

## **APPENDIX A – Summary of Capital Project Variances – March 31, 2022**

Listed below is a summary by Department/Program as to the status of each capital project portfolio. Projects with any projected final variance are detailed in a table within the respective Department/Program summary.

Project explanations denoted with “Project surplus/deficit” are projects that are in a position to be closed and the variance is likely to materialize. Those denoted with “Anticipated surplus/deficit” are projects that are still ongoing and not completed, as such the preliminary variance is merely an estimate which may fluctuate significantly before the project is complete.

### **Mayor’s Office**

#### *Mayor’s Office:*

There are three active capital projects in this area that are being administered by the Mayor’s Office. No project variance is anticipated as all of these projects are currently expected to come in on budget.

### **Office of the Commissioner of Economic Development & Innovation**

#### *Economic Development:*

There is one active capital project in this area that is being administered by the Economic Development department. No project variance is anticipated at this time.

#### *Information Technology:*

There are 20 active capital projects in this area that are being administered by the Information Technology department. Only one project, as identified below, is reporting a surplus variance of \$4,400.

<b>Projects with Projected Deficit/Surplus</b>	<b>(Deficit)/Surplus Amount</b>	<b>Brief Explanation</b>
S.W.I.F.T Network (7193001)	\$4,400	Project is complete and can be CLOSED. Funds will be returned to their original funding source, Fund 160.

#### *Planning and Building Development:*

There are 21 active capital projects in this area that are being administered by the Planning and Building department. No project variance is anticipated as all of these projects are currently expected to come in on budget.

Transportation Planning:

There are 12 active capital projects in this area that are being administered by the Planning department. One project variance, with a surplus of \$25,074, has been identified and is discussed in the table below.

Projects with Projected Deficit/Surplus	(Deficit)/Surplus Amount	Brief Explanation
Labelle / Northway Pedestrian Crossing (7191006)	\$25,074	Project is complete and can be CLOSED. Administration requests that surplus funds from this project be transferred to the Pedestrian Crossovers project, 7191010, to maximize the number of pedestrian crossovers that can be installed.

## Office of the Commissioner of Infrastructure Services

Street Lighting:

There is only one active capital project in this area that is being administered by the Engineering division. The Local Improvement – Street Lighting project (7161015) is expected to come in on budget.

Roadways:

There are 40 active capital projects in this area that are being administered by the Engineering department. There are two projects with a net deficit of (\$6,389), identified per below.

Projects with Projected Deficit/Surplus	(Deficit)/Surplus Amount	Brief Explanation
Ouellette Ave. Streetscape Phase 6 (7143003)	\$770	Project is complete and can be CLOSED. Administration recommends that surplus be used to mitigate shortfall in 2017 Connaught St. Reconstruction project, 7171064.
2017 Connaught St. Reconstruction (7171064)	(\$7,159)	Project is complete and can be CLOSED. Administration recommends that deficit be offset with transfers of \$6,389 and \$770, respectively, from the Memorial – Vimy East of Marentette project, 7171004, and the Ouellette Ave. Streetscape Phase 6 project, 7143003.

Sewer Rehabilitation:

There are 42 active capital projects in this area that are being administered by the Engineering department. In total, there are four projects with a net projected surplus of \$274,989. These projects are identified in the table below.

<b>Projects with Projected Deficit/Surplus</b>	<b>(Deficit)/Surplus Amount</b>	<b>Brief Explanation</b>
Francois – Seminole to Alice St. (7181006)	\$287,600	Projected surplus is a result of favourable tender pricing from initial budget estimates. Project can be CLOSED after surface asphalt charges are processed, with surplus funds being returned to the Sewer Surcharge Reserve, Fund 153.
South National Trunk Sanitary Relining (7134005)	\$50,000	Projected surplus is a result of favourable tender pricing from initial budget estimates. Project can be CLOSED after surface asphalt charges are processed, with surplus funds being returned to the Sewer Surcharge Reserve, Fund 153.
Memorial - Vimy East of Marentette (7171004)	\$6,389	Construction is complete. Project will remain open for final reconciliation. Upon closing, Administration is recommending that \$6,389 of surplus funds in this project be transferred to the 2017 Connaught St Reconstruction project, 7171064.
DMAF – Tranby Road and Park (7191038)	(69,000)	Project is complete and on maintenance. Projected deficit due to work on relocated baseball diamond. Administration will recommend funding sources to mitigate the deficit at a future point in time.

Storm Sewers:

There are 15 active capital projects in this area that are being administered by the Engineering department. All projects are anticipated to come in on budget.

Sanitary Sewers:

There are five active capital projects in this area that are being administered by the Engineering department. All projects are anticipated to come in on budget.

Corporate Projects:

There are 53 active capital projects in this area that are being administered by the Corporate Projects department. Five projects, detailed below, are anticipated to close with a net surplus of \$213,123.

<b>Projects with Projected Deficit/Surplus</b>	<b>(Deficit)/Surplus Amount</b>	<b>Brief Explanation</b>
Willistead Improvements – M264/2012 (7125002)	\$190,000	Administration is recommending that \$190,000 of unallocated funding be transferred to the Corporate Heating & Cooling Repair Program project, 7142000, for general Heating & Cooling work which is anticipated to cost more than anticipated due to inflation.
Stormwater Financing Study (7181013)	\$188,198	Project is complete and can be CLOSED. Administration recommends that funds be transferred to the Stormwater Financing Implementation project, 7229002.
350 CHS DEW Hook Up – 2011 (7141044)	\$157,166	Project is complete and can be CLOSED. Administration recommends that funds be transferred to the Corporate Heating & Cooling Repair Program project, 7142000, to support the related District Energy infrastructure and to mitigate budget variances due to overall increasing contract prices caused by inflation.
DND CCTV Security Camera (7211051)	\$24,925	Project is expected to be complete in Q2 2022 and can be CLOSED at that time. Upon closing, Administration recommends that surplus funds be transferred to a new project, specifically to meet capital repair needs as identified at Major F A Tilston, V C Armoury and Police Training Centre.
Corporate Heating & Cooling Repair Program	(\$347,166)	Projected deficit due to anticipated extra costs for general Heating & Cooling work and increased costs to support the District Energy infrastructure. A portion of this project's funding, budgeted for general Heating and Cooling projects throughout the City, was recently allocated to an HVAC cooling tower upgrade at Willistead Manor. This in turn, has reduced Administration's ability

		to complete much needed Heating and Cooling work across the City due to inflation. Administration is recommending transfers of surplus funding from the Willistead Improvements – M264/2012 project, 7125002, and the 350 CHS DEW Hook Up – 2011 project, 7141044, of \$190,000 and \$157,166, respectively, to address the anticipated shortfall.
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*Border Infrastructure Fund:*

There are only two active capital projects in this area that are being administered by the Corporate Projects department. These projects are expected to be on budget as they are fully funded by the Ministry of Transportation and are expected to be complete by the end of 2022.

*Development:*

There are five active capital projects in this area that are being administered by the Design & Development department. All projects are anticipated to come in at budget.

*Pollution Control:*

There are 76 active capital projects in this area that are being administered by the Pollution Control department. The majority of these projects are funded from the dedicated Pollution Control Reserve. There are three projects as identified in the table below with no net surplus or deficit.

<b>Projects with Projected Deficit/Surplus</b>	<b>(Deficit)/Surplus Amount</b>	<b>Brief Explanation</b>
LRPCP Dewater Polymer Batch System (7161022)	\$208,000	Project can be CLOSED. Administration recommends that remaining surplus be transferred to the LRPCP Sludge Pumphouse Valve project, 7192004.
LRWRP-Transformer Compound Wall (7211023)	\$24,000	Project can be CLOSED. Administration recommends that remaining surplus be transferred to the LRPCP Sludge Pumphouse Valve project, 7192004.
LRPCP Sludge Pumphouse Valve (7192004)	(\$232,000)	Consultant estimate came in much higher than anticipated. Administration recommends that remaining surplus funds in the LRPCP Dewater Polymer Batch System project, 7161022, and the LRWRP - Transformer Compound Wall project, 7211023, be transferred to this project to help mitigate the anticipated deficit. Additional funding, if required, will be requested in a future Council Report.

Environmental Services:

There is one capital project being administered by the Environmental Services division, which is expected to come in on budget.

Contracts & Field Services:

There are four active capital projects in this area that are being administered by the Public Works Operations department. No surpluses or deficits are anticipated at this time, as all four projects are expected to come in on budget.

Road Rehabilitation:

There are 19 active capital projects in this area that are being administered by the Public Works Operations department. There are nine projects as identified in the table below anticipating a net total project surplus of \$5,267,502.

<b>Projects with Projected Deficit/Surplus</b>	<b>(Deficit)/Surplus Amount</b>	<b>Brief Explanation</b>
2019 Bridge Rehabilitation Program (7191021)	\$1,934,582	The University @ CPR structure (#144) originally scheduled for reconstruction has been cancelled. Project is complete and can be CLOSED. Administration recommends that surplus funds be transferred to the 2022 Bridge Rehabilitation project, 7221009, to complete as much priority work as possible.
Connecting Links 5 Tecumseh - College (7202007)	\$1,490,000	Anticipated surplus is a result of favourable tender pricing from initial budget estimates. Project can be CLOSED once remaining funding from 2023 is transferred into project. Administration recommends transferring anticipated surplus funding of \$107,000 to the Traffic Signal Upgrades & Replacement project, 7209000, and \$1,383,000 to the 2022 Road Rehabilitation project, 7221001, to complete as much priority work as possible.
Connecting Links 4 - Malden - Pool (7192010)	\$948,000	Anticipated surplus is a result of favourable tender pricing from initial budget estimates. Project can be CLOSED once remaining funding from 2023 is transferred into project. Administration recommends transferring \$948,000 of surplus funds to the 2022 Road Rehabilitation project, 7221001, to complete as much priority work as possible.

2018 Bridge Rehabilitation Program (7181022)	\$590,812	Anticipated surplus is a result of favourable tender pricing from initial budget estimates. Project can be CLOSED once holdback is released. Administration recommends that remaining funding be transferred to the 2022 Bridge Rehabilitation project, 7221009, to complete as much priority bridge work as possible.
St Luke – Seminole to ETR (7184000)	\$140,968	Project is complete and can be CLOSED. Administration recommends that surplus funds be transferred to the 2022 Road Rehabilitation project, 7221001, to complete as much priority work as possible.
Wyandotte – St Rose to Janisse (7183019)	\$74,000	Project is complete and can be CLOSED, once remaining funding of \$440,000 is transferred to the project in 2023. Administration recommends transferring surplus funds to the 2023 Road Rehabilitation project to facilitate completion of as much priority road work as possible.
Courtland – South National to South National (7183020)	\$44,000	Project is complete and can be CLOSED, once remaining funding of \$120,000 is transferred into the project in 2023. Administration recommends transferring surplus funds to the 2023 Road Rehabilitation project to facilitate completion of as much priority road work as possible.
Annie St. – Tecumseh to Cul De Sac (7183021)	\$34,000	Project is complete and can be CLOSED, once remaining funding of \$90,000 is transferred into the project in 2023. Administration recommends transferring surplus funds to the 2023 Road Rehabilitation project to facilitate completion of as much priority road work as possible.
Victoria – Tecumseh to Jackson Park (7183006)	\$11,140	Project is complete and can be CLOSED. Administration recommends transferring remaining surplus to the 2022 Road Rehabilitation project, 7221001, to facilitate completion of as much priority road work as possible.

PW Maintenance:

There are seven active capital projects in this area that are being administered by the Public Works Operations department. All are projected to be on budget.

Traffic Operations and Parking Services:

There are 15 active capital projects in this area that are being administered by the Traffic Operations and Parking Services department, and in total is expecting to come in on budget.

Fleet Operations:

There are 21 active capital projects in this area that are being administered by the Public Works Operations department. There are three projects as identified in the table below anticipating a net total surplus of \$29,543:

<b>Projects with Projected Deficit/Surplus</b>	<b>(Deficit)/Surplus Amount</b>	<b>Brief Explanation</b>
Fuel Site Equipment Replacement (7221007)	\$36,550	Administration recommends that funds be transferred to mitigate the deficit in the Fuel Site Improvements Ward 3 project, 7181030, estimated at \$36,550.
Ditch Cutting Equipment – Vacant Properties (7181033)	\$29,543	Project is complete and can be CLOSED. Administration recommends that the remaining surplus be transferred to the Parks Equipment Reserve, Fund 197.
Fuel Site Improvements – Ward 3 (7181030)	(\$36,550)	Project is complete and can be CLOSED. Deficit is a result of unexpected remediation work that was required at the Fire Headquarters fuel site. Administration recommends that deficit be mitigated with a transfer of funds from the Fuel Site Equipment Replacement project, 7221007.

Technical Support:

There is only one active capital project in this area that is being administered by the Technical Support division. The Information Hansen Management System project (7209001) is expected to come in on budget.

Transit Windsor:

There are 20 active capital projects in this area that are being administered by Transit Windsor. No surpluses or deficits are being reported at this time.



## Office of the Commissioner of Legal & Legislative Services

### Fire and Rescue:

There are 15 active capital projects in this area that are being administered by the Fire and Rescue department. One project, with a surplus of \$2,674, is detailed in the table below.

Projects with Projected Deficit/Surplus	(Deficit)/Surplus Amount	Brief Explanation
Mobile Live Fire Training Simulator (7171088)	\$2,674	This project is complete and can be CLOSED. Administration recommends that surplus funds be transferred to the 2021 Fire Fleet Replacement project, 7211045.

### Legal Services:

There are 12 active capital projects in this area that are being administered by the Legal Department. No project deficits are being reported at this time.

### Records and Elections:

There are two active capital projects in this area that is being administered by the Council Services department. Both projects are anticipated to come in on budget.

## Office of the Commissioner of Corporate Services

### Corporate Asset Planning:

There are 26 active capital projects in this area that are being administered by the Asset Planning division. No project variance is anticipated as all of these projects are currently expected to come in on budget.

### Financial Accounting:

There are two active capital projects in this area that are being administered by the Financial Accounting division. No project variance is anticipated as all of these projects are currently expected to come in on budget.

### Financial Planning:

There are five active capital projects in this area that are being administered by the Financial Planning division. No project variance is anticipated as all of these projects are currently expected to come in on budget.

### Taxation and Financial Projects:

There are five active capital projects in this area that are being administered by the Taxation and Financial Projects division. No project variances are being reported at this time.

### Human Resources:

There are six active capital projects in this area that are being administered by the Human Resources department. No project variance is anticipated as all of these projects are currently expected to come in on budget.

## **Office of the Commissioner of Human & Health Services**

### Huron Lodge:

There are 11 active capital projects being administered by Huron Lodge. No project variance is anticipated as all of these projects are currently expected to come in on budget.

### Housing and Children's Services:

There are two active capital projects being administered by Housing and Children's Services. No project variance is anticipated as both of these projects are currently expected to come in on budget.

## **Office of the Commissioner of Community Services**

### Cultural Affairs:

There are six active capital projects in this area that are being administered by the Recreation and Culture department. No project variance is anticipated as all of these projects are currently expected to come in on budget.

### Recreation Facilities:

There are 15 active capital projects in this area that are being administered by the Recreation & Culture department. No project variance is anticipated as all of these projects are currently expected to come in on budget.

### Forestry:

There are three active capital projects in this area that are being administered by the Forestry division. No surpluses or deficits are projected at this time.

### Parks Operations:

There are eight active capital projects in this area that are being administered by the Parks Operations division. No surpluses or deficits are projected at this time.

Parks Design & Development:

There are 48 active capital projects in this area administered by the Parks Design & Development department. The department anticipates a total deficit of (\$158,076), as noted below:

<b>Projects with Projected Deficit/Surplus</b>	<b>(Deficit)/Surplus Amount</b>	<b>Brief Explanation</b>
Little River Corridor Bridges (7171027)	\$36,736	Project is complete and can be CLOSED. Administration recommends that surplus funds be used to mitigate a deficit, estimated at (\$31,636), in the Alexander Park Washroom project, 7171070, with remaining funds allocated to the Park Trails Capital Rehabilitation Program project, 7161026.
East Riverside Little River Rd (7171026)	\$20,490	Project is complete and can be CLOSED. Administration recommends that surplus funds be transferred to the Park Trails Capital Rehabilitation Program project, 7161026.
Park Trails Capital Rehab Prgm (7161026)	(25,590)	Administration recommends transfers of remaining surplus funding, estimated at \$20,490 and \$5,100, respectively, from the East Riverside Little River Road project, 7171026, and Little River Corridor Bridges project, 7171027, to complete trails identified for prioritized capital replacement.
Alexander Park Washroom (7171070)	(\$31,636)	Project is complete and can be CLOSED. Administration recommends mitigating the deficit with a transfer of surplus funding from the Little River Corridor Bridges project, 7171027.
Jackson Park Trails / Parking Lot / Washrooms / Maintenance (7181026)	(\$158,076)	Anticipated deficit due to inflationary costs for the final second layer of asphalt needed for the Jackson Park parking lot. Additional inflationary costs are anticipated for outstanding asphalt work required at the Parkside Tennis parking lot. Administration recommends that \$145,075.85 in unallocated funding from Canada Community Building Fund Reserve, Fund 176, as well as \$13,000 in funds from the Tennis Facility Jackson Park Reserve, Account 1780, be transferred to this project to mitigate this deficit.

Facilities Operations:

There are 9 active capital projects in this area administered by the Facilities Operations department. No surpluses or deficits are projected at this time.

Customer Service:

There is only one active capital project in this area that is being administered by the Customer Service department. The 311/211 Phone System Upgrades project (7191007) is reporting a surplus of \$37,485 as detailed below.

<b>Projects with Projected Deficit/Surplus</b>	<b>(Deficit)/Surplus Amount</b>	<b>Brief Explanation</b>
311/211 Phone System Upgrades (7191007)	\$37,485	Project is complete and can be CLOSED. Surplus funds will be returned to their original funding source, Fund 169 – Pay-As-You-Go Reserve.

Windsor Public Library:

There are 10 active capital projects in this area that are being administered by the Windsor Public Library and the Corporate Projects department. There are no project surpluses or deficits to report on at this time.

## **Agencies and Boards**

Windsor Police Services (WPS):

There are 19 active capital projects in this area that are being administered by various WPS divisions. All projects are anticipated to come in on budget.

Roseland Golf and Curling Club:

There are two active capital projects in this area that are being administered by the General Manager at Roseland. No project variance is anticipated as both of these projects are currently expected to come in on budget.

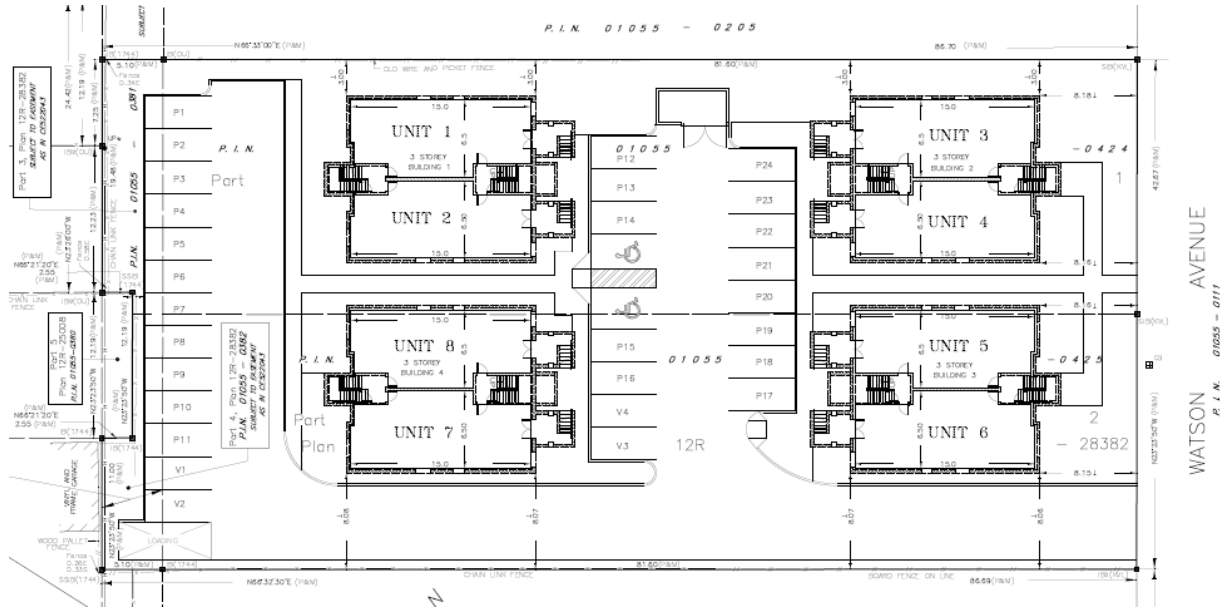
Windsor Airport:

There are five active capital projects in this area that are being administered by the Corporate Projects department. One project surplus, for \$416,362, is detailed below.

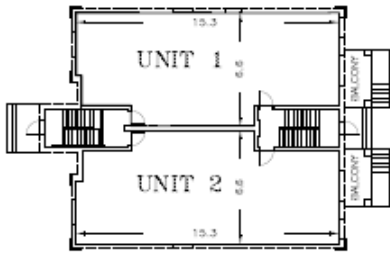
<b>Projects with Projected Deficit/Surplus</b>	<b>(Deficit)/Surplus Amount</b>	<b>Brief Explanation</b>
Kauth's Expansion Plan (7181035)	\$416,362	Project is complete and can be CLOSED. Administration recommends that surplus funds be transferred to the Cargo Road Extension (St. Etienne) project, 7211060.

Handi-Transit:

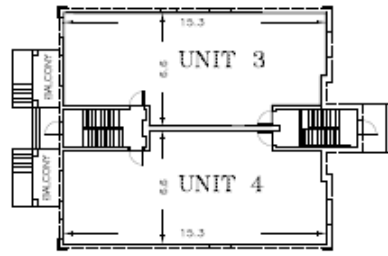
There is only one active capital project active in this area that is being administered by Handi Transit/Transit Windsor. The Handi-Transit Bus Acquisitions project (7191019) is expected to come in on budget.



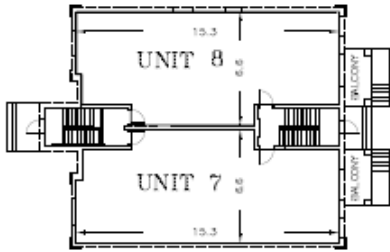
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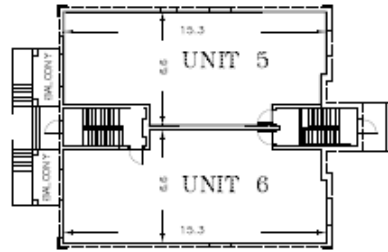
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BUILDING 1



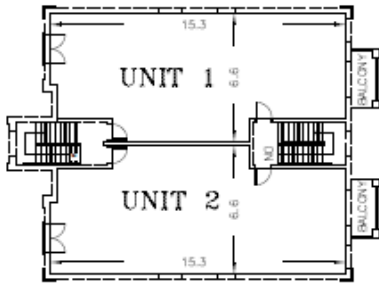
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LEVEL 2  
BUILDING 2



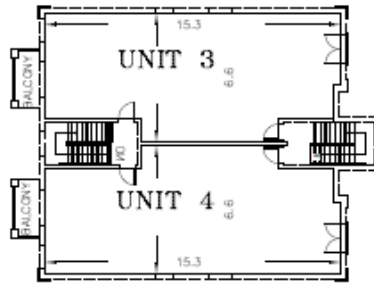
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BUILDING 4



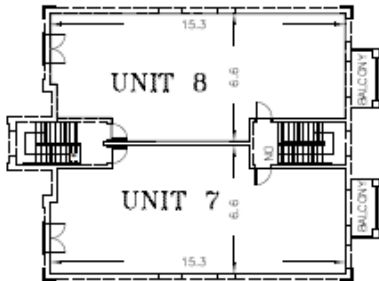
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LEVEL 2  
BUILDING 3



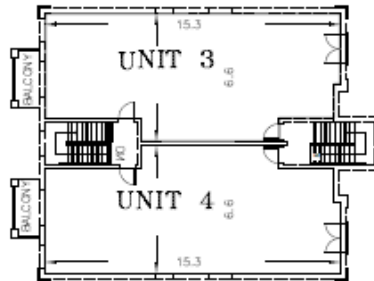
UNITS 1 and 2  
LEVEL 3  
BUILDING 1



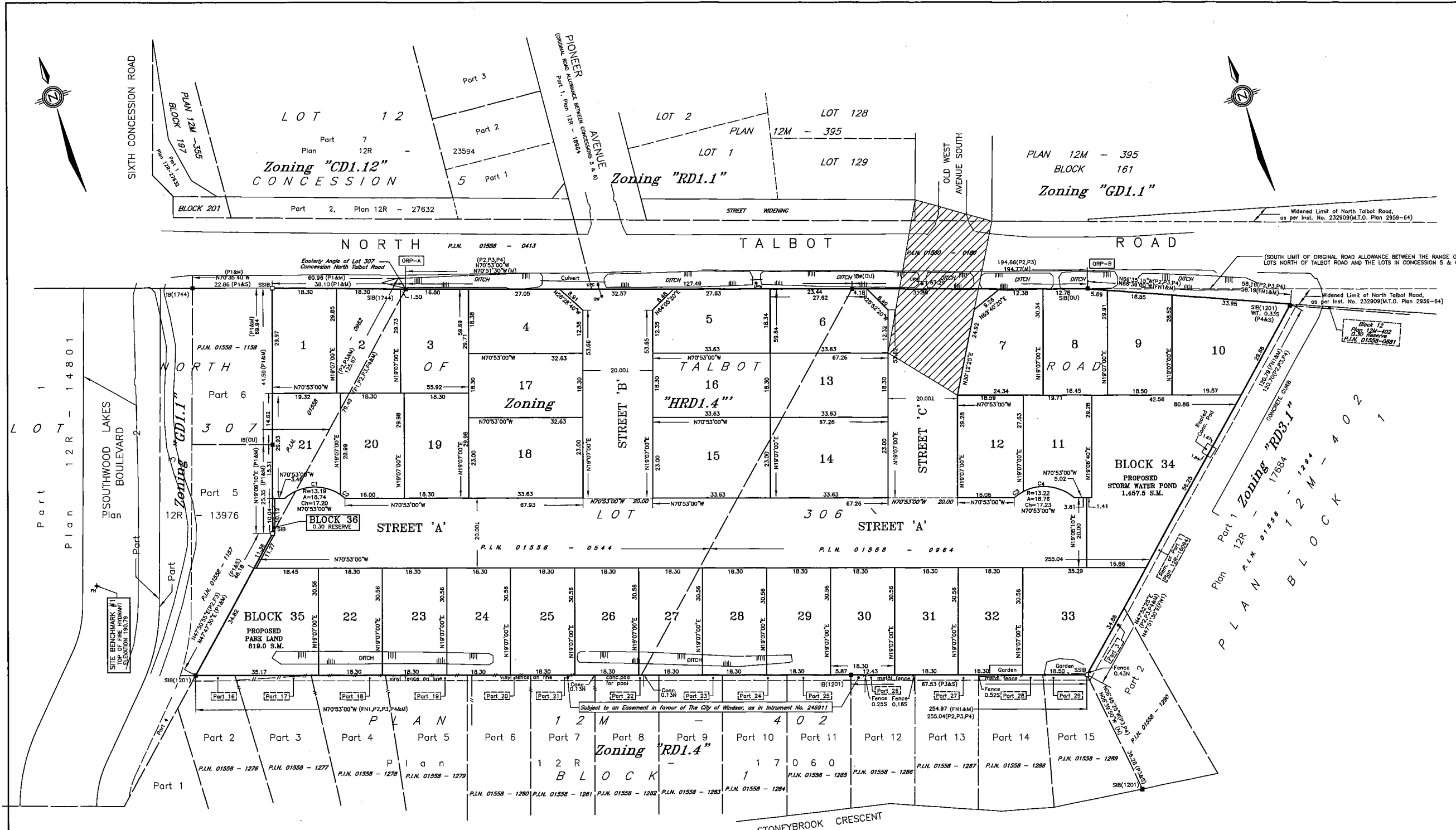
UNITS 3 and 4  
LEVEL 3  
BUILDING 2



UNITS 7 & 8  
LEVEL 3  
BUILDING 4



UNITS 5 and 6  
LEVEL 3  
BUILDING 3



KEY MAP NOT TO SCALE

**DRAFT PLAN OF SUBDIVISION**  
 OF  
 PART OF LOTS 306 AND 307  
 CONCESSION NORTH OF TALBOT ROAD  
 GEOGRAPHIC TOWNSHIP OF SANDWICH SOUTH  
 NOW IN THE  
 CITY OF WINDSOR  
 COUNTY OF ESSEX, ONTARIO  
 © VERHAEGEN LAND SURVEYORS  
 SCALE = 1:500

**LEGEND AND NOTES**  
 BEARINGS ARE UTM GRID DERIVED FROM OBSERVED REFERENCE POINTS "A" AND "B" BY REAL TIME NETWORK OBSERVATIONS AND ARE REFERRED TO UTM ZONE 17 (81° WEST LONGITUDE) NAD 83 (CGRS) (2010.0).  
 DISTANCES ON THIS PLAN ARE GRID AND CAN BE CONVERTED TO GRID BY MULTIPLYING BY THE COMBINED SCALE FACTOR OF 0.99990326.

