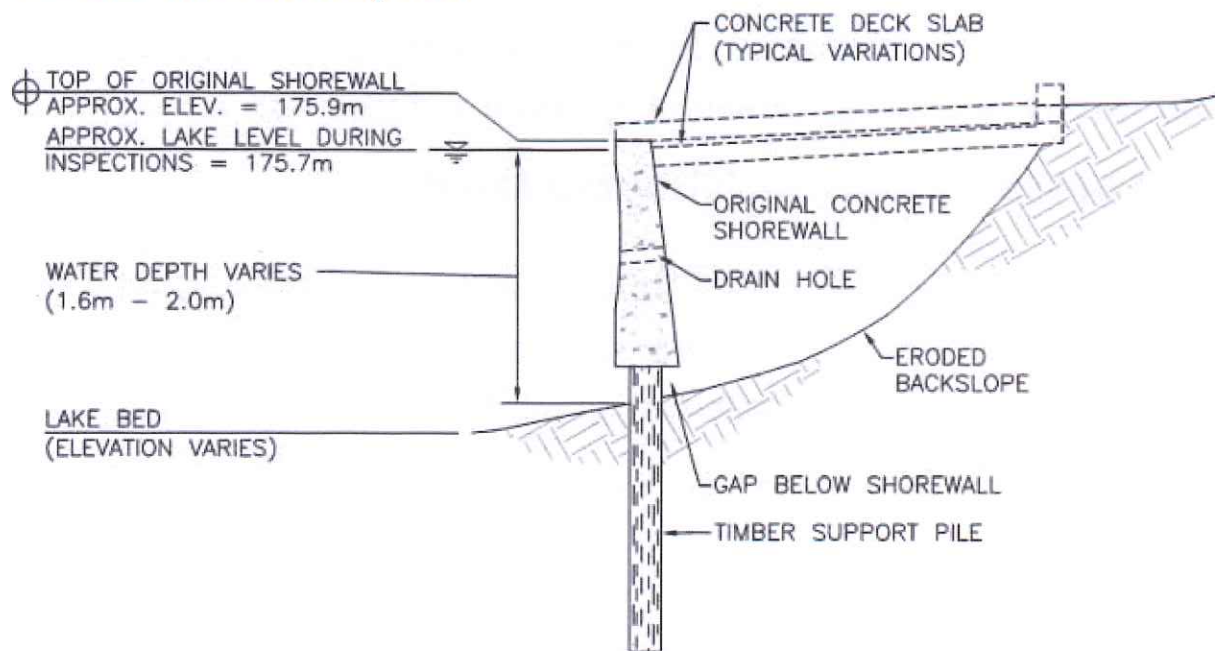
- Pay 100% of the cost per metre frontage for the repair and/or replacement of the shoreline structure abutting municipally owned property.
- Assess the costs plus rate of interest determined by the City Treasurer to the property owner, to be repayable as a one time payment or as a Local Improvement Charge assessed to the property owner for the desired repayment period of no longer than 20 years.

EAST RIVERSIDE SHOREWALL ASSESSMENT

From Report prepared by Landmark Engineers Inc.

Condition Assessment:

- The concrete wall itself is generally **in fair to good physical condition**, with some spalling and cracking noted at isolated locations – generally at or near the waterline.
- The timber support piles **appear to be in fair to good condition** at the locations where they were visible.
- There is **no evidence** of any significant settlement or misalignment (vertical or horizontal) anywhere along the length of the shorewall within the study area.



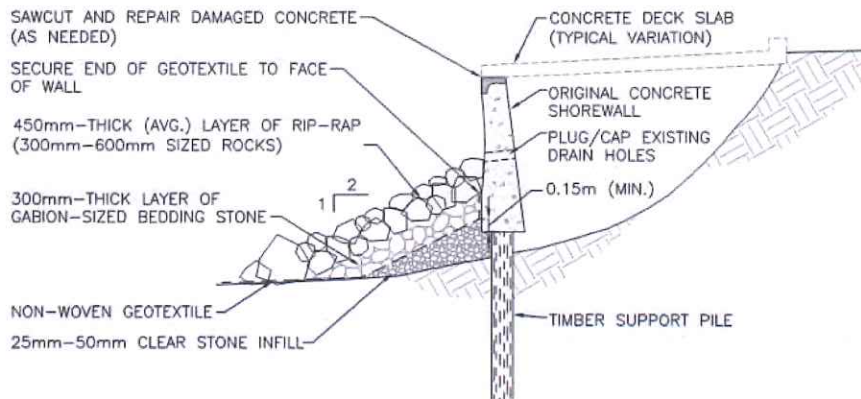
EXISTING CONDITIONS (TYP.)

Primary Concern:
Erosion / Undermining
of the wall and deck slab

EAST RIVERSIDE SHOREWALL FUNCTIONAL DESIGN

From Report prepared by Landmark Engineers Inc.

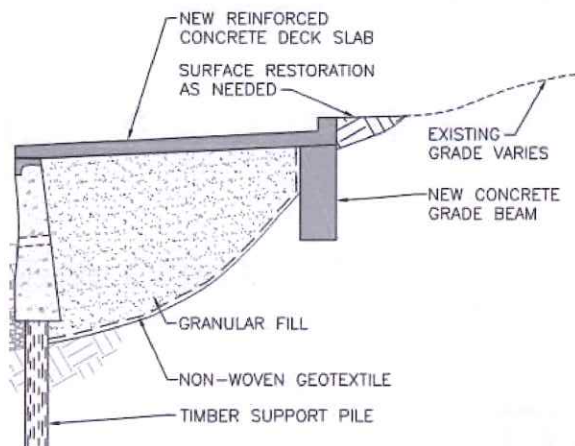
PROPOSED SHOREWALL REPAIR AND TOE PROTECTION



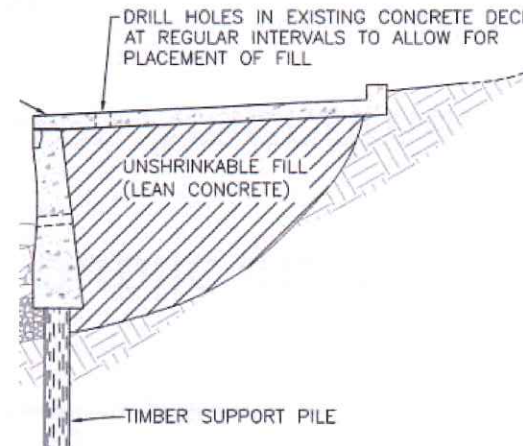
Stone Toe Protection:

- Stabilizes wall
- Prevents further undermining
- Breaks waves

PROPOSED DECK RESTORATION AND SLOPE STABILIZATION OPTIONS



FULL REPLACEMENT



STABILIZATION OF EXISTING SLABS

NEXT STEPS

1. Review comments from this public meeting.
2. Submit LIP Shoreline Structures Amendment for City Council review and approval.
3. Petition for Local Improvement of the shorewall.
4. If petition is successful, LIP process will be followed for design, construction, and payment assessment.