**LEGEND**  
 ■ DENOTES SURVEY MONUMENT FOUND  
 □ DENOTES SURVEY MONUMENT SET  
 SIB DENOTES STANDARD IRON BAR  
 SSB DENOTES SHORT STANDARD IRON BAR  
 IB DENOTES IRON BAR  
 PB DENOTES PLASTIC BAR  
 CP DENOTES STEEL PIN  
 DC DENOTES CUT-CROSS  
 WT DENOTES WITNESS  
 M DENOTES MEASURED  
 J DENOTES PERPENDICULAR  
 S DENOTES SET  
 ORP DENOTES OBSERVED REFERENCE POINT  
 ALL SET SIB AND PB MONUMENTS WERE DUE TO LACK OF OVERBURDEN AND/OR PROXIMITY OF UNDERGROUND UTILITIES IN ACCORDANCE WITH SECTION 11 (4) OF OREG. 525/91.  
 (OU) DENOTES ORIGIN UNKNOWN  
 (P1) DENOTES PLAN 12R-13976  
 (P2) DENOTES PLAN 12M-402  
 (P3) DENOTES PLAN 12R-17060  
 (P4) DENOTES PLAN 12R-17684  
 (FN1) DENOTES FIELD NOTE BY:  
 (1744) DENOTES VERHAEGEN LAND SURVEYORS OLS  
 (1201) DENOTES CLARKE SURVEYORS INC., O.L.S.  
 +FH DENOTES FIRE HYDRANT  
 +UPC DENOTES UTILITY POLE CONCRETE  
 +UPW DENOTES UTILITY POLE WOOD  
 +DW DENOTES DUT WIRE

**OWNER'S CERTIFICATE**  
 I HEREBY AUTHORIZE THE FILING OF THIS DRAFT PLAN FOR APPROVAL.  
 DATE: \_\_\_\_\_  
 I HAVE THE AUTHORITY TO BIND THE CORPORATION

**SURVEYOR'S CERTIFICATE**  
 I CERTIFY THAT:  
 THE BOUNDARIES OF THE LANDS TO BE SUBDIVIDED AND THEIR RELATIONSHIP TO THE ADJACENT LANDS HAVE BEEN ACCURATELY AND CORRECTLY SHOWN.  
 DATE: MAY 19, 2022  
 Roy Simons  
 A. SIMONE  
 ONTARIO LAND SURVEYOR

**INTEGRATION DATA**

COORDINATES ARE DERIVED FROM GRID OBSERVATIONS USING THE CAN-NET NETWORK SERVICE AND ARE REFERRED TO UTM ZONE 17 (81° WEST LONGITUDE) NAD83 (CGRS) (2010.0).  
 COORDINATE VALUES ARE TO AN URBAN ACCURACY IN ACCORDANCE WITH SECTION 14(2) O. REG 216/10

POINT ID	NORTHING	EASTING
ORP-A	N4678546.40	E336574.75
ORP-B	N4678562.53	E336758.76

COORDINATES CANNOT, IN THEMSELVES, BE USED TO RE-ESTABLISH CORNERS OR BOUNDARIES SHOWN ON THIS PLAN.  
 FOR BEARING COMPARISON, A ROTATION 0°00'25" CLOCKWISE WAS APPLIED TO (P2), (P3) & (P4) TO CONVERT TO GRID BEARINGS.  
 FOR BEARING COMPARISON, A ROTATION 0°09'20" CLOCKWISE WAS APPLIED TO (P1) TO CONVERT TO GRID BEARINGS.  
 FOR BEARING COMPARISON, A ROTATION 0°02'30" CLOCKWISE WAS APPLIED TO (FN1) TO CONVERT TO GRID BEARINGS.

**ADDITIONAL INFORMATION**  
 REQUIRED UNDER SECTION 51(17) OF THE PLANNING ACT, (R.S.O. 1990)

(A) AS SHOWN ON PLAN	(F) AS SHOWN ON PLAN
(B) AS SHOWN ON PLAN	(G) AS SHOWN ON PLAN
(C) AS SHOWN ON PLAN	(H) PIPED MUNICIPAL
(D) SINGLE FAMILY RESIDENTIAL	(I) CLAY LOAM
(E) NORTH - EXISTING RESIDENCE	(J) AS SHOWN ON PLAN
WEST - EXISTING RESIDENCE	(K) ALL SERVICES TO BE PROVIDED
EAST - EXISTING RESIDENTIAL	(L) AS SHOWN ON PLAN
SOUTH - EXISTING RESIDENTIAL	

**SUMMARY**  
 TOTAL NUMBER OF LOTS 33  
 TOTAL NUMBER OF BLOCKS 3  
 TOTAL AREA OF SITE 2.932 Hectares  
 DENSITY 11.3 Lots per Hectares

**CURVE SCHEDULE**

CURVE	RADIUS	ARC	CHORD	CHORD BEARING
C1	13.19	17.10	15.53	N74°26'30"W
C2	13.19	1.84	1.84	N33°44'40"W
C3	13.22	3.08	3.08	N76°05'30"E
C4	13.22	15.70	14.80	N64°14'50"W

"METRIC" DISTANCES AND COORDINATES SHOWN ON THIS PLAN ARE IN METRES AND CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048

MAP No. SDN-001/21-1

**VERHAEGEN** SURVEYING  
 LAND SURVEYORS  
 A DIVISION OF J.D. BARNES LTD.  
 944 OTTAWA STREET, WINDSOR, ON, N6X 3E1  
 T: (519) 258-1772 F: (519) 258-1791 www.jdbarnes.com

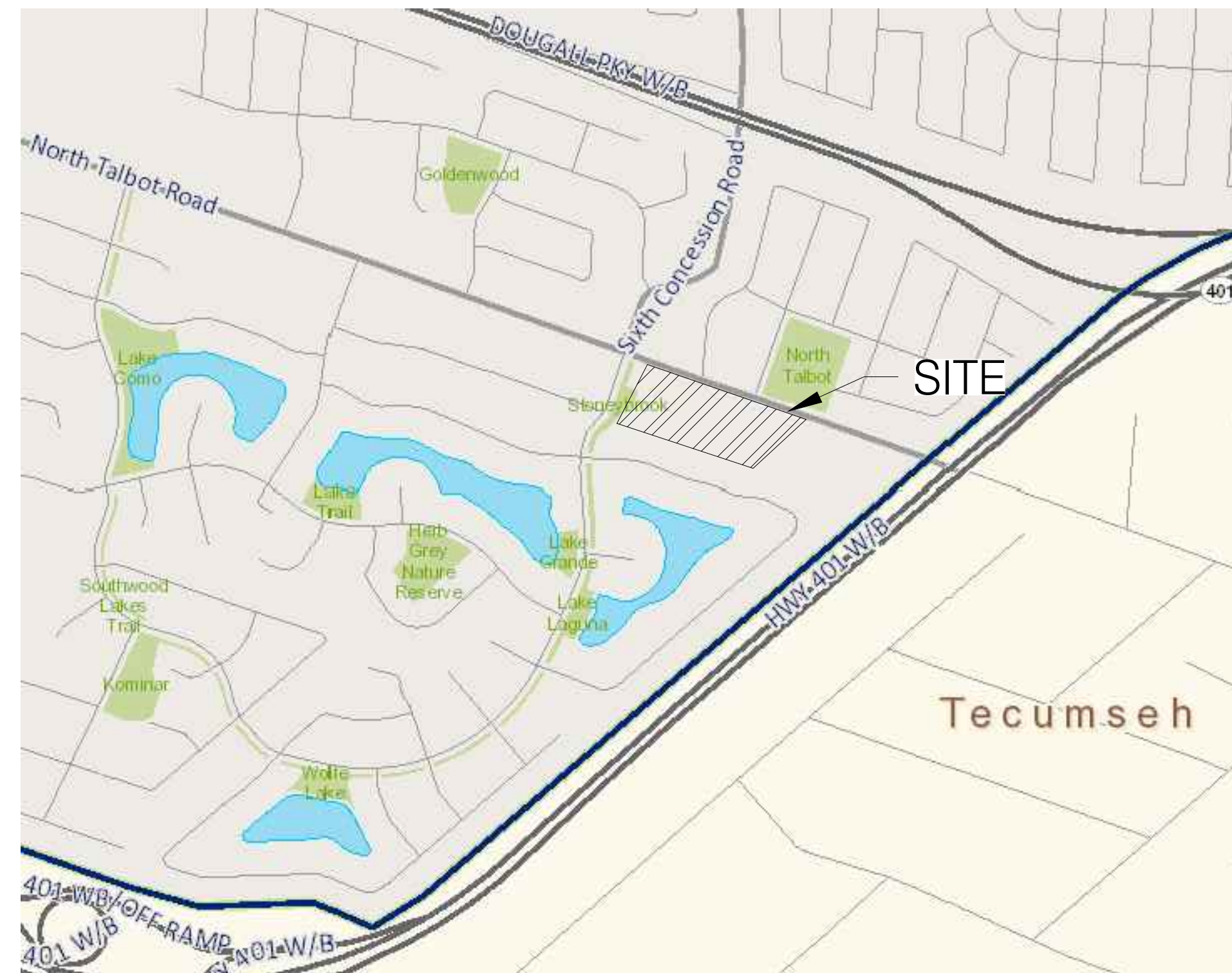
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CAD File: 21-47-019-02.dwg	E-WIND-NTR-306	CAD Date: May 20, 2022 10:23 AM



# NORTH TALBOT DEVELOPMENT

1095 NORTH TALBOT ROAD, WINDSOR

LEGEND		
DESCRIPTION	EXISTING	NEW
STORM SEWER	---	---
SANITARY SEWER	---	---
WATER MAIN	---	---
STORM SERVICE		○ STM
SANITARY SERVICE		○ SAN
WATER SERVICE		○ WSV
FIRE HYDRANT & WATER VALVE	⊗	⊗
GAS MAIN	---	---
CATCH BASIN	⊠	⊠
CURB INLET		⊠
STORM MANHOLE	●	●
SANITARY MANHOLE	●	●
WATER VALVE	⊗	⊗
EP ELEVATIONS		▽
STREET LIGHTS		✱



KEY PLAN  
N.T.S

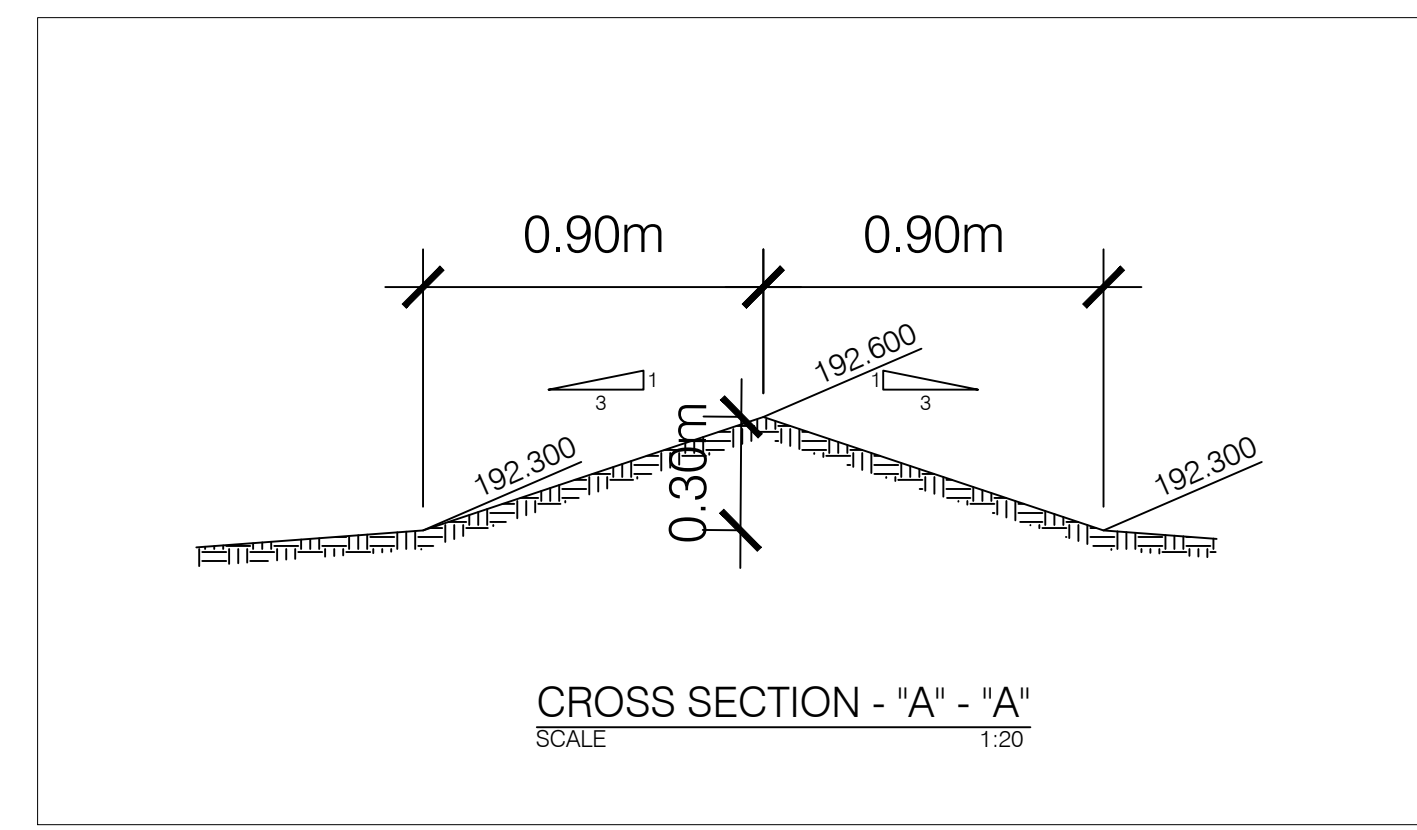
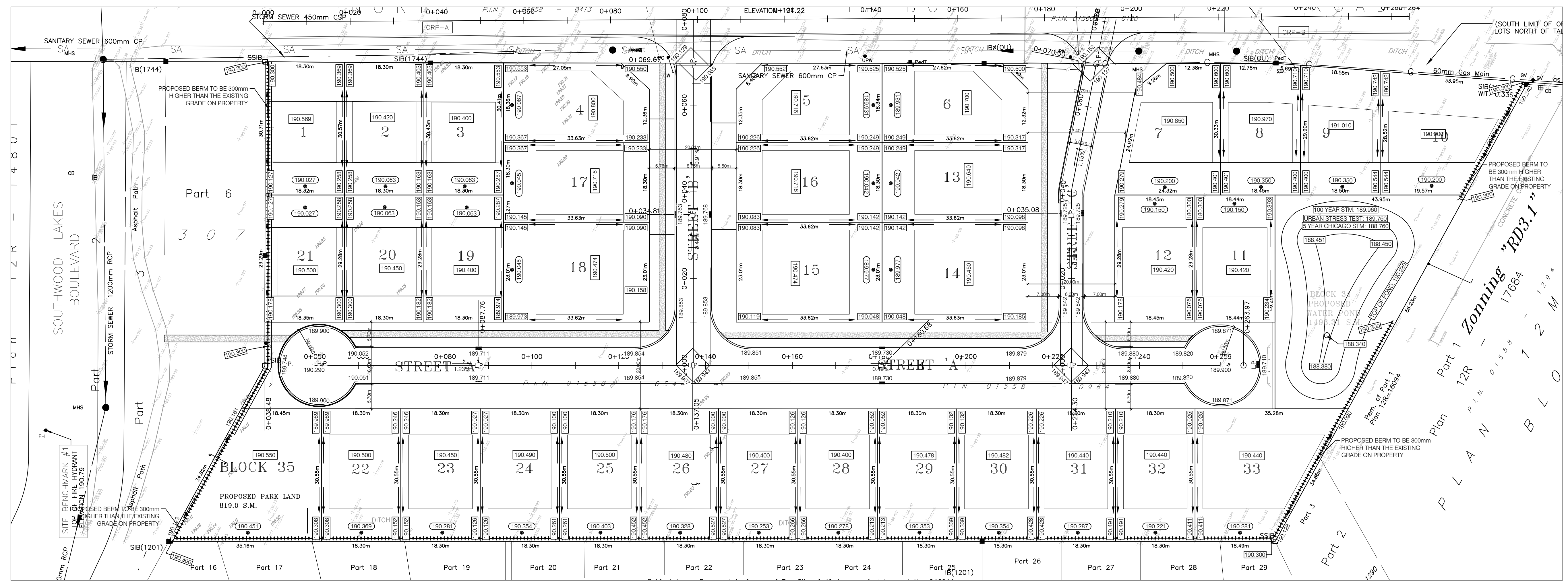
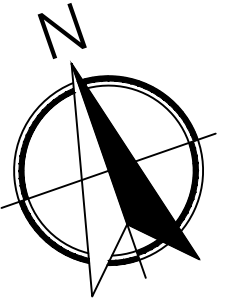
Sheet List Table	
Sheet Number	Sheet Title
--	TITLE PAGE
1	GRADING PLAN
2	OVERLAND FLOW ROUTE
3	SERVICING LAYOUT PLAN
4	STREET 'A' PLAN AND PROFILE 0+000 TO 0+170
5	STREET 'A' PLAN AND PROFILE 0+170 TO 0+310
6	STREET 'B' PLAN AND PROFILE 0+000 TO 0+108
7	STREET 'C' PLAN AND PROFILE 0+000 TO 0+105
8	POND PLAN AND PROFILE 0+000 TO 0+043
9	STORM DRAINAGE AREA PLAN
10	SANITARY DRAINAGE AREA PLAN
11	DETAILS 1
12	DETAILS 2

**NOTE:**  
ENVIRONMENTAL COMPLIANCE APPROVAL  
NUMBER: #####  
ISSUED DATE: #####

**ATTENTION**  
CONTRACTOR IS RESPONSIBLE FOR CONFIRMING  
THE EXACT LOCATION AND PROTECTION OF EXISTING  
UTILITIES DURING CONSTRUCTION.

**BENCH MARK**  
CITY OF WINDSOR BENCH MARK 1083 ELEVATION 191.32  
M.B. 1185 NORTH OF TALBOT ROAD: THE PLATE IS LOCATED ON THE WEST WALL OF THE CHIMNEY, 0.09 METER FROM THE SOUTH WALL OF THE CHIMNEY AND 0.43 METER ABOVE GRADE.  
SITE BENCH MARK #1 ELEVATION 190.79  
TOP OF FIRE HYDRANT AT SOUTHWEST CORNER OF PARCEL.  
SITE BENCH MARK #2 ELEVATION 191.22  
TOP OF FIRE HYDRANT AT NORTHEAST CORNER OF NORTH TALBOT ROAD AND PIONEER AVENUE.





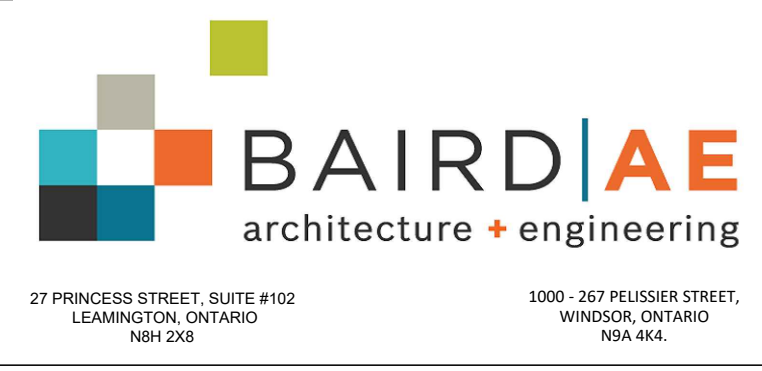
GRADING NOTES	
	OVER LAND FLOW DIRECTION
2.5%	PROPOSED SLOPE
186.563	PROPOSED ELEVATION
	EDGE OF ASPHALT ELEVATIONS
	EXISTING ELEVATIONS
	TOP OF BERM
	BOTTOM OF SWALE
	BERM

JAN 2022



DATE: MAY 27, 2022  
SHURJHEEL TUNIO, P. ENG.

DATE	REVISIONS
05/14/2021	SUBMITTED FOR APPROVALS
06/29/2021	REVISED AS PER ENWIN COMMENTS
22/04/2022	REVISED AS PER ERCA COMMENTS
27/05/2022	REVISED AS PER CITY COMMENTS
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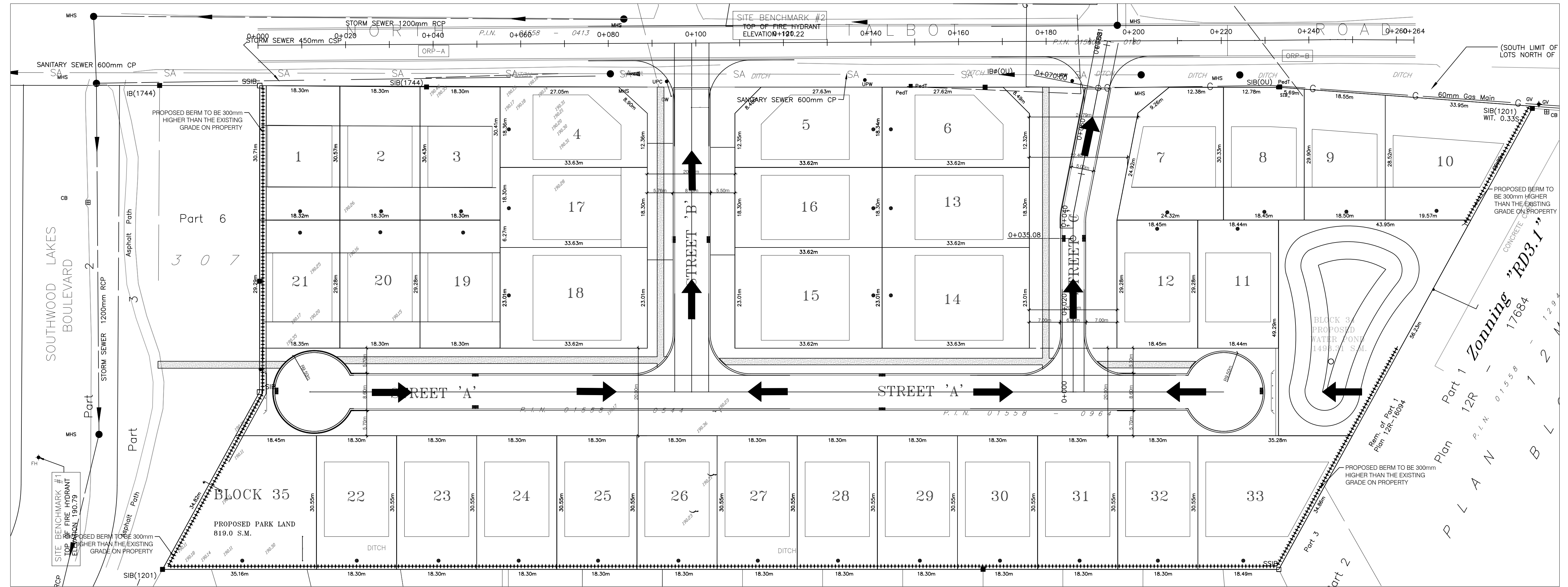
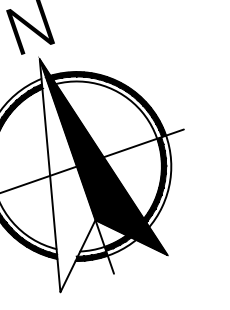


PROJECT TITLE:  
**NORTH TALBOT DEVELOPMENT**  
1095 NORTH TALBOT ROAD, WINDSOR  
SHEET TITLE:  
**GRADING PLAN**

DATE: MAY 27, 2022  
SCALE: 1:500  
DRAWN BY: B.T.  
CHECKED BY: S.T.

PROJECT NO:  
**21-021**  
SHEET NO:  
**1**

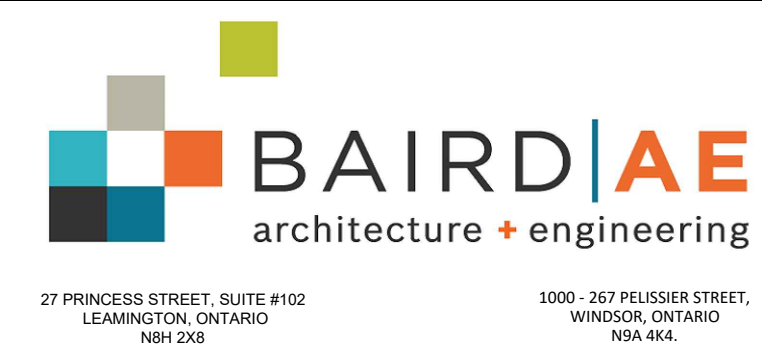
GRADING NOTES	
	—
2.5%	—
186.563	—
	—
186.563	—
	—
186.563	—
	—
	—



DATE: MAY 27, 2022

SHURJEEL TUNIO, P. ENG.

DATE	REVISIONS
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27/05/2022	REVISED AS PER CITY COMMENTS
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---	---
---	---

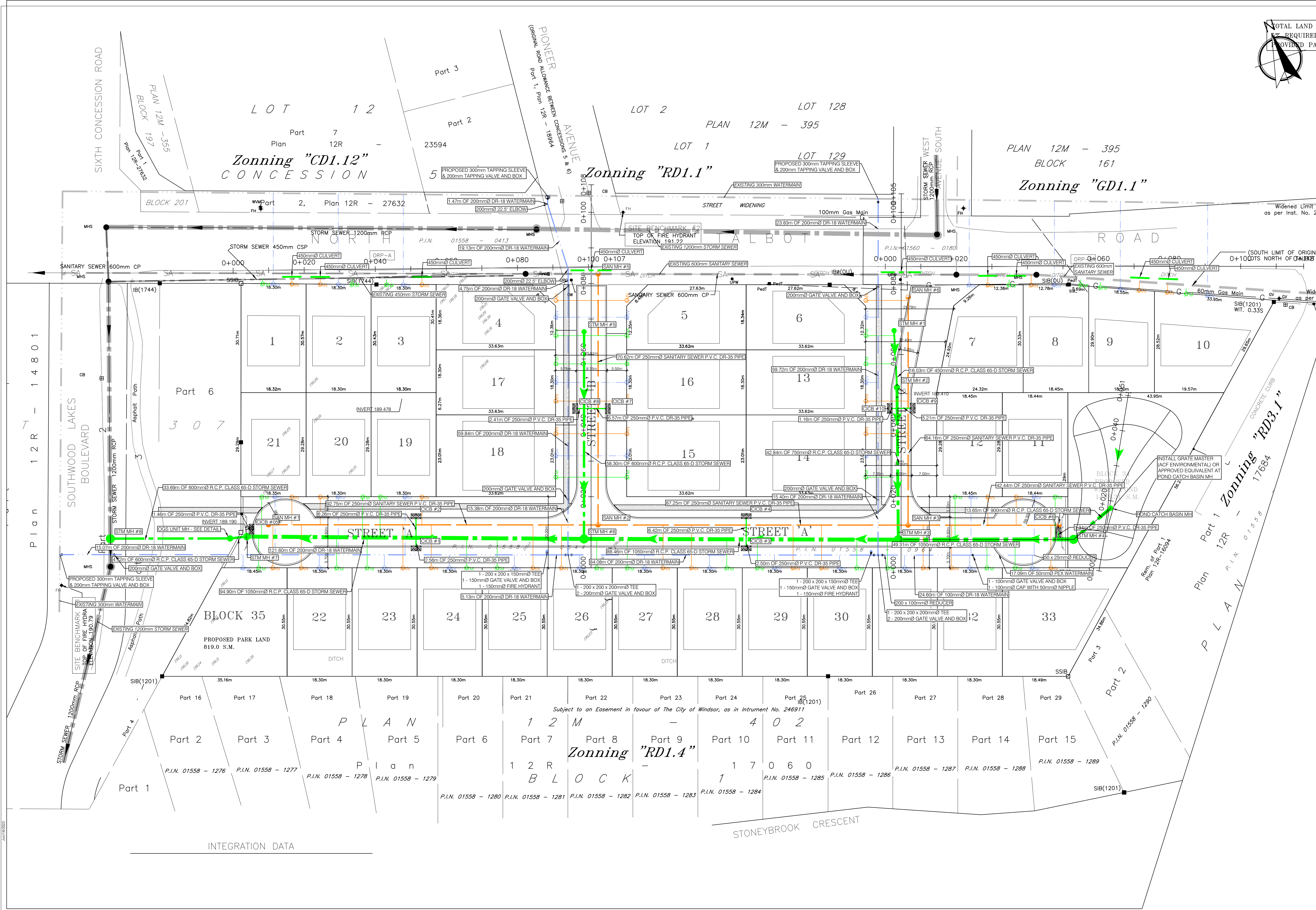


PROJECT TITLE:  
**NORTH TALBOT DEVELOPMENT**  
 1095 NORTH TALBOT ROAD, WINDSOR  
 SHEET TITLE:  
**OVERLAND FLOW ROUTE**

DATE: MAY 27, 2022	PROJECT NO: <b>21-021</b>
SCALE: 1:500	
DRAWN BY: B.T.	SHEET NO: <b>2</b>
CHECKED BY: S.T.	



DESCRIPTION	LEGEND	
	EXISTING	NEW
STORM SEWER		
SANITARY SEWER		
WATER MAIN		
STORM SERVICE		
SANITARY SERVICE		
WATER SERVICE		
FIRE HYDRANT & WATER VALVE		
GAS MAIN		
CATCH BASIN		
CURB INLET		
STORM MANHOLE		
SANITARY MANHOLE		
WATER VALVE		
EP ELEVATIONS		
STREET LIGHTS		



INTEGRATION DATA



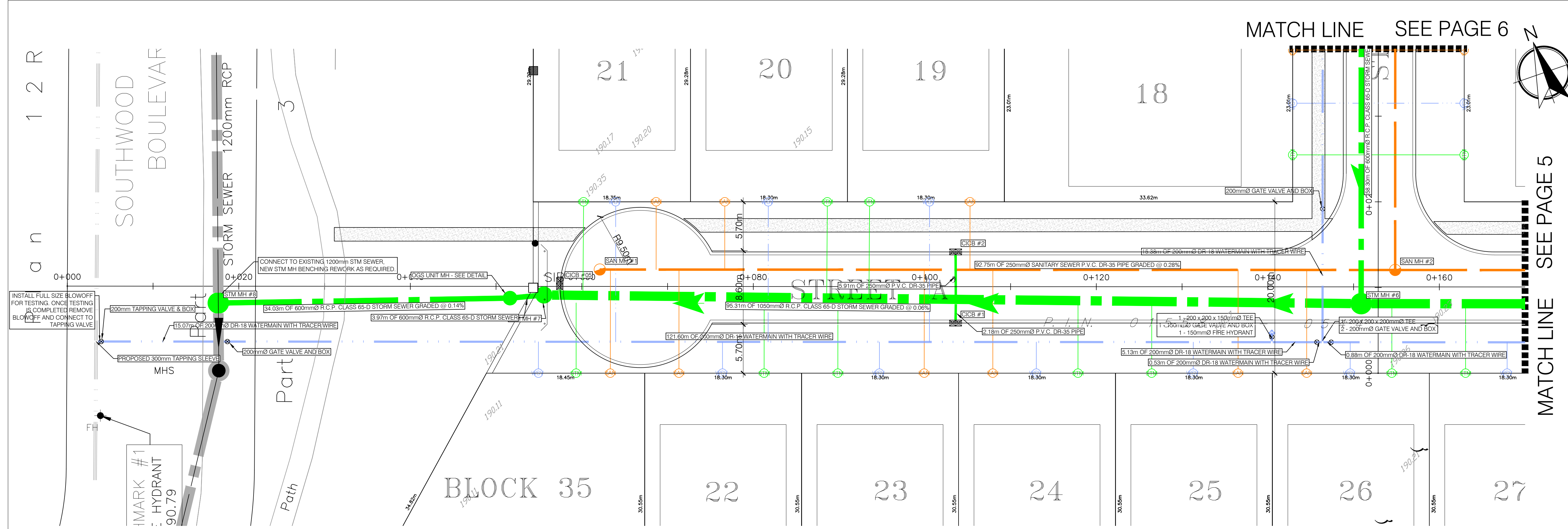
DATE: MAY 27, 2022  
 SHURJHEEL TUNIO, P. ENG.

DATE	REVISIONS
05/14/2021	SUBMITTED FOR APPROVALS
06/29/2021	REVISED AS PER ENWIN COMMENTS
22/04/2022	REVISED AS PER ERCA COMMENTS
27/05/2022	REVISED AS PER CITY COMMENTS

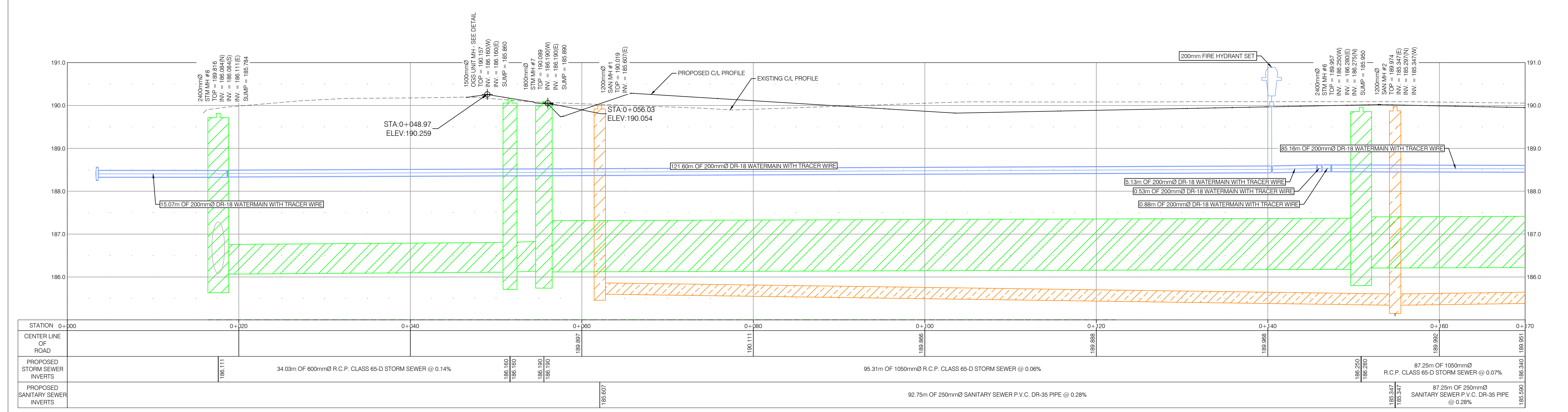


PROJECT TITLE: NORTH TALBOT DEVELOPMENT  
 1095 NORTH TALBOT ROAD, WINDSOR  
 SHEET TITLE: SERVICING LAYOUT PLAN

DATE: MAY 27, 2022	PROJECT NO: 21-021
SCALE: 1:500	
DRAWN BY: B.T.	SHEET NO: 3
CHECKED BY: S.T.	



LEGEND		
DESCRIPTION	EXISTING	NEW
STORM SEWER	---	---
SANITARY SEWER	---	---
WATER MAIN	---	---
STORM SERVICE		STM
SANITARY SERVICE		SAN
WATER SERVICE		WSV
FIRE HYDRANT & WATER VALVE		
GAS MAIN		gas
CATCH BASIN		
CURB INLET		
STORM MANHOLE		
SANITARY MANHOLE		
WATER VALVE		
EP		
ELEVATIONS		
STREET LIGHTS		



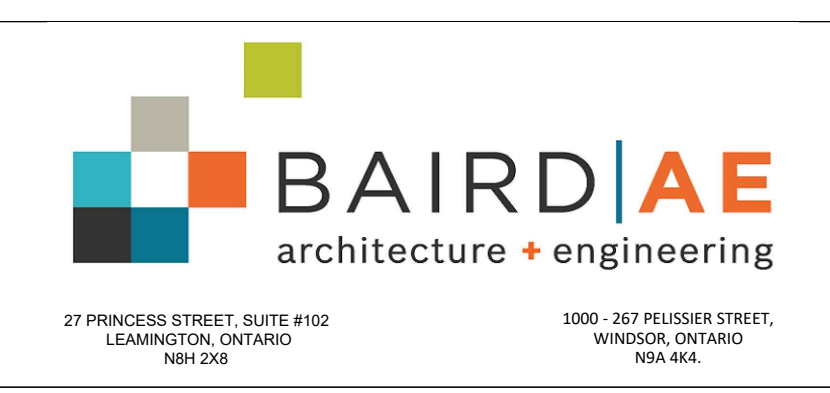
STATION	0+000	0+020	0+040	0+060	0+080	0+100	0+120	0+140	0+160	0+170
CENTER LINE OF ROAD					190.111	189.895	189.888	189.965	189.902	189.951
PROPOSED STORM SEWER INVERTS	186.111	34.03m OF 600mmØ R.C.P. CLASS 65-D STORM SEWER @ 0.14%	186.160 186.190	188.190 185.190	185.607	95.31m OF 1050mmØ R.C.P. CLASS 65-D STORM SEWER @ 0.06%		185.550 185.590	87.25m OF 1050mmØ R.C.P. CLASS 65-D STORM SEWER @ 0.07%	185.590 185.540
PROPOSED SANITARY SEWER INVERTS						92.75m OF 250mmØ SANITARY SEWER P.V.C. DR-35 PIPE @ 0.28%		185.547 185.547	87.25m OF 250mmØ SANITARY SEWER P.V.C. DR-35 PIPE @ 0.28%	



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22/04/2022	REVISED AS PER ERCA COMMENTS
27/05/2022	REVISED AS PER CITY COMMENTS
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---	---



PROJECT TITLE:  
**NORTH TALBOT DEVELOPMENT**  
1095 NORTH TALBOT ROAD, WINDSOR

SHEET TITLE:  
**STREET 'A' PLAN AND PROFILE 0+000 TO 0+170**

DATE: MAY 27, 2022	PROJECT NO: 21-021
SCALE: HOR: 1:250 VER: 1:50	
DRAWN BY: B.T.	SHEET NO: <b>4</b>
CHECKED BY: S.T.	

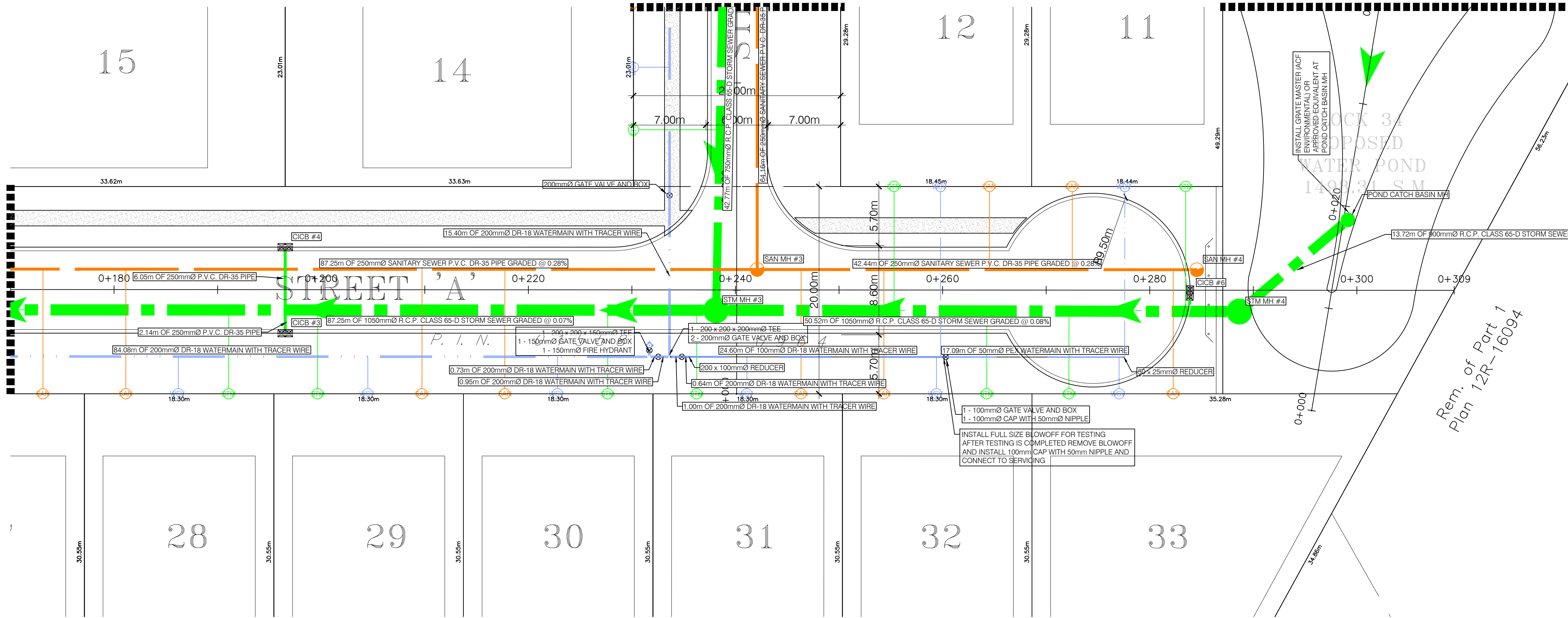
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MATCH LINE SEE PAGE 8

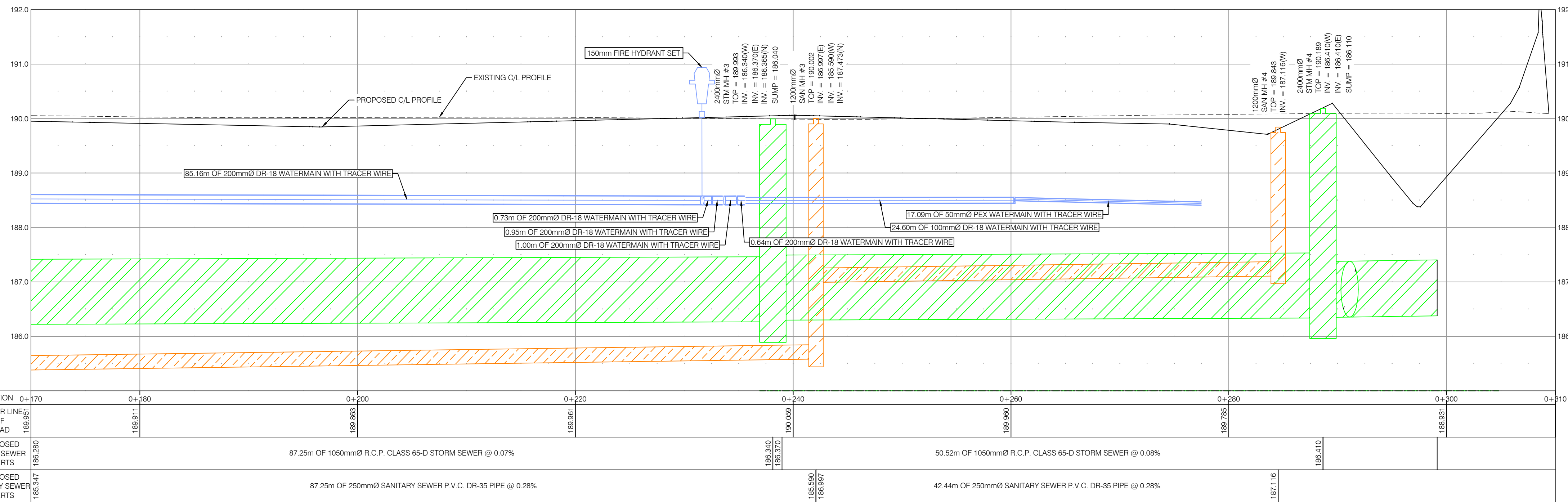


LEGEND		
DESCRIPTION	EXISTING	NEW
STORM SEWER	---	---
SANITARY SEWER	---	---
WATER MAIN	---	---
STORM SERVICE		STM
SANITARY SERVICE		SAN
WATER SERVICE		WSV
FIRE HYDRANT & WATER VALVE		
GAS MAIN		GAS
CATCH BASIN		
CURB INLET		
STORM MANHOLE		
SANITARY MANHOLE		
WATER VALVE		
EP ELEVATIONS		
STREET LIGHTS		

MATCH LINE SEE PAGE 4



Rem. of Part 1  
Plan 12R-16094

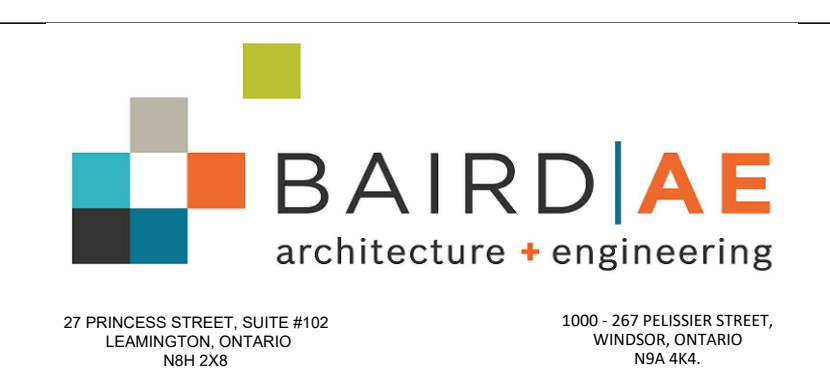


186 NORTH TALBOT DEVELOPMENT, 1095 NORTH TALBOT ROAD, WINDSOR, ONTARIO



DATE: MAY 27, 2022  
SHURJHEEL TUNIO, P.ENG.

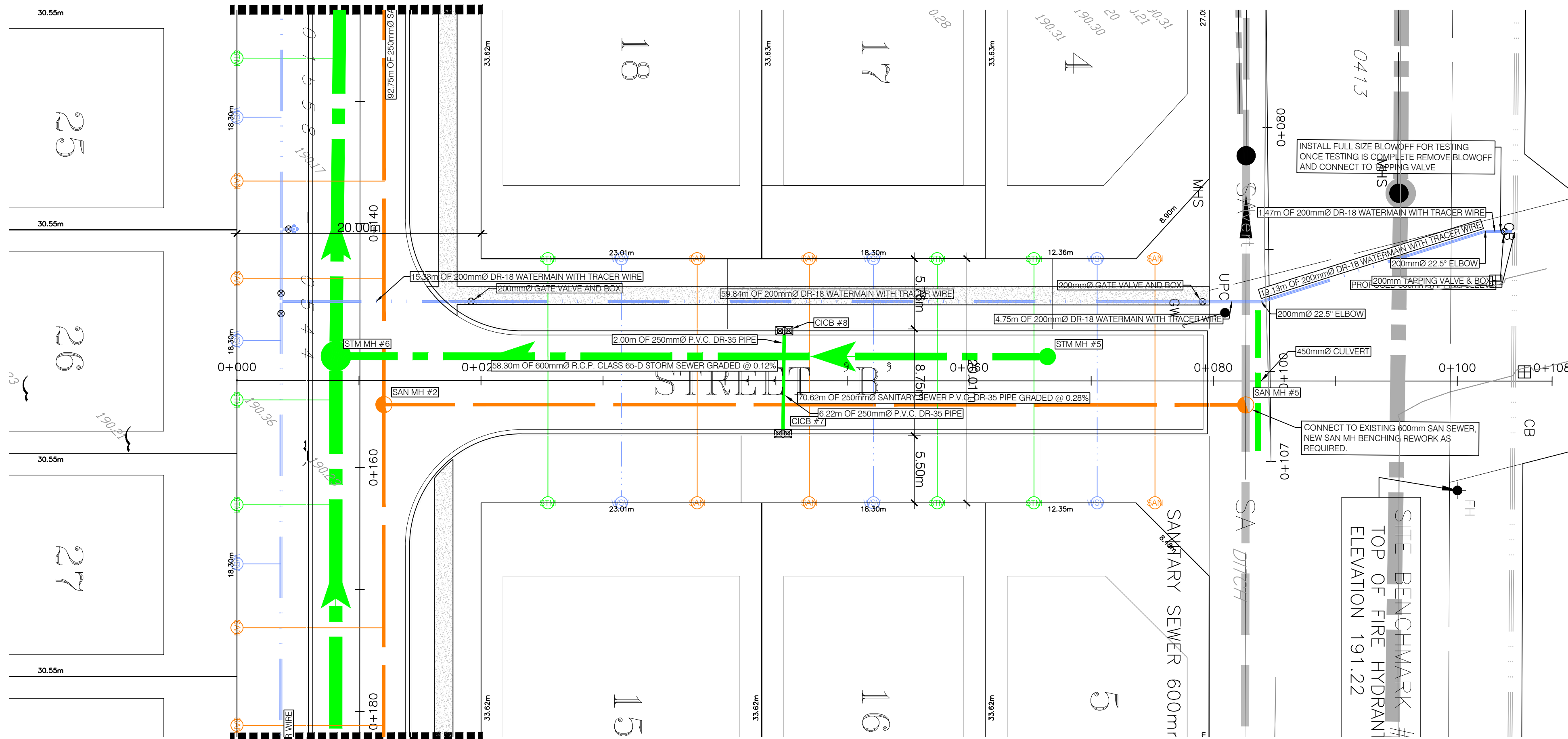
DATE	REVISIONS
05/14/2021	SUBMITTED FOR APPROVALS
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22/04/2022	REVISED AS PER ERCA COMMENTS
27/05/2022	REVISED AS PER CITY COMMENTS
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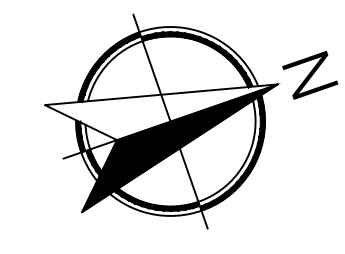
PROJECT TITLE:  
**NORTH TALBOT DEVELOPMENT**  
1095 NORTH TALBOT ROAD, WINDSOR  
SHEET TITLE:  
**STREET 'A' PLAN AND PROFILE 0+170 TO 0+310**

DATE: MAY 27, 2022  
SCALE: HOR: 1:250 VER: 1:50  
DRAWN BY: B.T.  
CHECKED BY: S.T.  
PROJECT NO: 21-021  
SHEET NO: **5**

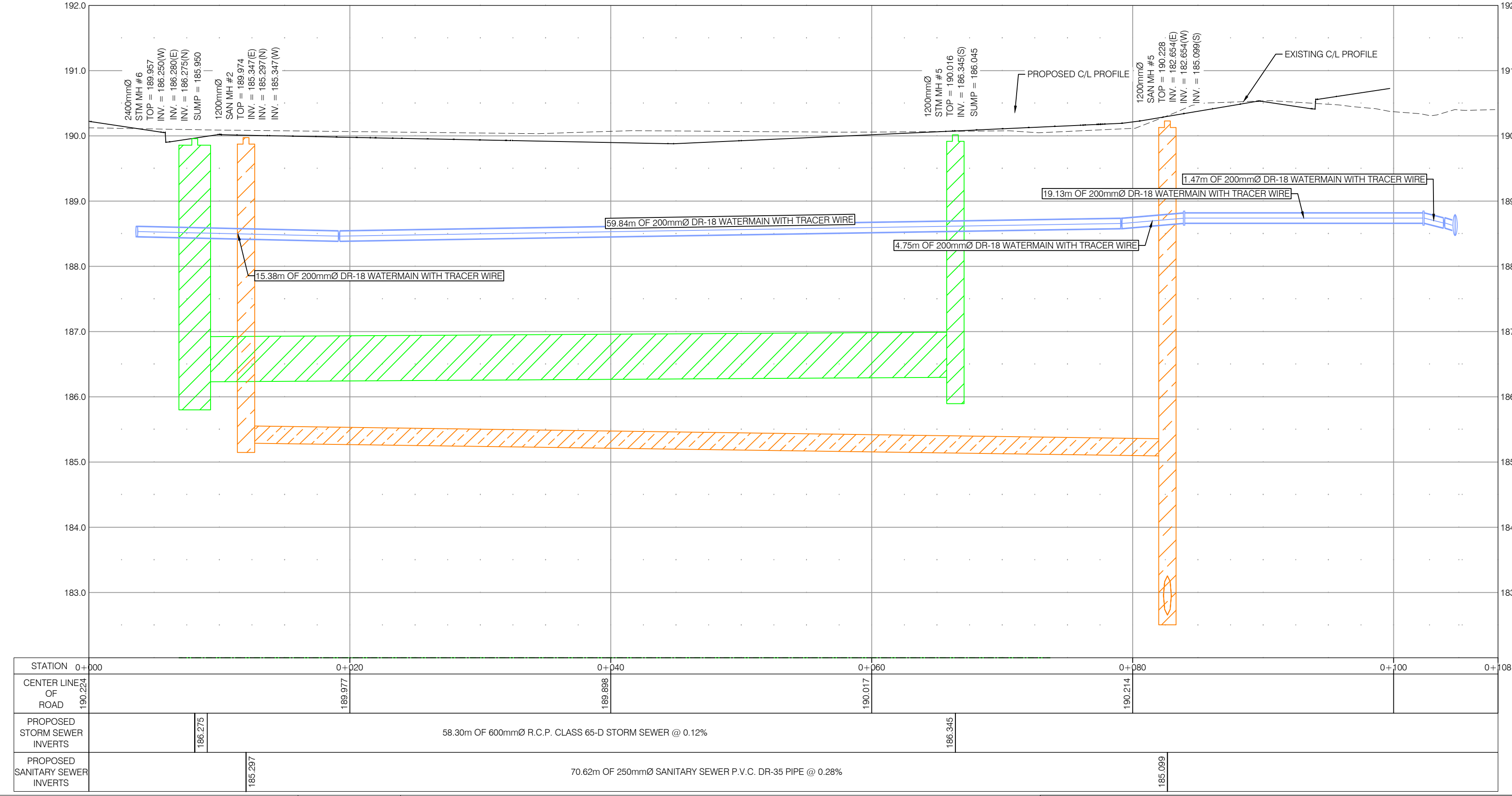
MATCH LINE SEE PAGE 4



MATCH LINE SEE PAGE 5



LEGEND		
DESCRIPTION	EXISTING	NEW
STORM SEWER		
SANITARY SEWER		
WATER MAIN		
STORM SERVICE		
SANITARY SERVICE		
WATER SERVICE		
FIRE HYDRANT & WATER VALVE		
GAS MAIN		
CATCH BASIN		
CURB INLET		
STORM MANHOLE		
SANITARY MANHOLE		
WATER VALVE		
EP ELEVATIONS		
STREET LIGHTS		

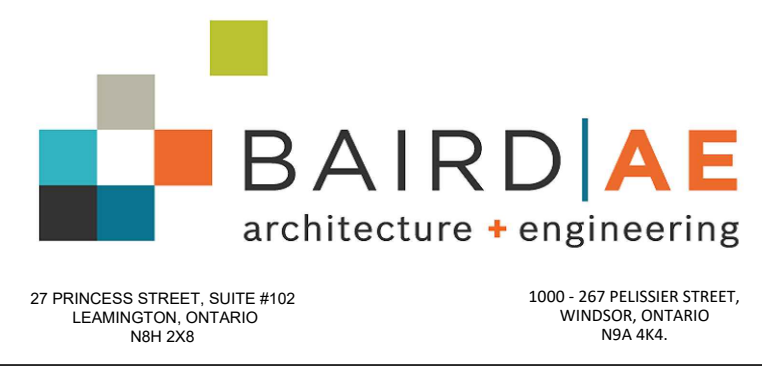


STATION	0+000	0+200	0+400	0+600	0+800	0+1000
CENTER LINE OF ROAD	189.272	189.277	189.288	189.017	188.214	188.008
PROPOSED STORM SEWER INVERTS	186.272					
PROPOSED SANITARY SEWER INVERTS	185.277					



DATE: MAY 27, 2022  
 SHURJHEEL TUNIO, P. ENG.

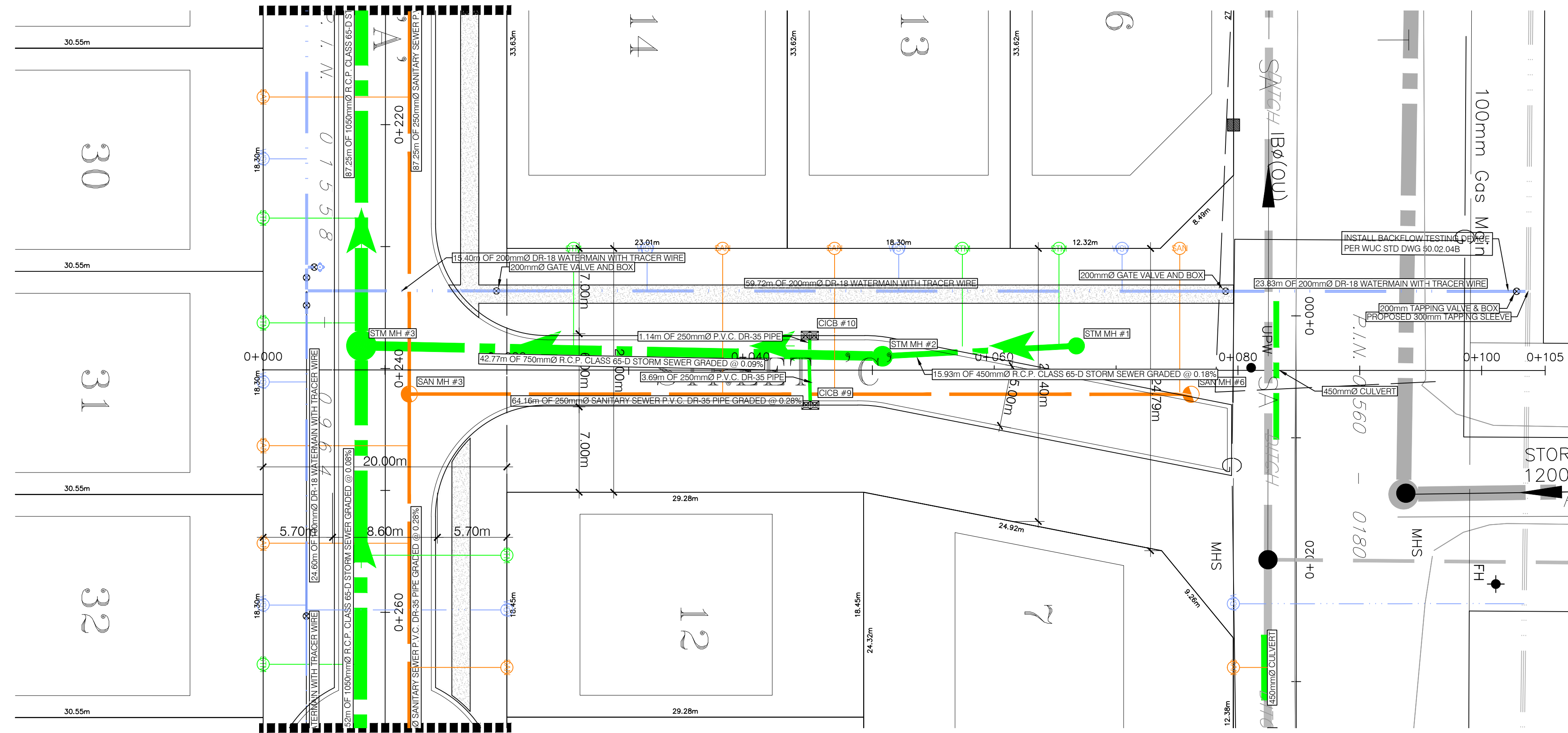
DATE	REVISIONS
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06/29/2021	REVISED AS PER ENWIN COMMENTS
22/04/2022	REVISED AS PER ERCA COMMENTS
27/05/2022	REVISED AS PER CITY COMMENTS



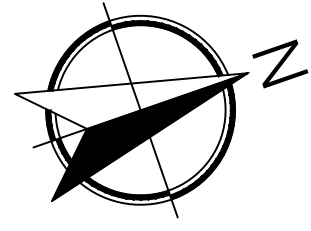
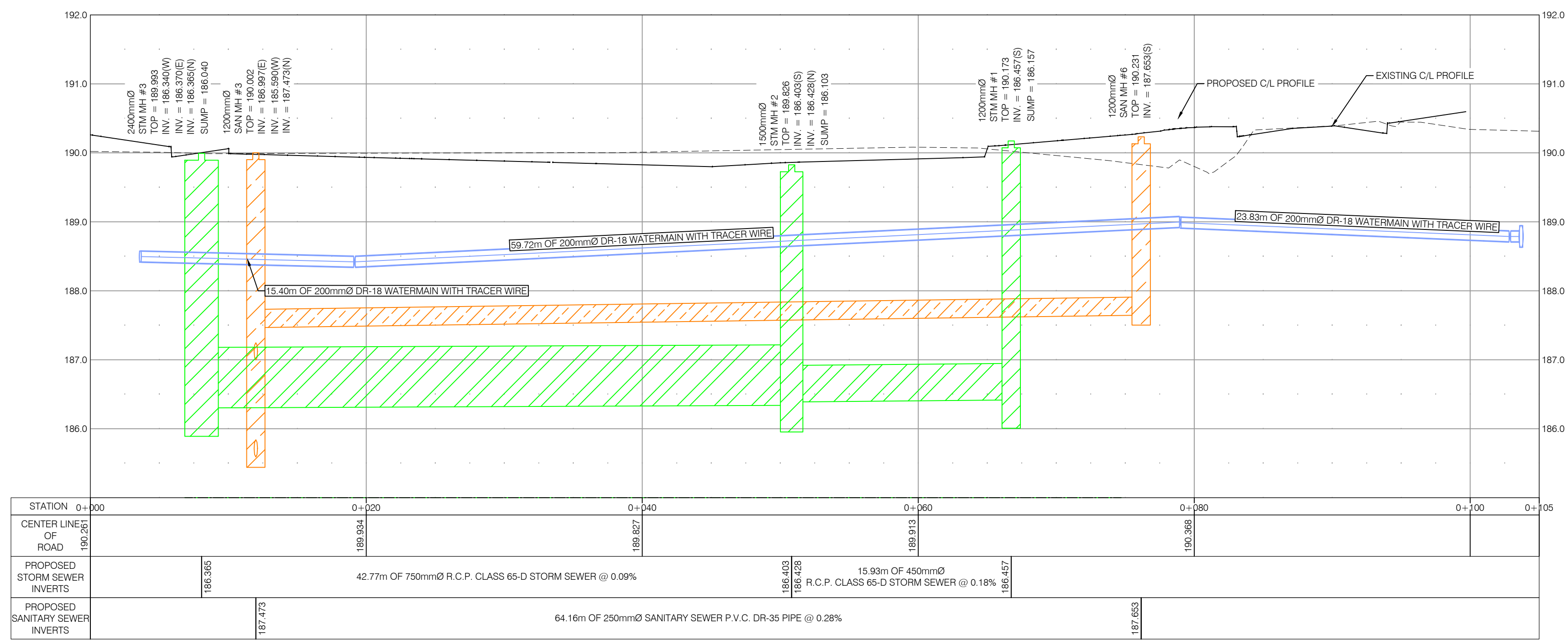
PROJECT TITLE:  
**NORTH TALBOT DEVELOPMENT**  
 1095 NORTH TALBOT ROAD, WINDSOR  
 SHEET TITLE:  
**STREET 'B' PLAN AND PROFILE 0+000 TO 0+108**

DATE: MAY 27, 2022	PROJECT NO: 21-021
SCALE: HOR: 1:250 VER: 1:50	
DRAWN BY: B.T.	SHEET NO: <b>6</b>
CHECKED BY: S.T.	

MATCH LINE SEE PAGE 5



MATCH LINE SEE PAGE 5



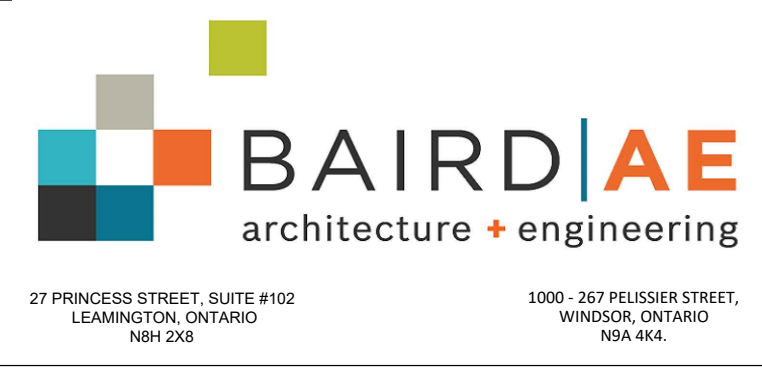
LEGEND		
DESCRIPTION	EXISTING	NEW
STORM SEWER	---	---
SANITARY SEWER	---	---
WATER MAIN	---	---
STORM SERVICE		○ STM
SANITARY SERVICE		○ SAN
WATER SERVICE		○ WSV
FIRE HYDRANT & WATER VALVE	⊗	⊗
GAS MAIN		— GAS —
CATCH BASIN	⊠	⊠
CURB INLET		⊠
STORM MANHOLE	●	●
SANITARY MANHOLE	●	●
WATER VALVE	⊗	⊗
EP		⚡
ELEVATIONS		▽
STREET LIGHTS		⊙

Jan 10/2022



DATE: MAY 27, 2022  
 SHURJHEEL TUNIO, P.ENG.

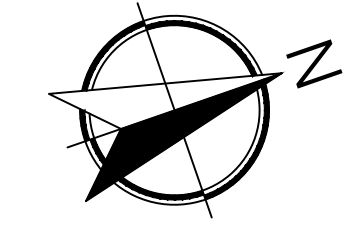
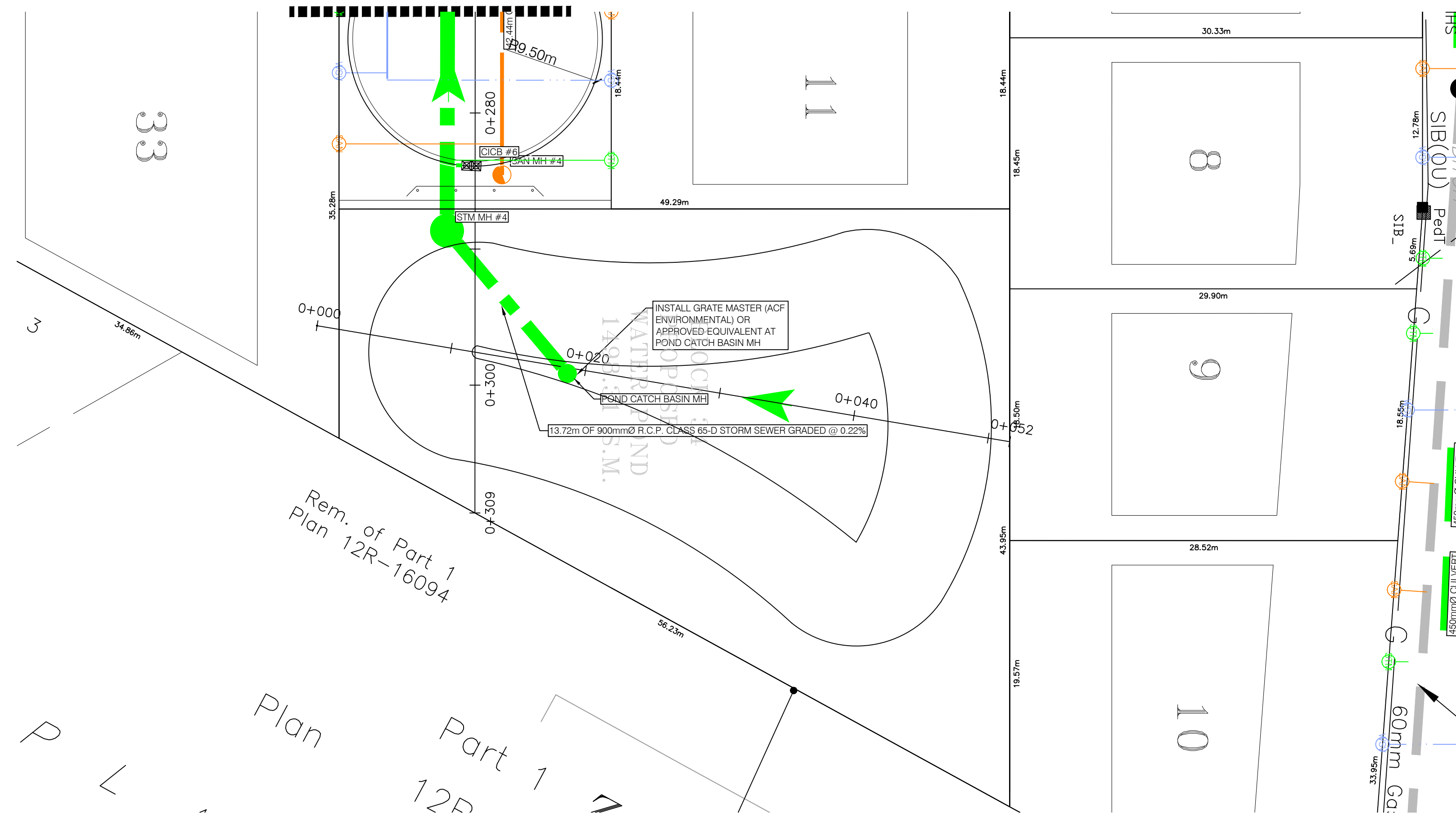
DATE	REVISIONS
05/14/2021	SUBMITTED FOR APPROVALS
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22/04/2022	REVISED AS PER ERCA COMMENTS
27/05/2022	REVISED AS PER CITY COMMENTS
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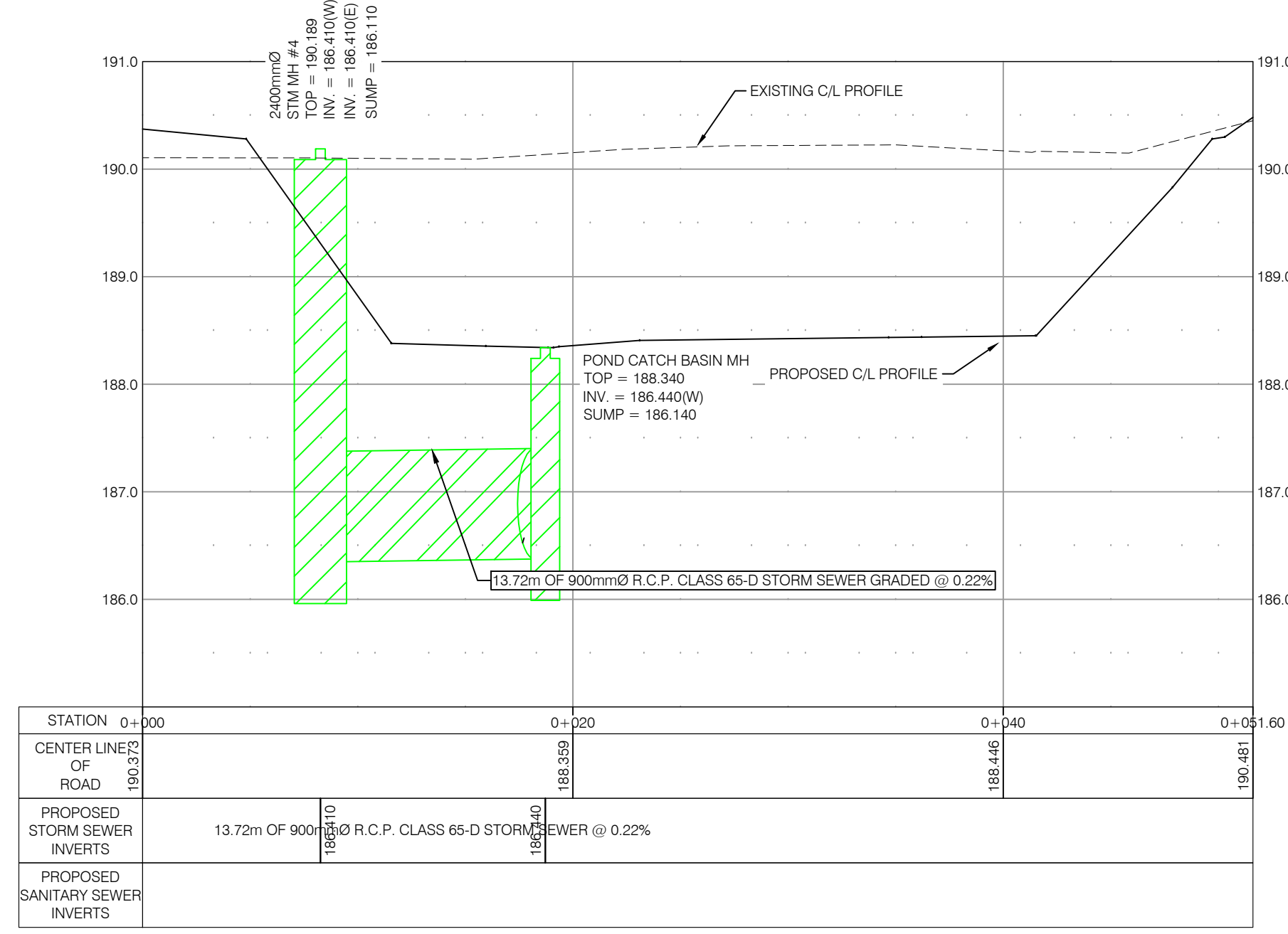
PROJECT TITLE:  
**NORTH TALBOT DEVELOPMENT**  
 1095 NORTH TALBOT ROAD, WINDSOR  
 SHEET TITLE:  
**STREET 'C' PLAN AND PROFILE 0+000 TO 0+105**

DATE: MAY 27, 2022  
 SCALE: HOR: 1:250 VER: 1:50  
 DRAWN BY: B.T.  
 CHECKED BY: S.T.  
 PROJECT NO: 21-021  
 SHEET NO: **7**

MATCH LINE SEE PAGE 5



LEGEND		
DESCRIPTION	EXISTING	NEW
STORM SEWER	---	---
SANITARY SEWER	---	---
WATER MAIN	---	---
STORM SERVICE		○ STM
SANITARY SERVICE		○ SAN
WATER SERVICE		○ WSV
FIRE HYDRANT & WATER VALVE	⊗	⊗
GAS MAIN		— GAS —
CATCH BASIN	⊠	⊠
CURB INLET		⊠
STORM MANHOLE	●	●
SANITARY MANHOLE	●	●
WATER VALVE	⊗	⊗
EP ELEVATIONS		⚡
STREET LIGHTS		⚡

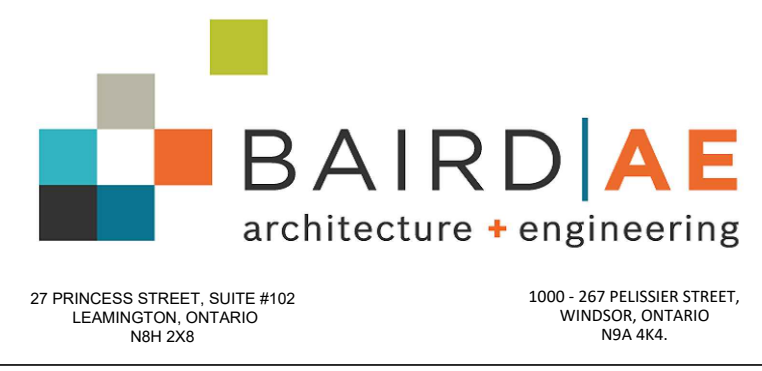


JAN 10 2022



DATE: MAY 27, 2022  
 SHURJHEEL TUNIO, P. ENG.

DATE	REVISIONS
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22/04/2022	REVISED AS PER ERCA COMMENTS
27/05/2022	REVISED AS PER CITY COMMENTS
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PROJECT TITLE:  
**NORTH TALBOT DEVELOPMENT**  
 1095 NORTH TALBOT ROAD, WINDSOR

DATE:  
 MAY 27, 2022

SCALE:  
 HOR: 1:250 VER: 1:50

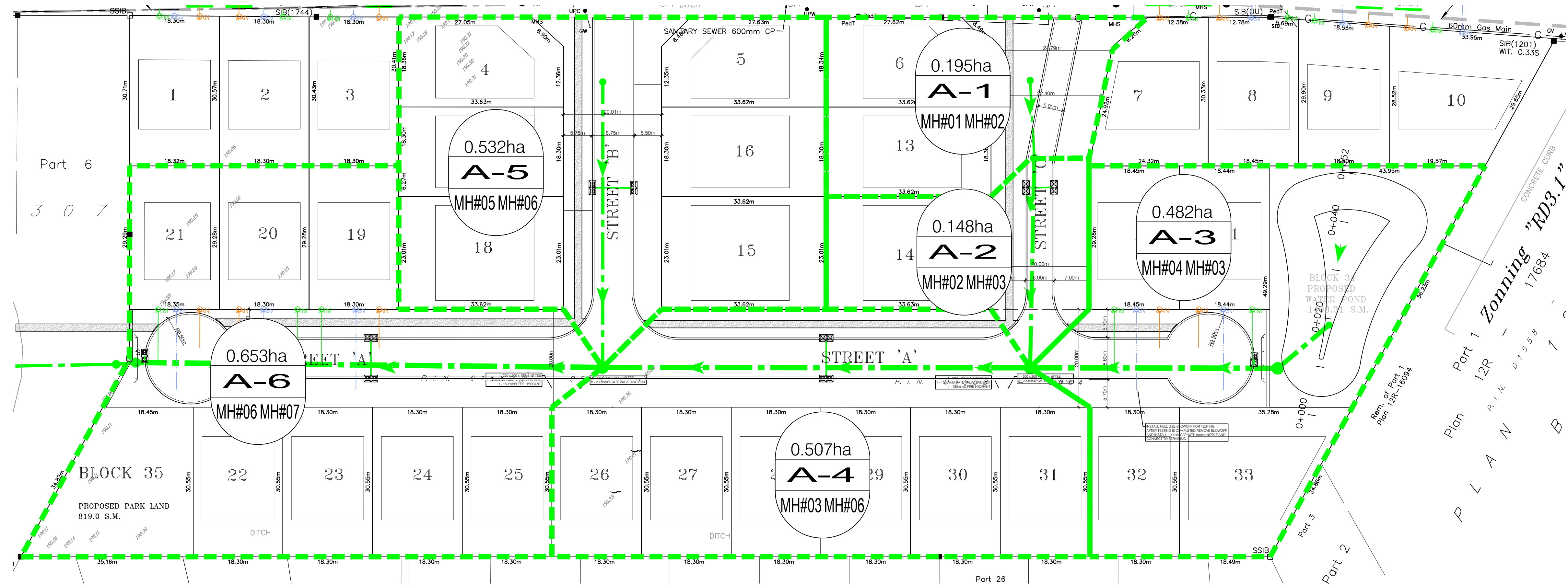
DRAWN BY:  
 B.T.

CHECKED BY:  
 S.T.

PROJECT NO:  
**21-021**

SHEET NO:  
**8**





1095 NORTH TALBOT ROAD STORM SEWER DESIGN SHEET (5-YEAR EVENT, Computed Tc)																									
LOCATION				AREA (ha)				FLOW						SEWER DATA						PROFILE					
Area ID	Area Included	From Node	To Node	C=0.20	C=0.60	C=0.80	C=0.95	Indiv 2.78 AC	Accum 5 2.78AC	Time of Conc.	Design Storm	Rainfall Intensity	Peak Flow (L/sec)	Qtotal (L/s)	Dia. (m) Actual	Dia. (mm)	Type	Slope (%)	Length (m)	Capacity (L/s)	Velocity (m/s)	Flow Time (min)	Ratio Q/Q full	Upstream Elevation	Downstream Elevation
<b>STREET 'C' MH#01 TO MH#02</b>																									
A1	RESIDENTIAL	MH#01	MH#02		0.184			0.31	0.31	20.00	5	75.35	23.12	23.12	0.450	450	R.C.P.	0.18	15.96	120.9	0.76	0.35	19%	186.440	186.411
<b>STREET 'C' MH#02 TO MH#03</b>																									
A2	RESIDENTIAL	MH#02	MH#03		0.148			0.25	0.55	20.35	5	74.59	41.30	41.30	0.750	750	R.C.P.	0.18	42.39	472.1	1.07	0.66	9%	186.411	186.335
<b>STREET 'A' MH POND TO MH#03</b>																									
		POND	MH#04		0.000			0.00	0.00	20.00	5	75.35	0.00	0.00	0.900	900	R.C.P.	0.22	13.72	848.7	1.33	0.17	0%	186.440	186.410
A3	RESIDENTIAL	MH#04	MH#03		0.482			0.80	0.80	20.17	5	74.97	60.28	60.28	1.050	1050	R.C.P.	0.07	66.03	722.2	0.83	1.32	8%	186.410	186.364
<b>STREET 'A' MH#03 TO MH#06</b>																									
A4	RESIDENTIAL	MH#03	MH#06		0.507			0.85	2.20	21.49	5	72.22	159.17	159.17	1.050	1050	R.C.P.	0.07	87.25	722.2	0.83	1.74	22%	186.340	186.279
<b>STREET 'B' MH#05 TO MH#06</b>																									
A5	RESIDENTIAL	MH#05	MH#06		0.532			0.89	0.89	20.00	5	75.35	66.86	66.86	0.600	600	R.C.P.	0.12	58.30	212.6	0.75	1.29	31%	186.320	186.250
<b>STREET 'A' MH#06 TO MH#07</b>																									
A6	RESIDENTIAL	MH#06	MH#07		0.653			1.09	4.18	23.23	5	68.92	288.09	288.09	1.050	1050	R.C.P.	0.07	92.46	722.2	0.83	1.85	40%	186.250	186.185

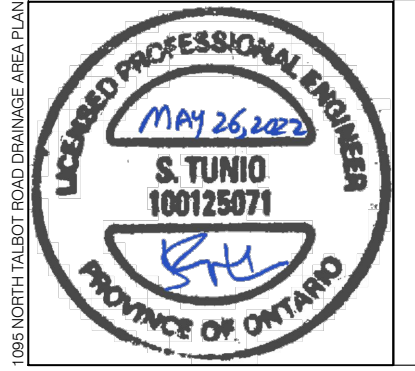
Q = 2.78 AIR, where  
 Q= Peak Flow in Litres per Second (l/s)  
 A= Area in hectares (ha)  
 I= Rainfall Intensity (mm/hr)  
 R= Runoff Coefficient

1) Windsor Rainfall-Intensity Curve  
 2) Min Pipe Velocity = 0.76 m/s  
 3) Max pipe Velocity = 3.0 m/s  
 4) Tc = 20 min (BASED ON 3.2.2.6 WERSMSM)  
 Intensity =  $I = a / (T+b)^c$

A= 1259  
 B= 8.8  
 C= 0.838

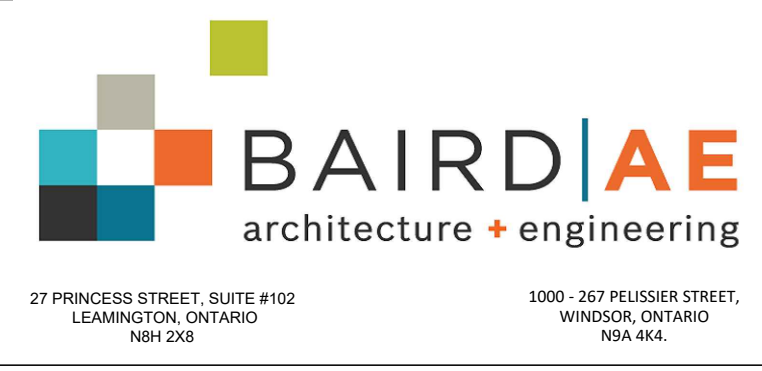


Consultant: Baird AE - Architects & Engineers		
Date:	May 27, 2022	
Design:	BILL FUERTH	
Project No:	Dwg. Reference:	Stamped:
21-021	1095 NORTH TALBOT	BFUERTH



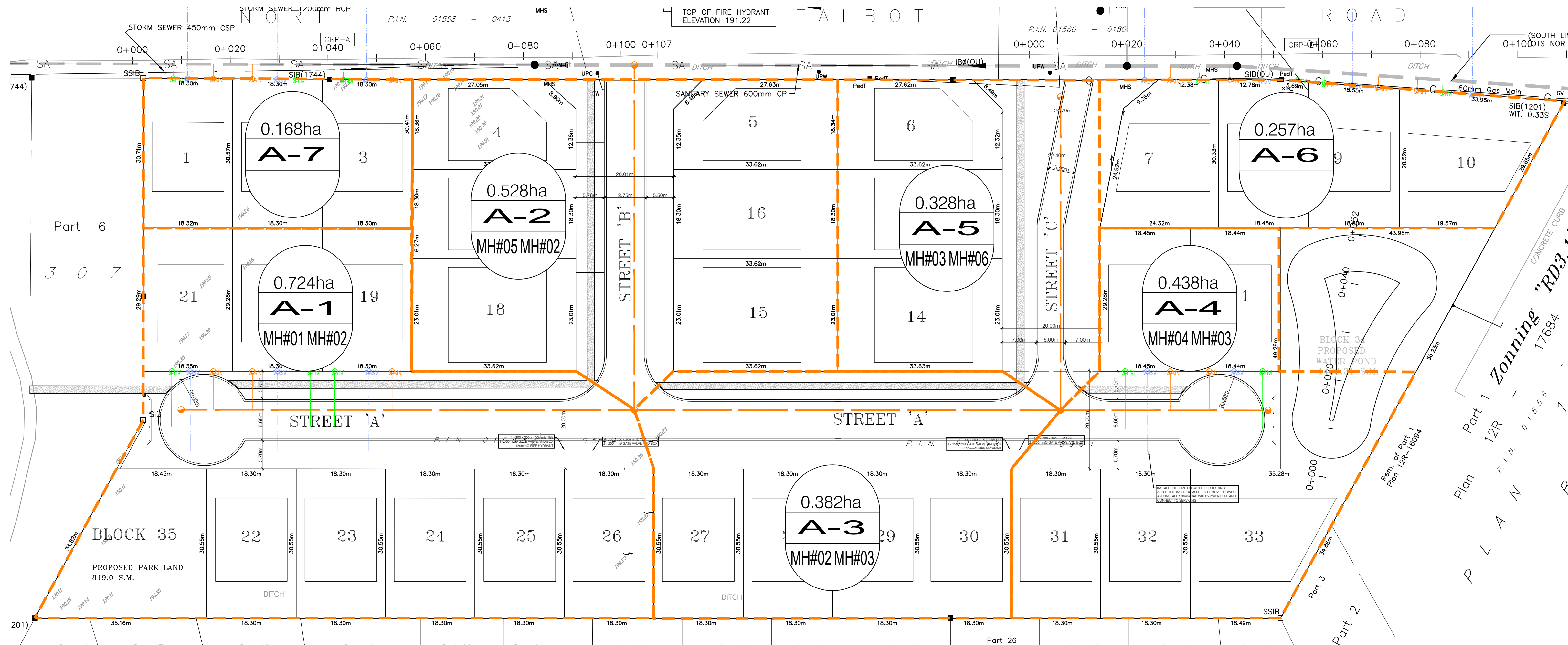
DATE: MAY 27, 2022  
 SHURJHEEL TUNIO, P. ENG.

DATE	REVISIONS
05/14/2021	SUBMITTED FOR APPROVALS
06/29/2021	REVISED AS PER ENWIN COMMENTS
22/04/2022	REVISED AS PER ERCA COMMENTS
27/05/2022	REVISED AS PER CITY COMMENTS
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PROJECT TITLE:  
 NORTH TALBOT DEVELOPMENT  
 1095 NORTH TALBOT ROAD, WINDSOR  
 SHEET TITLE:  
 STORM DRAINAGE AREA PLAN

DATE:	MAY 27, 2022	PROJECT NO:	21-021
SCALE:	1:500	SHEET NO.:	9
DRAWN BY:	B.T.	CHECKED BY:	S.T.



DEVELOPMENT	Area Included	From Node	To Node	DESIGN AREA SERVED (ha)		DESIGN POPULATION (PERSONS)		ULTIMATE FLOW FACTOR	MAXIMUM FLOW			SEWER DATA							PROFILE			
				INCREMENT	TOTAL	INCREMENT	TOTAL		INFILTRATION (L/s)	SEWAGE (L/S)	TOTAL SEWAGE AND INFILTRATION (L/s)	Dia. (m) Actual	Dia. (mm)	Type	Manning's "n"	Slope (%)	Length (m)	Capacity (L/s)	Velocity (m/s)	Ratio (%)	Upstream Elevation Invert (m)	Downstream Elevation Invert (m)
SAN MH#1 TO MH#2																						
STREET 'A'	A1	SAN MH 1	SAN MH 2	0.724	0.724	36	36	6	0.113	0.91	1.03	0.250	250	PVC	0.013	0.28%	92.75	31.471	0.64	3.26	185.607	185.347
SAN MH#4 TO MH#3																						
STREET 'A'	A4	SAN MH 4	SAN MH 3	0.438	0.438	22	22	6	0.068	0.55	0.62	0.250	250	PVC	0.013	0.28%	42.44	31.471	0.64	1.97	187.116	186.997
SAN MH#6 TO MH#3																						
STREET 'C'	A5	SAN MH 6	SAN MH 3	0.328	0.328	16	16	6	0.051	0.41	0.46	0.250	250	PVC	0.013	0.28%	64.16	31.471	0.64	1.48	187.653	187.473
SAN MH#3 TO MH#2																						
STREET 'A'	A3	SAN MH 3	SAN MH 2	0.382	1.148	19	57	6	0.179	1.45	1.63	0.250	250	PVC	0.013	0.28%	87.25	31.471	0.64	5.17	185.590	185.347
SAN MH#2 TO MH#5																						
STREET 'B'	A2	SAN MH 2	SAN MH 5	0.528	2.400	26	120	6	0.374	3.03	3.40	0.250	250	PVC	0.013	0.28%	70.62	31.471	0.64	10.80	185.297	185.099

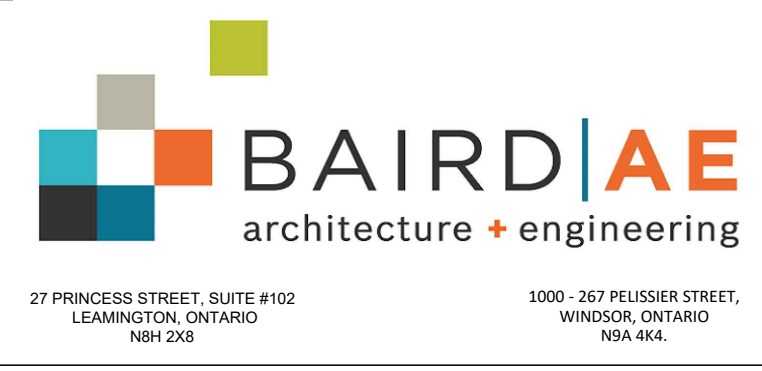


Design By: BILL FUERTH  
PROJECT NO: 21-021  
Checked and Stamped: BILL FUERTH



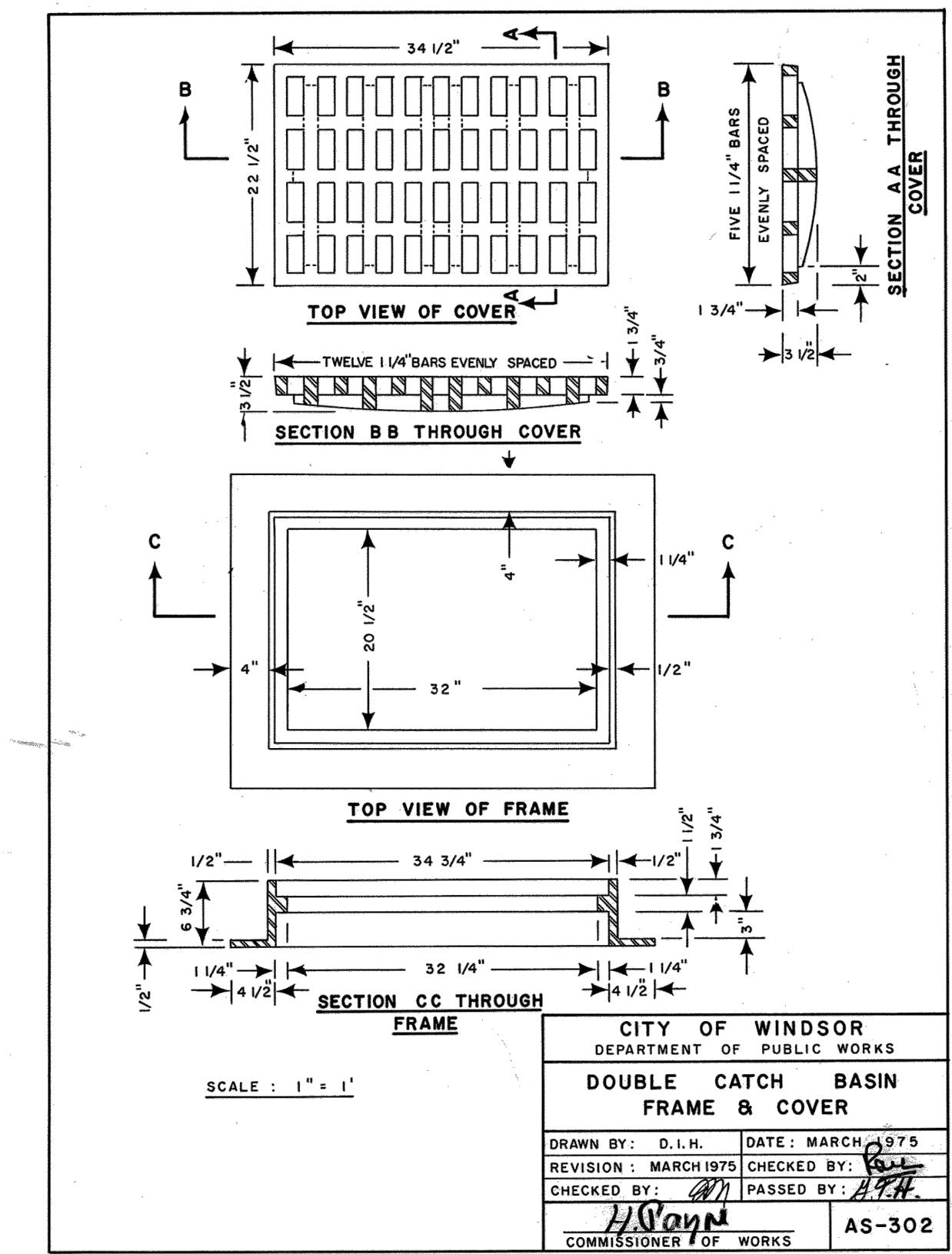
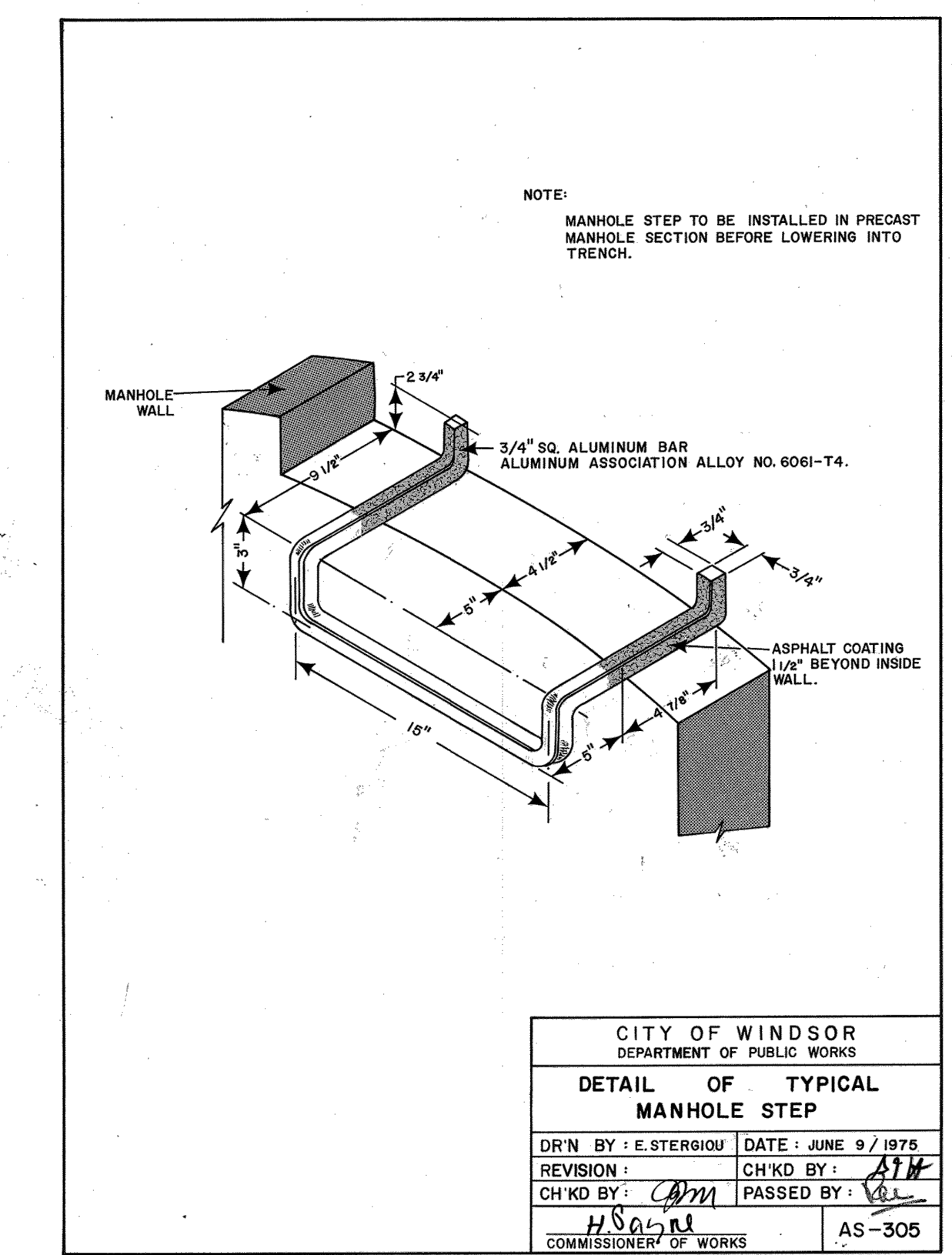
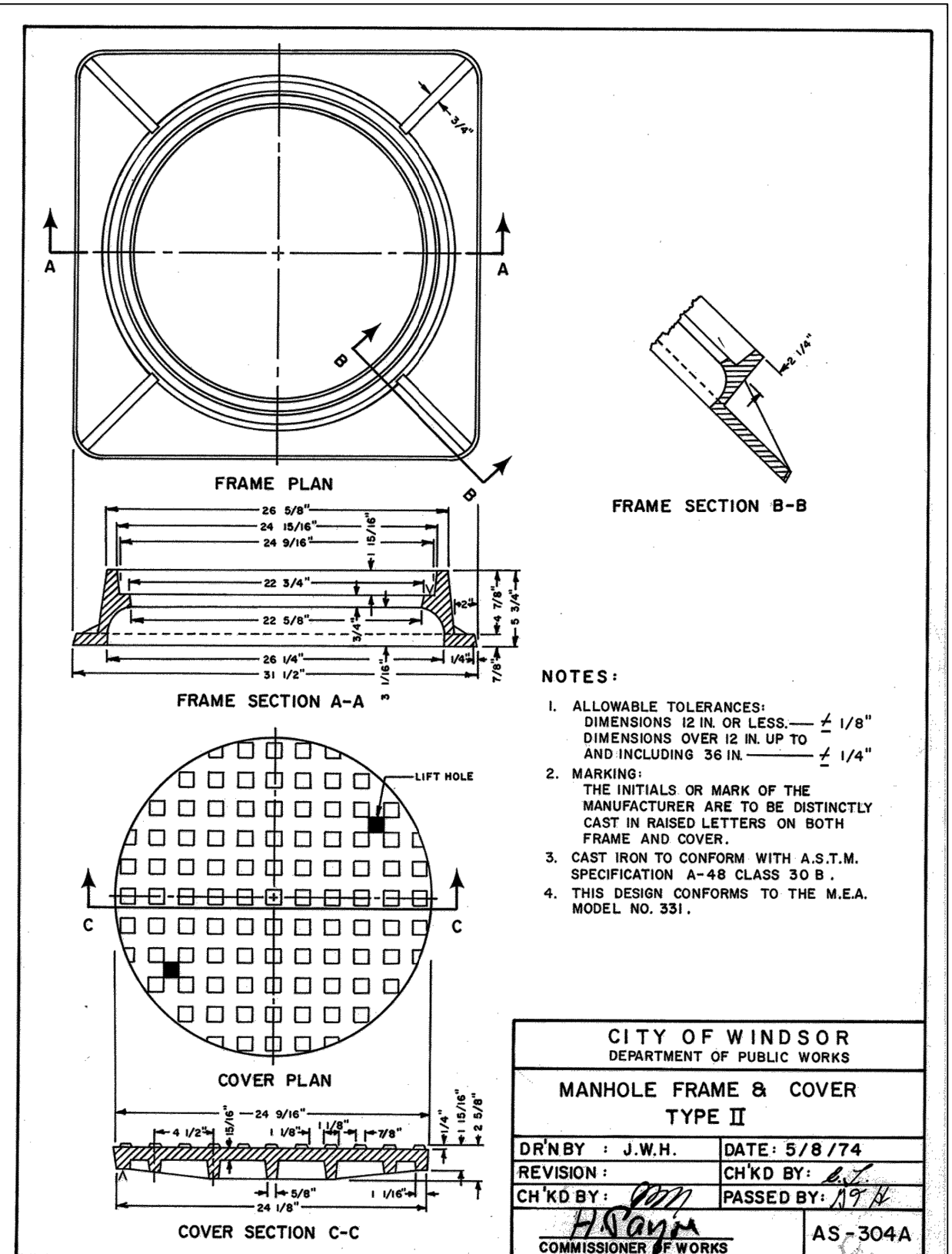
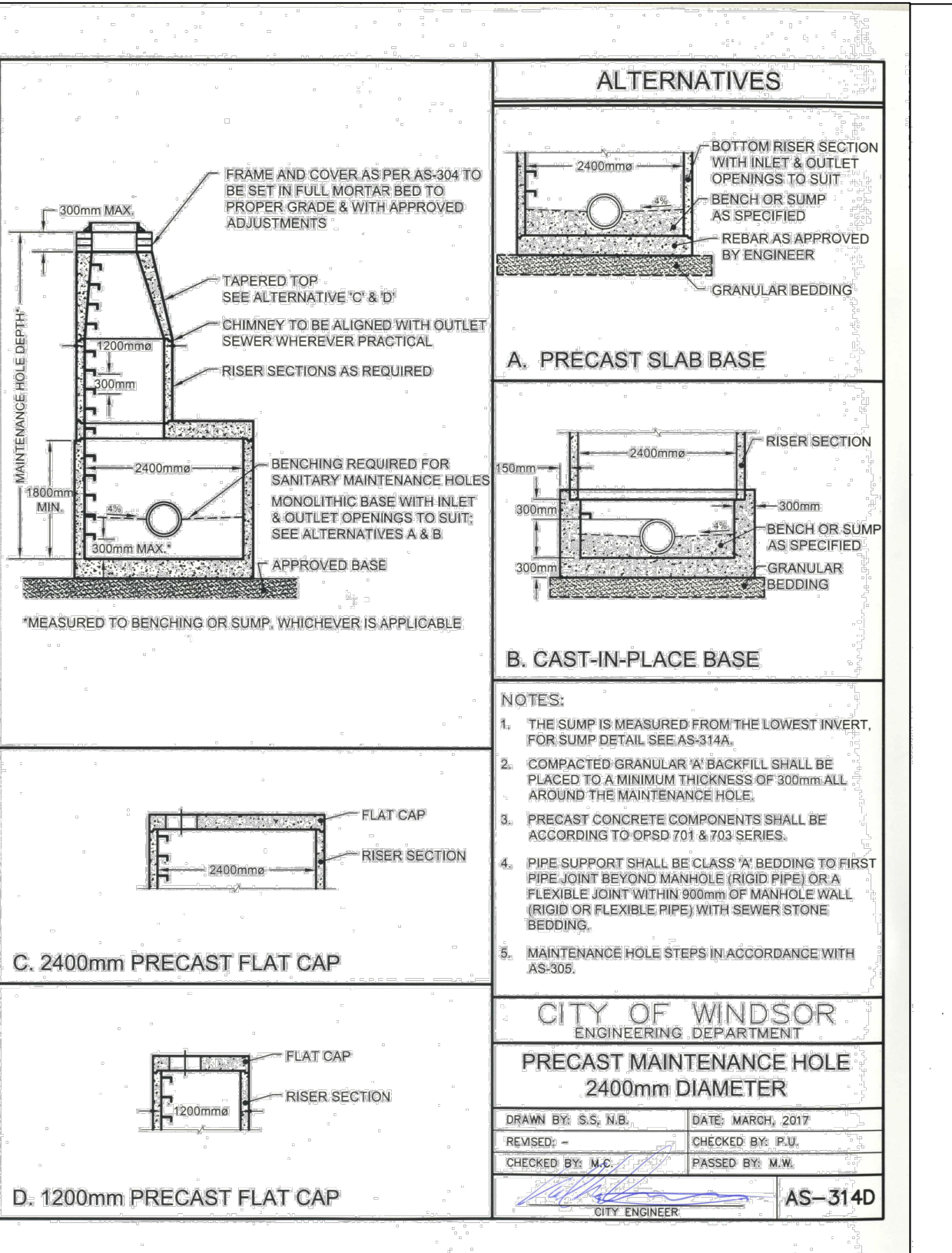
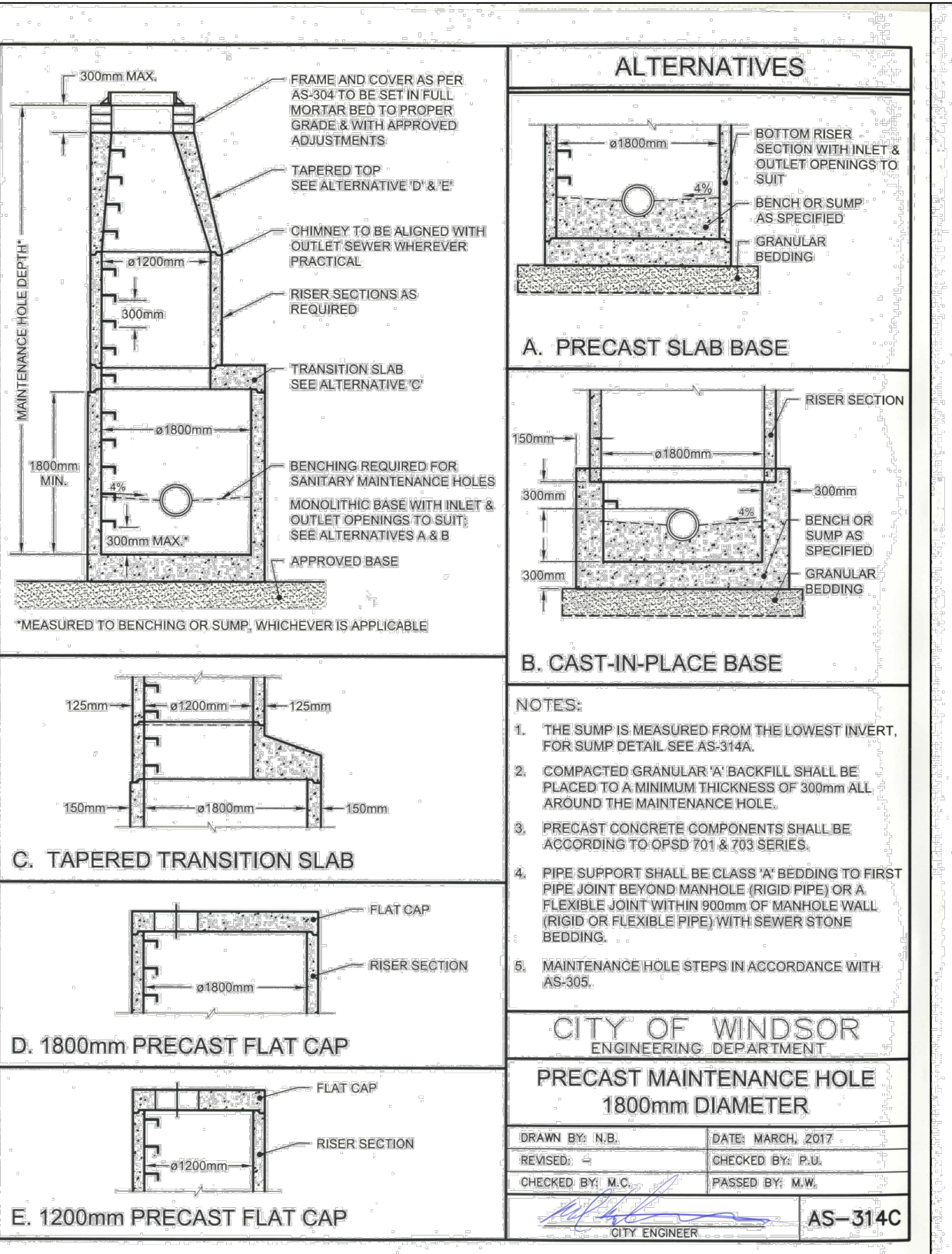
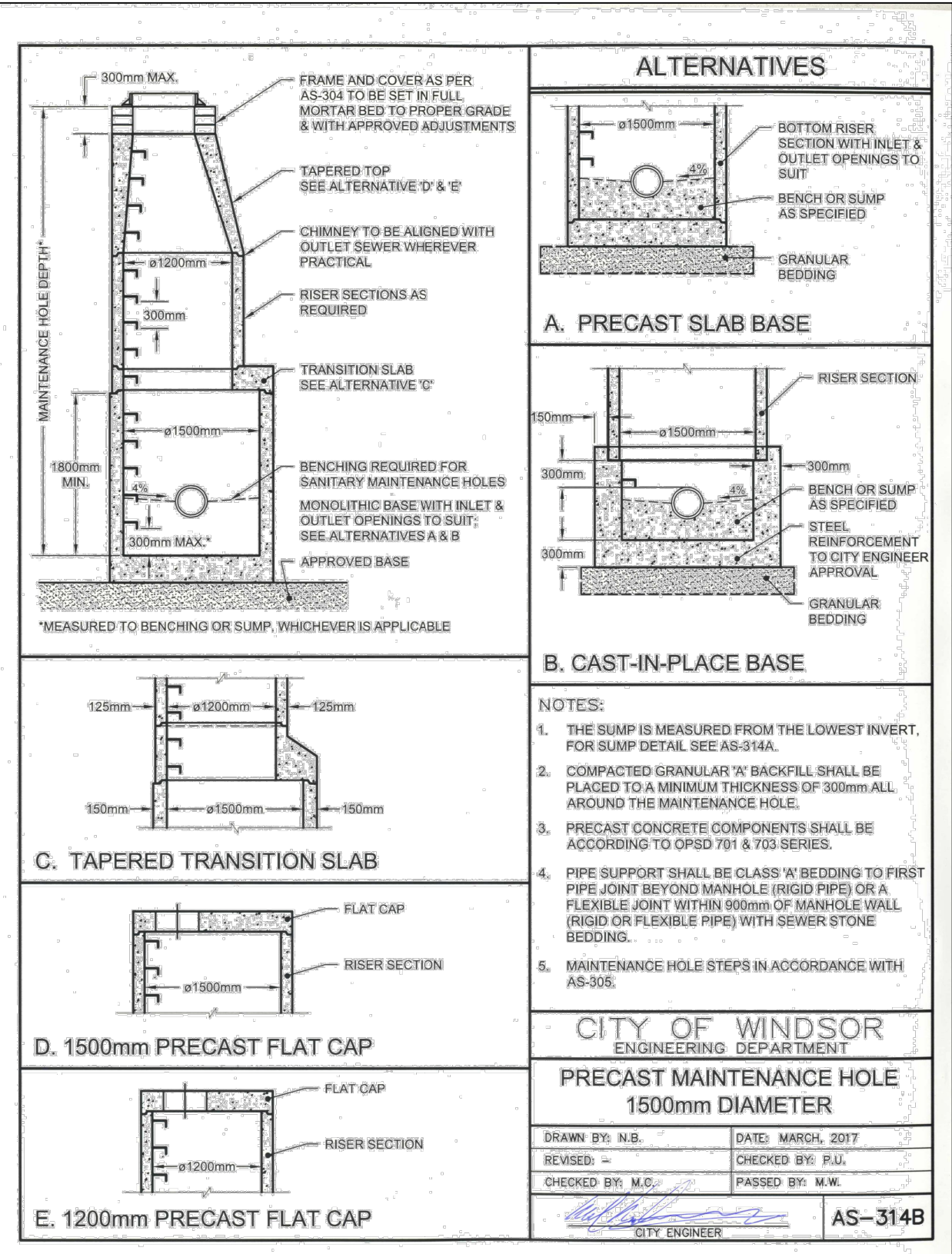
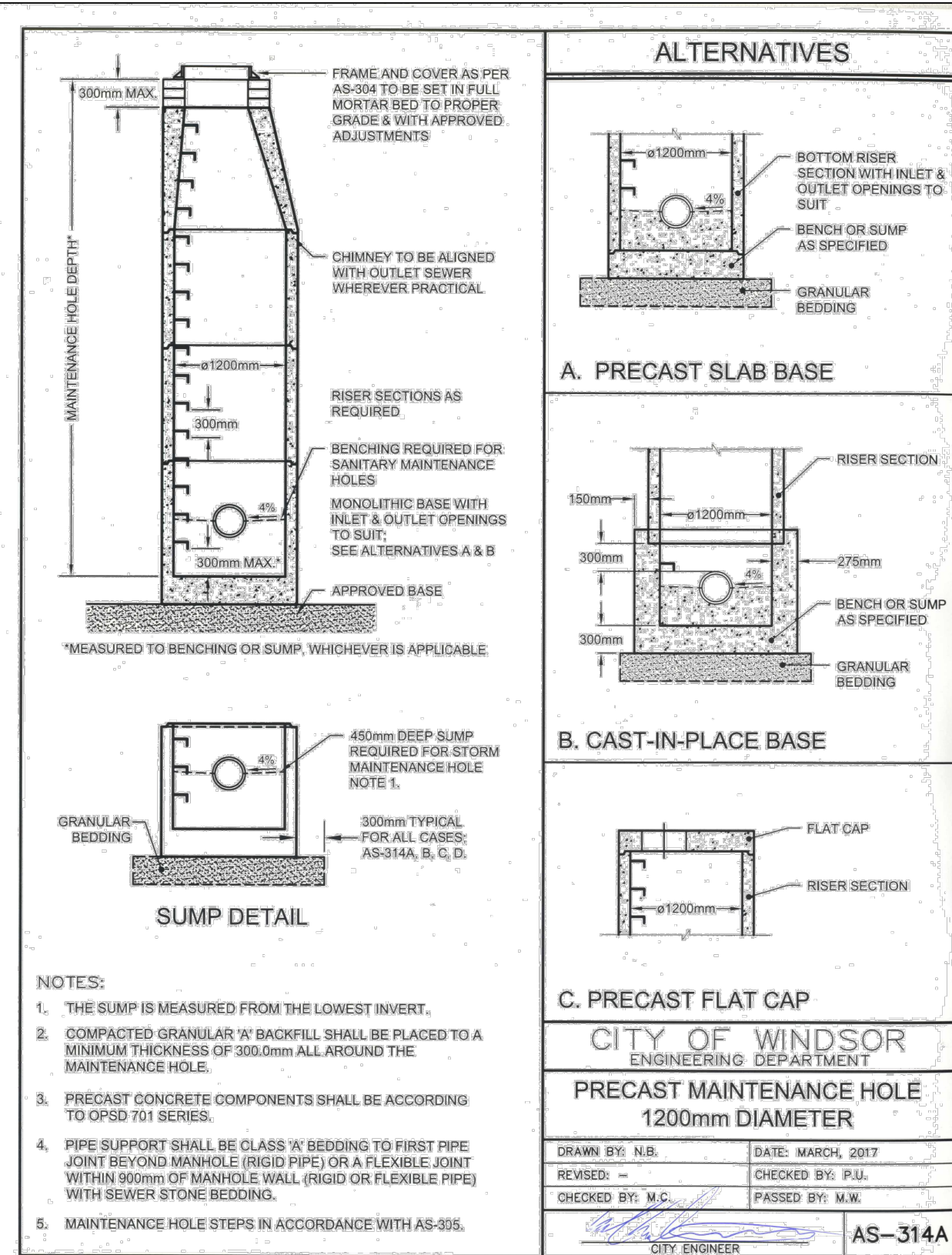
DATE: MAY 27, 2022  
SHURJHEEL TUNIO, P.ENG.

DATE	REVISIONS
05/14/2021	SUBMITTED FOR APPROVALS
06/29/2021	REVISED AS PER ENWIN COMMENTS
22/04/2022	REVISED AS PER ERCA COMMENTS
27/05/2022	REVISED AS PER CITY COMMENTS
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PROJECT TITLE: NORTH TALBOT DEVELOPMENT  
1095 NORTH TALBOT ROAD, WINDSOR  
SHEET TITLE: SANITARY DRAINAGE AREA PLAN

DATE: MAY 27, 2022  
SCALE: 1:500  
DRAWN BY: B.T.  
CHECKED BY: S.T.  
PROJECT NO: 21-021  
SHEET NO: 10



**ADS Canada**

**ADS OGS Sizing Summary**

Project Name: North Talbot Development  
Consulting Engineer: Baird AE  
Location: Windsor, ON

**Treatment Requirements**

Treatment Goal: Normal (MOE)  
Selected Parameters: 70% TSS 90% Volume  
Selected Unit: FD-5HC

**Summary of Results**

Model	TSS Removal	Volume Treated
FD-4HC	71.7%	86.7%
FD-5HC	75.1%	95.9%
FD-6HC	77.3%	99.7%
FD-8HC	81.6%	99.9%

**FD-5HC Specification**

Unit Diameter (A): 1,500 mm  
Inlet Pipe Diameter (B): 600 mm  
Outlet Pipe Diameter (C): 600 mm  
Height, T/G to Outlet Invert (D): 4190 mm  
Height, Outlet Invert to Sump (E): 1,500 mm  
Sediment Storage Capacity (F): 1.29 m<sup>3</sup>  
Oil Storage Capacity (G): 1,135 L  
Recommended Sediment Depth for Maintenance: 475 mm  
Max. Pipe Diameter: 600 mm  
Peak Flow Capacity: 566 L/s

**Site Elevations:**

Rim Elevation:	190.35
Inlet Pipe Elevation:	186.16
Outlet Pipe Elevation:	186.16

**ADS Canada**

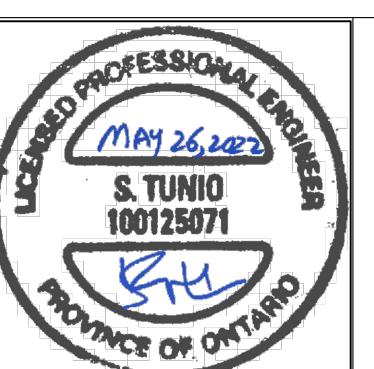
Project Name: North Talbot Development  
Consulting Engineer: Baird AE  
Location: Windsor, ON

**Net Annual Removal Efficiency Summary: FD-5HC**

Rainfall Intensity <sup>(1)</sup> mm/hr	Fraction of Rainfall <sup>(2)</sup> %	FD-5HC Removal Efficiency <sup>(2)</sup> %	Weighted Net-Annual Removal Efficiency %
3.00	13.2%	84.4%	11.1%
4.00	9.6%	82.2%	7.9%
5.00	7.5%	80.5%	6.0%
6.00	6.0%	79.2%	4.9%
7.00	4.8%	78.0%	3.7%
8.00	4.1%	77.1%	3.2%
9.00	3.6%	76.2%	2.7%
10.00	3.2%	75.5%	2.4%
11.00	2.8%	74.8%	2.1%
12.00	2.5%	74.2%	1.9%
15.00	6.6%	72.7%	4.8%
20.00	8.3%	70.8%	5.9%
25.00	5.8%	69.3%	4.0%
30.00	4.6%	68.2%	3.1%
35.00	3.8%	67.2%	2.6%
40.00	2.9%	66.4%	1.9%
45.00	2.4%	65.6%	1.6%
50.00	1.8%	65.0%	1.2%
65.00	6.6%	63.4%	4.2%
<b>Total Net Annual Removal Efficiency:</b>			75.1%
<b>Total Runoff Volume Treated:</b>			95.9%

**Notes:**

(1) Based on Windsor/Essex Region Stormwater Manual 2018, Table 3.4.1.5  
(2) Based on third party verified data and approximating the removal of a PSD similar to the STC Fine distribution



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SHURJHEEL TUNIO, P. ENG.

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22/04/2022	REVISED AS PER ERCA COMMENTS
27/05/2022	REVISED AS PER CITY COMMENTS



PROJECT TITLE:  
**NORTH TALBOT DEVELOPMENT**

1095 NORTH TALBOT ROAD, WINDSOR

SHEET TITLE:  
**DETAILS 1**

DATE: MAY 27, 2022

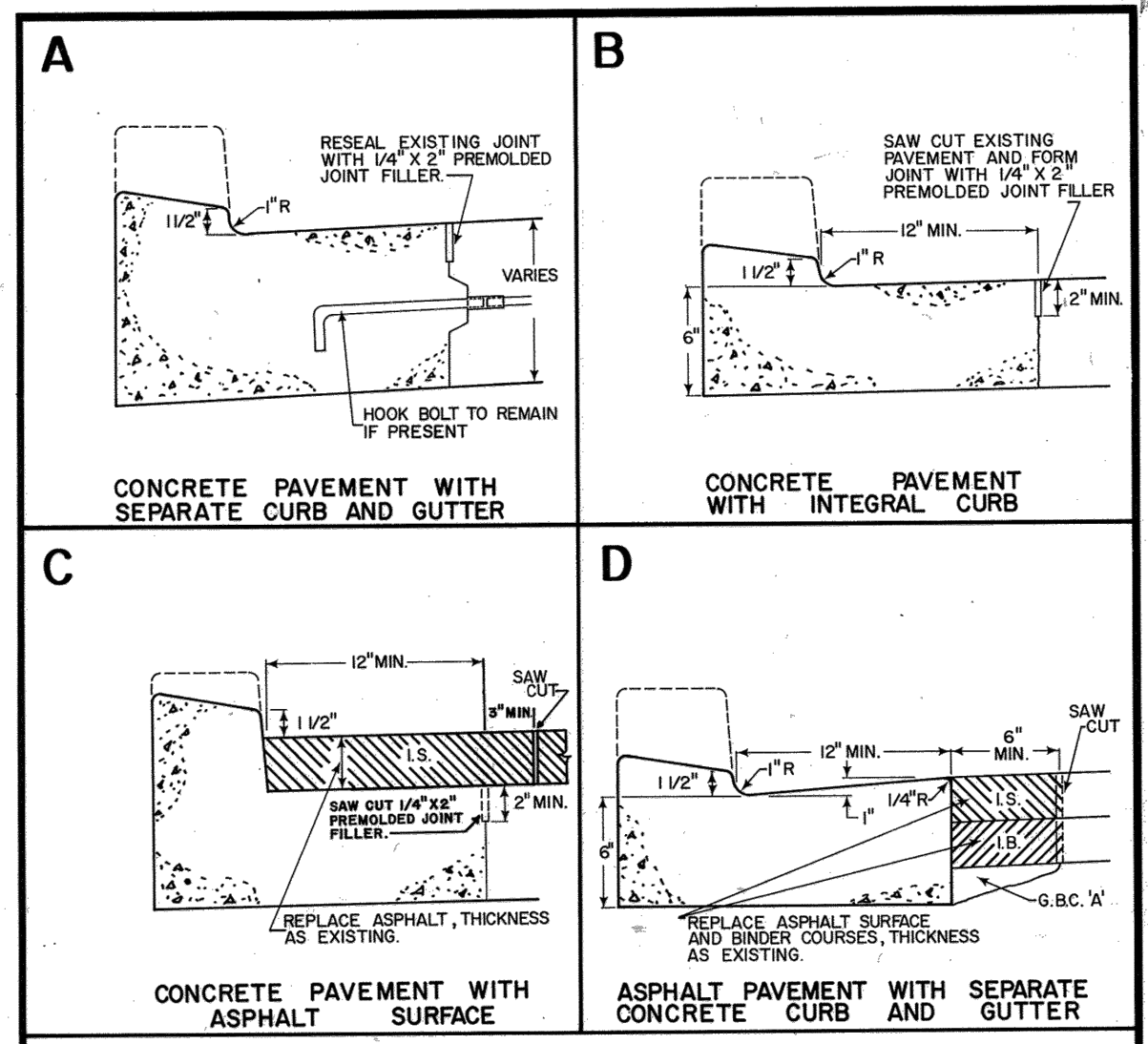
SCALE: N.T.S.

DRAWN BY: B.T.

CHECKED BY: S.T.

PROJECT NO: 21-021

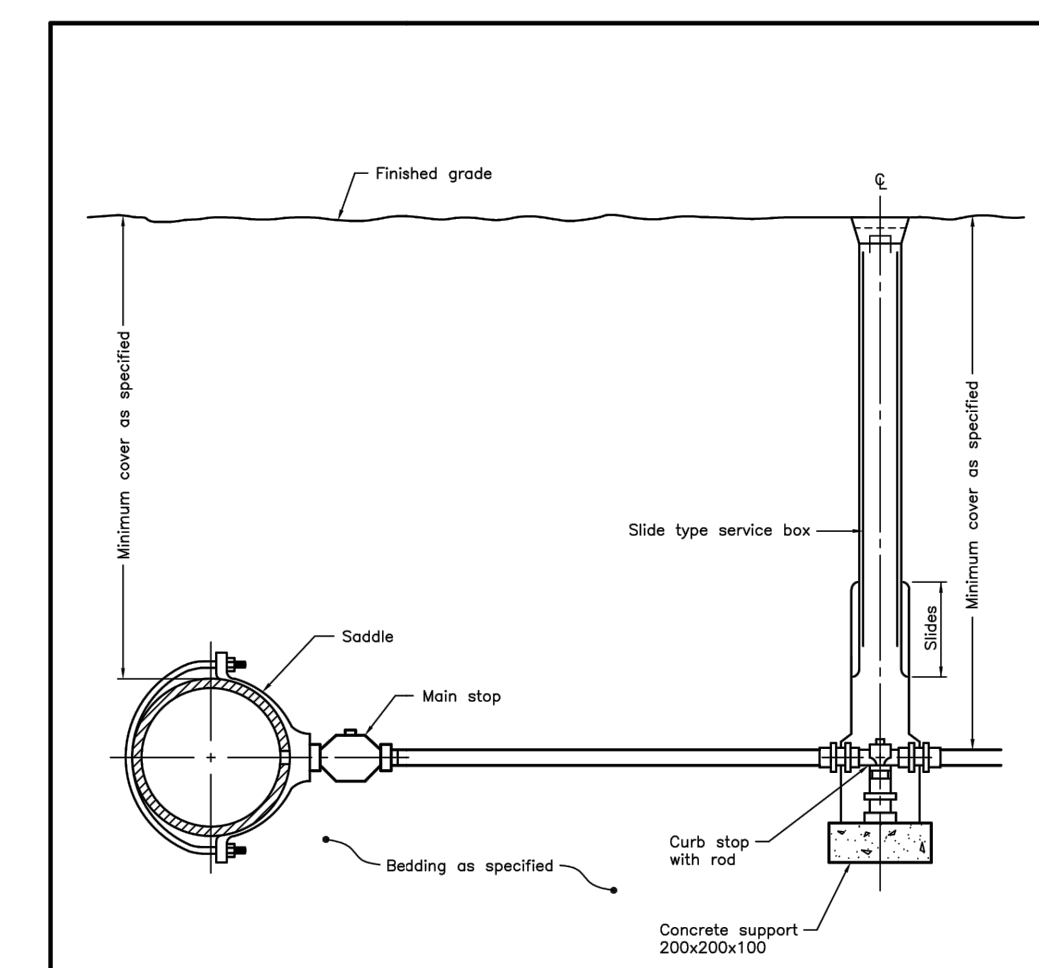
SHEET NO: 11



**NOTES:**

- SLOPE AND LENGTH OF DROPPED CURB TO BE DETERMINED BY THE ENGINEER.
- REMOVE EXISTING CURB AND GUTTER TO THE NEAREST JOINTS, REMOVE EXISTING ASPHALT AND GRANULAR BASE WHERE NECESSARY TO FORM GUTTER FACE.
- THICKNESS OF CONCRETE GUTTER IS TO BE THE SAME AS EXISTING ADJOINING GUTTER.
- CLEAN ALL FRACTURED FACES PRIOR TO PLACING OF CONCRETE.
- JOINT SPACING IN CONCRETE SHALL MATCH EXISTING. MAXIMUM SPACING IS 16 FT.
- ALL PRE-MOLDED JOINT FILLER TO CONFORM TO M.T.C. FORM 1508 EXCEPT AS OTHERWISE STATED. ALL JOINT FILLER TO BE 3/4" THICK AND FULL DEPTH.
- ALL WORK SHALL CONFORM TO CITY OF WINDSOR STANDARD SPECIFICATIONS S-4, S-5, S-9 AND S-10.
- CURB CUT TO BE CARRIED OUT AS ABOVE OR BY HORIZONTAL SAW CUTTING TO METHOD AS DIRECTED BY THE ENGINEER.

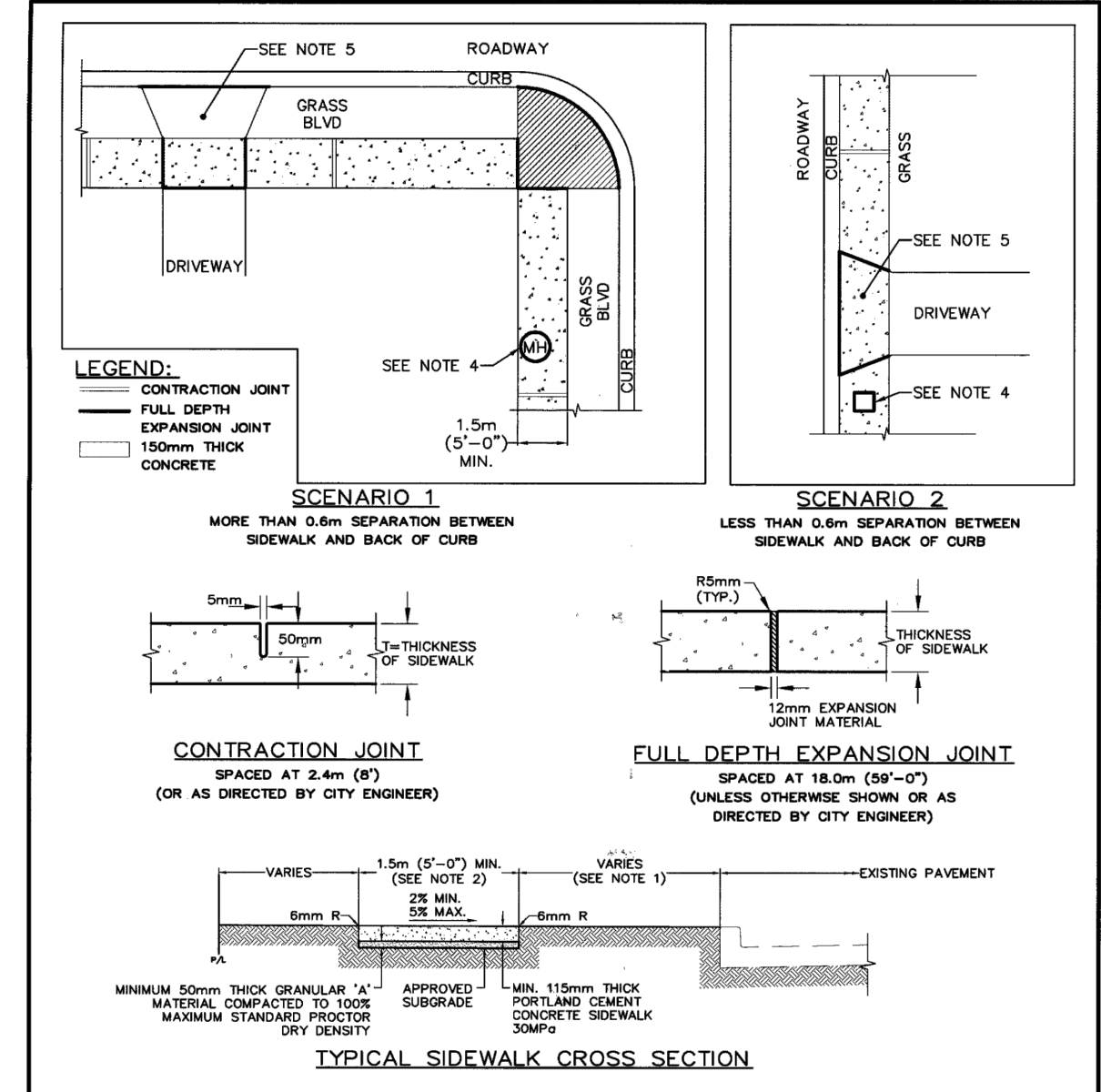
CITY OF WINDSOR DEPARTMENT OF PUBLIC WORKS	
CURB CUT REQUIREMENTS FOR VARIOUS PAVEMENTS	
DRN BY: K.A.J.	DATE: 6/23/72
REVISION: JAN. 1976	CHKD BY: S.S.
CHKD BY: P.J.U.	PASSED BY: H.P.
AS-216	



**NOTES:**

- Couplings shall not be permitted unless the service length exceeds 20m between the main stop and curb stop.
- All water services shall be installed 90° to the longitudinal axis of the watermain.
- Backfill material within 500mm of service box shall be native or imported, as specified.
- All dimensions are in millimetres unless otherwise shown.

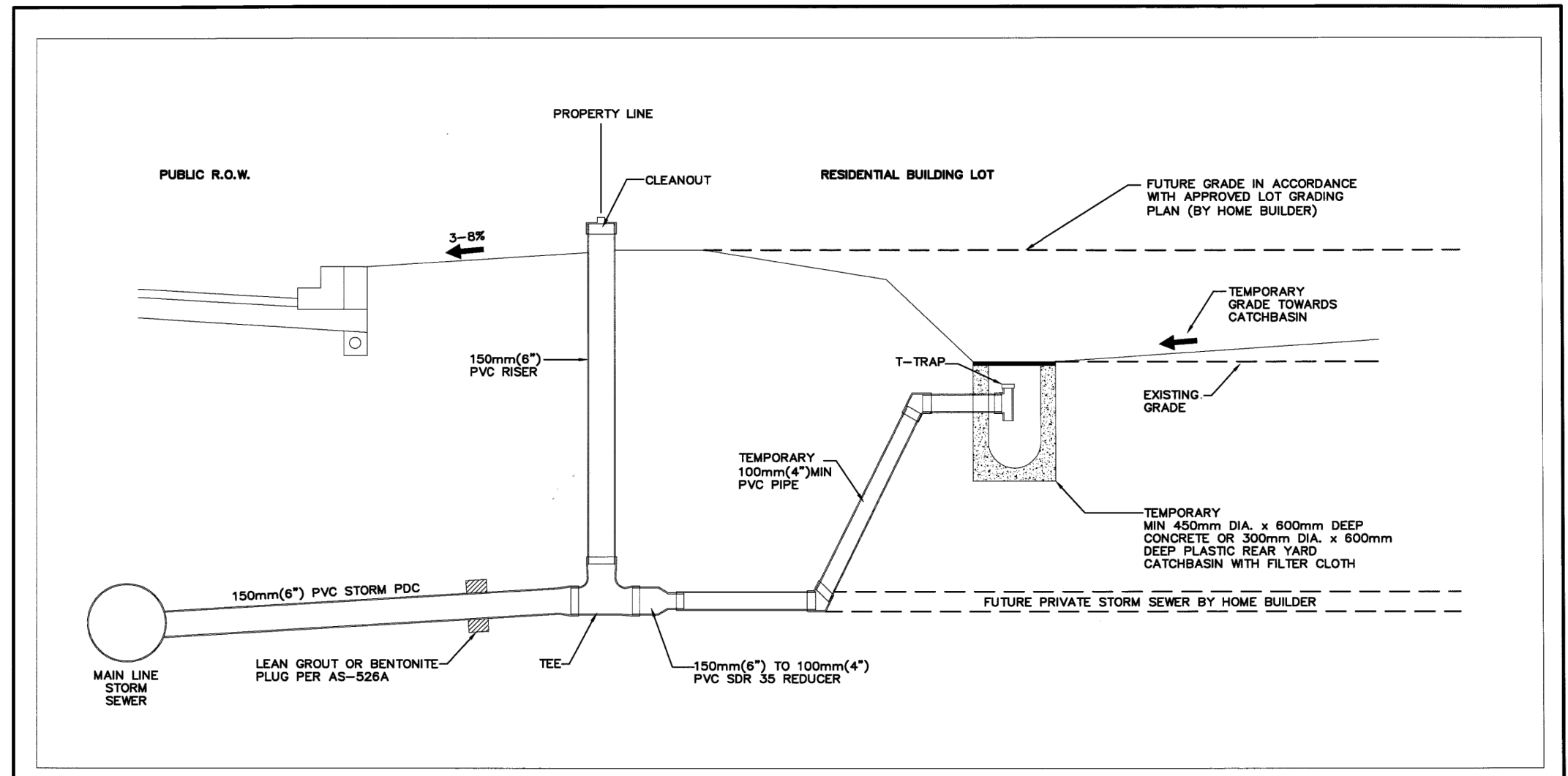
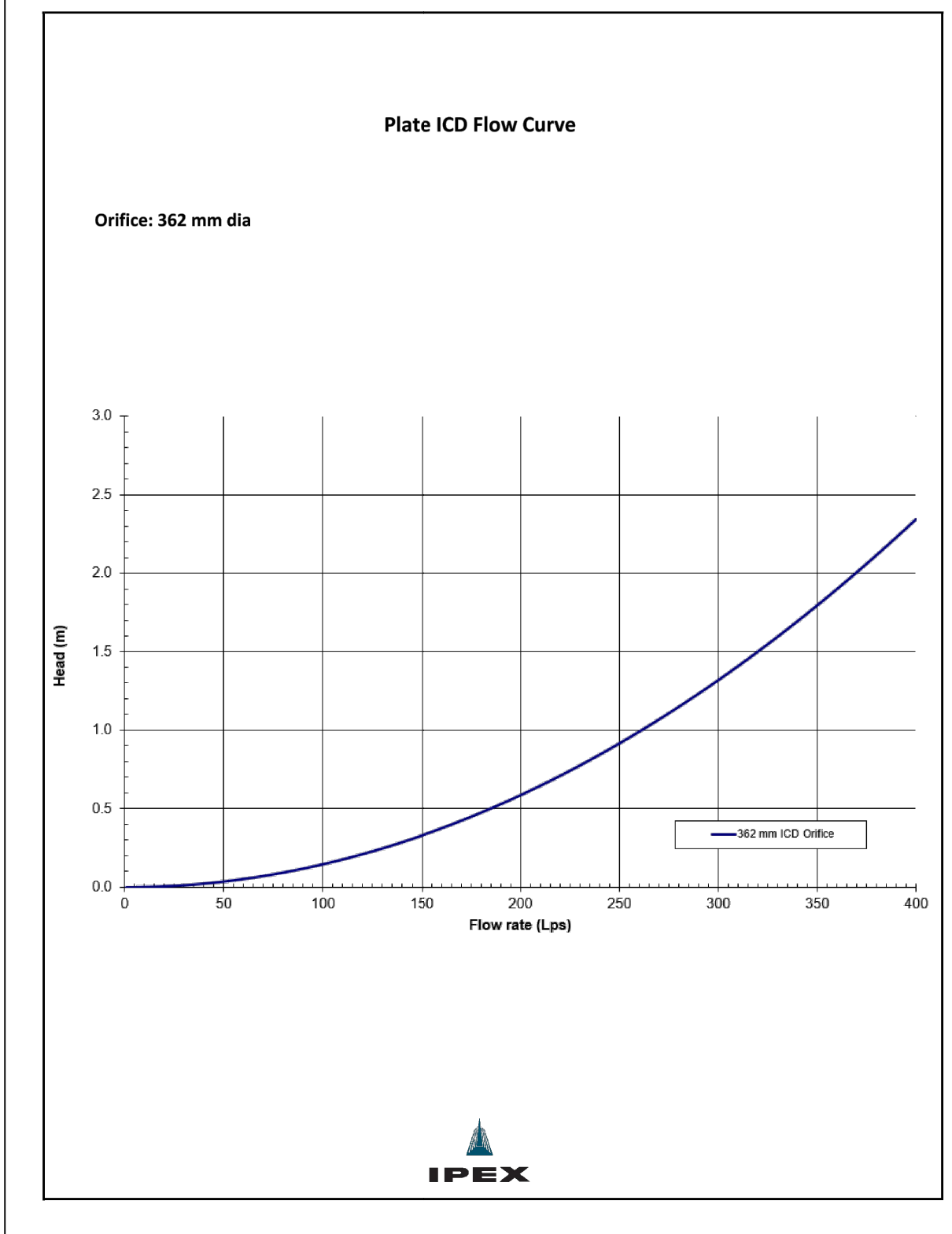
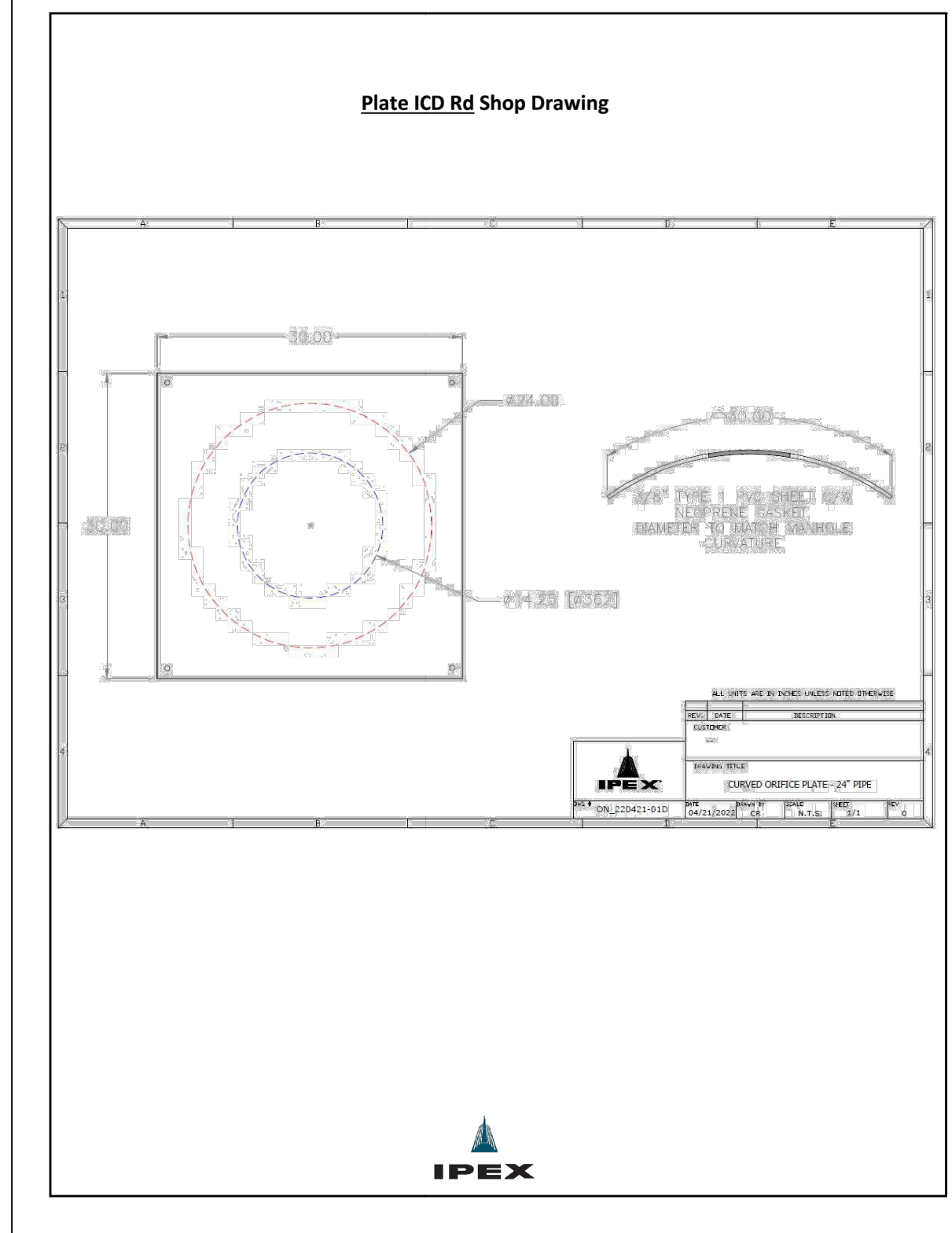
ONTARIO PROVINCIAL STANDARD DRAWING	Nov 2018	Rev 3
WATER SERVICE CONNECTION		
32, 38, and 50mm DIAMETER SIZES		
		OPSD 1104.020



**GENERAL NOTES:**

- Unless sidewalk abuts the curb, the distance between sidewalk and back of curb must be 600mm minimum.
- Any sidewalk width exceeding 3m must have a centered longitudinal contraction joint. Maximum spacing between longitudinal contraction joints to be 1.5m.
- Expansion joints are required at all fixed objects (poles, hydrants, manholes, structures, etc) and change in sidewalk direction.
- All manholes, boxes, valves, etc. to match finished grade of sidewalk.
- To be read in conjunction with AS-222, AS-402, AS-404, AS-406 and S-6.
- All work within public right-of-way to be completed to City of Windsor Standards and to the satisfaction of the City Engineer.

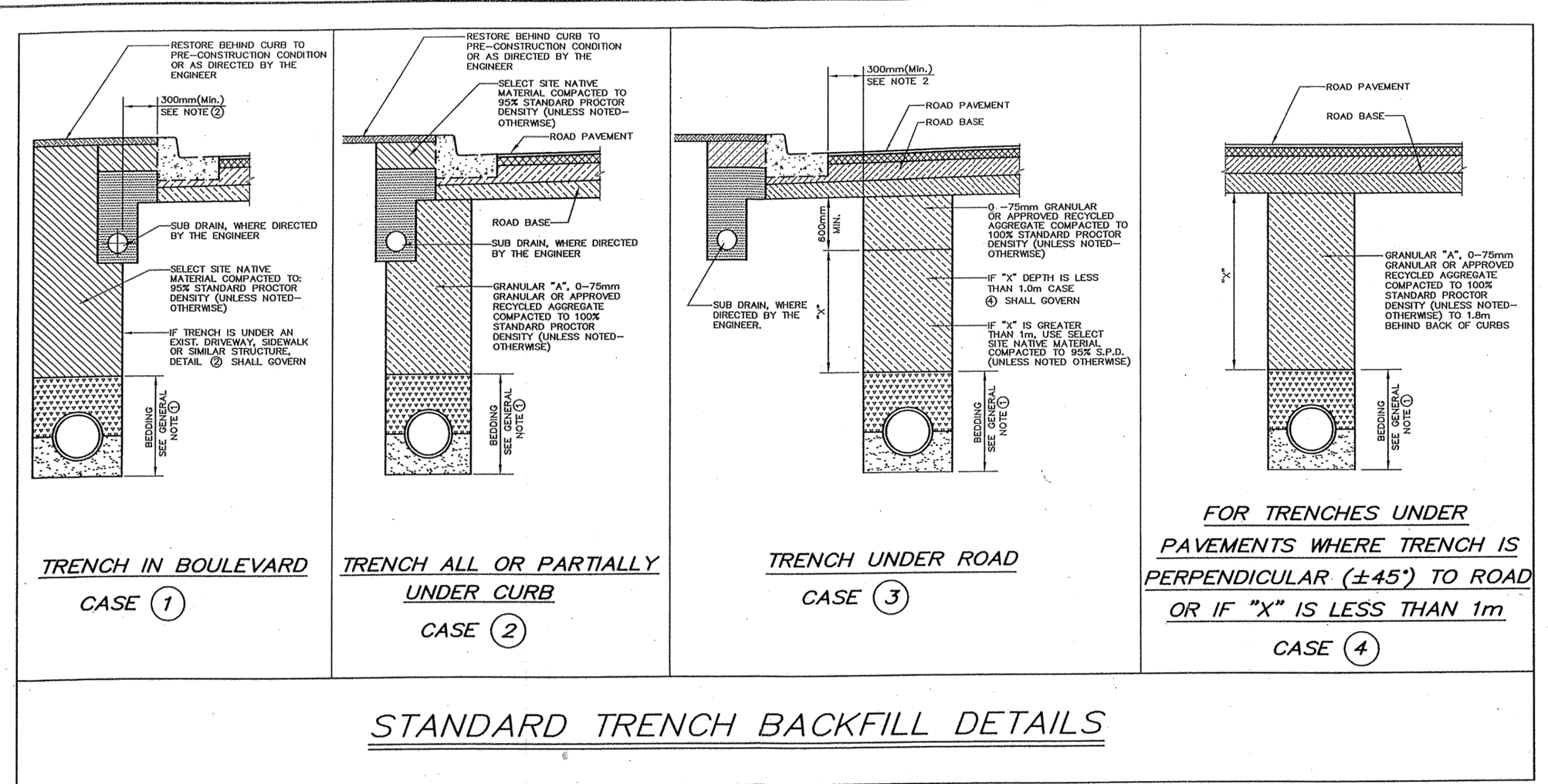
CITY OF WINDSOR ENGINEERING DEPARTMENT	
RESIDENTIAL CONCRETE SIDEWALK	
DRN BY: R.C. N.B.	DATE: APRIL 2004
REVISION: APRIL 2005	CHKD BY: W.B.
CHKD BY: P.J.U.	PASSED BY: T.M.
AS-401	



**NOTES:**

- DEVELOPER IS TO CONSTRUCT TEMPORARY LOT DRAINAGE FOR EACH BUILDING LOT AS SHOWN ABOVE AT THE TIME OF SEWER CONSTRUCTION.
- HOME BUILDER TO CONNECT PRIVATE STORM SEWER TO EXISTING TEE AT THE PROPERTY LINE AND REMOVE TEMPORARY CATCHBASIN AND PIPES ON PRIVATE PROPERTY.
- DETERMINATION OF WHETHER TEMPORARY LOT DRAINAGE IS REQUIRED SHALL BE AT THE DISCRETION OF THE CITY ENGINEER AND THE CHIEF BUILDING OFFICIAL.

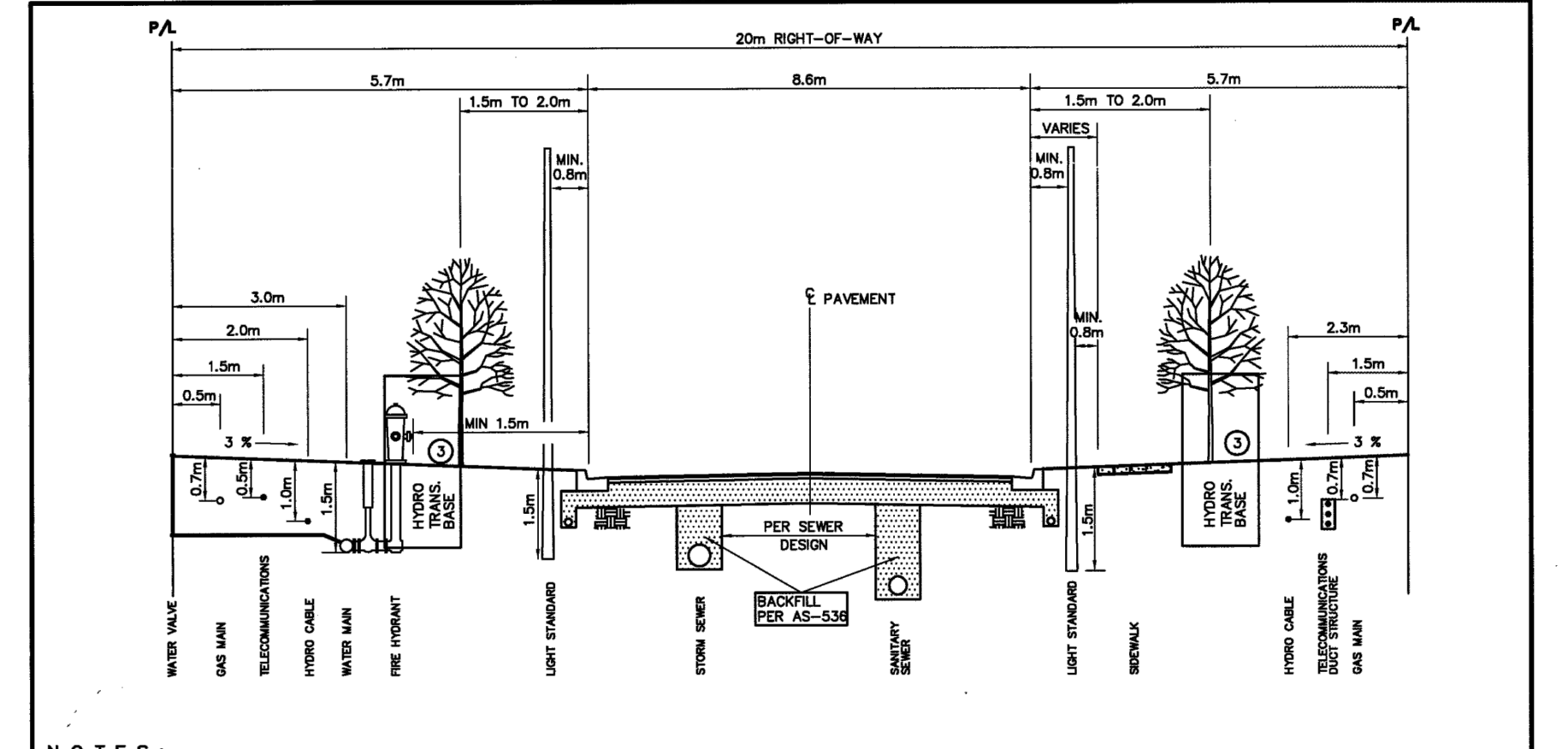
CITY OF WINDSOR ENGINEERING DEPARTMENT	
TEMPORARY LOT DRAINAGE	
DRN BY: J.L.	DATE: AUGUST, 2013
REVISION:	CHKD BY: S.S.
CHKD BY: P.J.U.	PASSED BY:
AS-550	



**GENERAL NOTES:**

- FOR SEWER AND PRIVATE SEWER CONNECTION CONSTRUCTION, ALL WORK TO BE IN ACCORDANCE WITH THE CITY OF WINDSOR STANDARD SPECIFICATIONS S-1 APPLIED TO ALL CASES (1), (2), (3) & (4) OF THIS STANDARD DRAWING.
- FOR ALL CASES, IF TRENCH WALL IS CLOSER THAN 300mm FROM THE BACK OF CURB (FUTURE OR EXISTING) OR EDGE OF PAVEMENT (FUTURE OR EXISTING), IN ANY DIRECTION, THEN CASE (2) SHALL GOVERN.
- FOR CASE (4), WHERE TRENCH CROSSES PERPENDICULAR TO ROADWAY (E-405) AND CROSSES UNDERNEATH AN EXISTING CURB, DRIVEWAY OR SIMILAR STRUCTURE, EXTEND GRANULAR BACKFILL TO 1.5m (MIN.) EITHER SIDE OF STRUCTURE.
- THIS DETAIL ADDRESSES TRENCH BACKFILL ONLY. SURFACE RESTORATION SHALL BE AS OTHERWISE APPROVED BY THE COMMISSIONER OF WORKS.

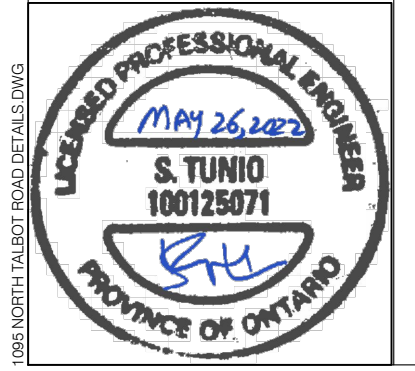
CITY OF WINDSOR DEPARTMENT OF PUBLIC WORKS	
STANDARD TRENCH BACKFILL DETAILS	
DRN BY: R.C.	DATE: OCTOBER, 2002
REVISION:	CHKD BY:
CHKD BY: P.J.U.	PASSED BY:
AS-538	



**NOTES:**

- IT IS NOTED THE ABOVE CROSS-SECTION IS MEANT TO BE A GUIDELINE ONLY AND CANNOT POSSIBLY APPLY IN EVERY INSTANCE. FOR EXAMPLE, NO PROVISION IS SHOWN FOR TELEPHONE MANHOLES AND IT IS THE RESPONSIBILITY OF THE DESIGN ENGINEER TO HAVE ALL UNDERGROUND LOCATIONS APPROVED BY THE UTILITY COMPANIES FOR EACH SUBDIVISION AS SET OUT IN THE CITY OF WINDSOR DEVELOPMENT MANUAL.
- VERTICAL DIMENSIONS TO UTILITY PLANT ARE GUIDELINES ONLY AND SHOULD BE CONFIRMED WITH THE UTILITY COMPANIES BY THE DESIGN ENGINEER. THESE DIMENSIONS ARE SHOWN TO TOP OF FINAL GRADE. IT IS THEREFORE ESSENTIAL THAT GRADING BE COMPLETED TO OUTER EDGE OF RIGHT-OF-WAY BEFORE UTILITY INSTALLATION AND TO BE WITHIN ±150mm OF FINAL GRADE.
- ABOVE GROUND TRANSFORMERS PEDESTALS TO BE LOCATED ON PLAN VIEWS BY ENGINEER. NO TWO (2) ABOVE GROUND UTILITIES ON THE SAME FRONTAGE.
- ALL TELECOMMUNICATIONS TO BE IN JOINT USE TRENCH FOR NEW CONSTRUCTION.
- SIDEWALKS TO BE IN ACCORDANCE WITH AS-401 TO AS-404 AS APPLICABLE.

CITY OF WINDSOR ENGINEERING DEPARTMENT	
STANDARD UTILITY CROSS-SECTION FOR 20m RIGHT-OF-WAY	
DRN BY: N.S.	DATE: JANUARY, 1980
REVISION: JULY, 2013	CHKD BY: S.S.
CHKD BY: P.J.U.	PASSED BY:
AS-206D	



DATE: MAY 27, 2022

SHURJEEEL TUNIO, P.ENG.

DATE	REVISIONS
05/14/2021	SUBMITTED FOR APPROVALS
06/29/2021	REVISED AS PER WIN COMMENTS
22/04/2022	REVISED AS PER ERCA COMMENTS
27/05/2022	REVISED AS PER CITY COMMENTS
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PROJECT TITLE:  
**NORTH TALBOT DEVELOPMENT**  
1095 NORTH TALBOT ROAD, WINDSOR

SHEET TITLE:  
**DETAILS 2**

DATE: MAY 27, 2022

SCALE: N.T.S.

DRAWN BY: B.T.

CHECKED BY: S.T.

PROJECT NO: 21-021

SHEET NO: 12

# Appendix B

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## Mitigation Measures

## **GENERAL MITIGATION MEASURES FOR CONSTRUCTION ADJACENT TO HABITAT FOR SAR REPTILES**

**1. Awareness** - Prior to conducting any work on site, project personnel and contractors should be made aware of the possible presence of protected snakes and their protection under the ESA 2007. Information materials to aid in species identification and encounter response should be provided to all personnel on site.

**2. Vegetation Clearing** – Vegetation clearing, including grubbing, will occur when weather conditions are suitable to allow reptiles to flee (sunny and at least 18°C). Vegetation clearing and grubbing will occur in an orderly and systematic manner to direct wildlife movement in one direction, and to reduce the possibility of wildlife encounters with equipment. Vegetation clearing will occur under the supervision of a qualified biologist to ensure no reptiles or other Protected Species are harmed. Clearing of vegetation can occur without the supervision of a qualified biologist if it occurs during the inactive season (between December 1 and March 31) and no grubbing or below-ground works are undertaken. Vegetation clearing during the inactive season should be performed in a manner that avoids soil compaction; vegetation can be cleared by hand, or cleared while the soil is frozen with light machinery that is equipped to reduce compaction. Removal of candidate bat maternity roost trees (trees with cavities or loose bark) must occur between September and April, outside the active bat season.

**3. Exclusion Fencing** – Once vegetation has been cleared, geotextile fencing should be installed as snake exclusion barrier along the construction boundary. The geotextile fence should be at least 1.0 meters high from grade at all locations and buried at least 0.2 meters below grade. Exclusion fencing should extend out from its terminal edges by a distance of at least 5 meters and angle out or back at a 45° angle (whichever is most beneficial) to direct wildlife away from the construction site. Installation of fencing during the active season (April 1 to November 30) will be supervised by a qualified biologist. Outside the active season, fencing may be installed without the supervision of a qualified biologist.

**4. Erosion Control** – To prevent entanglement of wildlife, including Protected Snakes, mesh or netting-type material must not be used for erosion control. Net-free materials, such as Curlex Net-Free blanket, riprap over geotextile fabric, or similar alternative is recommended.

**5. Equipment Inspection** - Between April 1 and November 30th, all equipment and machinery that is left idle for over 1 hour, or overnight, on the property must be visually examined prior to (re)ignition, to ensure reptiles are not present within the machinery. This visual examination should include all lower components of the machinery, including operational extensions and running gear.

**6. Encounters and Reporting** - Any SAR or protected wildlife that is encountered on site must be protected from harm and harassment. Should a protected reptile be observed in the work area and presumed to be unharmed, all project personnel and operating machinery should maintain a minimum 30-meter distance from it at all times until it has left the area. Contact MECP immediately if this cannot be done. A large Rubbermaid-type container with ventilated lid should be kept on site at all times in the

event a SAR is injured or killed during the project. If a SAR is injured, it should be immediately transported in the container to a licensed Wildlife Custodian. During transport, the snake inside the container should be maintained at a temperature between 10 and 30°C. MECP will be contacted immediately if any SAR are harmed or killed during construction.

### **7. Site Management**

The property should be clean and free of debris for any activities that occur during the active season for reptiles (April 1st to November 30th). Snakes may find and occupy materials and equipment stored on site and could be harmed when materials and debris are handled or used. The creation and duration of debris stockpiles within the development footprint should be limited. Materials such as excavated soils, lumber, and other construction materials should only be stored in areas that previously had understorey vegetation (1m or shorter), mowed to a height of 5 cm or shorter. Excavated soil should not be stored on the sites long term. Flat materials such as plywood or rubber mats should not be left lying on the ground. Any material stockpiles created on the property during the project must be visually examined for reptiles prior to disturbance or removal.

**8. Site Maintenance** – Cleared areas should be maintained at a height of 7-10cm. Allowing grass to grow greater than 15 cm in height could attract snakes to the construction sites.

## APPENDIX C - COMMENTS

### ENBRIDGE – Kelly Buchanan

It is Enbridge Gas Inc.'s (formerly Union Gas Ltd) request that as a condition of final approval that the owner/developer provide to Union the necessary easements and/or agreements required by Union for the provision of gas services for this project, in a form satisfactory to Enbridge.

### CANADA POST – Bruno DeSando

Thank you for contacting Canada Post regarding plans for a new subdivision in the City of Windsor. Please see Canada Post's feedback regarding the proposal, below.

#### **Service type and location**

1. Canada Post will provide mail delivery service to the subdivision through centralized Community Mail Boxes (CMBs).
2. If the development includes plans for (a) multi-unit building(s) with a common indoor entrance, the developer must supply, install and maintain the mail delivery equipment within these buildings to Canada Post's specifications.

#### **Municipal requirements**

1. Please update our office if the project description changes so that we may determine the impact (if any).
2. Should this subdivision application be approved, please provide notification of the new civic addresses as soon as possible.

#### **Developer timeline and installation**

Please provide Canada Post with the excavation date for the first foundation/first phase as well as the date development work is scheduled to begin. Finally, please provide the expected installation date(s) for the CMB(s).

**Please see Appendix A for any additional requirements for this developer.**

### **Appendix A**

#### **Additional Developer Requirements:**

- The developer will consult with Canada Post to determine suitable permanent locations for the Community Mail Boxes. The developer will then indicate these locations on the appropriate servicing plans.
- The developer agrees, prior to offering any units for sale, to display a map on the wall of the sales office in a place readily accessible to potential homeowners that indicates the location of all Community Mail Boxes within the development, as approved by Canada Post.
- The developer agrees to include in all offers of purchase and sale a statement which advises the purchaser that mail will be delivered via Community Mail Box. The developer also agrees to note the locations of all Community Mail Boxes within the development, and to notify affected homeowners of any established easements granted to Canada Post to permit access to the Community Mail Box.



- The developer will provide a suitable and safe temporary site for a Community Mail Box until curbs, sidewalks and final grading are completed at the permanent Community Mail Box locations. Canada Post will provide mail delivery to new residents as soon as the homes are occupied.
- The developer agrees to provide the following for each Community Mail Box site and to include these requirements on the appropriate servicing plans:
  - Any required walkway across the boulevard, per municipal standards
  - Any required curb depressions for wheelchair access, with an opening of at least two metres (consult Canada Post for detailed specifications)
  - A Community Mailbox concrete base pad per Canada Post specifications.

**ASSESSMENT MANAGEMENT OFFICER - Jose Mejalli**

No objection to the 34 lot subdivision and related roadways and change in zoning.

**TRANSIT WINDSOR - Jason Scott**

Transit Windsor has no objections to this development. The closest existing transit route to this proposed subdivision is with the Walkerville 8. The closest existing bus stops are located on North Talbot at Sixth Concession, Pioneer, and Old West. All of the proposed subdivision would be within our walking distance guidelines of 400 metres. The transit service will be changing in this area with our Council approved Transit Master Plan as the area will be getting a new 2 way conventional transit route along Southwood Lakes to replace the existing 1 way loop that is currently present along North Talbot. All of the proposed subdivision would still be within the walking distance guideline with this change.

**ENVIRONMENTAL & SUSTAINABILITY COORDINATOR - Jennifer Nantais**

In response to the application for a Plan of Subdivision there are no objections. Please also note the following comments for consideration:

**Energy Conservation, Air Quality and Climate Change:**

Please note PPS 2020 energy conservation and efficiency policies as they relate to long-term economic prosperity (1.7.1 (j)), as well as improved air quality and reduced greenhouse gas emissions (1.8.1). In addition, the City of Windsor Community Energy Plan (approved July 17 2017) aims to improve energy efficiency; modifying land use planning; reducing energy consumption and greenhouse gas emissions; and fostering green energy solutions throughout Windsor, while supporting local economic development.

As per these policies the developer should consider energy efficiency in the building design. This may include but not be limited to increased insulation, energy efficient appliances and fixtures, high efficiency windows and doors. In addition, consideration for EV charging infrastructure and opportunities to increase resiliency such as providing strategic back-up power capacity is warranted.

In addition, the large scale paving of natural space will increase the urban heat island in the area. It is recommended that the developer consider shade trees, white colour roofs or green roofs to mitigate this impact. For more suggestions please consult the following resources: LEED, Built Green Canada, and EnerGuide.

To promote the use of active transportation, bike racks should be included.

#### Stormwater Management:

Consideration should be given, as per PPS 2020 Section 1.6.6.7 to maximize the extent and function of vegetative and pervious surfaces; and promote stormwater management best practices, including stormwater attenuation and reuse, water conservation and efficiency, and low impact development.

#### Landscaping

Consideration for shade trees are recommended to minimize the urban heat island impacts. Consideration of native, drought resistant plants is encouraged to limit watering requirements.

The Environmental Sustainability & Climate Change team supports the comments submitted by the Landscape Architect on this application.

In addition we encourage the developer to consider community gardening space for residents. Local food production is very popular in Windsor and considering the size of this development a space for community garden boxes is warranted.

#### **HYDRO ONE – Dolly Shetty**

We are in receipt of Application SDN-001/21 [SDN/6575] - BelloCorp Inc., dated January 11, 2022. We have reviewed the documents concerning the noted Plan and have no comments or concerns at this time. *Our preliminary review considers issues affecting Hydro One's 'High Voltage Facilities and Corridor Lands' only.*

For proposals affecting 'Low Voltage Distribution Facilities' please consult your local area Distribution Supplier.

To confirm if Hydro One is your local distributor please follow the following link:

<http://www.hydroone.com/StormCenter3/>

Please select " Search" and locate address in question by entering the address or by zooming in and out of the map

If Hydro One is your local area Distribution Supplier, please contact Customer Service at 1-888-664-9376 or e-mail [CustomerCommunications@HydroOne.com](mailto:CustomerCommunications@HydroOne.com) to be connected to your Local Operations Centre.

#### **BUILDING DEPARTMENT- Barbara Rusan**

The Building Code Act, Section 8.(1) requires that a building permit be issued by the Chief Building Official for any construction or demolition of a building. It is strongly recommended that the owner

and/or applicant contact the Building Division to determine building permit needs for the proposed project. The City of Windsor Building Division can be reached by phone at 519-255-6267 or through email at [buildingdept@citywindsor.ca](mailto:buildingdept@citywindsor.ca)

### **WINDSOR MAPPING, ENBRIDGE - Sandro Aversa C.E.T.**

After reviewing the provided drawing and consulting our mapping system, please note that Enbridge Gas has active infrastructure in the proposed area. A PDF drawing has been attached for reference.

Please NOTE:

1. The shown piping locations are approximate and for information purposes only
2. The drawings are not to scale
3. This drawing does not replace field locates. Please contact Ontario One Call for onsite locates prior to excavating, digging, etc

Enbridge Gas requires a minimum separation of 0.6m horizontal and 0.3m vertical from all of our plant less than NPS 16 and a minimum separation 1.0m horizontal and 0.6m vertical between any CER-regulated and vital pipelines. For all pipelines (including vital pipelines), when drilling parallel to the pipeline, a minimum horizontal clearance measured from the edge of the pipeline to the edge of the final bore hole of 1 m (3.3 ft) is required. Please ensure that this minimum separation requirement is maintained, and that the contractor obtains locates prior to performing any work and utilizes safe excavation practices while performing any work in the vicinity.

Also, please note the following should you find any abandoned infrastructure in the area:

- Any pipe that is excavated, please assume that it is live
- If during the course of any job, any pipe is found that is not on the locate sheet and is in conflict with your work, please call our emergency number (1-877-969-0999), and one of our Union Gas representatives will respond to determine if that plant is in fact live or dead
- Please note that our Enbridge Gas representative will respond to the live or dead call within 1-4 hours, so please plan your work accordingly

### **HERITAGE PLANNER - Kristina Tang**

No supporting information required.

There is no apparent built heritage concern with this property and it is located on an area of low archaeological potential.

Nevertheless, the Applicant should be notified of the following archaeological precaution.

1. Should archaeological resources be found during grading, construction or soil removal activities, all work in the area must stop immediately and the City's Planning & Building Department, the City's Manager of Culture and Events, and the Ontario Ministry of Heritage, Sport, Tourism and Culture Industries must be notified and confirm satisfaction of any archaeological requirements before work can recommence.
2. In the event that human remains are encountered during grading, construction or soil removal activities, all work in that area must be stopped immediately and the site secured. The local police or coroner must be contacted to determine whether or not the skeletal remains are

human, and whether the remains constitute a part of a crime scene. The Local police or coroner will then notify the Ontario Ministry of Heritage, Sport, Tourism and Culture Industries and the Registrar at the Ministry of Government and Consumer Services if needed, and notification and satisfactory confirmation be given by the Ministry of Heritage, Sport, Tourism and Culture Industries.

Contacts:

Windsor Planning & Building Department:

519-255-6543 x6179, [ktang@citywindsor.ca](mailto:ktang@citywindsor.ca), [planningdept@citywindsor.ca](mailto:planningdept@citywindsor.ca)

Windsor Manager of Culture and Events (A):

Michelle Staaedegaard, (O) 519-253-2300x2726, (C) 519-816-0711,  
[mstaaedegaard@citywindsor.ca](mailto:mstaaedegaard@citywindsor.ca)

Ontario Ministry of Heritage, Sport, Tourism and Culture Industries

Archaeology Programs Unit, 1-416-212-8886, [Archaeology@ontario.ca](mailto:Archaeology@ontario.ca)

Windsor Police: 911

Ontario Ministry of Government & Consumer Services

A/Registrar of Burial Sites, War Graves, Abandoned Cemeteries and Cemetery Closures,  
1-416-212-7499, [Crystal.Forrest@ontario.ca](mailto:Crystal.Forrest@ontario.ca)

**PARKS DEVELOPMENT - Sherif Barsom**

We have reviewed the submitted application and noticed that there was a Tree Condition Report submitted for this site. At this point Parks Development has no comments pertaining this submission.

Noting that the City Forestry team has to review the report and comment on it if any.

**ERCA**

The following is provided, as a result of our review, to consider, Draft Plan of Subdivision (SDN-001-21 / SDN- 6575).

The applicant requests approval of a draft plan of Subdivision for the development of 34 residential building lots for single unit detached dwellings, on the parcels described, as, Part Lot 306 and Part Lot 307, Concession N Talbot Rd., Sandwich East, Windsor. The draft plan includes 3 new roadways (Streets A, B & C), and 3 Blocks (Block 35 for SWM facility; Block 36 & 37 for Reserves), identified as part of the proposed residential subdivision development. Two of the proposed new roads provide direct vehicular access from/to North Talbot Road.

The subject lands are designated Residential on Schedule D – Land Use, of the Official Plan and zoned Residential District 1.4 with a holding provision (HRD1.4) by Bylaw 8600. Subsection 20(1)85 of By-law 8600 applies to the subject lands and requires a minimum front yard depth of 9m for any lot fronting on North Talbot Rd between Southwood Lakes Blvd and HWY 401.

It is the ERCA understanding that the existing holding symbol maybe removed, when a Final Plan of Subdivision has been registered, for the site and when the applicant submits an application to remove the holding symbol.

## DELEGATED RESPONSIBILITY TO REPRESENT THE PROVINCIAL INTEREST IN NATURAL HAZARDS AND REGULATORY RESPONSIBILITIES ASSOCIATED WITH THE CONSERVATION AUTHORITIES ACT

The following comments reflect our role as representing the provincial interest in natural hazards as outlined by Section 3.1 of the *Provincial Policy Statement of the Planning Act*, as well as our regulatory role as defined by Section 28 of the *Conservation Authorities Act*.

We have reviewed our floodline mapping for this area and it has been determined this site is not located within a regulated area that is under the jurisdiction of the ERCA (Section 28 of the *Conservation Authorities Act*).

As a result, a permit is not required from ERCA for issues related to Section 28 of the *Conservation Authorities Act*, Development, Interference with Wetlands and Alteration to Shorelines and Watercourses Regulation under the *Conservations Authorities Act*, (Ontario Regulation No. 158/06).

Please note the ERCA will still require a *Development Review Clearance* for this proposal, given the size and scale of the proposed residential development.

## PLANNING ADVISORY SERVICE TO PLANNING AUTHORITIES - NATURAL HERITAGE POLICIES OF THE PPS, 2020

The following comments are provided from our perspective, as an advisory service provider to the Planning Authority, on matters related to natural heritage and natural heritage systems, as outlined in Section 2.1 of the *Provincial Policy Statement (PPS)* of the *Planning Act*. The comments in this section do not necessarily represent the provincial position and are advisory in nature for the consideration of the Planning Authority.

The subject property contains a natural heritage feature that may meet criteria for significant woodland, significant wildlife habitat and/or habitat of endangered species and threatened species. A 'species at risk study' and a "tree inventory" is not sufficient, in order to meet the requirements of PPS natural heritage policies. The proposal is to develop the entire Natural Heritage feature. This would not meet the requirements of PPS policies to demonstrate no negative impact.

Therefore, this application should be *Deferred*, pending completion of a *Environmental Evaluation Report (EER / EIA)*, documenting ALL species on the site and evaluating the site in accordance with EIA guidelines.

## WATERSHED BASED RESOURCE MANAGEMENT AGENCY

The following comments are provided in an advisory capacity, as a public commenting body on matters related to watershed management.

### SECTION 1.6.6.7 Stormwater Management (PPS, 2020)

ERCA has concerns with the potential impact of the quality and quantity of runoff in the downstream watercourse due to the proposed development on this site.

ERCA recommends that stormwater quality and stormwater quantity will need to be addressed, up to and including the 1:100 year storm event and be in accordance with the guidance provided by the "*Stormwater Management Planning and Guidance Manual, prepared by the Ministry of the*

*Environment (MOE, March 2003)*” and the "*Windsor-Essex Region Stormwater Management Standards Manual*".

### FINAL RECOMMENDATIONS

This application should be "*Deferred*", pending completion of an *Environmental Evaluation Report (EER / EIA)*, documenting ALL species on the site and evaluating the site in accordance with EIA guidelines.

In addition, we therefore request inclusion of the following draft conditions, in the Subdivision Agreement:

1. That the subdivision agreement between the Owner and the Municipality contain provisions, to the satisfaction of the "Municipality" and the "Essex Region Conservation Authority", that stipulate that the Owner will finalize the engineering analysis to identify stormwater quality and quantity measures as necessary to control any increase in flows in downstream watercourses, up to and including the 1:100 year design storm and in accordance with the *Windsor-Essex Stormwater Management Standards Manual*.

2. That the subdivision agreement between the Owner and the Municipality contain provisions that requires that the Owner installs the stormwater management measures identified in the engineering analysis completed as part of the development for the site and undertake to implement the recommendations contained therein, to the satisfaction of the "Municipality" and the "Essex Region Conservation Authority".

3. That prior to undertaking construction or site alteration activities, any necessary permits or clearances be received from the Essex Region Conservation Authority (ERCA), in accordance with Section 28 of the *Conservation Authorities Act*. Alternatively, if not regulated, by the ERCA, any required development review clearances.

4. That the subdivision agreement between the Owner and the Municipality contain provisions that require the Owner to implement any and / or all recommendations of a final *Environmental Evaluation Report (EER / EIA)*, to the satisfaction of the Municipality and the ERCA. Recommendations from this report shall be implemented in the design and construction phases of the development (*note: this draft condition is subject to the ERCA request for a Deferral of this application at this time*).

5. That prior to final approval, the Essex Region Conservation Authority shall require a copy of the fully executed subdivision agreement between the Owner and the Municipality, in wording acceptable to the Essex Region Conservation Authority, containing provisions to carry out the recommendations of the final plans, reports and requirements noted above.

We have no objections to the application for Draft Plan of Subdivision at this time, subject to the draft conditions noted above and the consideration of a *deferral* at this time, pending submission of an *Environmental Evaluation Report (EER / EIA)*, documenting ALL species on the site and evaluating the site in accordance with EIA guidelines.

We ask the County of Essex or the approval authority, in this case, to forward a copy of the Notice of Decision, including a copy of the Draft Approved Plan for our records.

We also request to be notified, on any future circulations for this application or notifications, regarding this proposed plan of subdivision.

The ERCA has also provided some preliminary comments on the Stormwater Management Plan to facilitate your review (see attached).

Reviewed the following for SWM considerations:

- 1095 North Talbot Residential SWM and FS Report (Project No. 21-021)

Provide the following SWM comments:

- Please confirm is the 100 year SCS event modelled?
- Why is the UST HWL lower than the 100 yr HWL?
- Discharge is to be restricted via a 377 mm diameter plate. Does this size exist? Or will it be custom made?
- Is there an overland spill route to the pond or the ROW? Proposed Street A – Has a HP on the west boundary edge of 190.35 m and on the east edge of 190.08 m. The dry pond has a TOB of 190.27 m. It appears that the overland flow route will spill into the existing adjacent property (1255 North Talbot Rd) for events exceeding the stress test.
- Is an easement required behind blocks 7, 9, 9, 35 for the 250 mm storm sewer. If this is the case, please provide confirmation that the easement is satisfactory to the City.
- Sewer design sheet is missing A-7, Tc is selected as 10 mins but inlet time shows 20 mins. Please clarify.
- What is the purpose of the ditches on the North and South end of the property? Are these for storage or conveyance? Where is the water directed to?
- Since the proposed development is adjacent to existing developed blocks on the southern edge, can you please confirm if the existing grading will outlet into the proposed rear yard cbs? If so, will the provided SWM need to account for additional drainage requirements posed by these lots?

Please NOTE: At this time, ERCA is requesting a **deferral** of the planning application, pending completion of an *Environmental Evaluation Report (EER/EIA)*, documenting ALL species on the site and evaluating the site in accordance with EIA guidelines.

### **Rania Toufeili – Transportation**

- North Talbot Road is classified as a Class I Collector with a required right-of-way width of 25.3 meters per Schedule X of the Official Plan. The current right-of-way is sufficient per the Sixth Concession Road/North Talbot Road Environmental Assessment and therefore no conveyance is required along the roadway.
- Corner cut-offs of 4.6 meters are required on North Talbot Road and Streets B and C (lots 4,5, 6 and 7). The submitted plan shows corner cut-offs.
- The new proposed Street B and Street C should align with the intersections to the north of Pioneer Avenue and Old West Avenue South respectively.
- Driveways for lots 4, 5, 6 and 7 should be set as far back as possible from the intersections. Furthermore, lots that are abutting two streets should have driveways on the lower classification street (local road).
- Sidewalk contributions are to be made as required and outlined by Engineering Right-of-Way.
- Detailed and dimensioned drawings showing the proposed driveways, curb cuts and cul-de-sac design are required to provide further comments on conveyances and additional

requirements. All roadways should be constructed to City of Windsor Standards Engineering Drawings.

- A pedestrian connection should be provided from Street A to the trail on Southwood Lakes Boulevard. A standard cul-de-sac bulb will need to be provided for Street A and therefore a sidewalk connection should be accommodated with sufficient space at this cul-de-sac.
- Parking restrictions and required by-law amendments will be reviewed at the engineering drawings review stage.
- All accesses shall conform to the TAC Geometric Design Guide for Canadian Roads and the City of Windsor Standard Engineering Drawings.
- All exterior paths of travel must meet the requirements of the *Accessibility for Ontarians with Disabilities Act* (AODA).

### **Bell Canada – Circulations**

We have reviewed the circulation regarding the above noted application and have no objections to the application as this time. However, we hereby advise the Owner to contact Bell Canada at [planninganddevelopment@bell.ca](mailto:planninganddevelopment@bell.ca) during detailed design to confirm the provisioning of communication/telecommunication infrastructure needed to service the development. We would also ask that the following paragraph be included as a condition of approval:

“The Owner agrees that should any conflict arise with existing Bell Canada facilities where a current and valid easement exists within the subject area, the Owner shall be responsible for the relocation of any such facilities or easements at their own cost.”

It shall also be noted that it is the responsibility of the Owner to provide entrance/service duct(s) from Bell Canada’s existing network infrastructure to service this development. In the event that no such network infrastructure exists, in accordance with the Bell Canada Act, the Owner may be required to pay for the extension of such network infrastructure.

If the Owner elects not to pay for the above noted connection, Bell Canada may decide not to provide service to this development.

To ensure that we are able to continue to actively participate in the planning process and provide detailed provisioning comments, we note that we would be pleased to receive circulations on all applications received by the Municipality and/or recirculations.

Please note that WSP operates Bell’s development tracking system, which includes the intake of municipal circulations. WSP is mandated to notify Bell when a municipal request for comments or for information, such as a request for clearance, has been received. All responses to these municipal circulations are generated by Bell, but submitted by WSP on Bell’s behalf. WSP is not responsible for Bell’s responses and for any of the content herein.

If you believe that these comments have been sent to you in error or have questions regarding Bell’s protocols for responding to municipal circulations and enquiries, please contact [planninganddevelopment@bell.ca](mailto:planninganddevelopment@bell.ca).

### **Enwin**

**Hydro Engineering:** No Objection, however, a hydro easement may be required to accommodate the existing hydro anchor on the northwest corner of Street B on the draft plan. See attached sketch.



**Water Engineering:** Water Engineering has no objections.

### **Patrick Winters – Engineering**

The applicant is requesting approval for a Draft Plan of Subdivision consisting of 34 lots for single unit detached dwellings. The draft plan includes 3 new roadways (Streets A, B & C), and 3 blocks (Block 35 for SWM facility; Block 36 & 37 for Reserves) identified as part of the proposed residential subdivision development. The subject land is currently zoned (H) RD1.4 under By-law 8600 and designated as residential use under the City of Windsor Official Plan.

After reviewing the servicing requirements of the subject lands pertinent to the subject application, we have the following comments:

#### **Roads and Right-of Way**

North Talbot Road between Sixth Concession Road and the City limits is a municipal roadway classified as a Class I Collector. The section of road falls within the boundary of the 6<sup>th</sup> Concession Road/North Talbot Environmental Study Report [by Dillon Consulting dated April 2016]. The ESR does not identify any property requirements across the frontage of the subject lands

The new proposed Street B and Street C should align with the intersections to the north of North Talbot Road, Pioneer Avenue and Old West Avenue, respectively. Proper cul-de-sacs bulbs would be required at both ends of Street A. The driveways for lots 4, 5, 6 and 7 should be set as far back as possible from the intersections. Furthermore, lots that are abutting two streets should have driveways on the lower classification street (local road). This section of North Talbot Road currently has a rural cross section, and as such contributions are requested in the amounts of \$33,500.00 and \$17,750.00 towards future construction of concrete sidewalks as well as curbs and gutters, respectively, on the North Talbot Road frontage of the subject lands.

There are City trees and hydro poles in the right-of-way; therefore, the City Forester and respective utility companies should be contacted to see what measures need to be taken to resolve these impediments in the right-of-way prior permit issuance.

The subject lands fall within the Ministry of Transportation (MTO) permit control area. The owner will be required, prior to the issuance of a construction permit, to contact MTO and obtain any necessary permits and approvals.

Additionally, a geotechnical report may be required to determine the capacity of the soil below the road bed and building envelopes.

#### **Sewers**

There are municipal storm and sanitary sewers within the abutting road way, available to service the subject property as follows:

- North Talbot Road
  - Storm Sewer: 450mm Corrugated Steel Sewer, 1200mm Reinforced Concrete Pipe
  - Sanitary Sewer: 600mm Concrete Pipe and Reinforced Concrete

Dillon Consulting Ltd. is currently doing detailed engineering design for the North Talbot Road Corridor. The post-development land use parameters used within the model to consider future condition peakflow and volume through the system are as follows:

- East half of the property
  - Area = 1.39 ha
  - Max. Allowable Release Rate = 201 L/s
- West half of the property
  - Area = 1.42 ha
  - Max. Allowable Release Rate = 206 L/s

A servicing study is required for this development and a stormwater strategy supported by the City. Detailed civil servicing drawings are required to provide further comments on additional requirements.

We have no objection to the Subdivision Application, subject to the following conditions:

Subdivision Agreement - The applicant agrees to enter into a Development Agreement with the Corporation of the City of Windsor with the General Provisions of Council Resolutions 233/98 and any other specific requirements.

Corner Cut-Off – The owner(s) agrees, prior to the issuance of a construction permit, to gratuitously convey a 4.6m x 4.6m (15' x 15'), corner cut-off at the intersection of North Talbot Road and Street 'B' as well as North Talbot Road and Street 'C' in the Draft Plan, in accordance with City of Windsor Standard Drawing AS-230.

Servicing Study - The owner agrees, at its own expense, to retain a Consulting Engineer to provide a detailed servicing study report on the impact of the increased flow to the existing municipal sewer systems, satisfactory in content to the City Engineer and prior to the issuance of a construction permit. The study shall review the proposed impact and recommend solutions to addressing the problems and ultimate implementation of solutions should there be a negative impact to the system. The study shall be finalized in agreement with the City Engineer.

Site Servicing Plans – The owner agrees to submit a site servicing plan for the subject lands to the satisfaction of the Chief Building Official, the City Engineer, and ERCA in regulated areas, prior to the issuance of any construction permits for the subject lands.

Video Inspection (Mainline) - The applicant shall agree to conduct at its entire expense a video inspection, or pay the cost of similar inspection, of ALL EXISTING sanitary/storm sewers on North Talbot Road which will be tapped to service the development, all to the satisfaction of the City Engineer.

MTO Corridor Management (MTO Requirement) - The owner will be required, prior to the issuance of a construction permit, to contact the Ministry of Transportation (MTO) Corridor Management Section at 1-800-265-6072 to obtain the necessary permits, clearances and/or approvals in accordance with the Public Transportation & Highway Improvement Act.

Sidewalks -The owner(s) agrees to pay to the Corporation, prior to the issuance of a Building Permit, the sum of \$33,500.00 being the Owner's contribution towards the future construction of a concrete sidewalk on the North Talbot Road frontage of the subject lands.

Curbs and Gutters – The owner further agrees to pay to the Corporation, prior to the issuance of a construction permit, the sum of \$17,750.00 being the Owner's contribution towards the construction of concrete curb and gutter on the North Talbot Road frontage of the subject lands.

### **LANDSCAPE ARCHITECT & URBAN DESIGN - Stefan Fediuk**

Pursuant to the application for a Plan of Subdivision (**SDN-001/21**) for the properties situated on the south side of North Talbot Road, between Southwood Lakes Blvd and HWY 401 (0 North Talbot Rd; 1095 North Talbot Rd and 1185 North Talbot Rd), please note no objections from a landscape architectural or urban design perspective.

The applicant has provided several documents required through the Pre-submission process as requested by the Landscape Architect, including; Species at Risk Study, Topographic Plan of Survey, Tree inventory and Preservation Plan, and Planning Rationale.

The Applicant's landscape architect has also provided an updated Tree Preservation Report No 2, dated 18 April 2022, and addendum to that report dated May 17, 2022, as well as a revised Subdivision plan and Landscape/Tree Preservation Plan. Through consultation with the Executive Director of Parks, please also note the following comments:

#### Urban Design:

The proposal's Planning Rationale highlights these options through its response to the required studies as well as the revised subdivision plan to include parkland where the existing trees are the densest, abutting the existing Stoneybrook Park. This preservation and enhance of the natural wooded lots positively respond to the following Provincial and Civic policies related to development:

- Policy of PPS Section 1.1.1 (h) recommends Healthy, livable and safe communities are sustained by promoting development and land use patterns that conserve biodiversity.
- Section 8 Urban Design of the OP, recommends that development within the city provide a memorable, attractive and liveable city, and that development is to maintain and improve the quality of life for present and future generations by integrating the principles of sustainability and place making.

#### Species at Risk:

The Planning Rationale outlines the mitigation measures for Species at Risk (SAR) Section 4.2.2 including:

- Mitigation measures to avoid potential impact to the Yellow-breasted Chat and protected reptiles should be followed to prevent against potential contraventions of the Endangered Species Act, 2007 or Migratory Bird Convention Act, 2010
- To reduce the potential for impacts, vegetation removal will occur between October 1 and March 31, outside of the active season for bats, and replacement bat roosting habitat (two rocket boxes) will be installed under the direction of a qualified professional.

These are supported by the Landscape Architect.

#### Parks and Parkland Dedication:

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Per By-law 12780 and the Planning Act, Residential subdivisions require the provision of parkland dedication representing 5% of the subject lands in the form of land and/or cash-in-lieu, to the satisfaction of the Commissioner of Parks.

Through consultation with the Executive Director of Parks, and Planning Staff and the developer's consultants, opportunities for parkland development; physically connecting the proposed development to the Stoneybrook Park multiuse trail, and help preserve a large portion of the existing natural environment, as per Section 6.3.2.4 of the OP are now being proposed.

Block 35 at the western end of the Street "A", consists of 819.1 m<sup>2</sup>, will represent a Parkland conveyance of 2.7% of the overall site development. The difference from the required Parkland of 5% shall be though a provision of cash-in-lieu. In addition, the developer is aware that a Parkland Development contribution is also to be provided.

The proposal found in Section 3.1 of the Planning Rationale identifies a stormwater management facility to be located on Block 35 and further detailed in Section 4.2.5. The addendum and subdivision landscape plan identify that the SWM facility will be 1,498.2 m<sup>2</sup> in area and surrounded by trees that will help to absorb stormwater before entering the municipal stormwater infrastructure. The location and solution is supportable and will a potential attraction for the development, but the developer is to be aware that Parks does not accept stormwater management facilities as Parkland Dedication.

#### **Required**

- Special Provision of the Subdivision Agreement pertaining to G-5(5) Parkland Conveyance identifying Block 35 to be conveyed to the City of Windsor as 2.7% Parkland along with cash-in-lieu representing the 2.3% remaining Parkland Conveyance.
- Special Provision of the Subdivision Agreement pertaining to G-2(25) Sidewalks, that the owner(s) shall pay to the Corporation, prior to the issuance of a Subdivision Agreement, the sum of \$16,500.00, being the Owner's contribution towards the future construction of a 3.0m wide multi-use trail connection from the Southwood Lakes Multi-use Trail, in Stoneybrook Park, to the proposed cul-de-sac at the western end of the proposed Street 'A'.

#### Climate Change & Tree Preservation:

As identified in the Planning Rationale provided, *"The Site is grassed and has areas of clustered mature trees predominantly near the western half of the Site and scattered near the eastern portion of 1185 North Talbot Road. The two vacant parcels also have areas of clustered mature trees throughout the Site."* The report also recognizes PPS policy 2.1.1 which requires natural features and area to be protected for the long term.

The development recognizes that most of the vegetation will be removed from the site for development. Removal of vegetation and developing with paved roads and buildings will reduce the stormwater resiliency of the property and increase the heat island effect in the area. Preservation of as many trees as possible has been highly recommended. Where unable to preserve trees, the developer is to provide measures to improve and replace the tree canopy loss that will help mitigate climate change ( Section 1.1.3.2 clauses c) and d), as well as 1.6.1 of the PPS).

The Revised Tree Inventory and Conditions Report No 2 prepared by Bezair Partners, identified 44 instances (including 3 on adjacent municipal land) of desirable species of trees of

significant size and health, to be impacted, by the development (representing a total of 21,160mm of total tree caliper). The western half of the development consists of the most notable number of the trees inventoried consisting of desirable native species providing suitable habitat for urban wildlife (such as squirrels, bats and song birds).

An amendment to that Conditions Report No2, itemizes the trees species, size (dbh in cm), and which could possibly be saved through preservation methods and through the conveyance of Block 35 to the Corporation as parkland. Proposed is the removal of 50 tree (13,700mm of tree caliper) and 16 trees plus one cluster of trees to be preserved (7460mm of tree caliper).

While the developer has suggested planting new trees within the development as compensation for tree loss, residential building permits are completed on a lot-by-lot basis by individual homebuilders. This process would make this difficult to accomplish by the developer and for city staff to administer, once the developer has relinquished ownership to the individual builders. Five Lots (2, 3, 4, 20 & 21) would require special permits, individual agreements and site-specific securities, and inspections by the Parks and Planning staff on private single-family residential properties. This process is generally conducted through Site Plan Control; however, the Planning Act precludes private single-family residential properties from that process.

Any new trees to be planted to repopulate the urban tree canopy, is to be completed by City Parks Forestry staff on public lands to ensure that the trees planted are the same as those desirable species removed from the site, and that the trees are more readily maintained by Forestry for the long-term. Block 35 require to be preserved. This will unfortunately, increase the number of trees lost to 57 (17,530mm caliper). It will however, ensure additional new trees to be planted, and maintained through the City Forestry staff, instead of risking expending a great deal of time and resources of the developer and the corporation, only to have the home owner potential remove the preserved tree once the builder sells the home.

### **Required**

- Special Provisions of the Subdivision Agreement pertaining to G-3(2) Preservation of Existing Trees,
  - The owner shall provide a Landscape and Tree Preservation Plan identifying the proposed locations of all existing trees removed from the development and those to be retained in Block 35, to the satisfaction of the Executive Director of Parks and the Executive Director/City Planner, prior to the final subdivision plan approval.
  - The owner shall pay to the Corporation prior to the issuance of a Subdivision Agreement, the sum of \$130,000.00 towards compensation, at a rate of one (1) street tree for every 70mm caliper (dbh) of desirable tree removed, in accordance with the Corporation's Fees and Changes By-law 392-2002.
  - Individual lot owner(s) shall also provide cash-in-lieu for one (1) new 70mm caliper native tree per each lot for planting a boulevard tree (per CR 332/79) , in accordance with the Corporation's Fees and Changes By-law 392-2002, prior to the issuance of any construction permit.

In addition to Special Provisions for preservation of the existing desirable trees in Lot22:

- Special Provisions of the Subdivision Agreement pertaining to G-6 the owner shall:
  - Prior to the final subdivision plan approval, provide a Performance Security in the amount of \$25,000.00 in the form of cash or a certified cheque to be liquid to ensure that the nine desirable trees located on Block 35 are preserved during the construction process are preserved.

- Request inspection by Corporation's City Forester to ensure that the proposed tree protection and appropriate method of protection has been completed to the satisfaction of the Executive Director of Parks, prior to release of the Security.
- Where trees, proposed to be preserved, have been removed from development, at the time of inspection by the Corporation's City Forester, compensation will be drawn from the Performance Security at a rate of one tree for every 70mm of tree caliper (dbh) or portion thereof missing, in accordance with the Corporation's Fees and Charges By-law 392-2002.

### **Barry Horrobin – Police Services**

I have reviewed the drawings associated with this plan of subdivision application and hereby offer my revised comments for inclusion into the application with other reviewers:

The following comments on the proposed draft plan of subdivision are provided with a particular focus on public safety impact. These comments are in two general categories as follows:

1. The importance of establishing and maintaining proper emergency vehicular access/response capability
2. All other important public safety and security implications relating to the application to ensure a development that is safe for all

### **EMERGENCY VEHICULAR ACCESS**

- The Windsor Police Service generally supports the roadway layout and access connections proposed for this land development to existing roadway infrastructure, most notably the connections back to North Talbot Road. The positioning of Streets 'A' and 'B', 'C' properly connect to ensure overall ease of police incident response and general police patrolling activities for this newly developed neighbourhood.
- One issue that may arise as relates to road safety however is that Street 'C' appears to be offset slightly from its alignment with Old West Avenue on the other side of North Talbot Road. Once connected, this could lead to difficulties for drivers making left hand turns onto North Talbot. The offset is not substantial, yet it is enough to divert driver sightlines that could make turns more challenging. As a result, safety could be reduced for all users of the immediate roadway area. If there was a way to better align Street 'C' with Old West, that would seem to present a safer layout

### **SPECIFIC SAFETY ISSUES & CONSIDERATIONS**

The following issues, in no particular order, are hereby raised for consideration, with the goal being to optimize public safety in a practical manner:

- There is a parcel of land proposed for the storm water management facility (block 35). Given this lot will not be a regularly occupied space within the broader neighbourhood, it is important that it be properly constructed/modified and maintained to help discourage any risks to its use in an unlawful or undesirable way (such as trespassing, loitering, etc.). The orientation of this lot as shown on the draft plan is capable of leveraging adequate ongoing natural surveillance, most notably on its southern boundary near the eastern terminus of Street 'A', and this should be maintained as the minimum standard going forward. It would also be prudent to ensure some measure of access control is considered

(by way of a fence for example) on the sides of this lot that abut, residential lots #8, #9, #10, and #11 to provide a functional degree of separation between public and private spaces. Ideally, such fencing should allow for some degree of observation capacity – a steel picket or chain link style fence of 1.8 m would be preferred over an opaque, wood privacy style fence. Fencing along any other lot line/section of lot #35 would not be recommended from a safety and security perspective.

- Ensuring prompt and effective response capability by police responders is directly correlated to accurately locating the right address where an emergency call for assistance is required. Therefore, it is very important that each separate dwelling have a prominently displayed address number that is at least 5” high, is of a contrasting colour to the backdrop onto which it is mounted, and can be easily seen from the adjacent roadway by police without obstruction. This will optimize the address identification by Police/Fire/Ambulance during an emergency response.
- Pedestrian safety is very important in all residential neighbourhoods. This includes appropriate sidewalk infrastructure to connect to adjacent areas and proper street lighting as well. Lighting provided should be LED, in keeping with the current municipal standard, which helps in promoting public safety

In summary, a clause(s), if possible or appropriate, would ideally help as one of the conditions of approval for this application to address the important issues raised here to ensure they are incorporated.